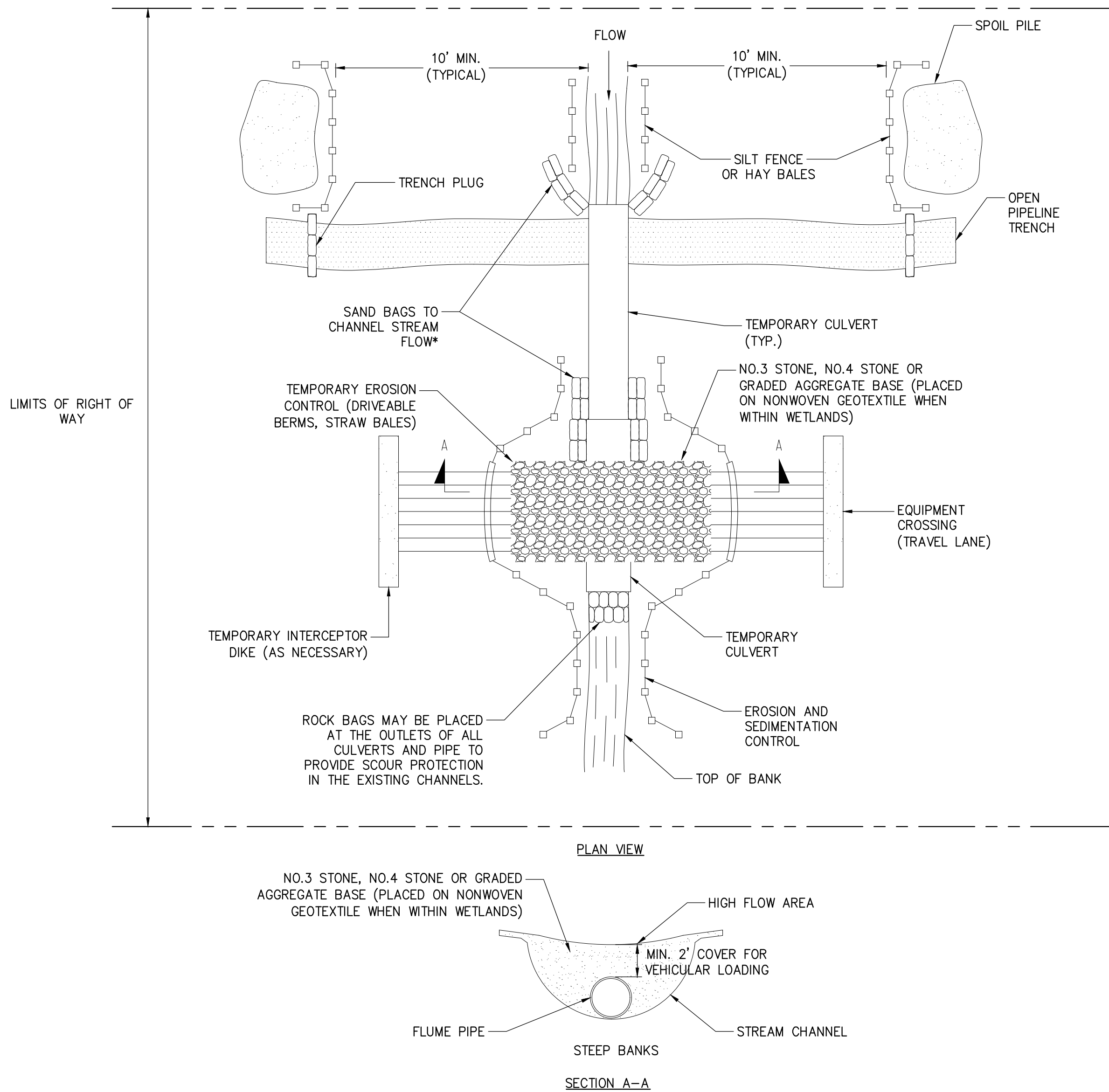


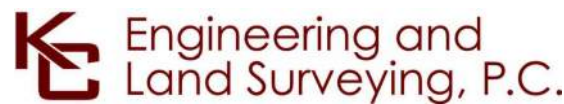
DATE	06/19/2023
SH.NO.	OF

File: P:\120174-CHPE\CABLE_INSTALL-KIEWIT\60_CAD\20_ENGINEERING_CAD_FILES\PACKAGE_7A\NY_ENVIRONMENTAL_(EROSION_CONTROL)\01_KCE\SHEET_FILES\21162_7A_ESC-DETAILS.DWG Saved: 6/15/2023 12:02:23 PM Plotted: 6/16/2023 12:37:13 PM Current User: Benson Lam LastSavedBy: blam



1 FLUMED CROSSING DETAIL
SCALE: N.T.S.

* IF WELDED PIPE IS USED SAND BAGS AT JOINTS NOT REQUIRED. ACTUAL NUMBERS OF FLUMES AND CULVERT PIPE REQUIRED TO BE DETERMINED BY STREAM WIDTH.



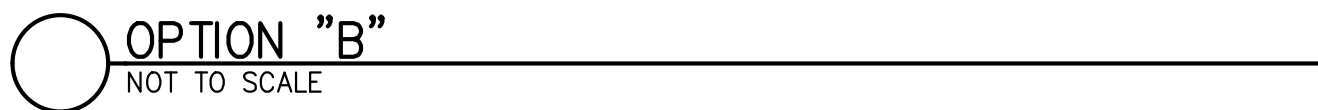
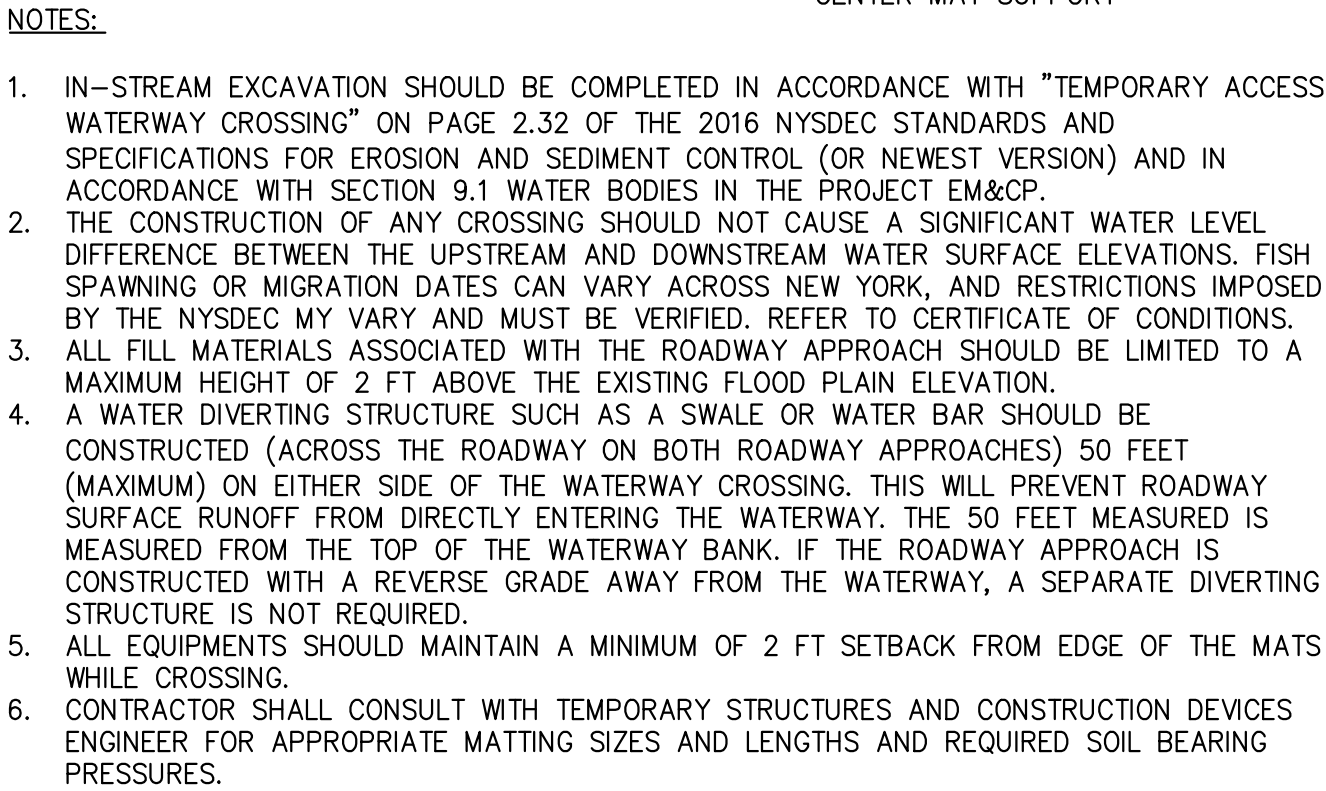
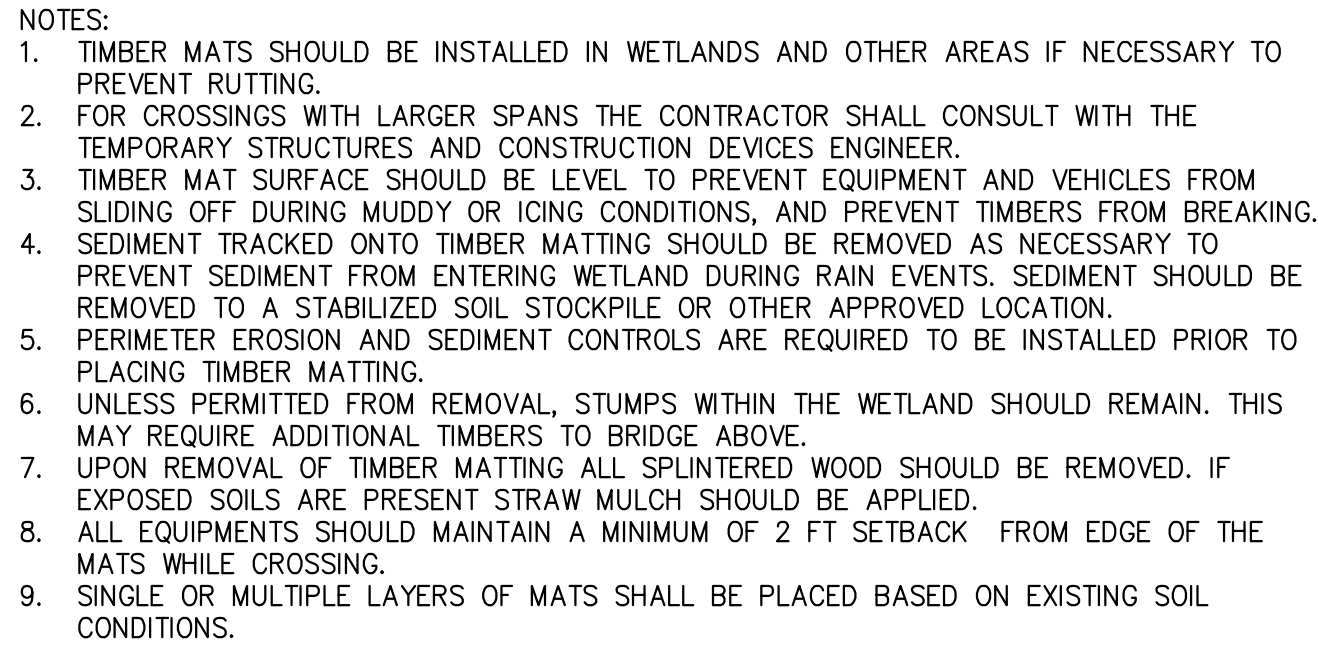
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

	06/19/2023	ISSUED FOR CONSTRUCTION SUBMISSION	SL	JL	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 11 (PACKAGE 7A) - CSX: CATSKILL
EROSION AND SEDIMENT CONTROL DETAILS

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-605
DATE	06/19/2023
SH.NO.	OF

DRAWN BY:	BL	DESIGNED BY:	SL	APPROVED BY:	JL	SCALE	AS SHOWN	DATE	06/19/2023
						REV. NO.	0	SH.NO.	OF



SCALE: N.T.S

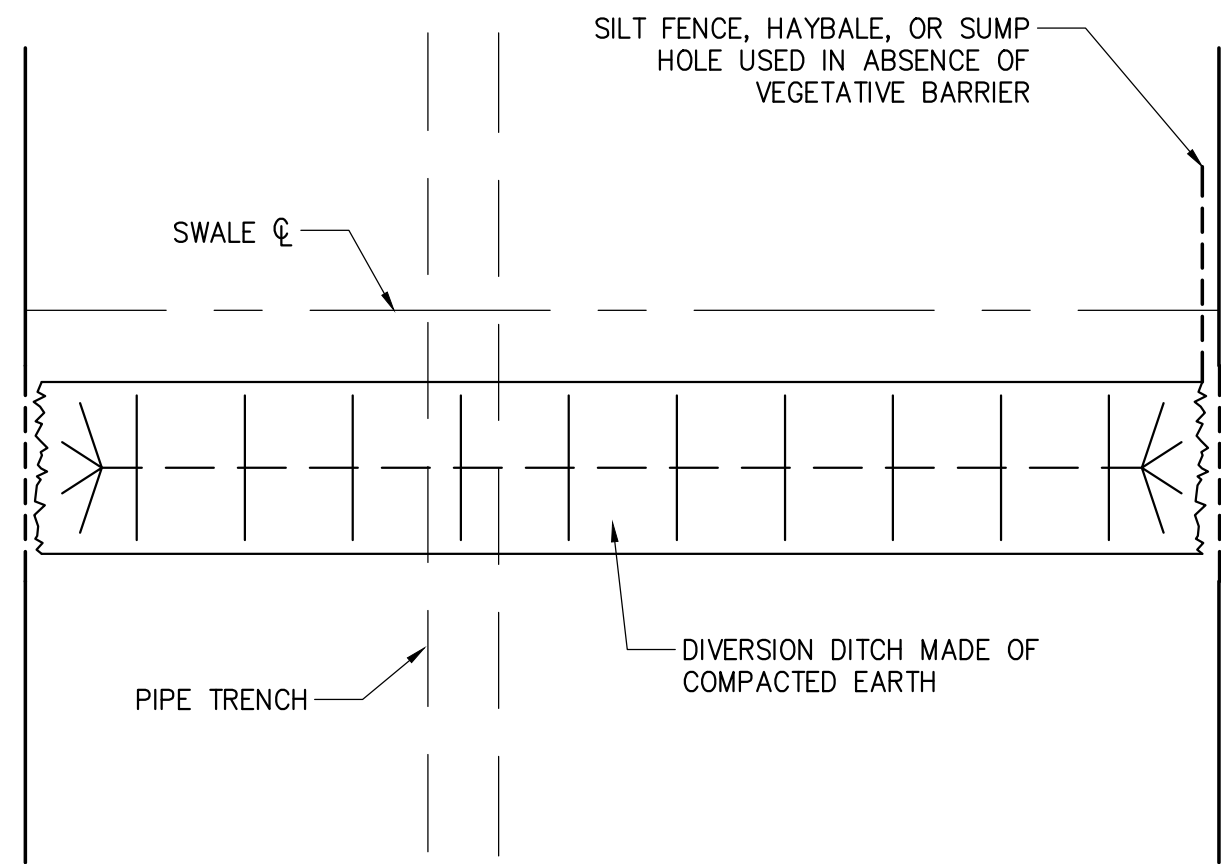
GENERAL NOTES

1. TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.
2. CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER IMMEDIATELY.

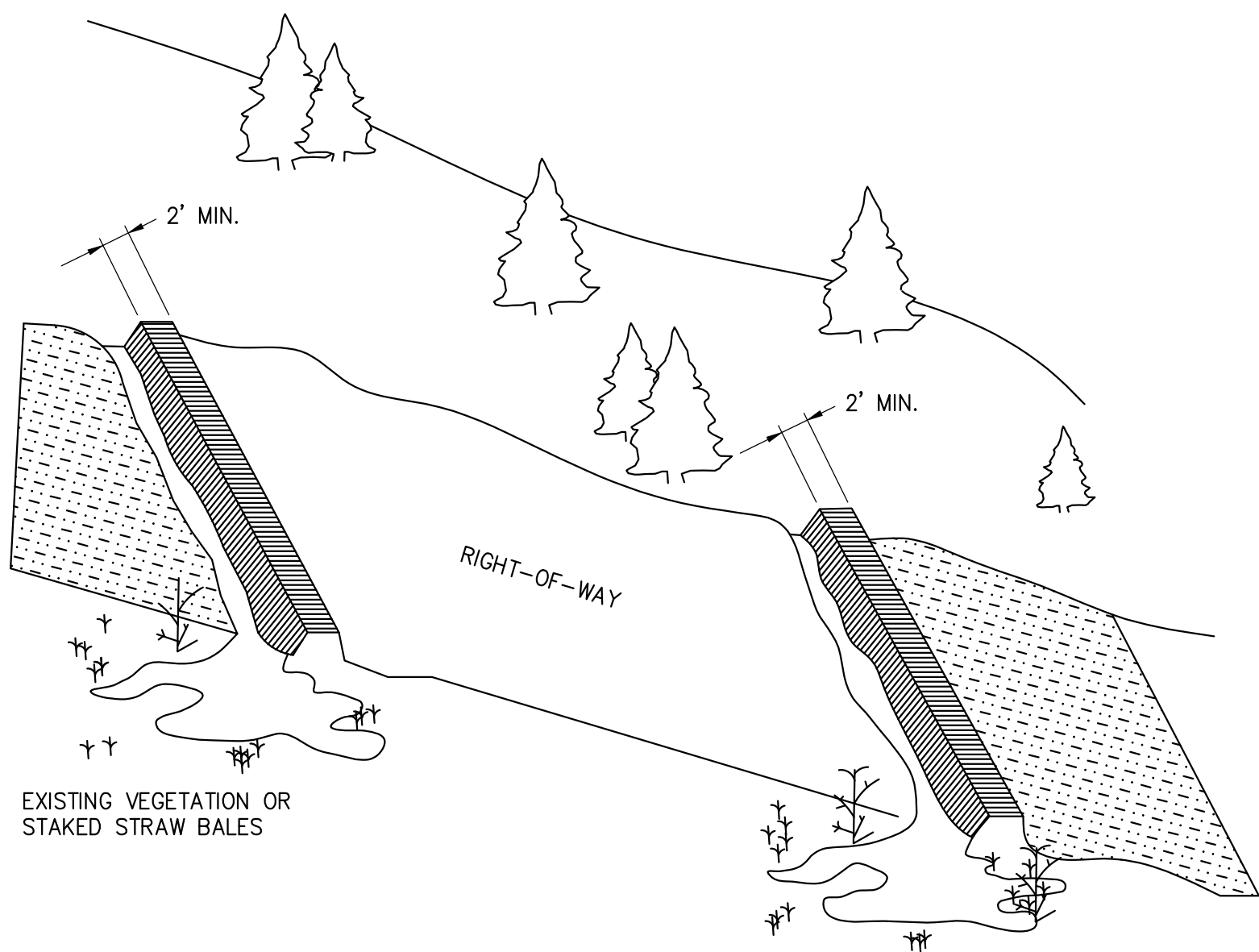


IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

[illegible]



OVERHEAD VIEW

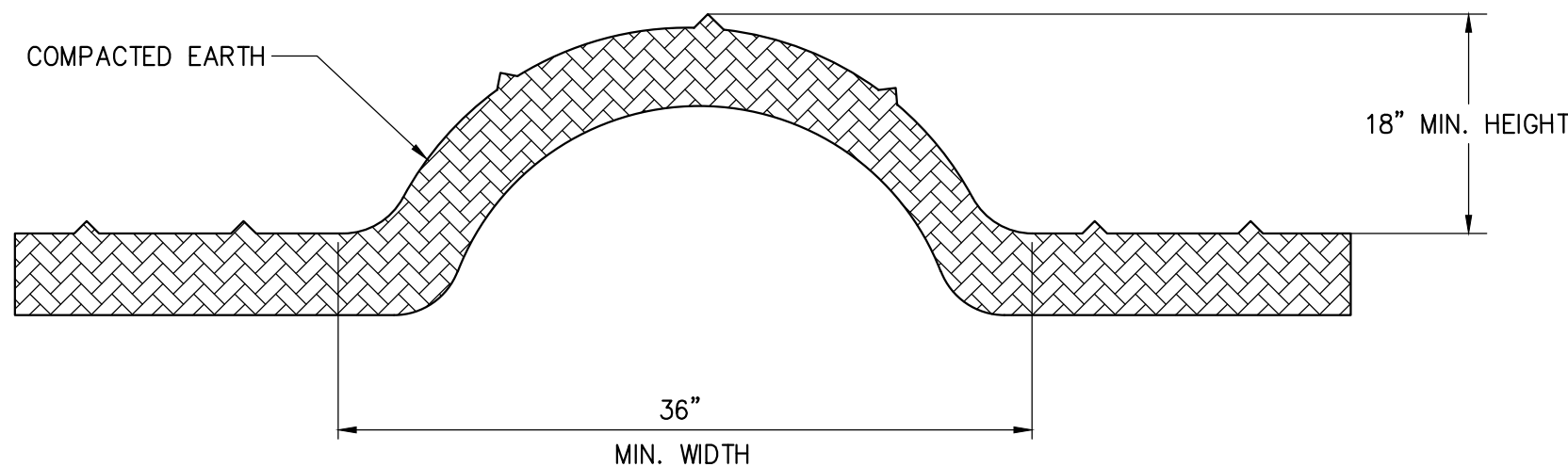


1. AT THE PROPOSED INTERCEPTOR DITCH LOCATION ESTABLISH A HORIZONTAL CONTOUR LINE (USING A POCKET TRANSIT OR HAND LEVEL) WHICH EXTENDS COMPLETELY ACROSS THE DISTURBED RIGHT-OF-WAY. THIS LINE WILL ALWAYS BE PERPENDICULAR TO THE DIRECTION OF WATER FLOW AND SHOULD BE PARALLEL TO THE MAP CONTOURS SHOWN ON THE PLAN DRAWINGS.
2. DETERMINE WHICH SIDE OF THE RIGHT-OF-WAY IS BEST SUITED FOR THE DITCH OUTLET (EVALUATE VEGETATION DENSITY, LOCAL TOPOGRAPHY, ETC.) AND DEVIATE DIKE AWAY FROM THE HORIZONTAL CONTOUR LINE SLIGHTLY DOWNWARD TOWARD THE SELECTED OUTLET SIDE MAINTAINING A THREE TO FIVE PERCENT SLOPE. AS AN EXAMPLE, THE CHART AT THE RIGHT SHOWS DIMENSIONS ASSUMING A FOUR PERCENT SLOPE.
3. WHEN OUTLETTING NEAR WATER BODIES, STREAMS, DITCHES, & CROP FIELDS, A FILTER FENCE OR STRAW BALE FENCE SHOULD BE PLACED ON OUTLET END OF THE DIVERSION DITCH.

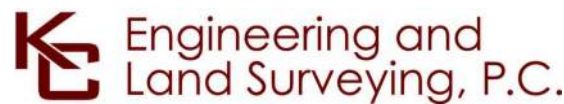
NOTES:

1. TEMPORARY DIVERSION DITCH SHOULD BE BUILT SIMILAR TO THE PERMANENT DITCH CONFIGURATION BUT THE DIMENSION CAN BE SCALED BACK.
2. MAXIMUM HEIGHT SHOULD BE 12" AND SHOULD BE COMPACTED.
3. SPACING BETWEEN DIVERSION DITCHES AND SKEW OF THE DIVERSION DITCHES CAN VARY FROM THE PERMANENT DIVERSION DITCHES.
4. WHEN CONSTRUCTING TEMPORARY DIVERSION DITCHES THEY SHOULD BE FUNCTIONAL, WHILE MAINLINE CONSTRUCTION IS PROCEEDING, UNTIL RESTORATION BEGINS AND PERMANENT DIVERSION DITCHES ARE THEN CONSTRUCTED.

SCALE: N.T.S.



SCALE: N.T.S.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

	06/19/2023	ISSUED FOR CONSTRUCTION SUBMISSION	SL	JL
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

KIEWIT PROJECT NO.
21162
KC PROJECT NO.
120174
DRAWING NO.

C-612

DRAWN BY: BL	DESIGNED BY: SL	APPROVED BY: JL	SCALE	AS SHOWN	DATE	06/19/2023
			REV. NO.	0	SH. NO.	OF

WETLAND AND AGRICULTURAL LANDS WORKING SURFACE PLAN - OPTION A
NOT TO SCALE

1
C-613

NOTES:

1. UNDERCUT AND REMOVE TOP SOIL PRIOR TO PLACING GEOTEXTILE FABRIC.
2. A LAYER OF CLEAN CRUSHED STONE SHALL BE LAID ON TOP OF THE GEOTEXTILE FABRIC.
3. GEOTEXTILE FABRIC SHALL EXTEND AT LEAST 3 FT TO 5 FT BEYOND THE EDGE OF STONE PLACEMENT TO MINIMIZE STONE ENTERING THE WETLAND AND FACILITATE REMOVAL OF THE ROAD.
4. SUITABLE CROSS DRAINING SHALL BE PROVIDED ACROSS THE ROAD FOR STREAM CHANNELS AND SURFACE FLOW.
5. REFER TO EM+CP PLANS FOR EROSION CONTROL DETAILS.
6. REFER TO EM+CP PLANS FOR RESTORATION OF WETLAND.

GENERAL NOTES:

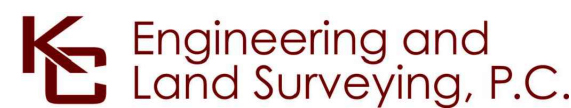
1. **TIMBER:**
 - A. **TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.**
2. **CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE ENGINEER IMMEDIATELY**
3. **NONWOVEN GEOTEXTILE SHALL BE MIRAFI 180N OR EQUIVALENT APPROVED BY EOR.**

WETLAND AND AGRICULTURAL LANDS WORKING SURFACE PLAN - OPTION B

2
C-613

NOTES:

1. TIMBER MAT SHOULD BE INSTALLED IN WETLANDS, AGRICULTURAL LANDS AND OTHER AREAS IF NECESSARY TO PREVENT RUTTING.
2. BASED ON ACTUAL SITE CONDITIONS, NUMBER OF TIMBER MAT LAYERS TO BE DETERMINED ON SITE.
3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING.
4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND AND AGRICULTURAL LAND DURING RAIN EVENTS. SEDIMENT SHOULD BE REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION.
5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.
6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.
7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.
8. REFER TO EM+CP PLANS FOR EROSION CONTROL DETAILS.
9. REFER TO EM+CP PLANS FOR RESTORATION OF WETLAND.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

	06/19/2023	ISSUED FOR CONSTRUCTION SUBMISSION	SL	JL
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

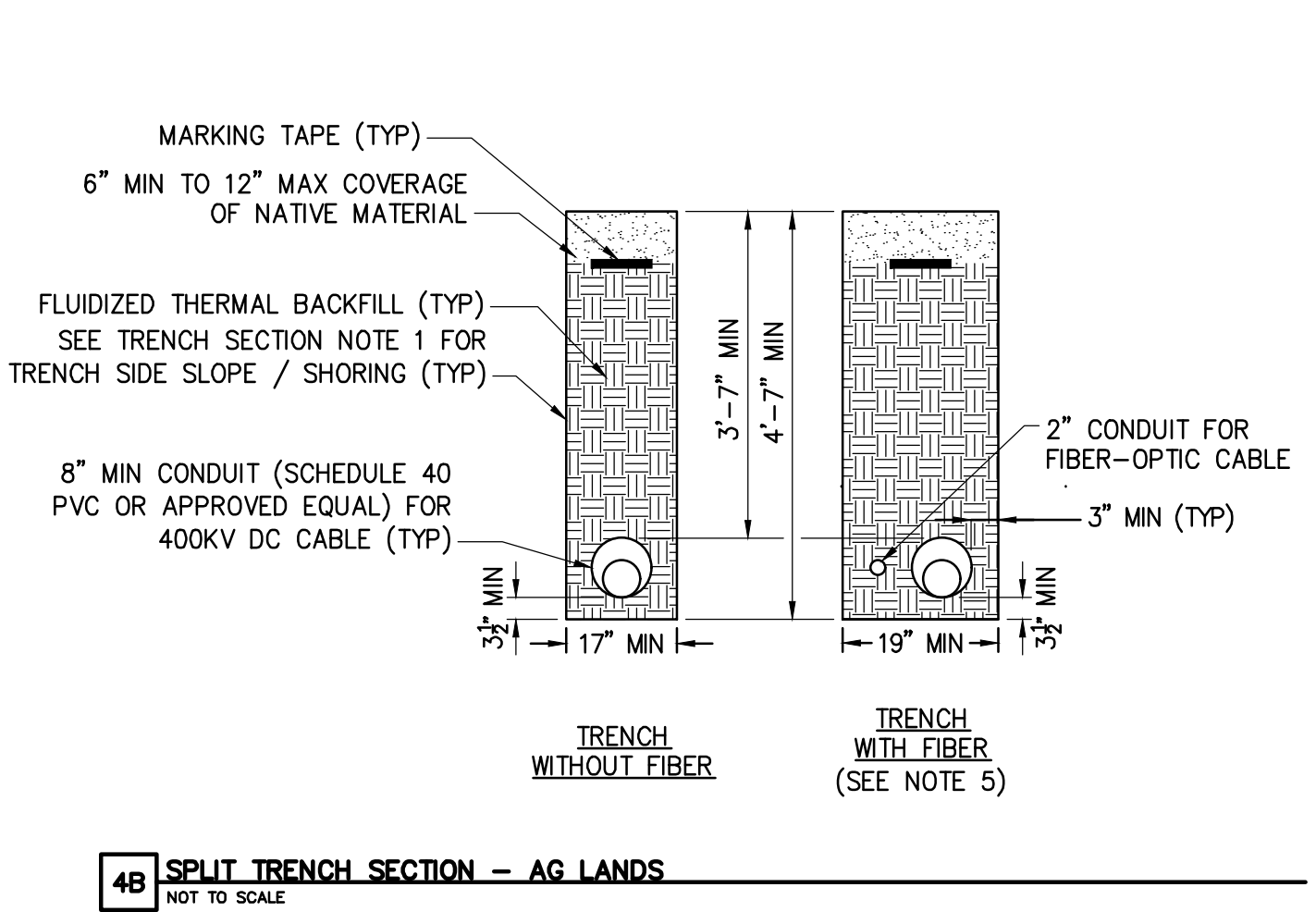
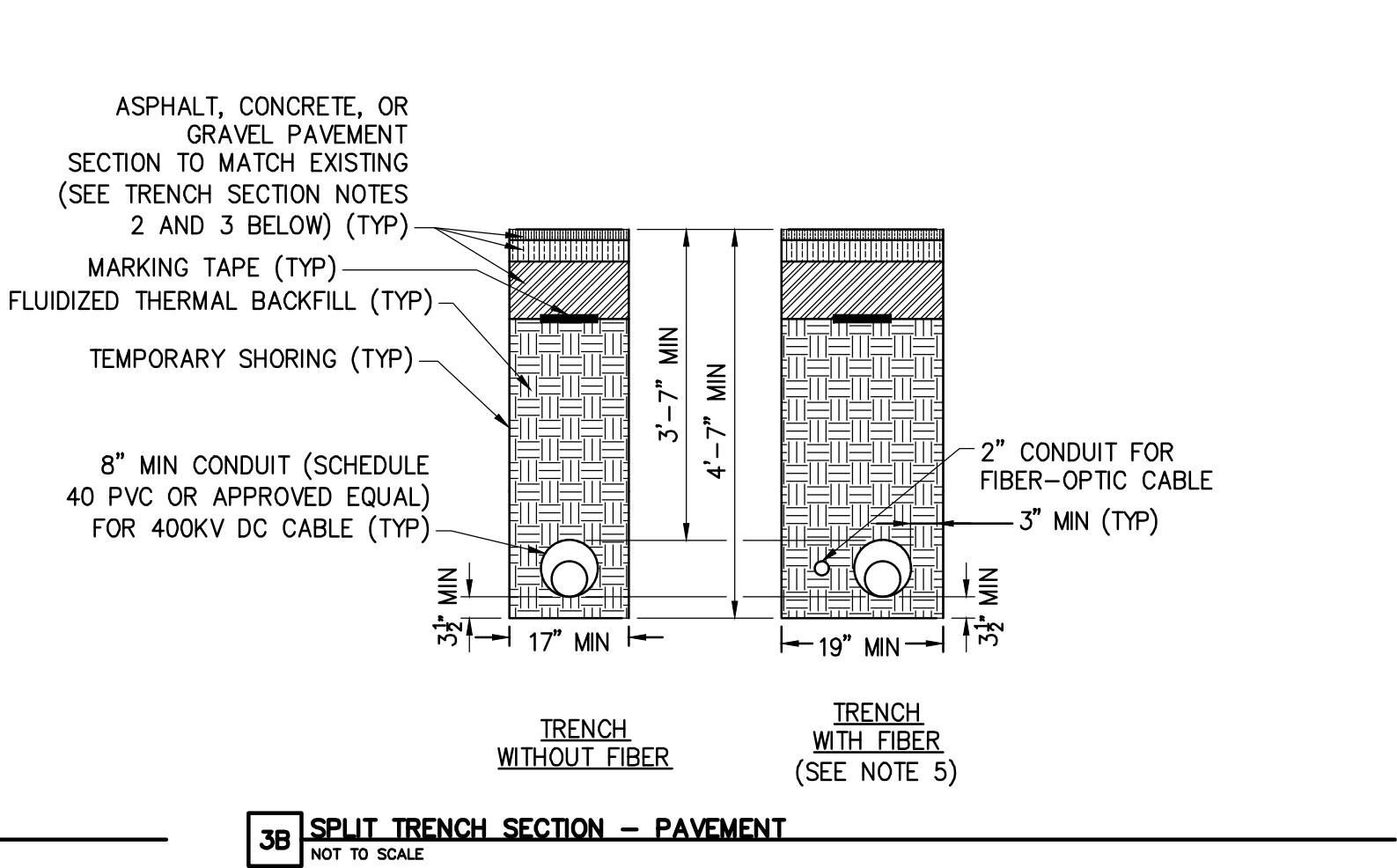
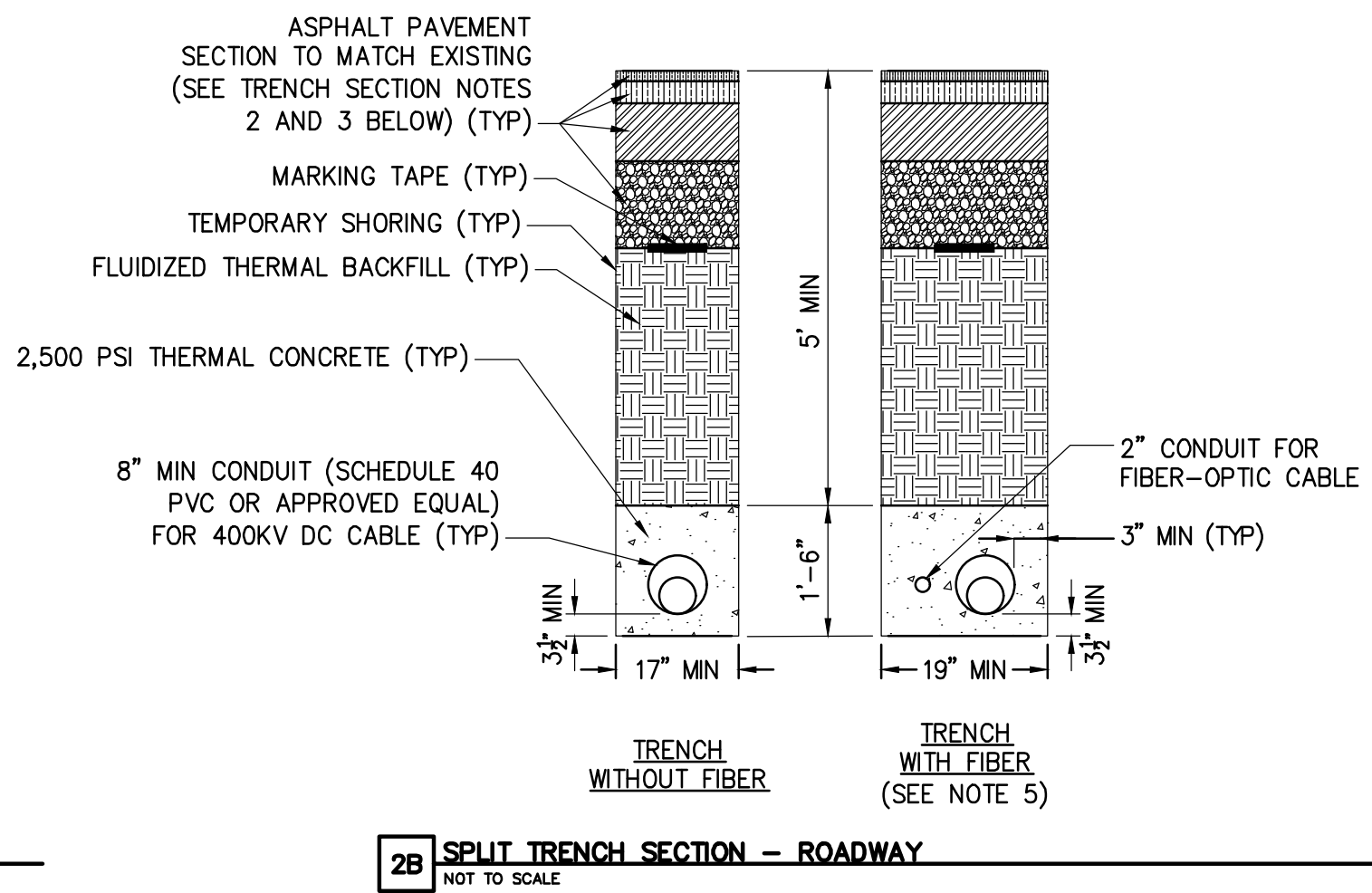
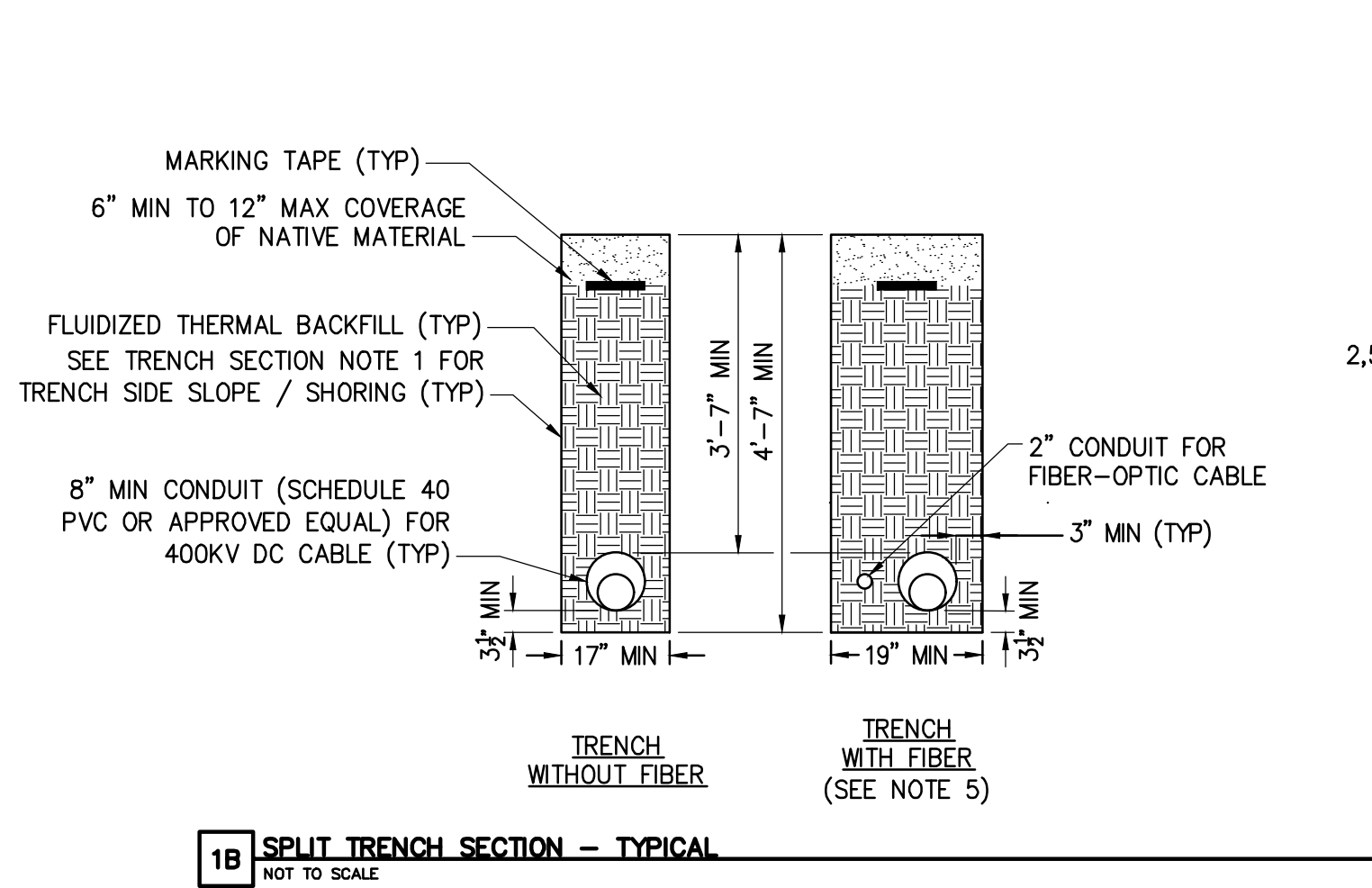
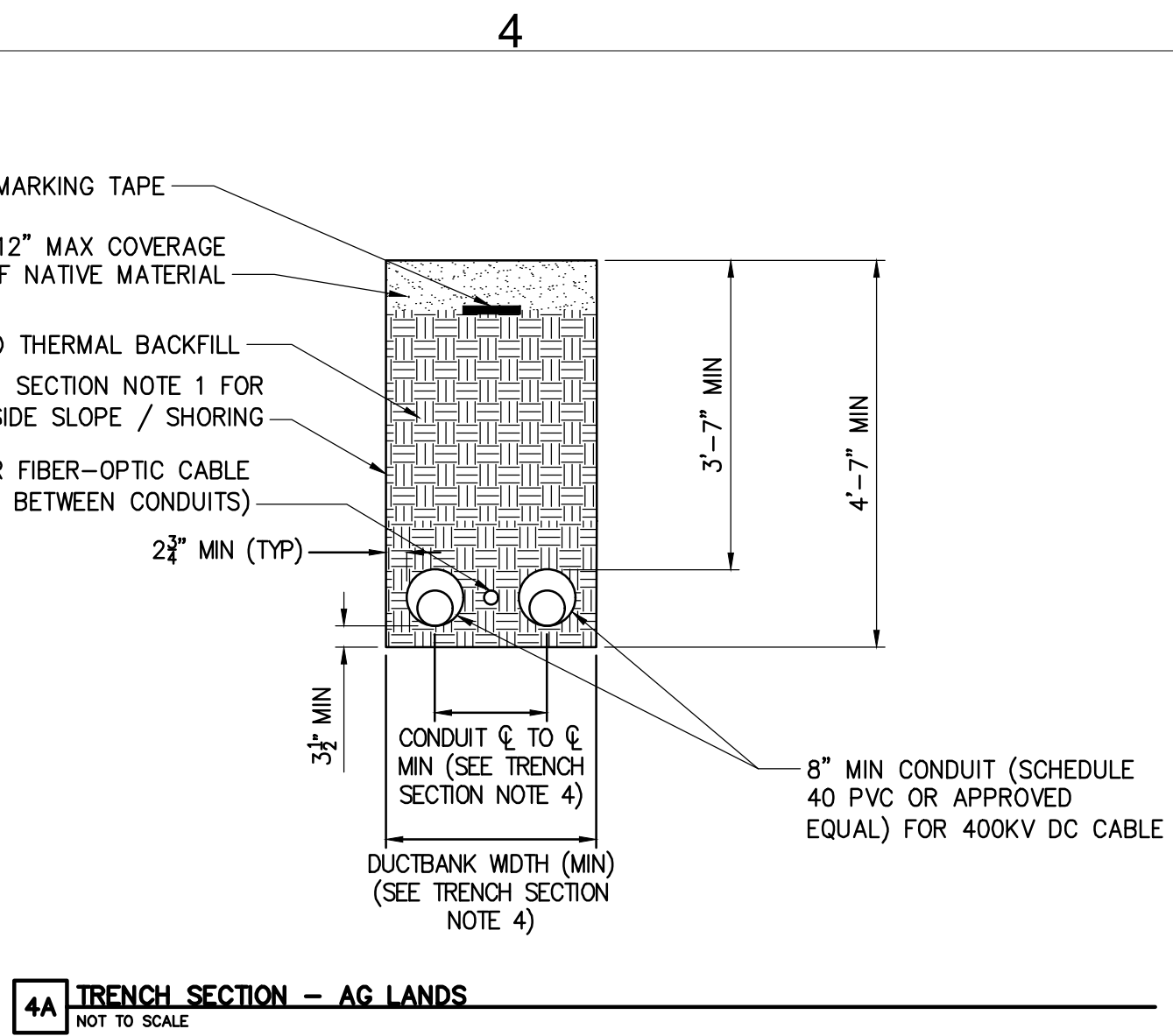
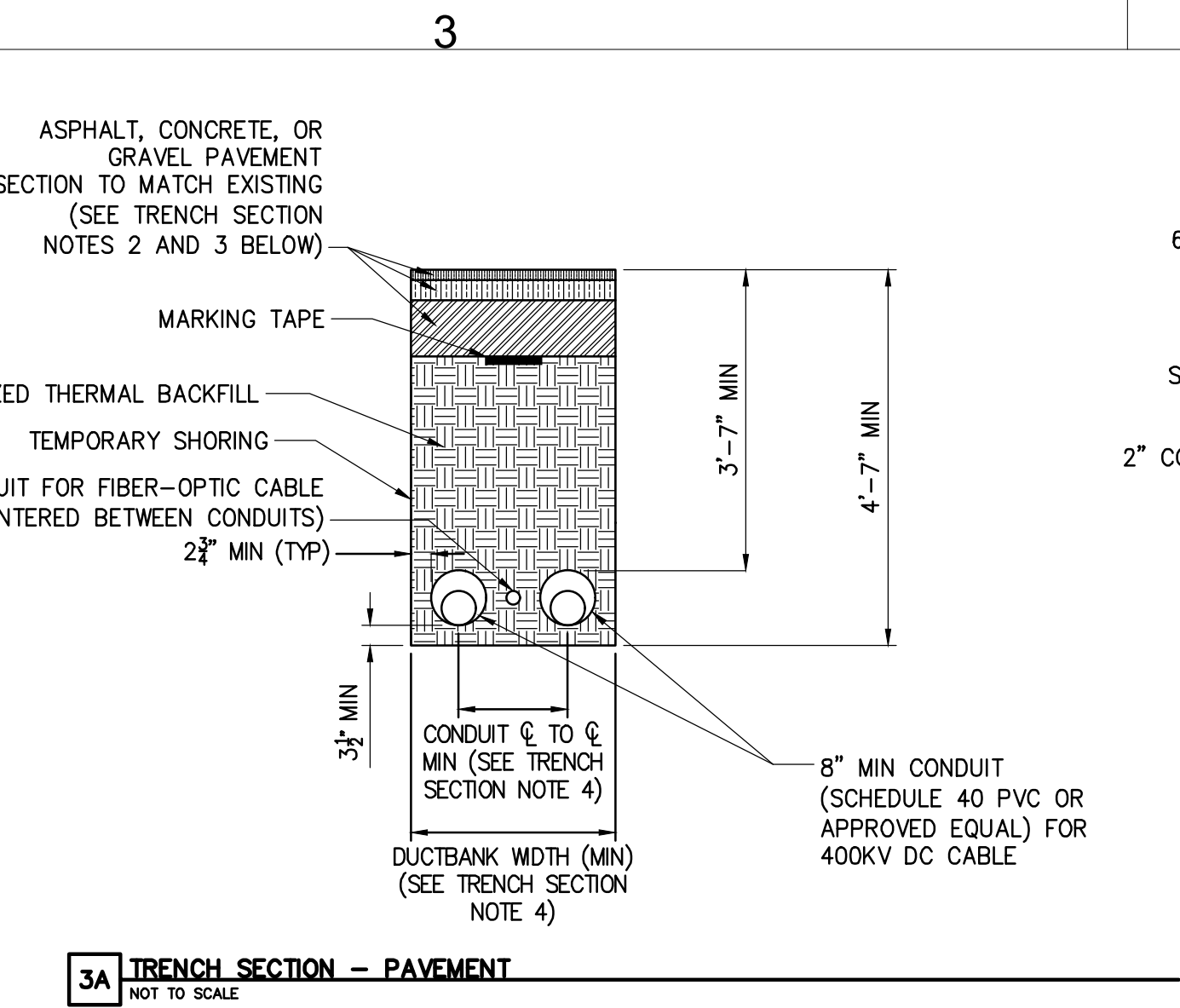
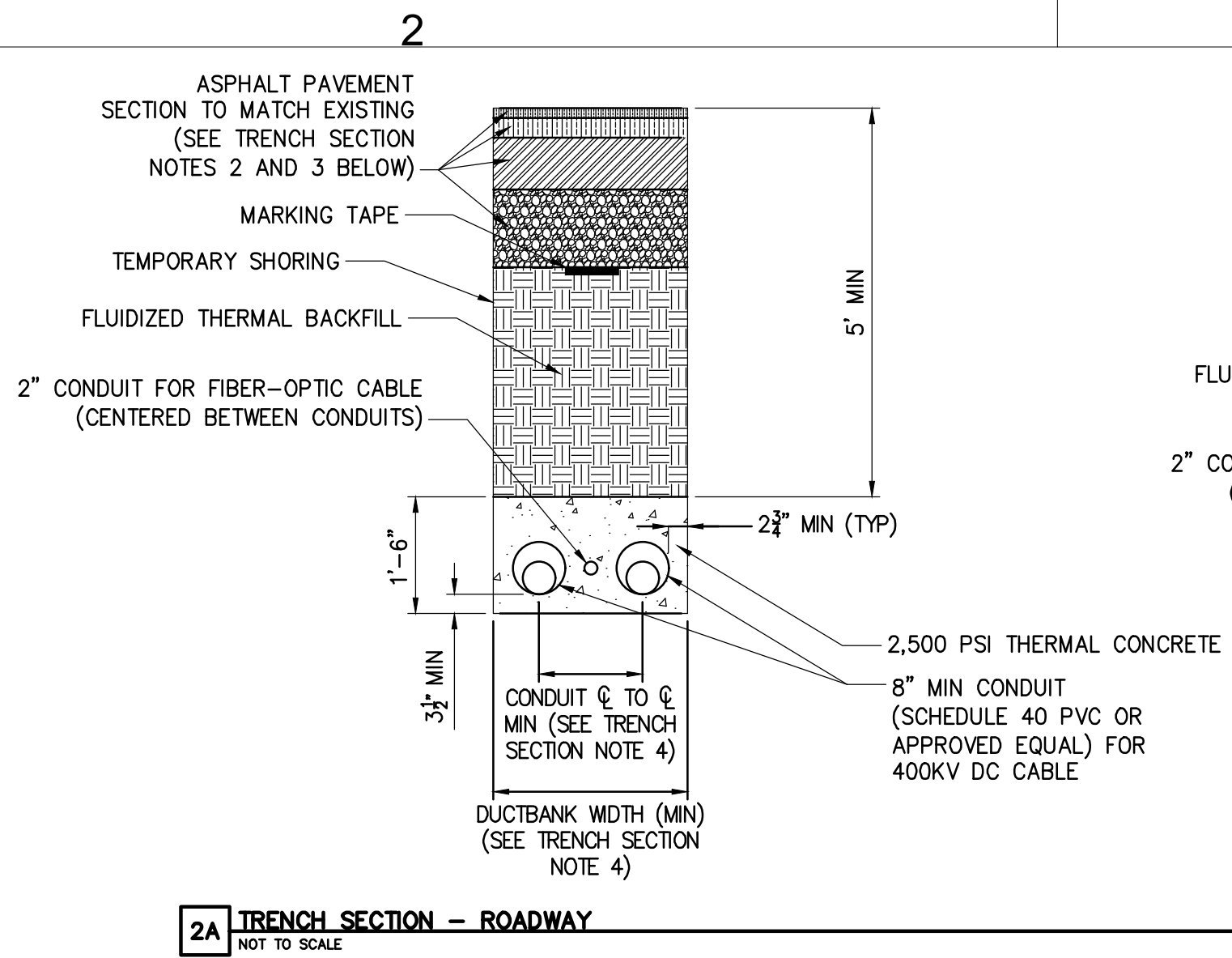
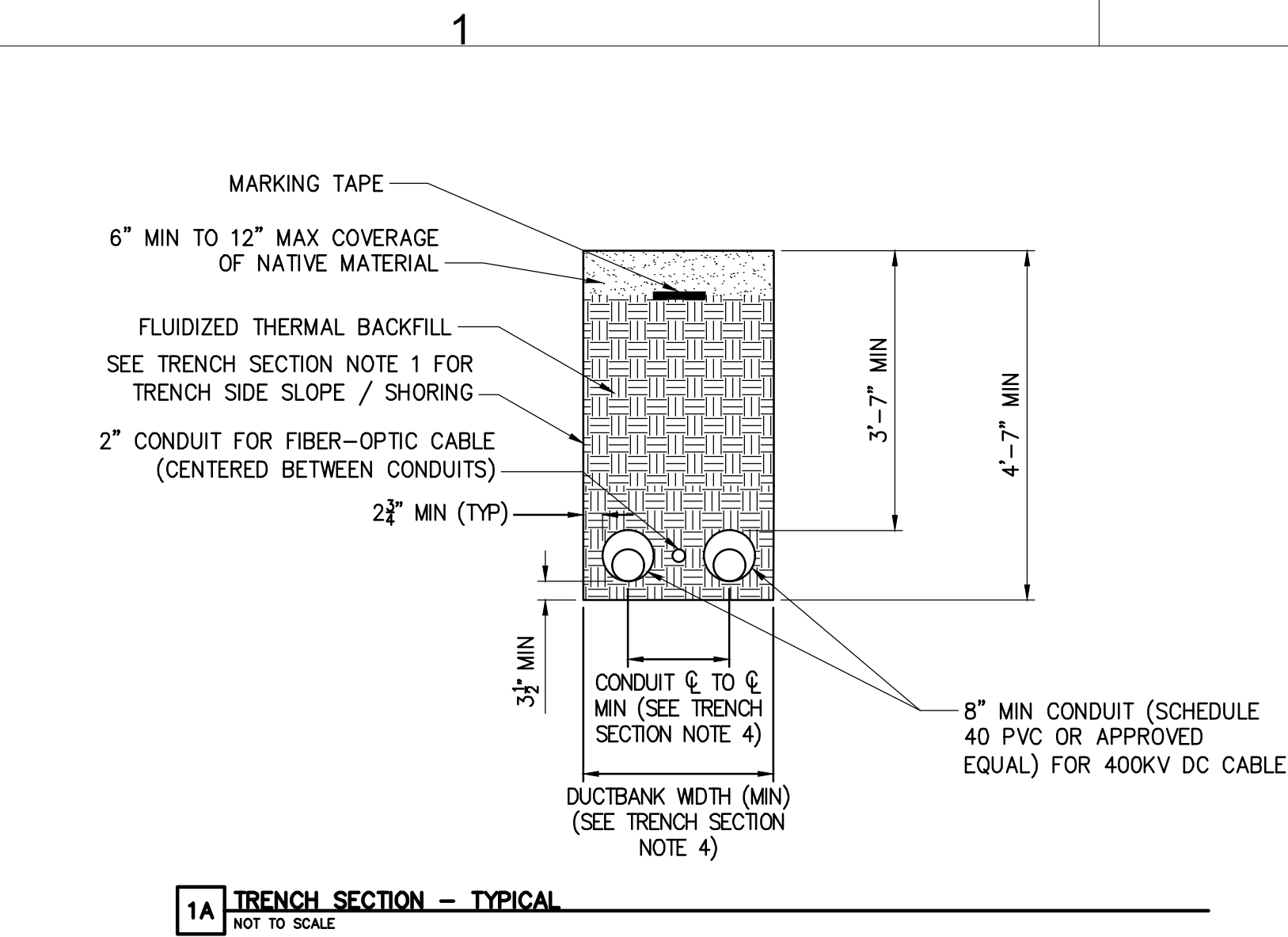
CHAMPLAIN HUDSON POWER EXPRESS

SEGMENT 11 (PACKAGE 7A) - CSX: CATSKILL

WETLAND AND AGRICULTURAL LAND
WORKING SURFACE

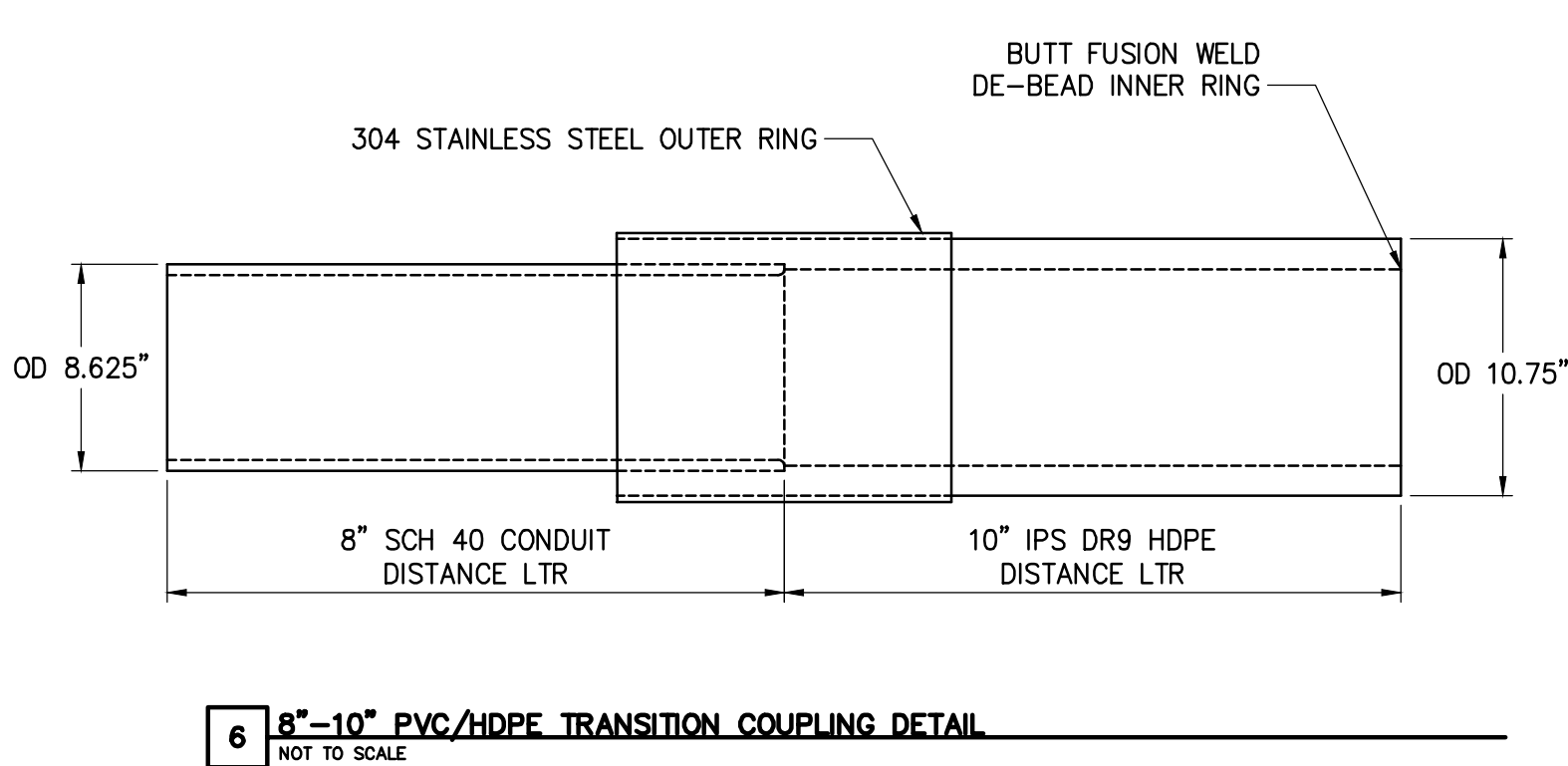
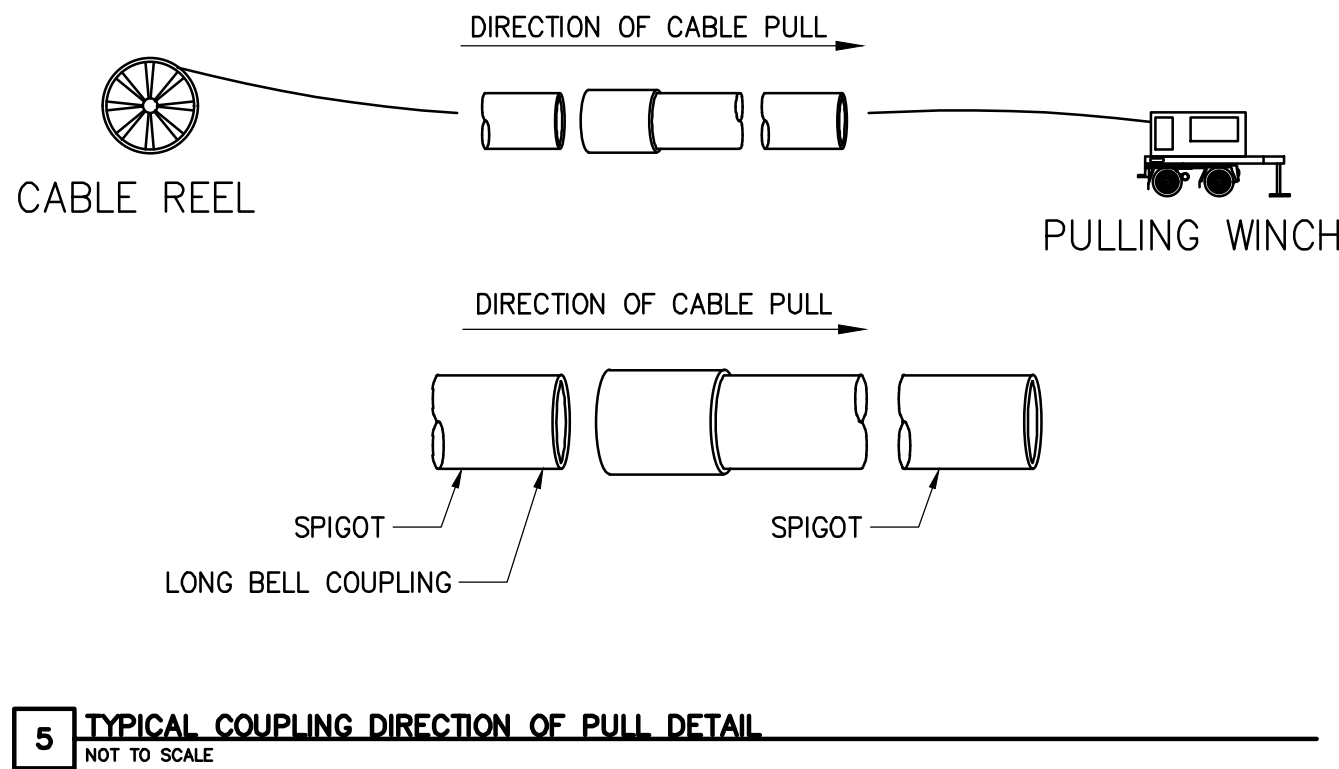
KIEWIT PROJECT NO.	
21162	
KC PROJECT NO.	
120174	
DRAWING NO.	
C-613	
TE	06/19
NO.	OF

File: C:\P\WORKING\REV\DWG\514\21162_7A_C-621.DWG Saved: 6/14/2023 2:07:34 PM Plotted: 6/15/2023 3:31:10 PM Current User: Le, Julian LastSavedBy: Mike Wager



TRENCH SECTION NOTES:

1. SLOPING, BENCHING, OR SHORING SHALL BE IN ACCORDANCE WITH OSHA EXCAVATION STANDARDS, 29 CFR PART 1926, SUBPART P. AT LOCATIONS WHERE THE TRENCH IS NOT SHORED, SLOPING AND/OR BENCHING WILL DEPEND ON TYPE OF SOILS ENCOUNTERED ON SITE. SLOPE FROM EDGE OF ROADWAY TO BOTTOM OF EXCAVATIONS MAY BE FLATTER THAN 2:1 (H:V) FOR AASHTO HS-20 LOADING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EVALUATING SLOPE STABILITY BASED ON ACTUAL EQUIPMENT FOR SITE OPERATIONS AS DETERMINED BY A GEOTECHNICAL ENGINEER.
2. SEE DETAIL 4 ON DETAIL SHEET C-631 FOR PAVEMENT TRANSITION DETAIL.
3. SEE SHEET C-631 FOR SURFACE RESTORATION DETAILS.
4. SEE PLAN AND PROFILE SHEETS FOR CONDUIT Ø TO Ø AND DUCTBANK WIDTH (NOTE ABOVE PROFILE VIEW).
5. FIBER TO LEAD IN AND OUT OF (-) POLARITY HDD BORE, AS DENOTED ON PLAN AND PROFILE SHEETS.



NOTE:
THIS TRANSITION COUPLING COMES ASSEMBLED AS A UNIT. POLY-CAM, ISCO INDUSTRIES P/N: 737-1008PVC40PVI09 TRANSITION COUPLING SHALL ARRIVE FROM VENDOR WITH NO ROUGH EDGES OR PROTRUSIONS ON INTERIOR. INTEGRITY OF COUPLING TO BE FIELD-VERIFIED PRIOR TO INSTALLATION. IF UNSATISFACTORY, CONTRACTOR TO SHAPE OR SAND MINOR IRREGULARITIES PRIOR TO INSTALLATION.



TETRA TECH ENGINEERING AND SURVEYING P.C.
(A NEW YORK PROFESSIONAL CORPORATION)



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

0	06/19/2023	ISSUED FOR CONSTRUCTION SUBMISSION	BV	TK					
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP					

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 11 (PACKAGE 7A) - CSX: CATSKILL
TRENCHING DETAILS

DRAWN BY:	AR	DESIGNED BY:	BV	APPROVED BY:	TK	SCALE	AS SHOWN	DATE	06/19/2023
						REV. NO.	0	SH.NO.	OF

KIEWIT PROJECT NO.

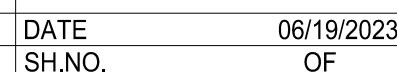
21162

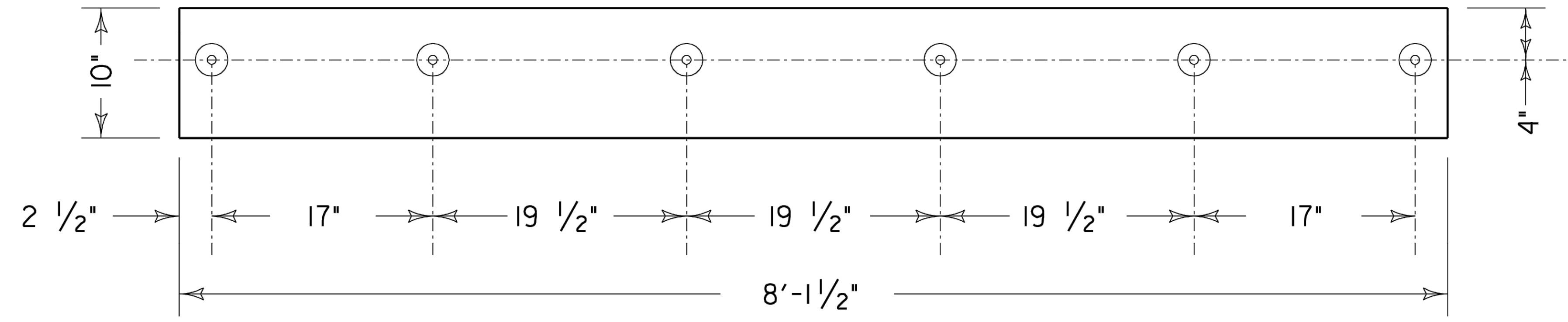
TT PROJECT NO.

204-3701

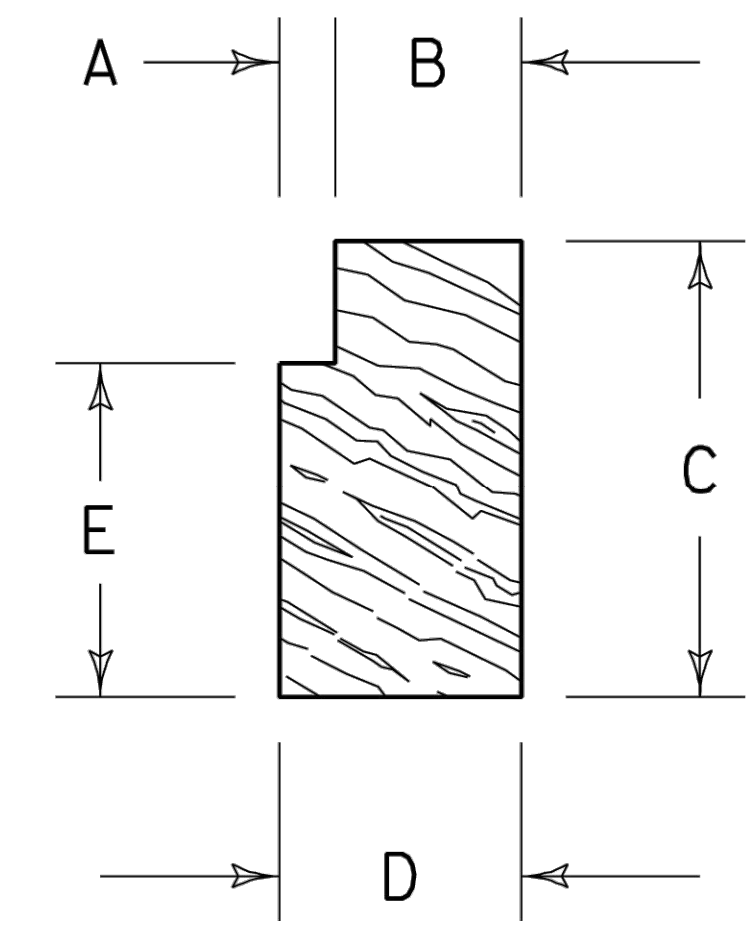
DRAWING NO.

C-621

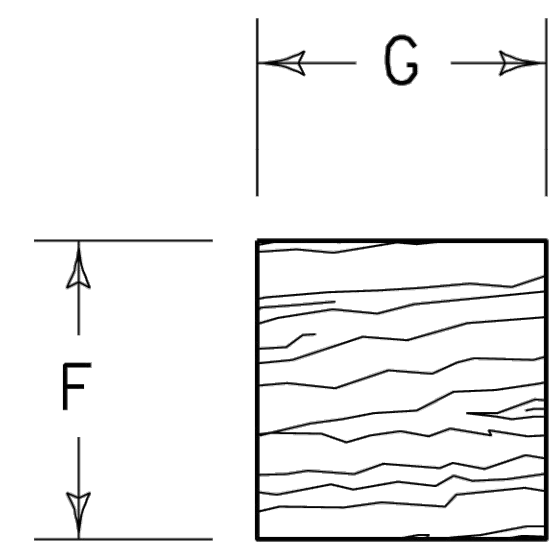




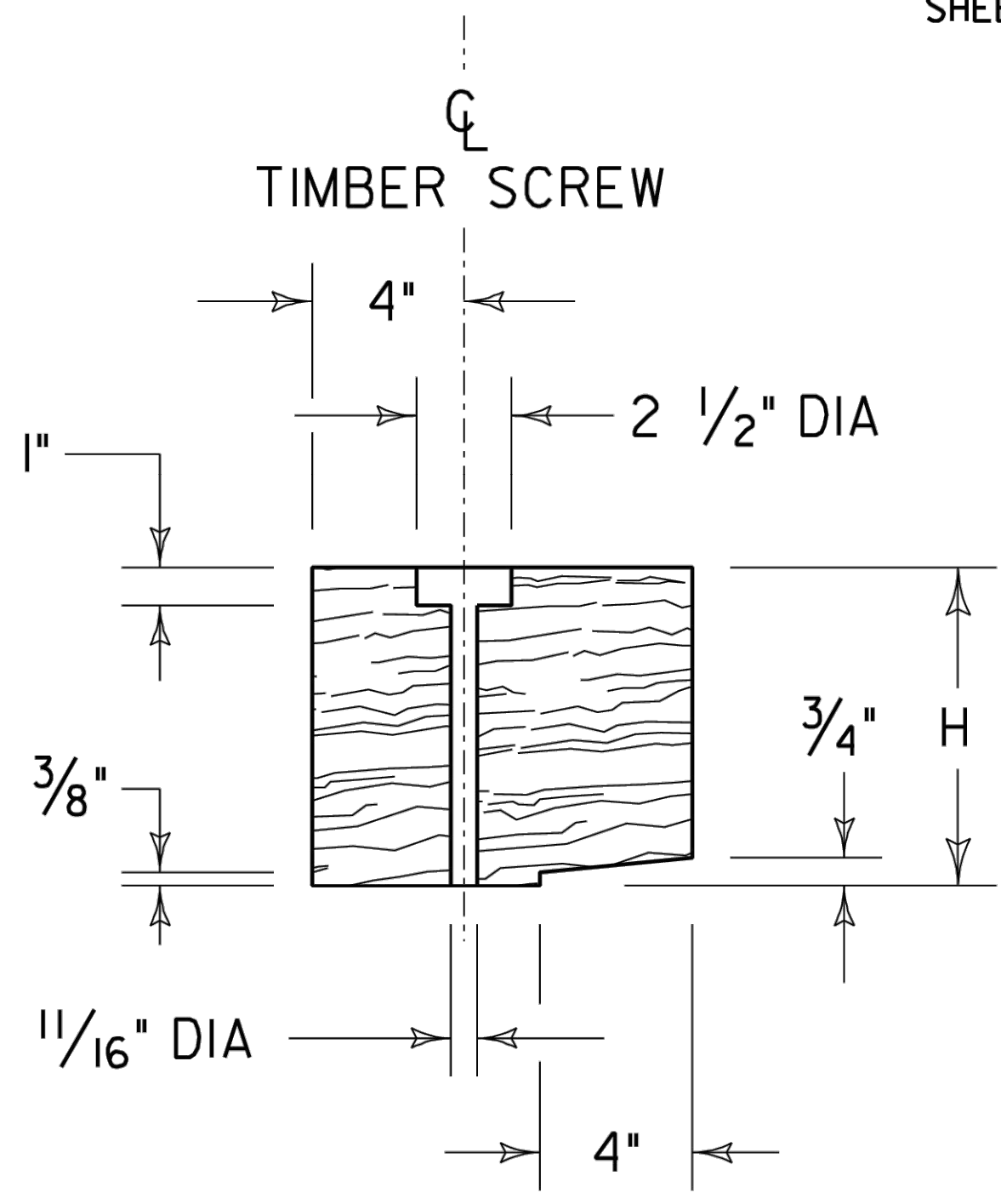
CROSSING TIMBER
PLAN VIEW



FIELD SIDE
FILLER BLOCK DETAIL



GAGE SIDE
FILLER BLOCK DETAIL



CROSSING TIMBER
SECTION AT SCREW LOCATION

- NOTES
- 1. TIMBERS ARE NOT PREDRILLED UNLESS SPECIFIED IN THE REQUISITION.
 - 2. GAGE AND FIELD TIMBERS ARE IDENTICAL.
 - 3. TOLERANCES:
A, E, AND G : 1/16" +/-
ALL OTHERS : 1/8" +/-
 - 4. CROSSING TIMBER TO BE OAK OR GUM.
TREATMENT PER MW SPEC 9900I
LIKE CROSSTIES
 - 5. FILLER BLOCKS TO BE SOUTHERN
YELLOW PINE GRADE 2 WITH
10 LB / CU FT TREATMENT
 - 6. TIMBERS & FILLERS
TO BE MARKED
FOR RAIL SIZE

RAIL WGT	A	B	C	D	E	F	G	H
115 RE	3/4	2 3/4	5 3/8	3 1/2	3 7/8	3 3/8	4	7 1/2
122 CB	3/4	2 3/4	5 5/8	3 1/2	4	3 5/8	4	7 1/2
132 RE	5/8	2 7/8	6	3 1/2	4 1/2	3 7/8	3 7/8	8
136 RE	5/8	2 7/8	6 1/4	3 1/2	4 1/2	3 7/8	3 7/8	8 3/8
140 RE	3/4	2 3/4	6 1/16	3 1/2	4 5/16	3 7/8	4	8 3/8
141 RE	1 1/16	2 13/16	6 3/8	3 1/2	4 1/2	3 7/8	3 7/8	8 3/8

ALL DIMENSIONS ARE IN INCHES.



TIMBER AND ASPHALT CROSSING
CROSSING TIMBER AND FILLER BLOCK DETAILS

APPROVED - CHIEF ENGINEER
MAINTENANCE OF WAY

APPROVED - VICE PRESIDENT
ENGINEERING

PREPARED BY: J. E. BEYERL
ISSUED: MARCH 22, 2005
REVISED: NOVEMBER 14, 2005

File: C:\PWORKING\RE\1\0\5\5141\21162_7A_C-626.DWG Saved: 6/14/2023 5:36:40 PM Plotted: 6/14/2023 5:36:54 PM Current User: Weger, Mike Last Saved By: Mike Weger



TETRA TECH ENGINEERING AND SURVEYING P.C.
(A NEW YORK PROFESSIONAL CORPORATION)

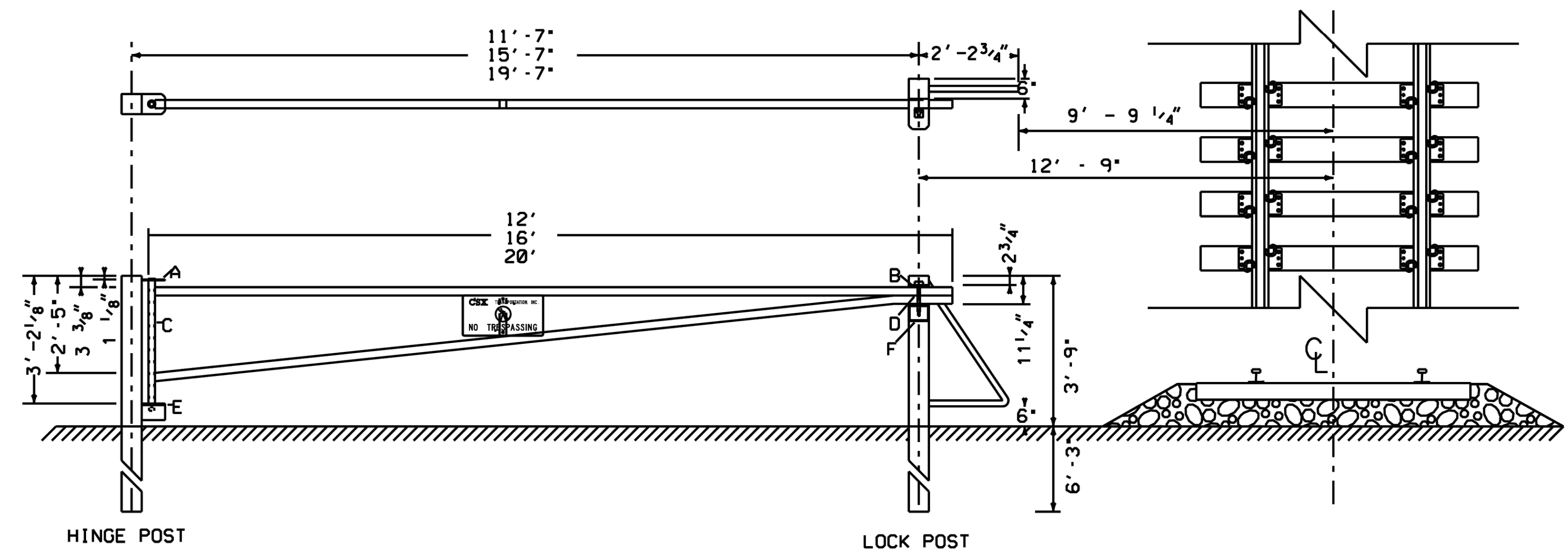
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

0	06/19/2023	ISSUED FOR CONSTRUCTION SUBMISSION	BV	TK	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 11 (PACKAGE 7A) - CSX: CATSKILL
CSX CROSSING - DETAIL 2 OF 2

DRAWN BY: AR DESIGNED BY: BV APPROVED BY: TK
SCALE AS SHOWN REV. NO. 0 SH. NO.

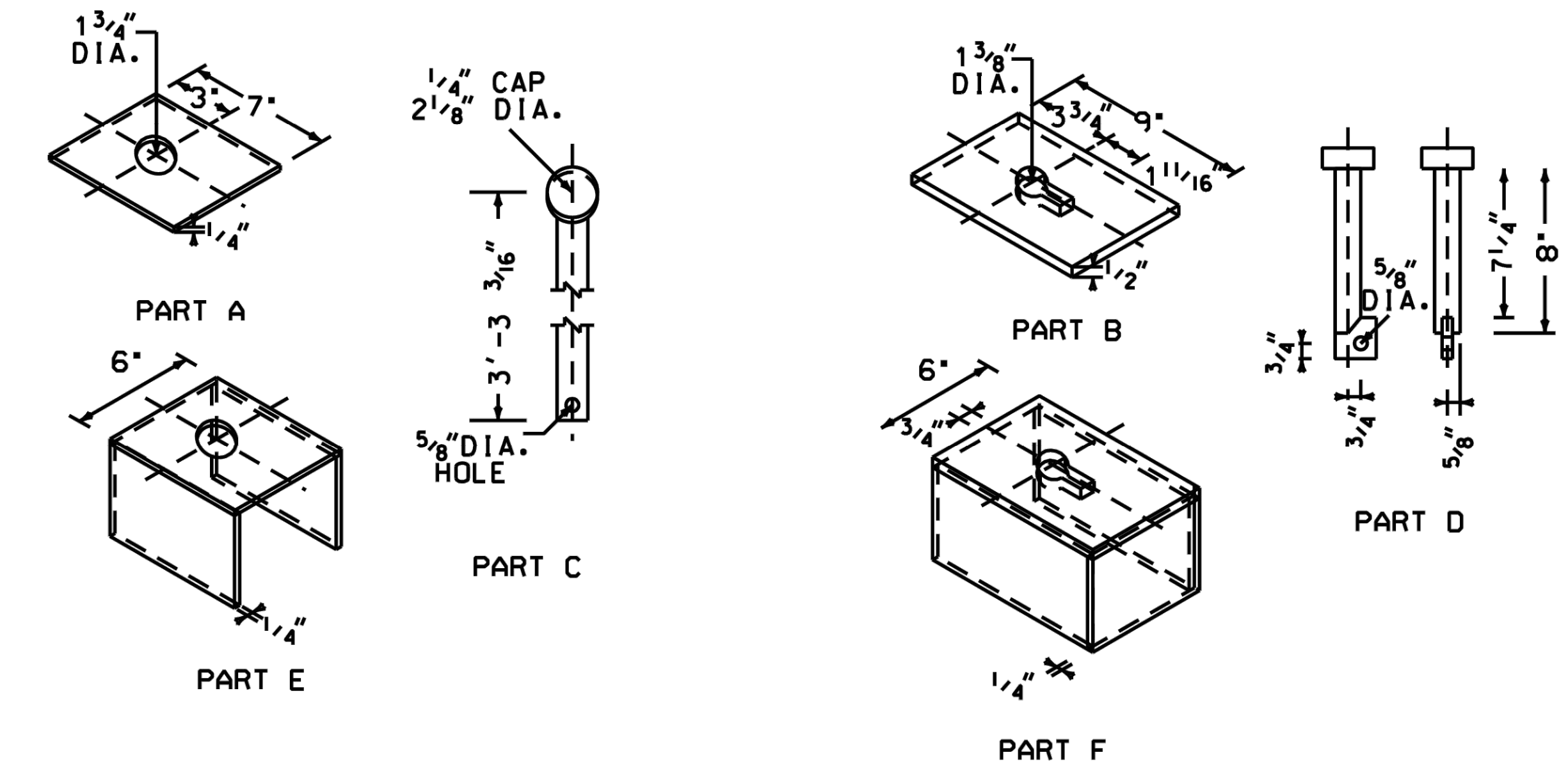
KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	C-626
DATE	06/19/2023
SH. NO.	OF



2614

NOTES

- 1. METAL WEDGE SIGN MOUNT SHALL BE 2 1/2" WIDE, WELDED IN THE CENTER OF THE GATE WITH HOLES DRILLED FOR THE ATTACHMENT OF NO TRESPASSING SIGN
- 2. SIGN SPECIFICATION IS FOUND IN STANDARD DRAWING 2703
- 3. ENTIRE GATE ASSEMBLY TO BE PAINTED AREMA YELLOW.
- 4. CSX SWITCH LOCK TO BE USED FOR GATE LOCK.



DESCRIPTION	UNITS	CLASS	ITEM NUMBER
GATE, RIGHT OF WAY, 12'	EACH	014	0409045
GATE, RIGHT OF WAY, 16'	EACH	014	0409043
GATE, RIGHT OF WAY, 20'	EACH	014	0409041
LOCK, SWITCH AMERICAN H10	EACH	450	0008580



RIGHT-OF-WAY SECURITY GATE

J. E. BEYERL FOR

APPROVED - DIRECTOR

ENGINEERING STANDARDS

APPROVED - VICE PRESIDENT

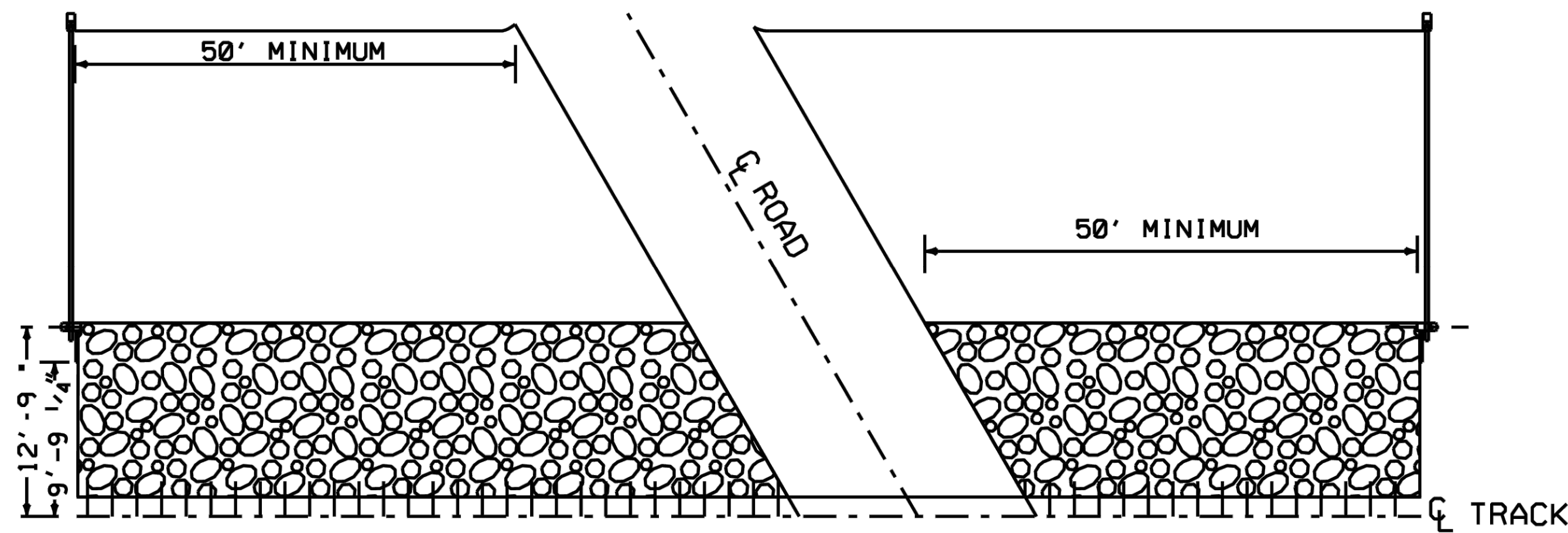
ENGINEERING

PREPARED BY:

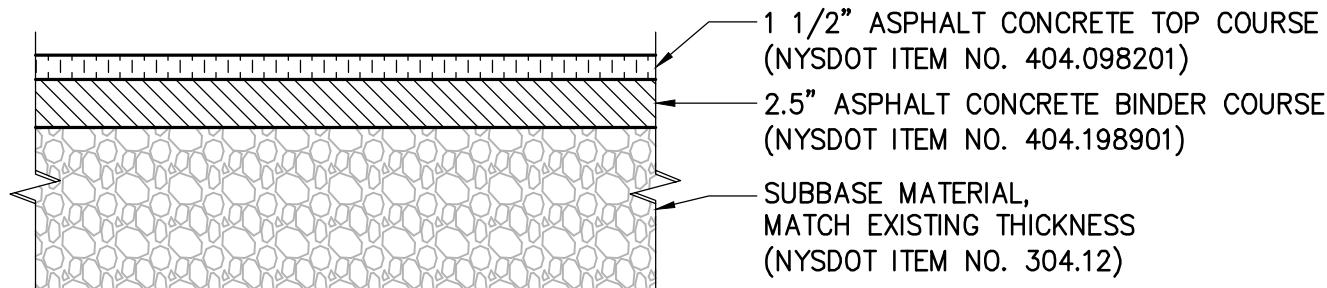
M. E. AUSTIN

ISSUED: JANUARY 10, 2011

REVISED : INITIAL ISSUE



File: C:\PWORKING\RE\10155141\21162_7A_C-627.DWG Saved: 6/14/2023 4:27:20 PM Plotted: 6/14/2023 5:45:31 PM Current User: Wager, Mike LastSavedBy: Mike Wager

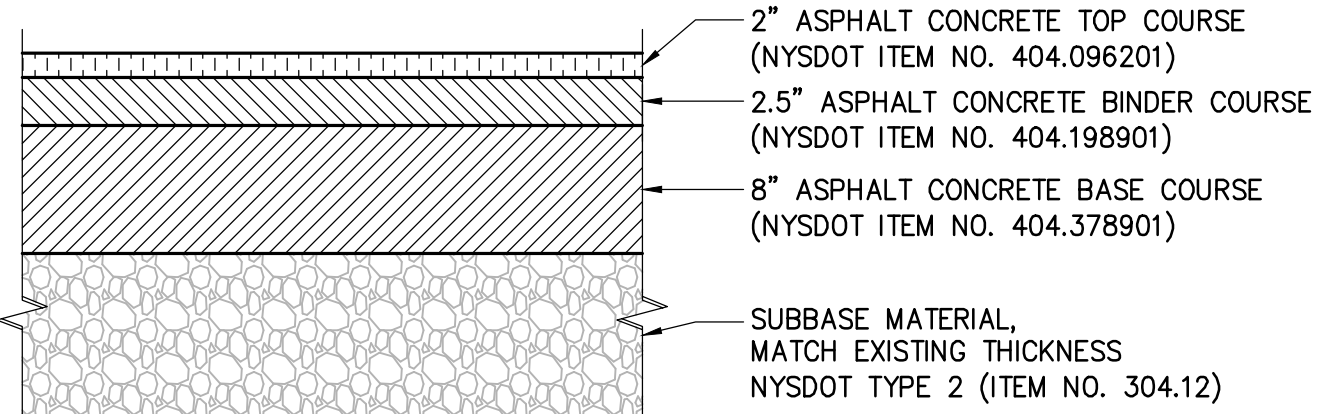


- NOTES:
1. ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION IF EXISTING THICKNESS IS GREATER
 2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS.
 3. APPLY STRAIGHT TACK COAT TO BETWEEN PAVEMENT SECTIONS NYSDOT ITEM407.0103

ASPHALT CONCRETE PAVEMENT DETAIL
(PRIVATE DRIVEWAY)

1

SCALE: N.T.S.

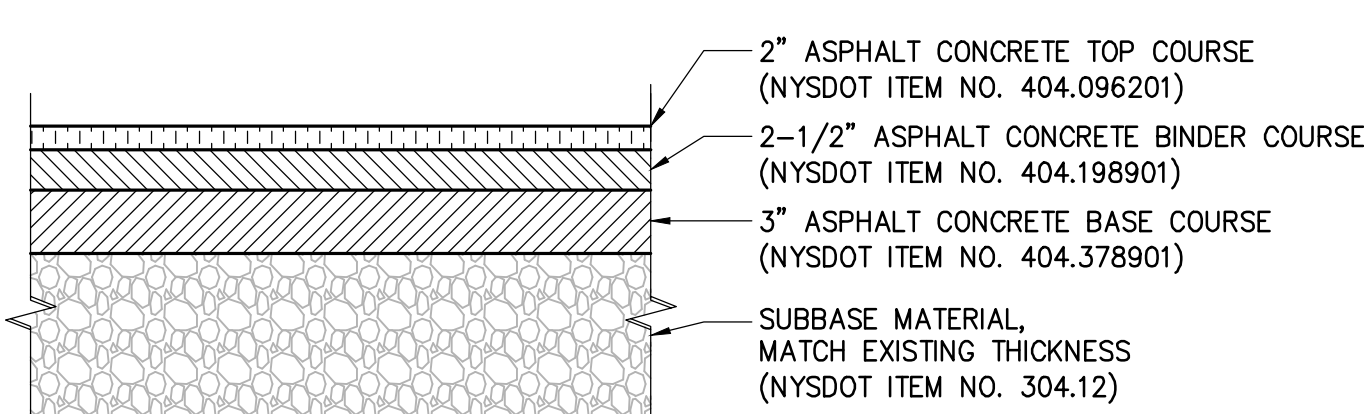


- NOTES:
1. ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION IF EXISTING THICKNESS IS GREATER
 2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS
 3. APPLY STRAIGHT TACK COAT TO BETWEEN PAVEMENT SECTIONS NYSDOT ITEM 407.0103

ASPHALT CONCRETE PAVEMENT
(WITHIN NYSDOT ROADWAYS)

2

SCALE: N.T.S.

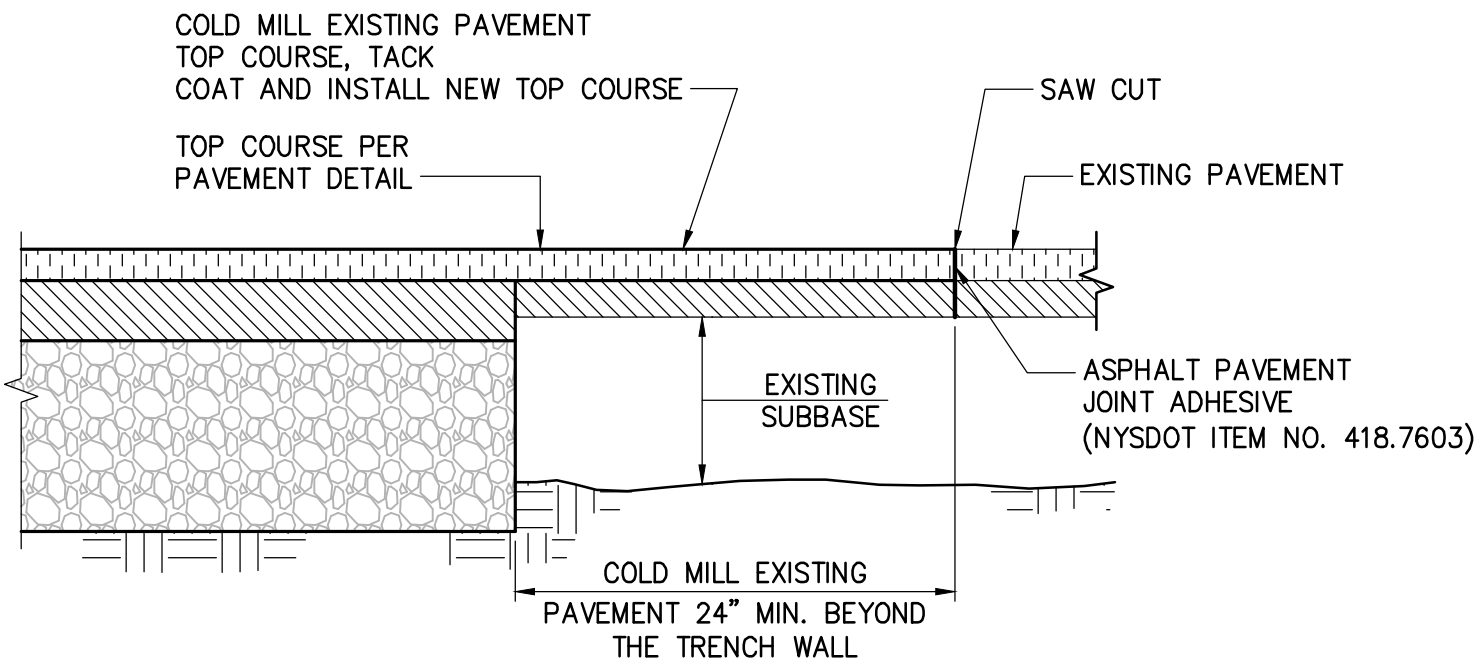


- NOTES:
1. ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION IF EXISTING THICKNESS IS GREATER
 2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS
 3. APPLY STRAIGHT TACK COAT TO BETWEEN PAVEMENT SECTIONS NYSDOT ITEM407.0103

ASPHALT CONCRETE PAVEMENT
(WITHIN COUNTY OR TOWN ROADWAYS)

3

SCALE: N.T.S.

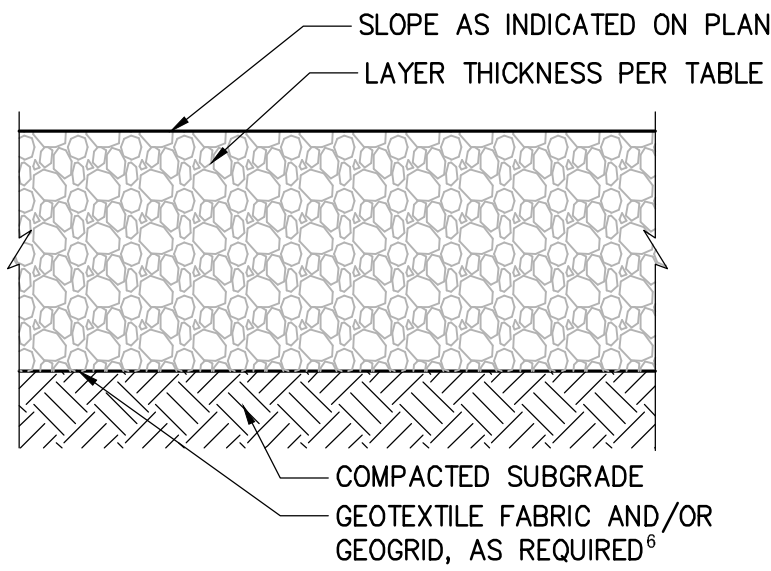


- NOTE:
1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

PAVEMENT TRANSITION DETAIL
(WITHIN NYSDOT, COUNTY, OR TOWN ROADWAYS)

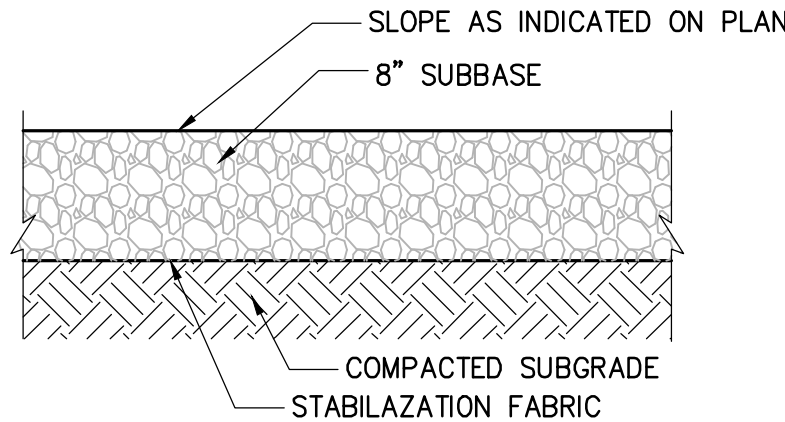
4

SCALE: N.T.S.



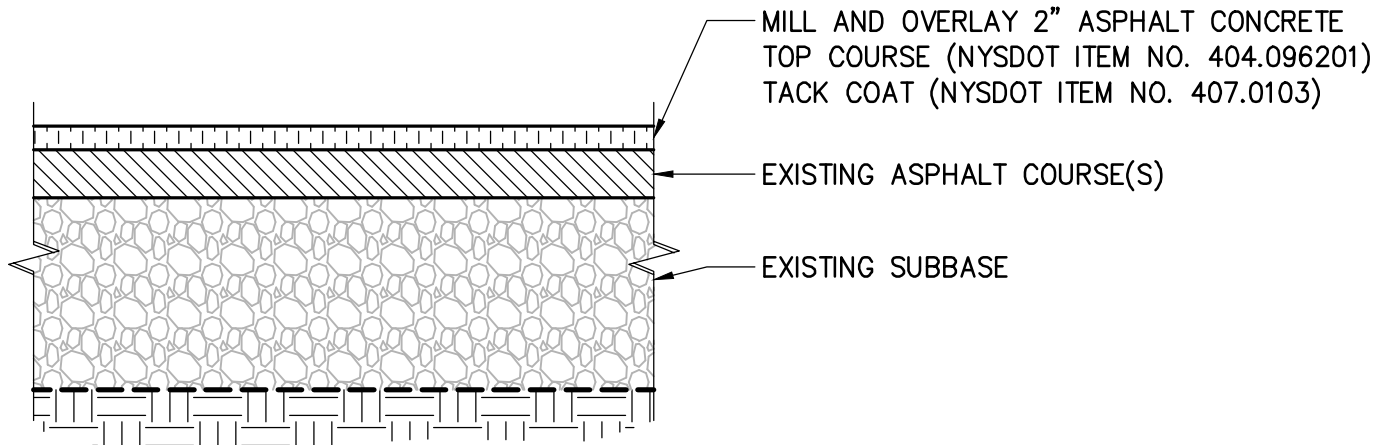
TEMPORARY ACCESS ROAD SECTION ^{1,2,3,4}				
CBR ⁵	UNSTABILIZED	MIRAFI 180N GEOTEXTILE ⁶	TENSAR BX1200 GEOGRID ⁶	MIRAFI RSI SERIES GEOTEXTILE ⁶
0.5	--	20 INCH RIP RAP ⁷ + 6 INCH AGGREGATE	--	20 INCH RIP RAP ⁷ + 4 INCH AGGREGATE (RS580I)
1.0	--	18 INCH AGGREGATE	12 INCH AGGREGATE	15 INCH AGGREGATE (RS280I)
1.5	--	12 INCH AGGREGATE	9 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
2.0	18 INCH AGGREGATE	11 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
3.0+	15 INCH AGGREGATE	8 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)

- NOTES:
- 1 TEMPORARY ACCESS ROAD SECTIONS PER KIEWIT ENGINEERING (NY) CORP.
 - 2 AGGREGATE SHALL BE NYSDOT TYPE 2 CRUSHED AGGREGATE OR APPROVED ALTERNATIVE.
 - 3 DESIGN CONSIDERS 1,000 PASSES OF MAXIMUM 22-KIP AXLE LOAD AND A DESIGN RUT DEPTH OF 3 INCHES. ADDITIONAL AXLE PASSES, HEAVIER AXLE LOADS, AND DETERIORATED SUBGRADE CONDITIONS MAY REQUIRE THICKER AGGREGATE SECTIONS OR ADDITIONAL MAINTENANCE.
 - 4 ALTERNATE TEMPORARY ACCESS ROAD DESIGNS MAY BE PROVIDED BY KIEWIT ENGINEERING, AS REQUIRED, BASED ON FIELD CONDITIONS AND TRAFFIC LOADING.
 - 5 ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER OR ALTERNATIVE METHOD APPROVED BY GEOTECHNICAL ENGINEER OF RECORD (EOR). CBR OF IN-SITU SOIL MAY VARY SEASONALLY DUE TO FREEZE/THAW AND BASED ON MOISTURE CONDITIONS.
 - 6 GEOGRID AND GEOTEXTILE
 - A GEOGRID AND GEOTEXTILES SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS.
 - B SPECIFIED GEOTEXTILE OR GEOGRID MAY BE REPLACED BY EQUIVALENT MATERIAL APPROVED BY EOR.
 - C GEOTEXTILE IS REQUIRED IN REGULATED WETLANDS AND AGRICULTURAL LANDS.
 - D GEOTEXTILE SEPARATOR FABRIC IS REQUIRED BENEATH GEOGRID ON COHESIVE SUBGRADE
 - 7 RIP RAP
 - A RIP RAP SHALL BE NYSDOT LIGHT STONE FILL OR APPROVED ALTERNATIVE.
 - B A LAYER OF #57 STONE IS RECOMMENDED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE GEOTEXTILE FABRIC WHERE RIP RAP IS USED.



GRAVEL PAVEMENT

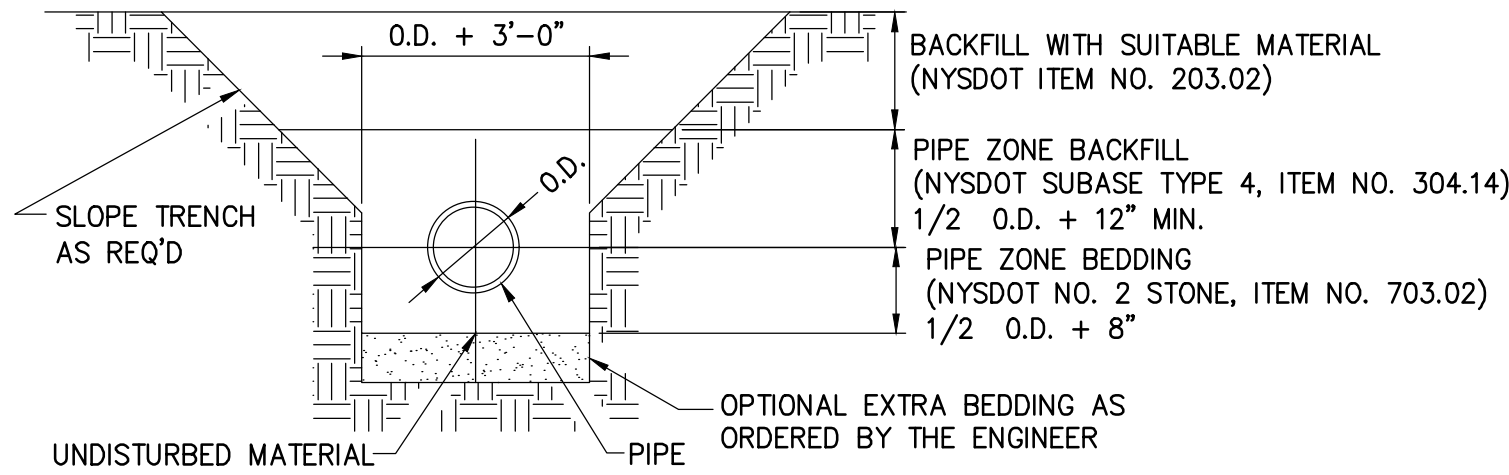
SCALE: N.T.S.



- NOTES:
1. APPLY TACKCOAT TO MILLED SURFACE PRIOR TO PLACING ASPHALT CONCRETE TOP COURSE.
 2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

MILL AND OVERLAY ASPHALT CONCRETE PAVEMENT DETAIL

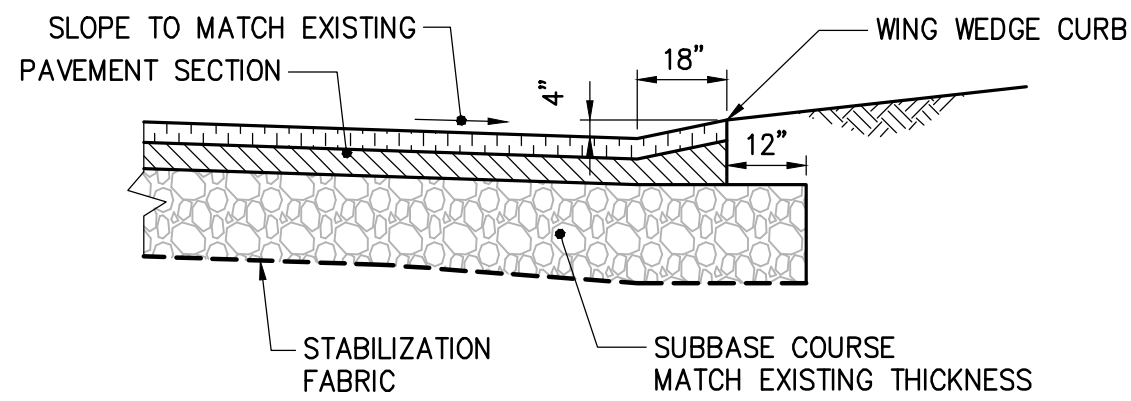
SCALE: N.T.S.



- NOTES:
1. WHERE IDENTIFIED ON PLANS, CULVERT REPLACEMENTS AND/OR REPAIR TO BE COMPLETED IN ACCORDANCE WITH NYSDOT STANDARD SHEETS (NYSDOT STANDARD SHEET GROUP 603 CULVERTS AND STORM DRAINS AND NYSDOT BRIDGE DETAIL SHEETS BD-CB1 THRU BD-CB13)

CULVERT REPLACEMENT

SCALE: N.T.S.



- NOTE:
1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

WING WEDGE CURB DETAIL

SCALE: N.T.S.



TETRA TECH ENGINEERING AND SURVEYING P.C.
(A NEW YORK PROFESSIONAL CORPORATION)



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED. THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

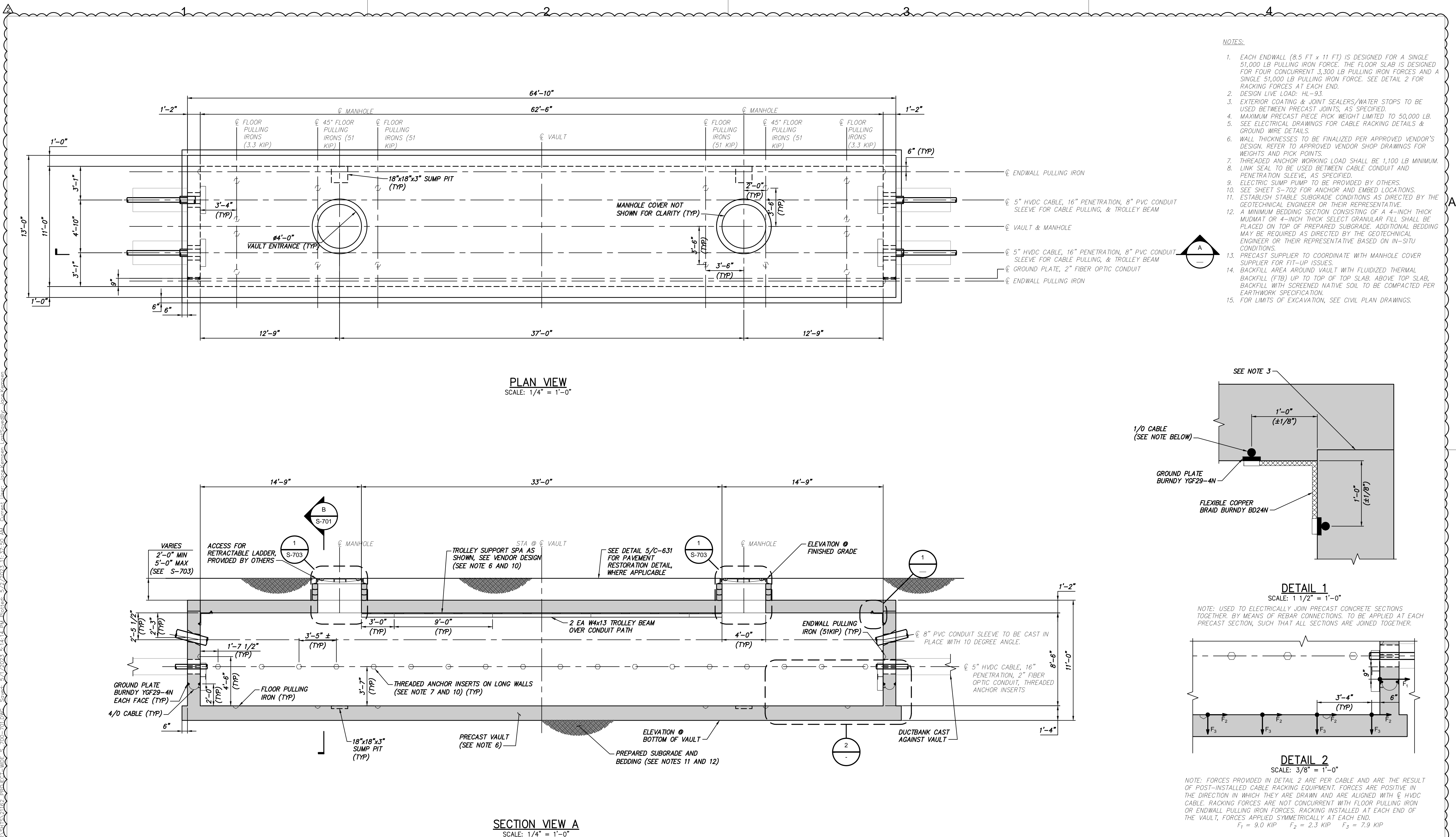
0	06/19/2023	ISSUED FOR CONSTRUCTION SUBMISSION	BV	TK	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	


CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 11 (PACKAGE 7A) - CSX: CATSKILL
SURFACE RESTORATION DETAILS

DRAWN BY:	DESIGNED BY: BV	APPROVED BY: TK	SCALE: AS SHOWN	DATE: 06/19/2023
			REV. NO. 0	SH.NO. -- OF


KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	

C-631






Champlain Hudson Power Express



Kiewit



STATE OF NEW YORK
MARK WANCHIN
LICENSED PROFESSIONAL ENGINEER
090036

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
2	05/03/2023	RFC - VAULT UPDATES	JNK	OO
1	02/10/2023	REV 1 - IFC - NYSDOT HWP, SPLICE VAULT UPDATES	JNK	OO
0	12/21/2022	IFC SUBMISSION	JNK	OO

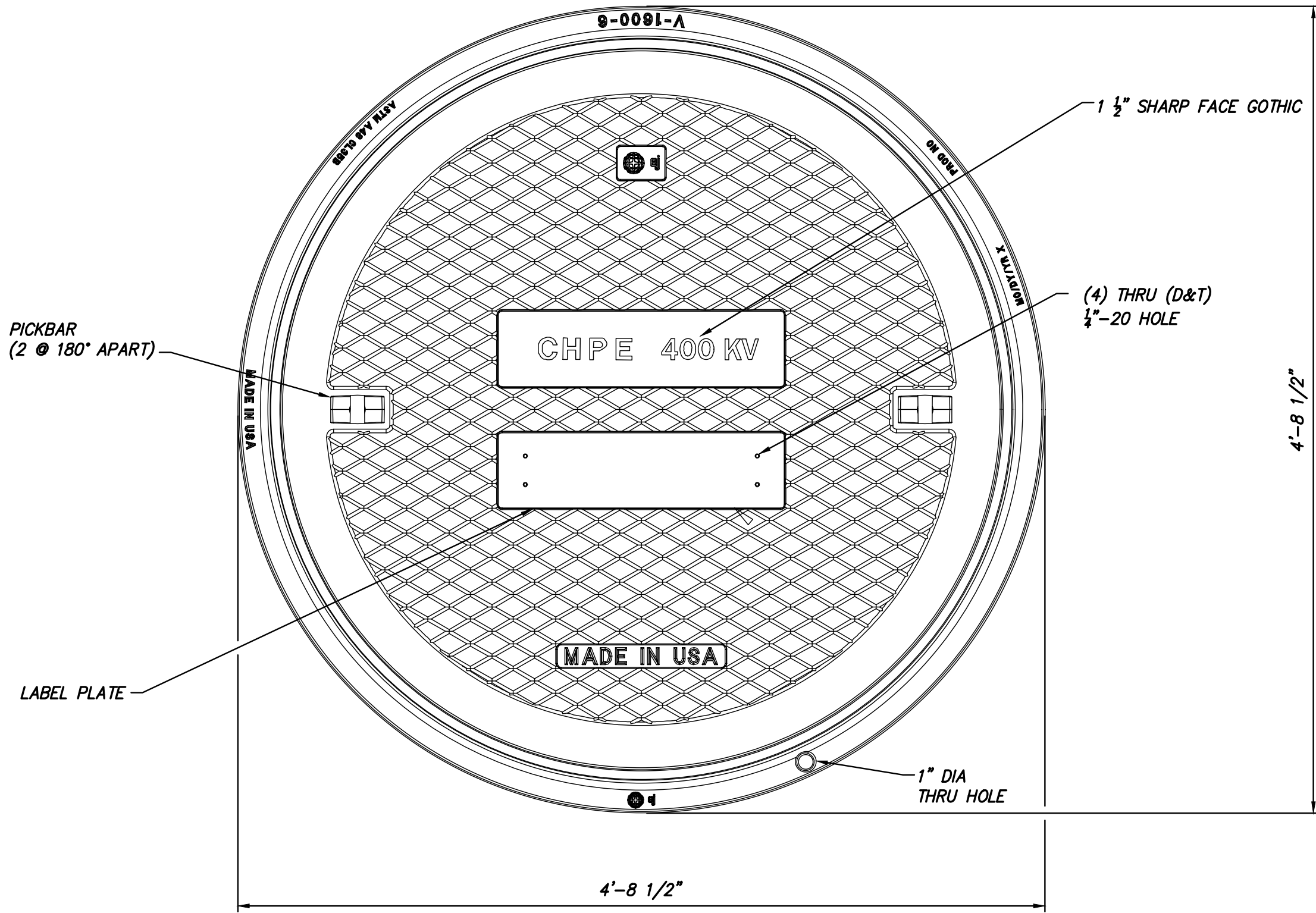
CHAMPLAIN HUDSON POWER EXPRESS

SPLICE VAULT PLAN AND ELEVATION

DRAWN BY: DRH	DESIGNED BY: JNK	APPROVED BY: OO	SCALE: AS SHOWN	DATE: 2
			REV. NO.	SH.NO.

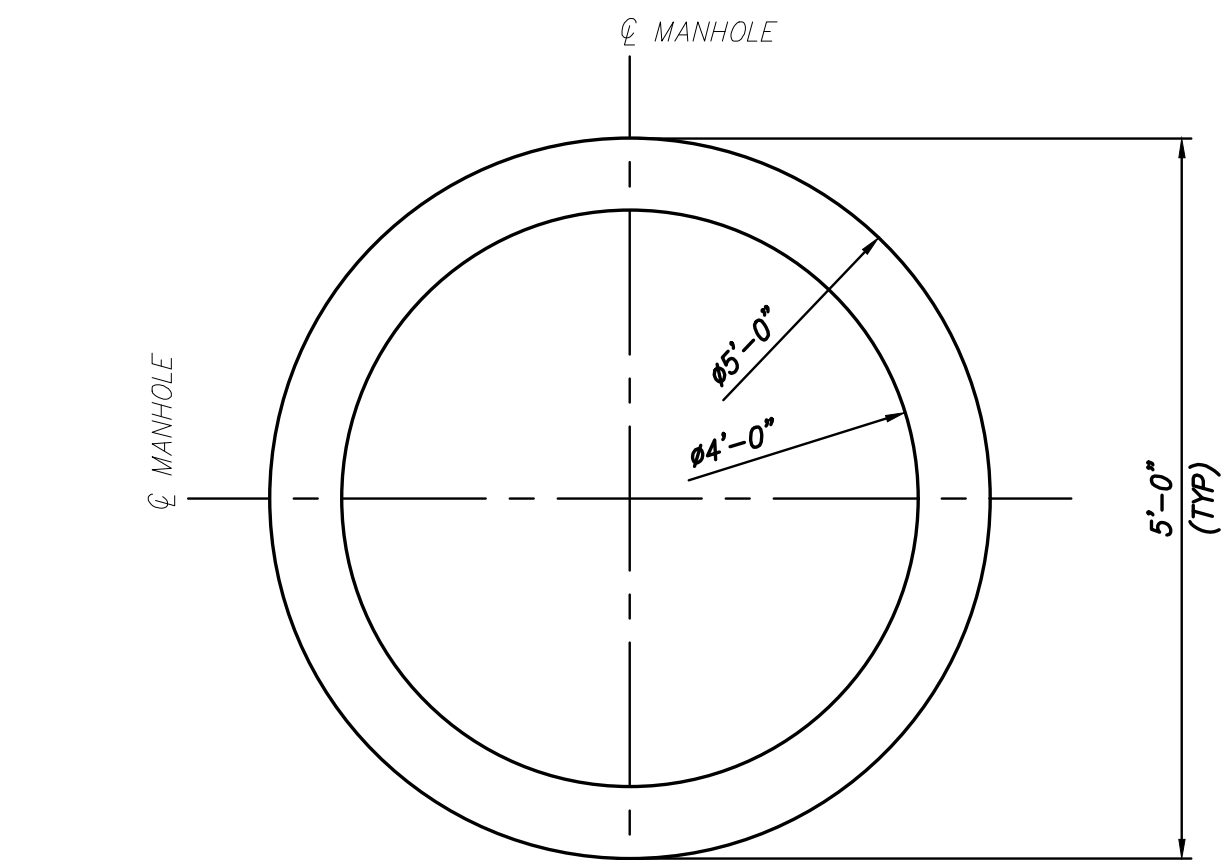
KIEWIT PROJECT NO. 21162

DRAWING NO. S-700

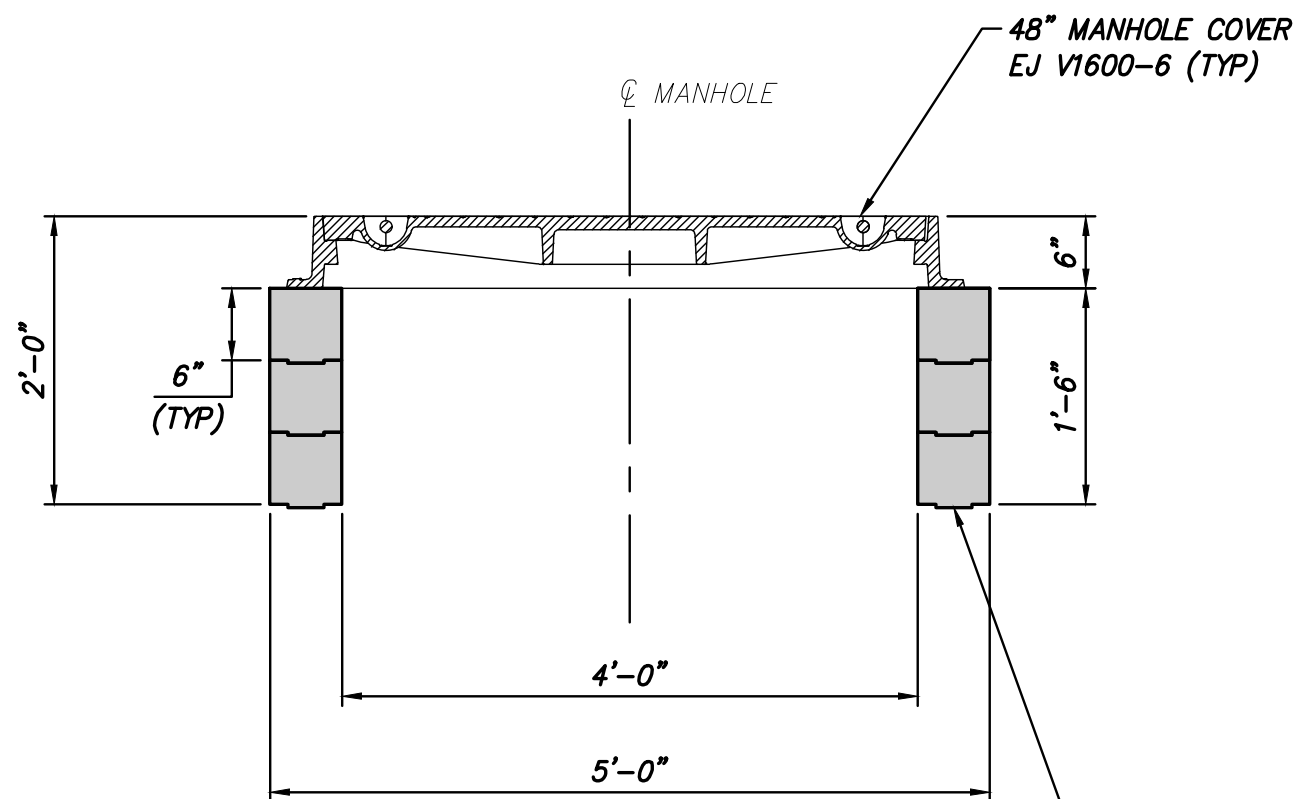


MANHOLE COVER

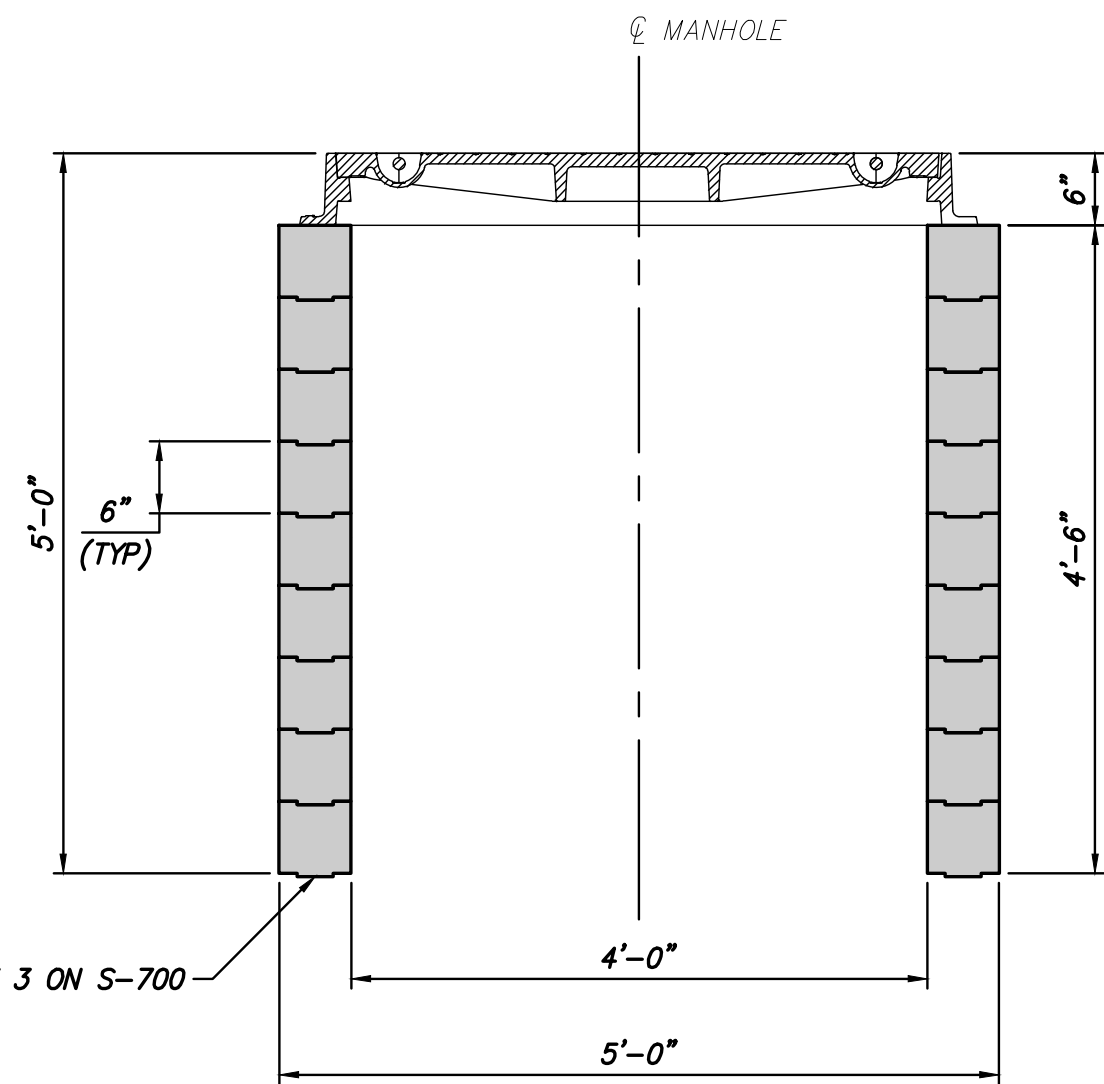
SCALE: 1 1/2" = 1'-0"



PRECAST CAP/RISER RINGS



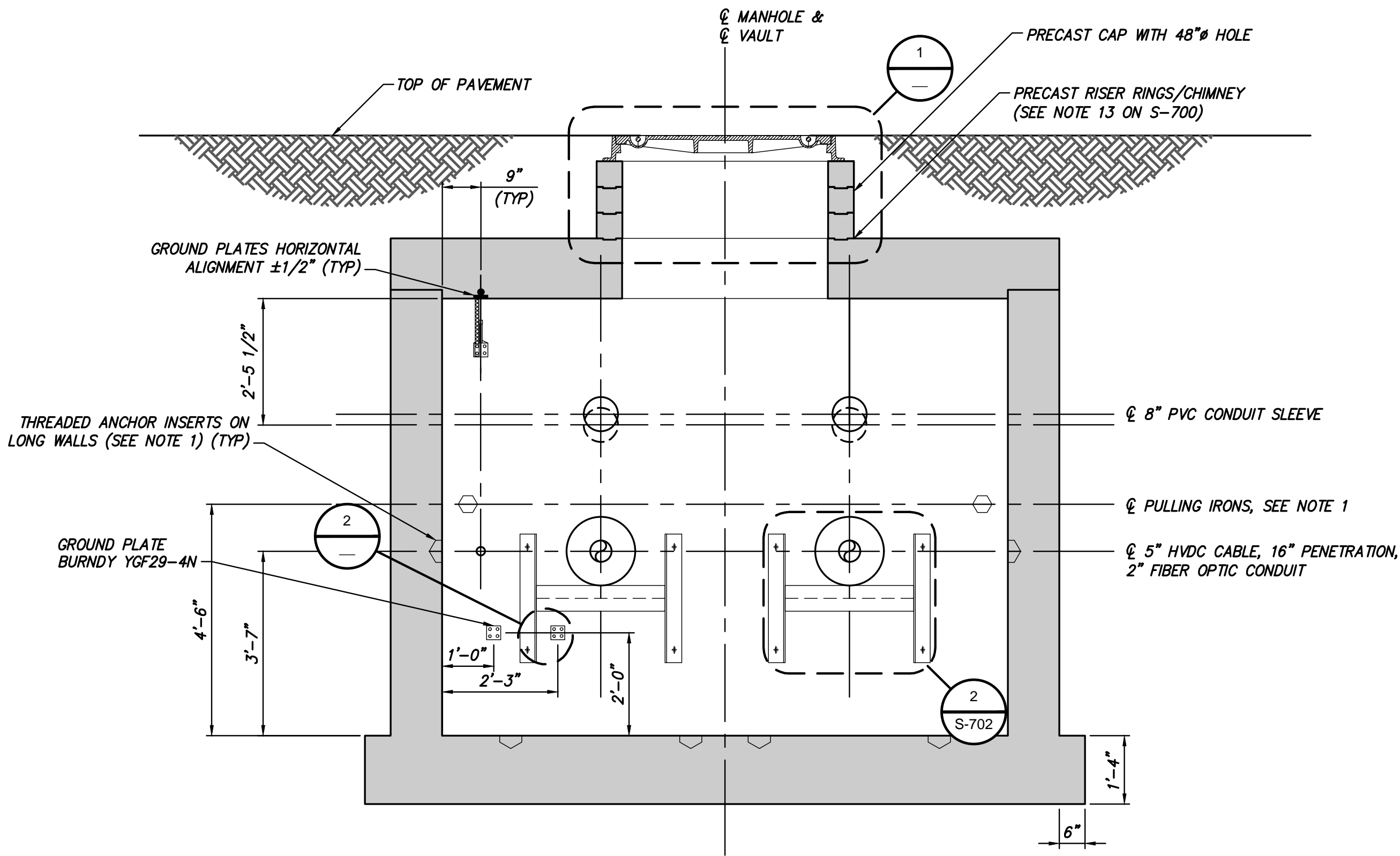
MINIMUM HEIGHT RISER DETAIL



MAXIMUM HEIGHT RISER DETAIL

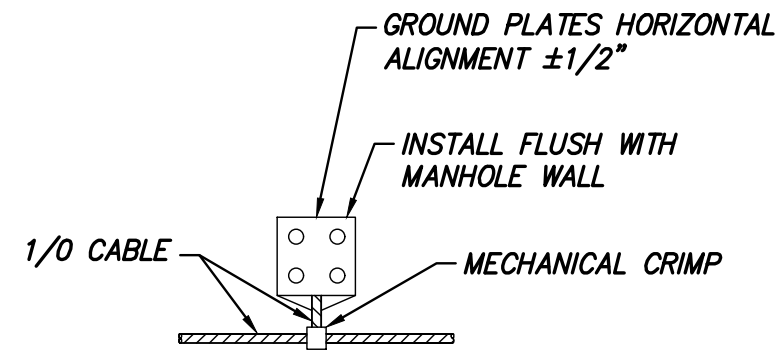
DETAIL 1

SCALE: 3/4" = 1'-0"



SECTION B

SCALE: 1/2" = 1'-0"



DETAIL 2

SCALE: 1 1/2" = 1'-0"

NOTES:

1. REFER TO NOTES ON SHEET S-700.
2. MANHOLE COVER SHALL BE RATED FOR HL-93 WHEEL LOADING MINIMUM PER S-700 NOTES AND S-705 DESIGN LOADS.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
2	05/03/2023	RFC - VAULT UPDATES	JNK	OO
1	02/10/2023	REV 1 - IFC - NYSDOT HWP, SPLICE VAULT UPDATES	JNK	OO
0	12/21/2022	IFC SUBMISSION	JNK	OO

CHAMPLAIN HUDSON POWER EXPRESS

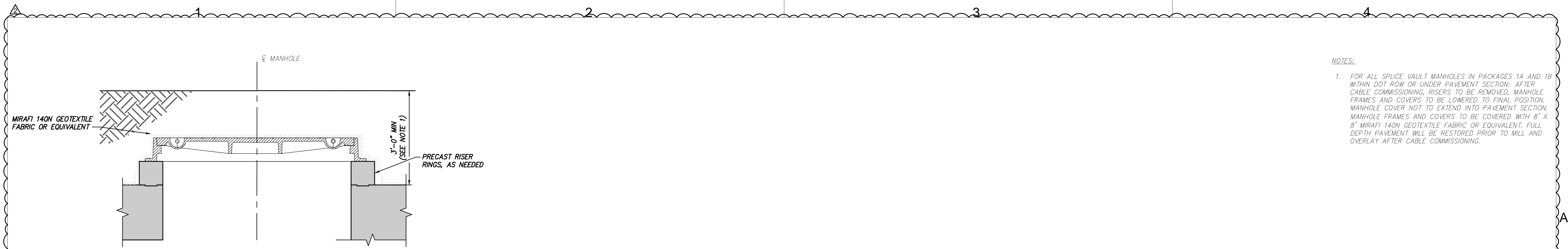
SPLICE VAULT SECTION AND DETAILS

DRAWN BY: DRH	DESIGNED BY: JNK	APPROVED BY: OO	SCALE: AS SHOWN	DATE: 2
REV. NO.				SH.NO.

PROJECT NO. 21162

DRAWING NO.

S-701



- NOTES:
1. FOR ALL SPLICE VAULT MANHOLES IN PACKAGES 1A AND 1B WITHIN DOT ROW OR UNDER PAVEMENT SECTION: AFTER CABLE COMMISSIONING, RISERS TO BE REMOVED, MANHOLE FRAMES AND COVERS TO BE LOWERED TO FINAL POSITION. MANHOLE COVER NOT TO EXTEND INTO PAVEMENT SECTION. MANHOLE FRAMES AND COVERS TO BE COVERED WITH 8' X 8' MIRAFI 140N GEOTEXTILE FABRIC OR EQUIVALENT. FULL DEPTH PAVEMENT WILL BE RESTORED PRIOR TO MILL AND OVERLAY AFTER CABLE COMMISSIONING.

File: P:\KIE-CHPE\3-DES\07 STATIONING\CHPE-SPICEVAULTS.dwg, Date: 5/2/2023, 11:35:45 AM, User: JNK, Plot: 5/2/2023, 11:35:45 AM, Scale: 1"=1'-0", Plot Size: 11.00 x 17.00, Plot Range: Full, Plot Device: HP DesignJet T1100, Plot Driver: HP DesignJet T1100, Plot Status: Success



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

2	05/03/2023	RFC - VAULT UPDATES	JNK	OO	
1	02/10/2023	REV 1 - IFC - NYSDOT HWP, SPLICE VAULT UPDATES	JNK	OO	
0	12/21/2022	IFC SUBMISSION	JNK	OO	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS

SPLICE VAULT DETAILS

DRAWN BY: DRH DESIGNED BY: JNK APPROVED BY: OO SCALE: AS SHOWN REV. NO. 2 DATE: SH.NO.

KIEWIT PROJECT NO.	21162
DRAWING NO.	S-703
DATE	
SH.NO.	

File: P:\VIE-CHPE\3-DES\07 STA\CHPE\CHPE-STRUCTURE-AND-S-705-DRAWING-Sheet-5-1\2023-5-14-4 PM-Printed-8-12-2023-11:35:45 AM-C:\Users\pkw\OneDrive\Documents\CHPE-STRUCTURE-AND-S-705-DRAWING-Sheet-5-1\2023-5-14-4 PM-Printed-8-12-2023-11:35:45 AM

DESIGN SPECIFICATIONS

1. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 9TH EDITION, 2020
2. NYSDOT LRFD BRIDGE DESIGN SPECIFICATIONS, 2021
3. NYSDOT LRFD BLUE PAGES, 2021
4. AREMA MANUAL FOR RAILWAY ENGINEERING, VOLUME 2 STRUCTURES, 2016
5. ASCE 7-16 MINIMUM DESIGN LOADS AND ASSOCIATED CRITERIA FOR BUILDINGS AND OTHER STRUCTURES

DESIGN LOADS

1. DEAD LOADS
 - 1.1. CONCRETE UNIT WEIGHT = 150 PCF
 - 1.2. STEEL UNIT WEIGHT = 490 PCF
2. SUPERIMPOSED DEAD LOADS
 - 2.1. WEIGHT OF TWO HVDC CABLES + ONE FIBER OPTIC CABLE AND ASSOCIATED CONDUIT = 47.53 PLF
 - 2.2. WEIGHT OF CABLE TRAY = 20.57 PLF
 - 2.3. PULLING IRON, TROLLEY BEAM, ANCHORAGE, EMBED FORCES NOTED ON DRAWINGS WHERE APPLICABLE.
3. EARTH LOADS
 - 3.1. SOIL BACKFILL UNIT WEIGHT = 125 PCF
4. LIVE LOAD SURCHARGE
 - 4.1. 100 PSF MINIMUM
5. LIVE LOADS
 - 5.1. 300 PSF MINIMUM AT GROUND SURFACE OF TRENCH SECTIONS AND VAULTS.
 - 5.2. HL-93
 - 5.3. COOPER E-80
 - 5.4. ANSI-SCTE TIER 22 (FOR HANDHOLES ONLY, SEE MATERIALS 7.1 BELOW)
6. WIND LOADS
 - 6.1. 50 PSF TRANSVERSE
 - 6.2. 10 PSF LONGITUDINAL
7. SNOW LOADS
 - 7.1. 50 PSF
8. WATER
 - 8.1. STRUCTURES ARE ASSUMED TO BE SUBMERGED.
9. THERMAL LOADS
 - 9.1. STRUCTURES ARE SUBJECT TO THERMOMECHANICAL LOADING FROM HVDC CABLES.
 - 9.2. TEMPERATURE GRADIENT.
10. SEISMIC LOADING
 - 10.1. BURIED STRUCTURES ARE NOT SUBJECT TO SEISMIC PROVISIONS.

MATERIALS:

1. REINFORCED CONCRETE
 - 1.1. f'c = 4,500 PSI AT 28 DAYS, UNO
 - 1.2. F2 FREEZE-THAW CATEGORY WHERE NOTED
2. REINFORCING STEEL
 - 2.1. ASTM A706, GRADE 60, UNO
3. STRUCTURAL STEEL
 - 3.1. ASTM A36, UNO
4. BOLTS
 - 4.1. ASTM A325, UNO
5. NUTS
 - 5.1. ASTM A563, UNO
6. WASHERS
 - 6.1. ASTM F436, UNO
7. POLYMER CONCRETE
 - 7.1. ANSI-SCTE 77 2017
8. REINFORCED THERMOSETTING RESIN CONDUIT
 - 8.1. NEC 355
9. PVC
 - 9.1. SCH 40

ABBREVIATIONS:

APPR	APPROACH
BRG	BEARING
CIP	CAST IN PLACE
CL	CENTERLINE
CLR	CLEAR COVER
DIA	DIAMETER
EL	ELEVATION
G	GIRDER
HS	HIGH STRENGTH
ICS	INTERMEDIATE CONDUIT SUPPORT
ID	INSIDE DIAMETER
IPS	IRON PIPE SIZE
KSI	KIPS PER SQUARE INCH
LLV	LONG LEG VERTICAL
NOM	NOMINAL
OD	OUTSIDE DIAMETER
PC	PRECAST
PL	PLATE
PROT	PROTECTIVE
PVC	POLYVINYL CHLORIDE
RT	ROUTE
STA	STATION
SW	STANDARD WALL
T	THICKNESS
UNO	UNLESS NOTED OTHERWISE



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

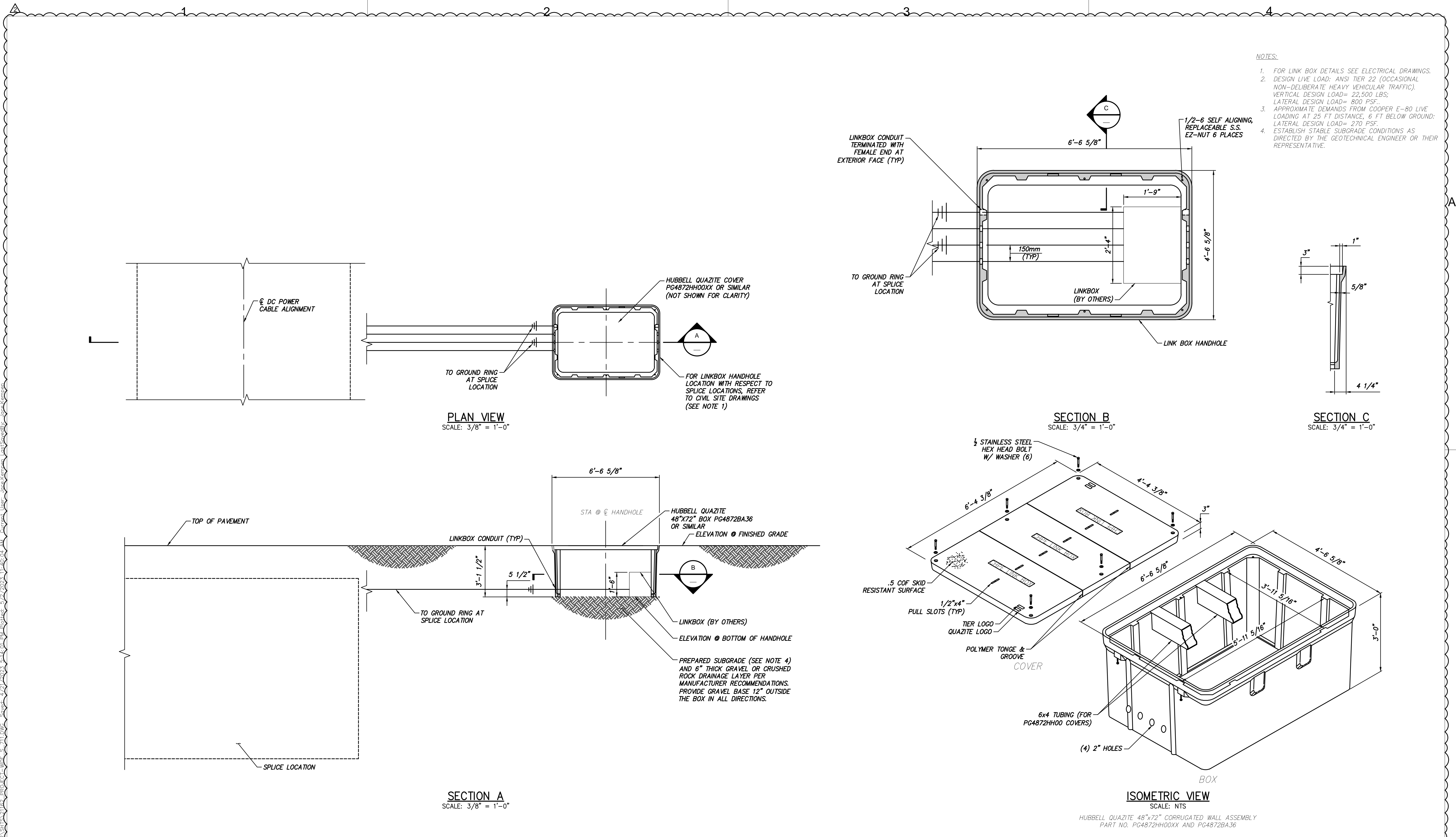
2	05/03/2023	RFC - VAULT UPDATES	JNK	OO	
1	02/10/2023	REV 1 - IFC - NYSDOT HWP, SPLICE VAULT UPDATES	JNK	OO	
0	12/21/2022	IFC SUBMISSION	JNK	OO	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	


CHAMPLAIN HUDSON POWER EXPRESS

STRUCTURAL GENERAL NOTES AND ABBREVIATIONS


DRAWN BY: DRH	DESIGNED BY: JNK	APPROVED BY: OO	SCALE REV. NO.	AS SHOWN 2	DATE SH.NO.
---------------	------------------	-----------------	-------------------	---------------	----------------

KIEWIT PROJECT NO. 21162
DRAWING NO. S-705






CHPE
Champlain Hudson
Power Express



Kiewit



MARK WANCHUN LI
LICENSED PROFESSIONAL ENGINEER
000036

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
2	05/03/2023	RFC - VAULT UPDATES	JNK	OO
1	02/10/2023	REV 1 - IFC - NYSDOT HWP, SPLICE VAULT UPDATES	JNK	OO
0	12/21/2022	IFC SUBMISSION	JNK	OO

CHAMPLAIN HUDSON POWER EXPRESS

FRP LINK BOX HANDHOLES

DRAWN BY: DRH

DESIGNED BY: JNK

APPROVED BY: OO

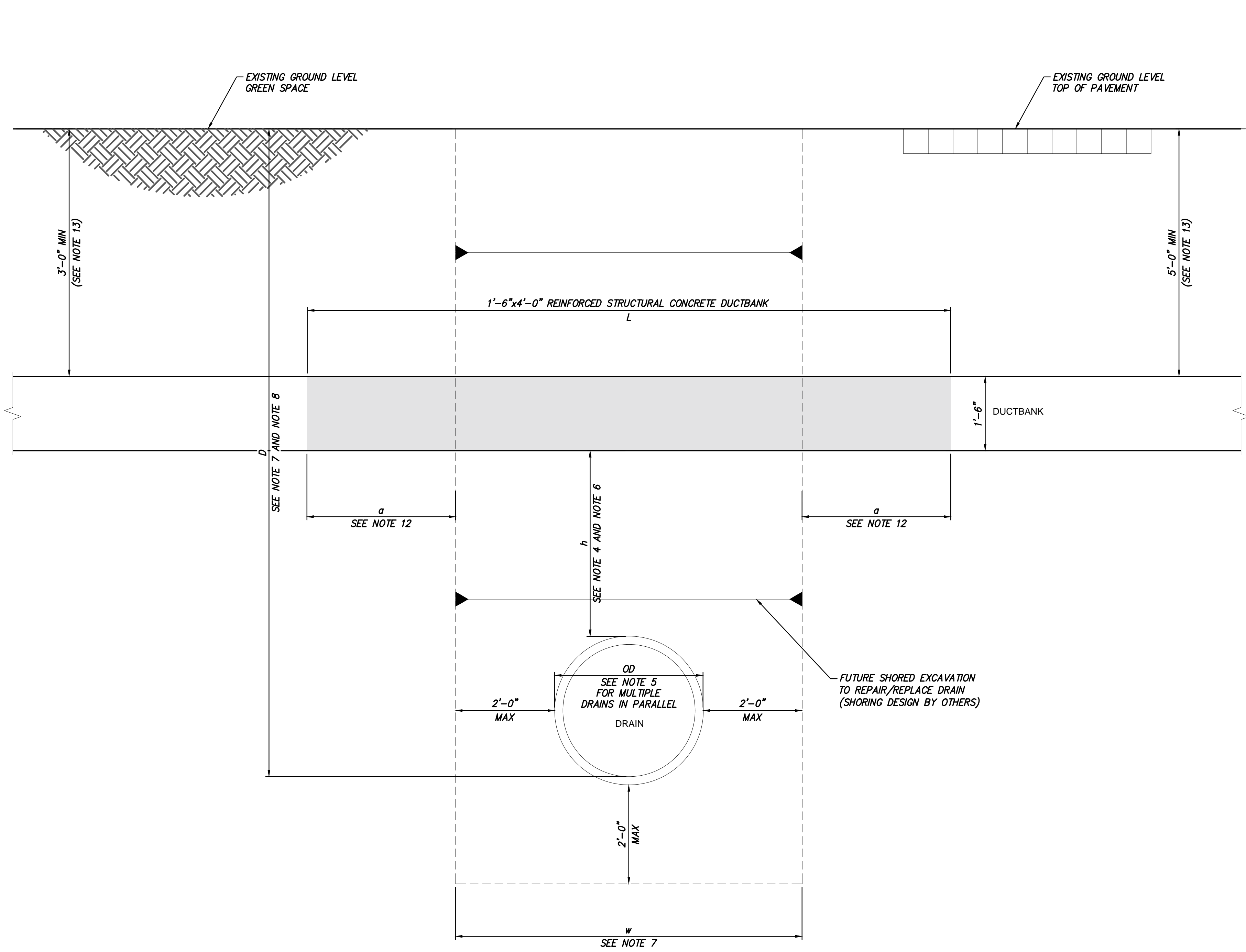
SCALE: AS SHOWN
REV. NO. 2

KIEWIT PROJECT NO. 21162

DRAWING NO. S-711

DATE 5/03/2023
SH.NO.

File: P:\VIE-CHPE\3-DES\07 STA 8000+21.65-REINFORCING TRAY OVER UTILITIES.dwg, 5/20/2023, 4:52:04 PM, Plotted: 5/27/2023, 11:54:45 AM, Current User: J. Kiewit, Last Saved By: J. Kiewit



TYPICAL DUCT BANK SECTION OVER UTILITIES
(FOR D < 14FT)
SCALE: NTS

- NOTES:
1. TYPICAL SECTION AND FOLLOWING NOTES PROVIDED PROPOSED DESIGN CRITERIA TO BE APPLIED FOR DRAINS OR OTHER UTILITIES LOCATED BELOW THE DUCT BANK WITHIN THE NYSDOT RIGHT OF WAY AND/OR OTHER PRIVATE PROPERTY OWNERS AND JURISDICTIONS, PENDING APPROVAL.
 2. EXCAVATION FOR / CONSTRUCTION OF FUTURE DRAIN REPAIRS/REPLACEMENTS, AND RELATED SHORING CALCULATIONS BY OTHERS.
 3. D = DISTANCE FROM EXISTING GROUND LEVEL TO DRAIN INVERT.
 4. h = DISTANCE FROM BOTTOM OF TRENCH PROTECTIVE CONCRETE/BOTTOM OF BRIDGING SUPPORT, TO TOP OF EXISTING DRAINAGE PIPE.
 5. FOR SINGLE PIPE, OD = OUTSIDE DIAMETER OF PIPE; FOR MULTIPLE PIPES IN PARALLEL, OD = DISTANCE BETWEEN OUTSIDE EDGES OF EXTERIOR PIPES.
 6. REFER TO CIVIL PLAN & PROFILE DRAWINGS FOR VALUE OF "h".
 7. FOR D≤14FT, ALL FUTURE EXCAVATIONS TO REPAIR/REPLACE EXISTING DRAINAGE PIPES WILL BE SHORED EXCAVATION, WITH MAXIMUM EXCAVATION WIDTH OF w=2FT+OD+2FT (DESIGN/CONSTRUCTION BY OTHERS).
 8. FOR D>14FT, TRENCH-LESS METHOD WILL BE USED TO REPAIR/REPLACE EXISTING DRAINAGE PIPES (DESIGN/CONSTRUCTION BY OTHERS).
 9. BRIDGING SUPPORT IS PROVIDED OVER LENGTH L = w+2a.
 10. BRIDGING SUPPORT IS NOT PROVIDED WHEN D>14FT.
 11. BRIDGING SUPPORT IS DESIGNED TO CARRY DUCT BANK WEIGHT, ONLY. SOIL ABOVE THE DUCT BANK IS REMOVED BEFORE EXCAVATING UNDER THE DUCT BANK.
 12. "a" DIMENSION IS 3'-0" MINIMUM.
PER GEOTECHNICAL TRENCH REPORT, 1.5 KSF MINIMUM BEARING CAPACITY ASSUMED.
 13. SEE NOTE 11/S-721, SPECIFIC TO PACKAGE 8:
FOR STA. 80000+50 TO 80001+50 (ADJACENT TO WILLIS AVE. BRIDGE), MINIMUM COVER IS 2'-0". TYPICAL COVER IN PACKAGE 8 IS 3'-0" MINIMUM.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

2	05/03/2023	RFC - VAULT UPDATES	JNK	OO	
1	02/10/2023	REV 1 - IFC - NYSDOT HWP, SPLICE VAULT UPDATES	JNK	OO	
0	12/21/2022	IFC SUBMISSION	JNK	OO	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS

REINFORCING TRAY OVER UTILITIES

DRAWN BY: DRH DESIGNED BY: JNK APPROVED BY: OO SCALE AS SHOWN DATE

KIEWIT PROJECT NO.	21162
DRAWING NO.	S-720
REV. NO.	2
SH.NO.	