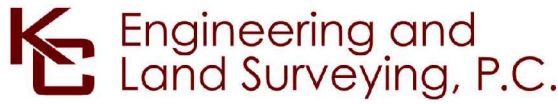


WORK ZONE TRAFFIC CONTROL – RANDALL’S ISLAND
SCALE: 1" = 50'

- LEGEND**
- TEMPORARY PEDESTRIAN/BIKE DETOUR USING EXISTING WALKWAY
 - TEMPORARY PAVEMENT
 - CLOSED AREA
 - NIGHT TIME WORK AREA
 - ACCESS ROAD TO EXISTING PARKING LOT

- NOTES:**
- BRONX SHORE ROAD WB WILL BE DETOURED TO BRONX SHORE ROAD EB AS SHOWN ON THE PLAN VIEW ABOVE.
 - EXISTING PEDESTRIAN/BIKE WALKWAY BETWEEN APPROXIMATE CONDUIT STATION 80036+00 AND 80038+00 SHOWN WITH SOLID FILL HATCH SHALL BE CLOSED COMPLETELY DURING CONSTRUCTION.
 - CONTRACTOR SHALL PLATE OVER OPEN TRENCHES AT THE END OF EACH SHIFT.
 - ALL EXISTING CURBS IMPACTED BY THE PROPOSED WORK SHALL BE REPLACED IN ACCORDANCE WITH APPLICABLE STANDARDS FROM NYCDOT, NYCDPR AND AS PER THE DETAIL SHOWN ON C-631.
 - SEE SHEET NO. C-504 FOR SECTION A-A.
 - CONTRACTOR SHALL MAINTAIN MINIMUM 12' LANE FOR ACCESS TO EXISTING PARKING LOT AT ALL TIMES DURING CONSTRUCTION.
 - TEMPORARY PAVEMENT TO BE REMOVED AND SITE TO BE RESTORED TO THE EXISTING CONDITION AT THE END OF CONSTRUCTION. REFER TO RESTORATION DETAIL ON C-631.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

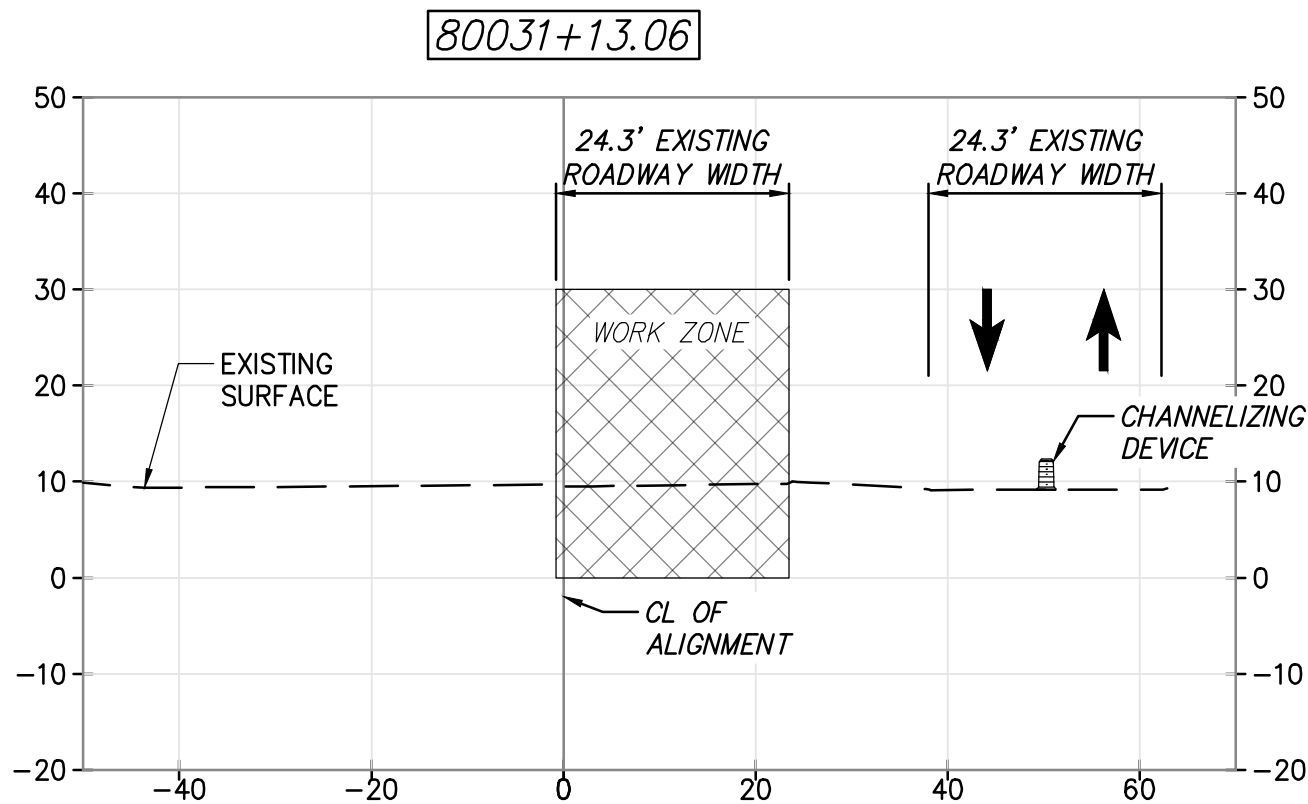
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
WORK ZONE TRAFFIC CONTROL PLAN 02
RANDALL'S ISLAND

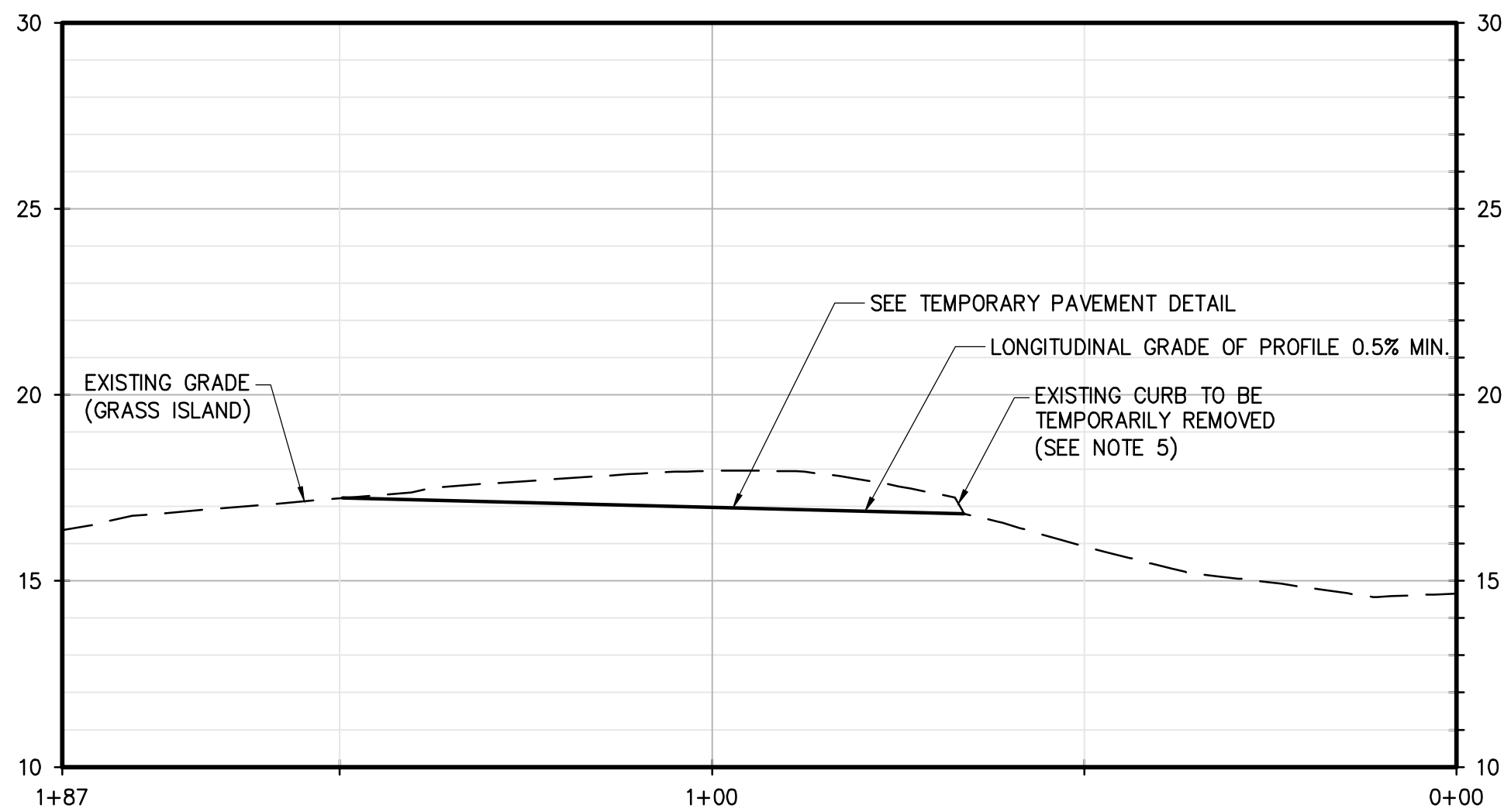
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REV. NO. 0 SH. NO. OF

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KC PROJECT NO.	120174
DRAWING NO.	C-503

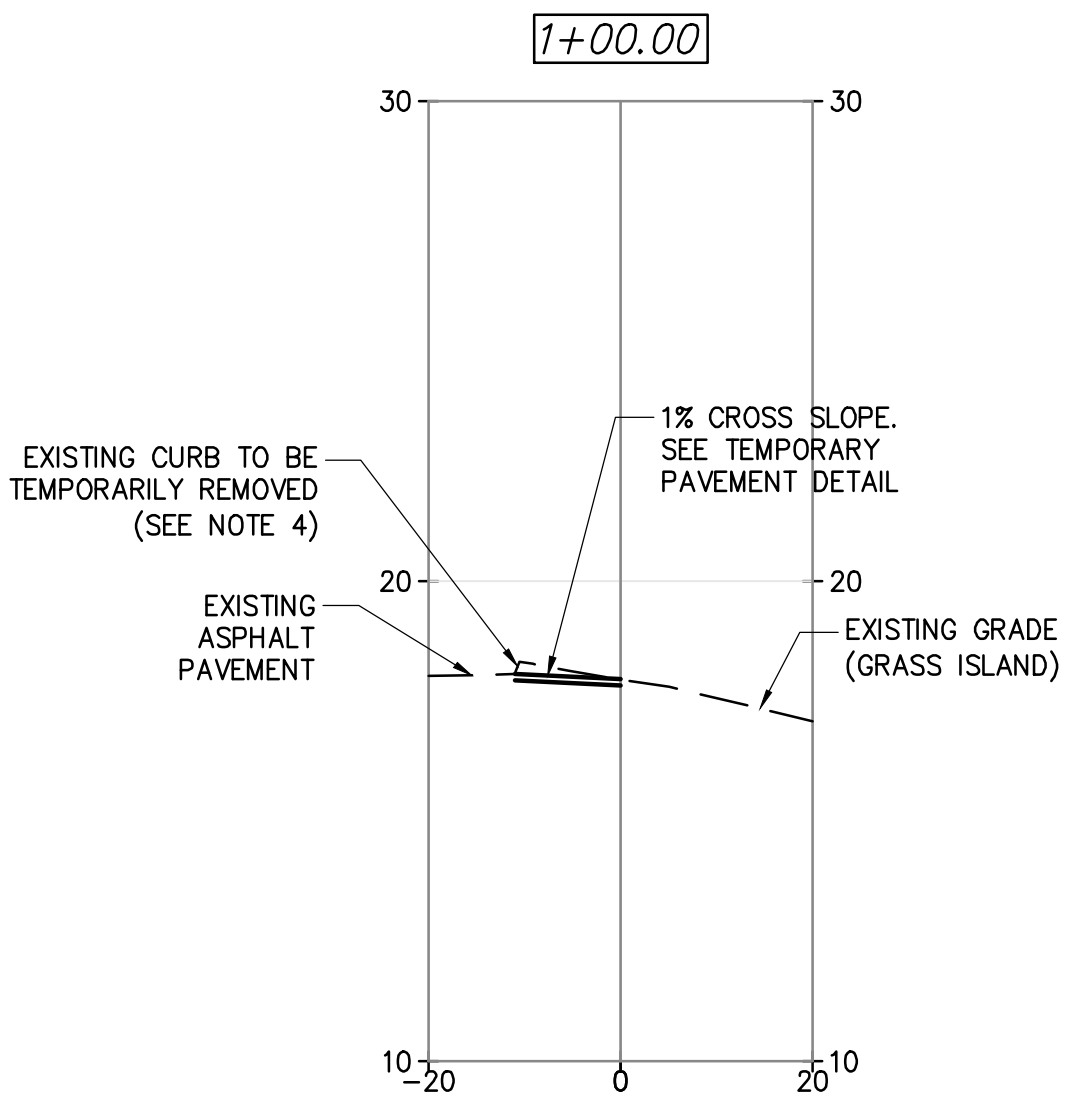
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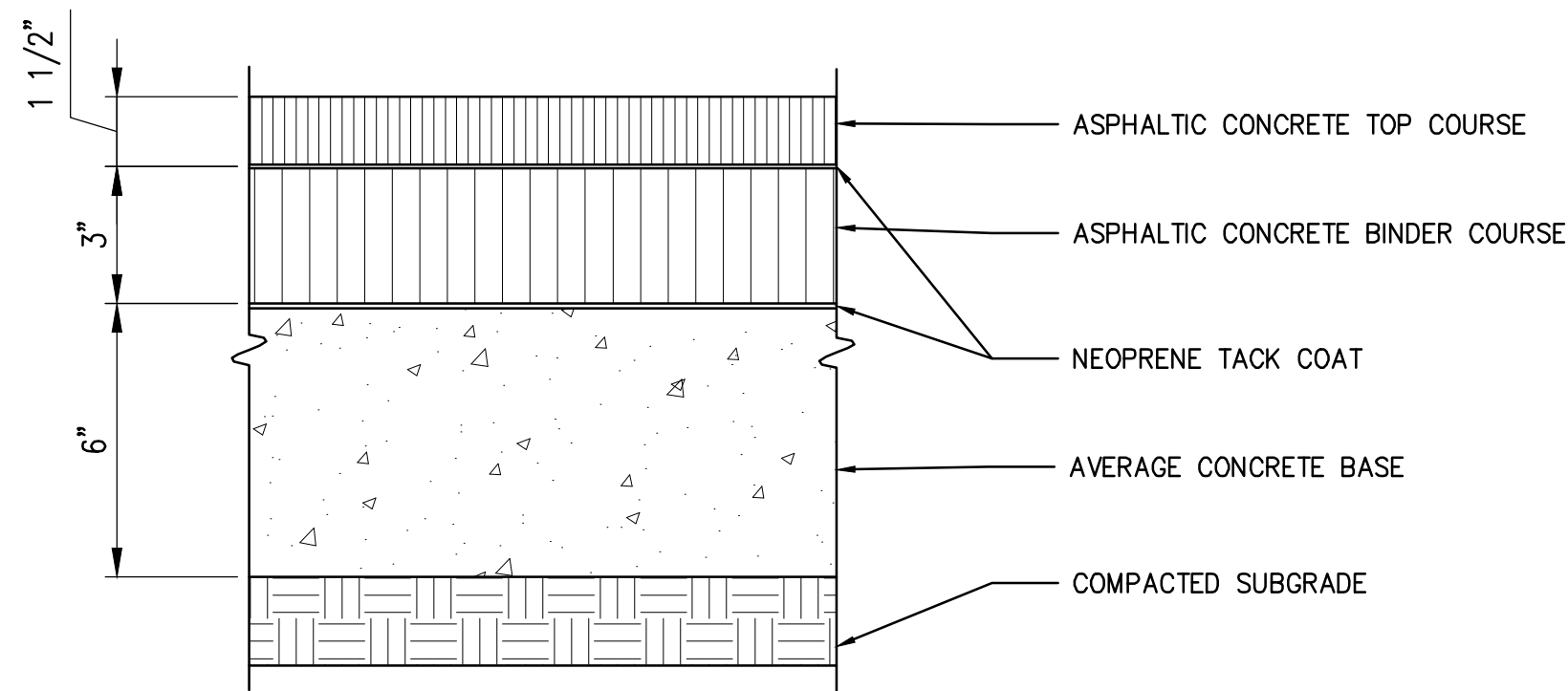
SECTION A-A: STA. 80031+13.06
SCALE: H:1" = 20' V:1" = 20'



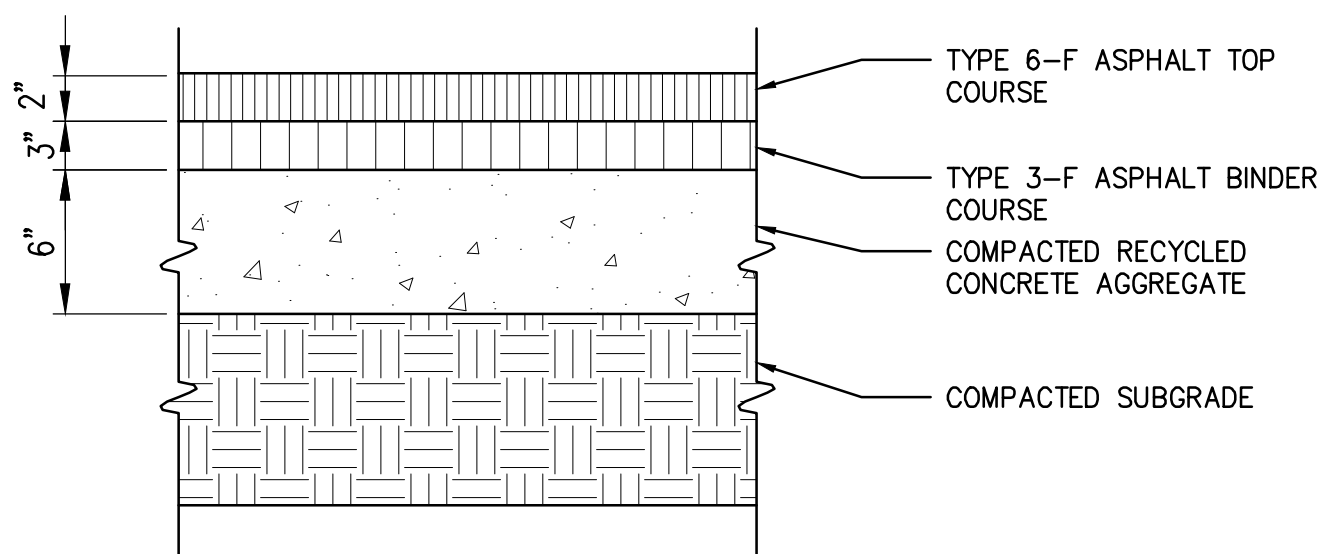
TEMPORARY ROAD PAVEMENT PROFILE
SCALE: H:1" = 20' V:1" = 10'



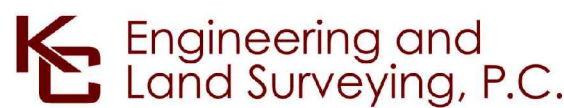
TEMPORARY ROAD PAVEMENT SECTION VIEW AT STA. 1+00
SCALE: H:1" = 20' V:1" = 10'



TEMPORARY ROAD PAVEMENT DETAIL
NOT TO SCALE



TEMPORARY PEDESTRIAN WALKWAY ASPHALT PAVEMENT
NOT TO SCALE



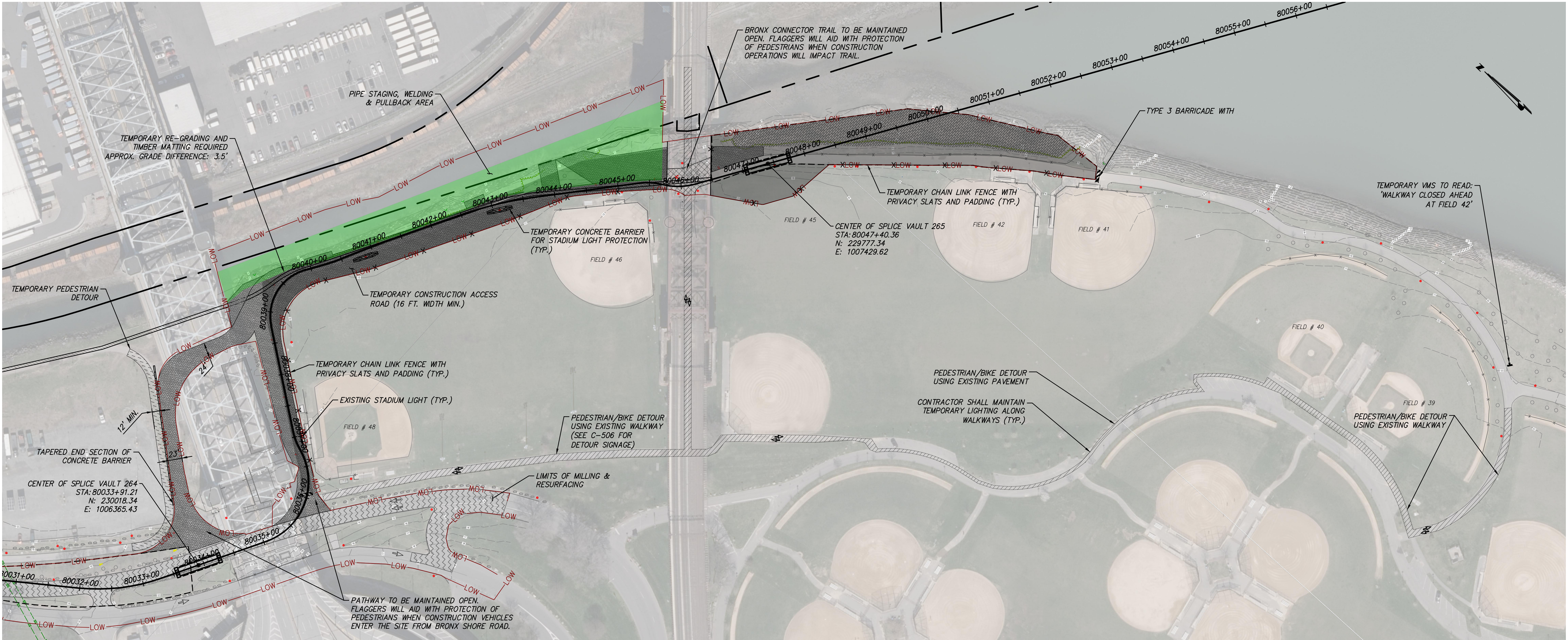
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
WORK ZONE TRAFFIC CONTROL PLAN 02
RANDALL'S ISLAND PROFILE AND SECTION VIEW

DRAWN BY:	MK	DESIGNED BY:	MK	APPROVED BY:	CV	SCALE	AS SHOWN	DATE	07/31/2023
						REV. NO.	0	SH.NO.	OF

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-504

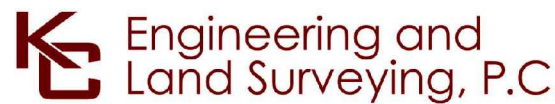


WORK ZONE TRAFFIC CONTROL – RANDALL’S ISLAND OUTSIDE OF RECREATION PERIOD (DECEMBER 1 THROUGH MID-MARCH)
SCALE: 1" = 80'

LEGEND

- TEMPORARY PEDESTRIAN/BIKE DETOUR USING EXISTING WALKWAY
- CLOSED AREA
- NIGHT TIME WORK AREA
- BRONX KILL PIPE STAGING, WELDING & PULLBACK AREA

- NOTES:**
- EXISTING PEDESTRIAN/BIKE WALKWAY BETWEEN APPROXIMATE CONDUIT STATION 80036+00 AND 80046+00 SHOWN WITH SOLID FILL HATCH SHALL BE CLOSED COMPLETELY DURING CONSTRUCTION.
 - CONDUIT INSTALLATION BETWEEN APPROXIMATE CONDUIT STATION 80045+00 AND 80047+00 SHOWN WITH CROSS HATCH SHALL BE CONDUCTED USING NIGHT TIME OPERATION.
 - CONTRACTOR SHALL PLATE OVER OPEN TRENCHES AT THE END OF EACH SHIFT FOR THE ROADWAY TO BE USED FOR DAY TIME OPERATIONS.

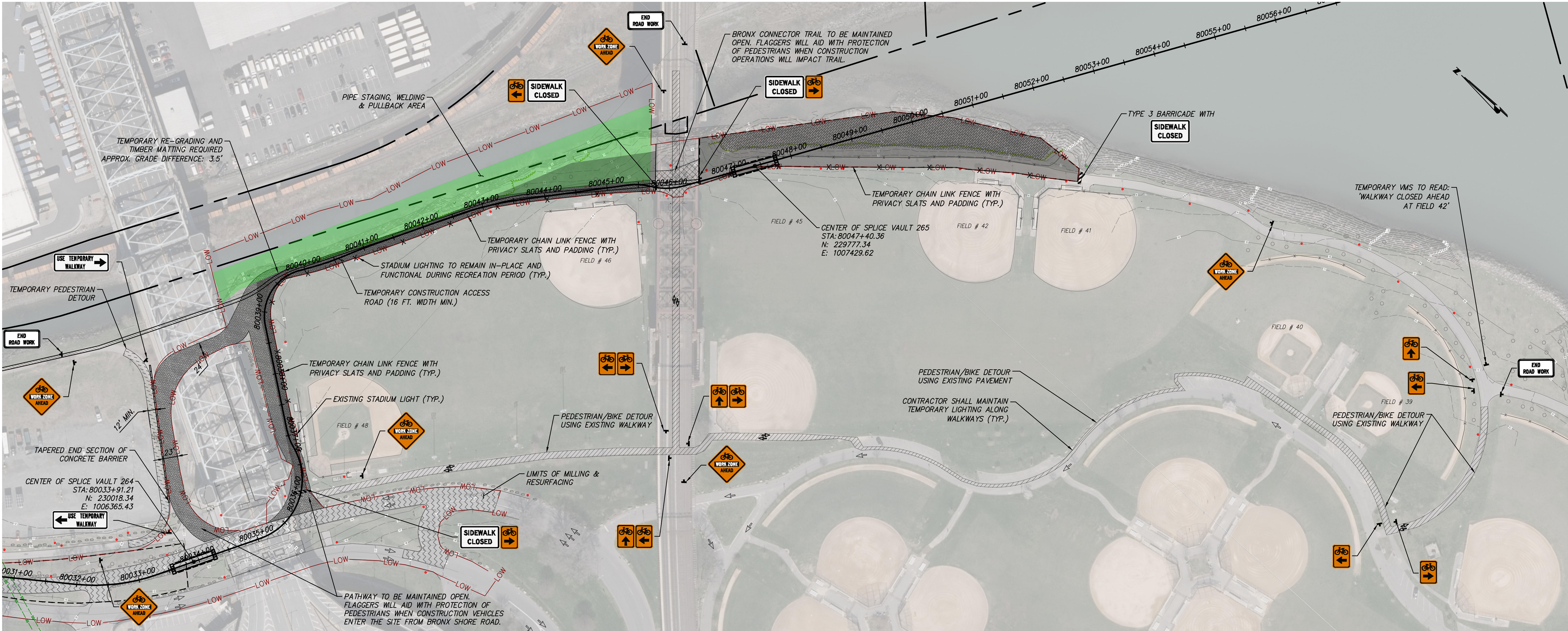


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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
WORK ZONE TRAFFIC CONTROL PLAN 03
RANDALL'S ISLAND - OUTSIDE OF RECREATION PERIOD

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-505
DATE	07/31/2023
SH.NO.	OF



WORK ZONE TRAFFIC CONTROL – RANDALL’S ISLAND DURING PEAK RECREATION PERIOD (MID-MARCH – NOVEMBER 30)
SCALE: 1" = 80'



LEGEND

- TEMPORARY WALKWAY
- TEMPORARY PEDESTRIAN/BIKE DETOUR USING EXISTING WALKWAY
- CLOSED AREA
- NIGHT TIME WORK AREA
- BRONX KILL PIPE STAGING, WELDING & PULLBACK AREA



Engineering and Land Surveying, P.C.



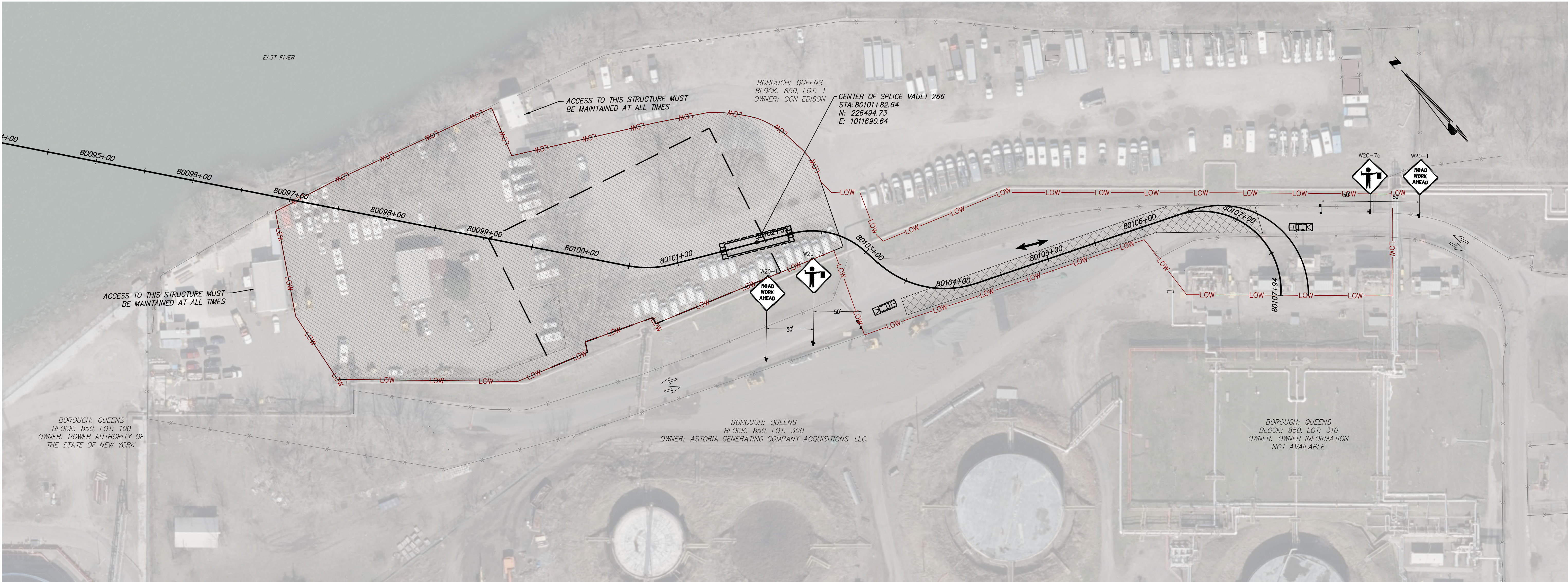
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
WORK ZONE TRAFFIC CONTROL PLAN 03
RANDALL'S ISLAND - PEAK RECREATION PERIOD

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-506
DRAWN BY:	MK
DESIGNED BY:	MK
APPROVED BY:	CV
SCALE	AS SHOWN
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DATE	07/31/2023
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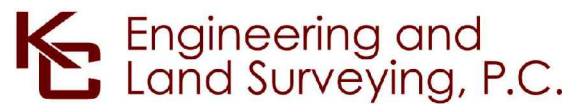
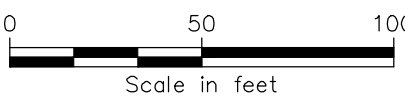
LEGEND

WORK AREA

- NOTES:
- CONDUIT INSTALLATION BETWEEN APPROXIMATE CONDUIT STATION 80103+00 AND 80107+50 SHOWN WITH CROSS HATCH SHALL BE CONDUCTED USING NIGHT TIME OPERATION.
 - CONTRACTOR SHALL PLATE OVER OPEN TRENCHES AT THE END OF EACH SHIFT FOR THE ROADWAY TO BE USED FOR DAY TIME OPERATIONS.

WORK ZONE TRAFFIC CONTROL – ASTORIA

SCALE: 1" = 50'



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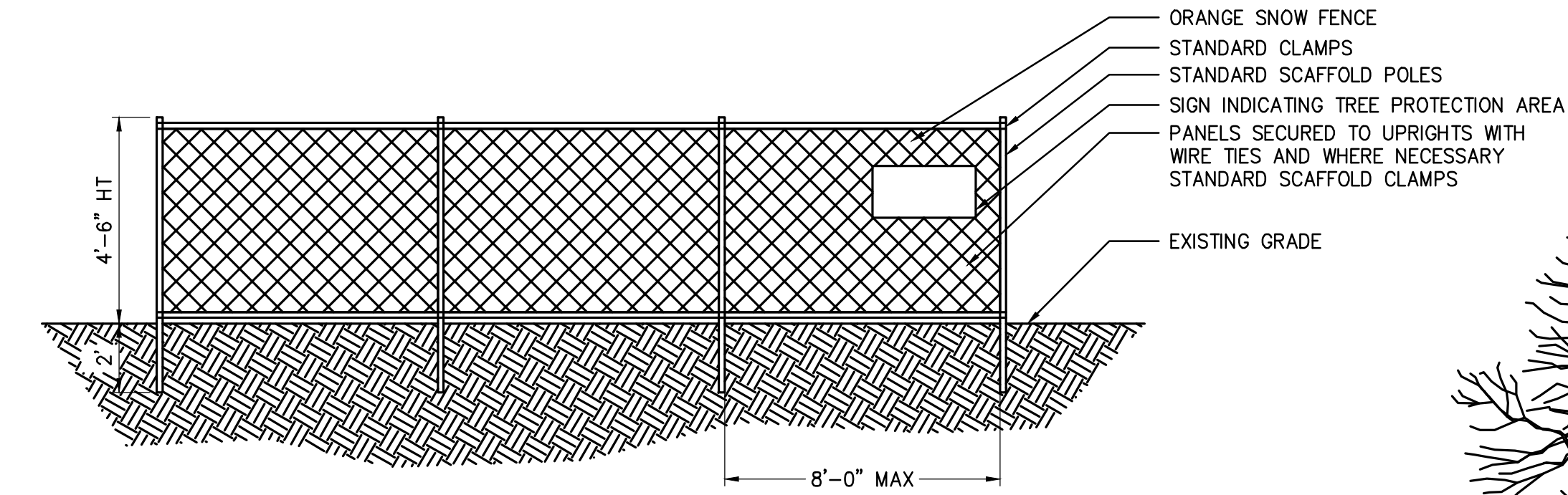
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
WORK ZONE TRAFFIC CONTROL PLAN 04
ASTORIA

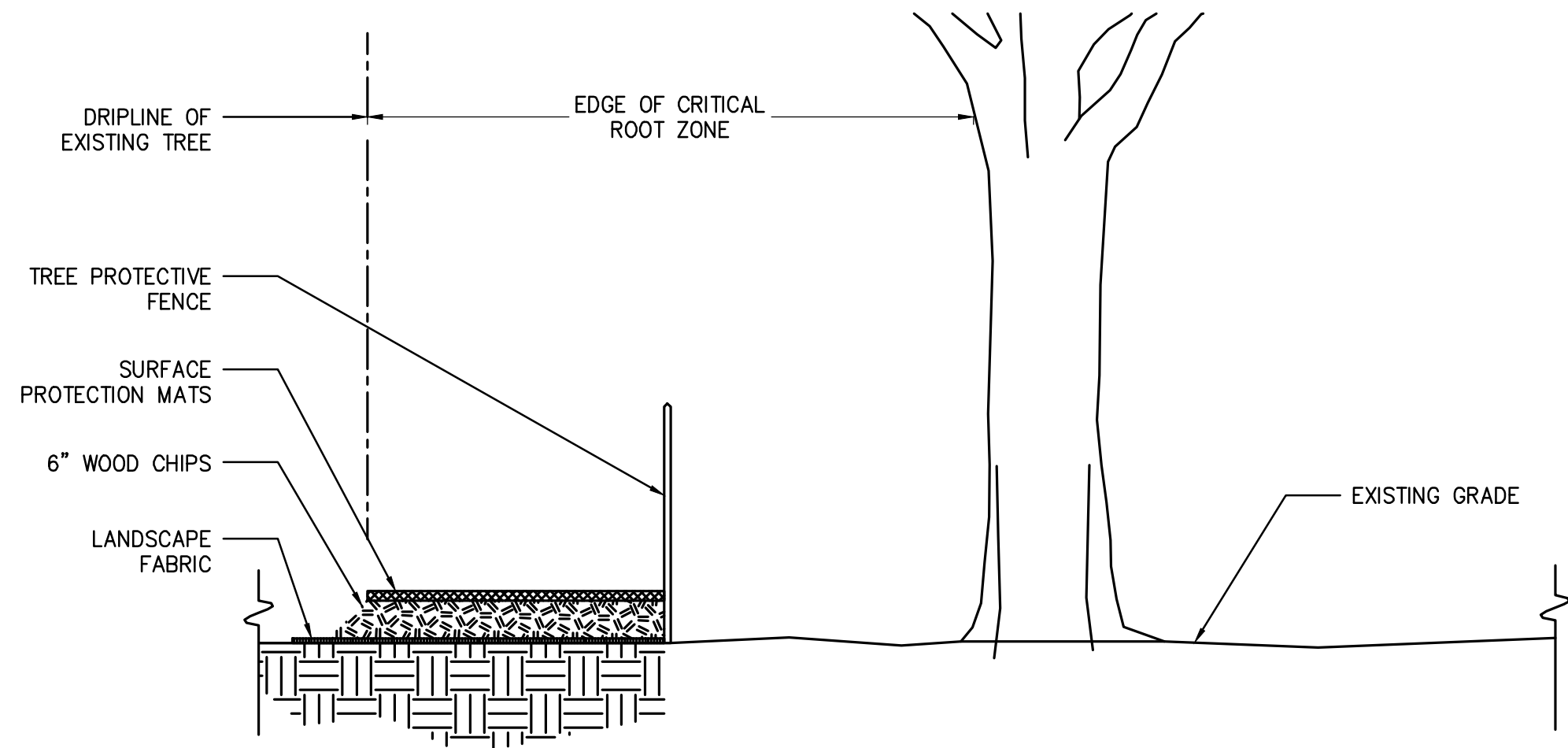
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KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-507

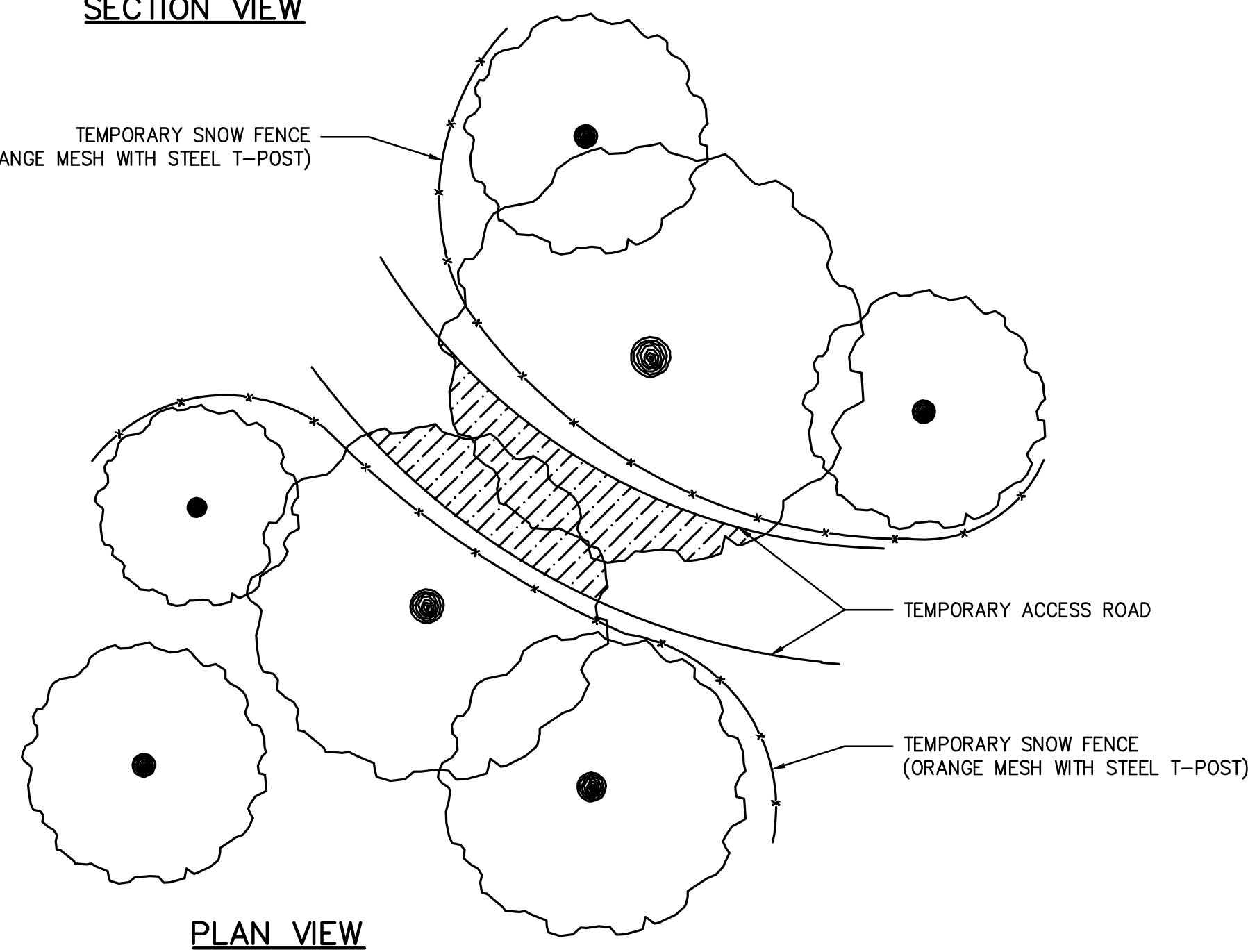
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1 TREE PROTECTIVE FENCE
SCALE: N.T.S.

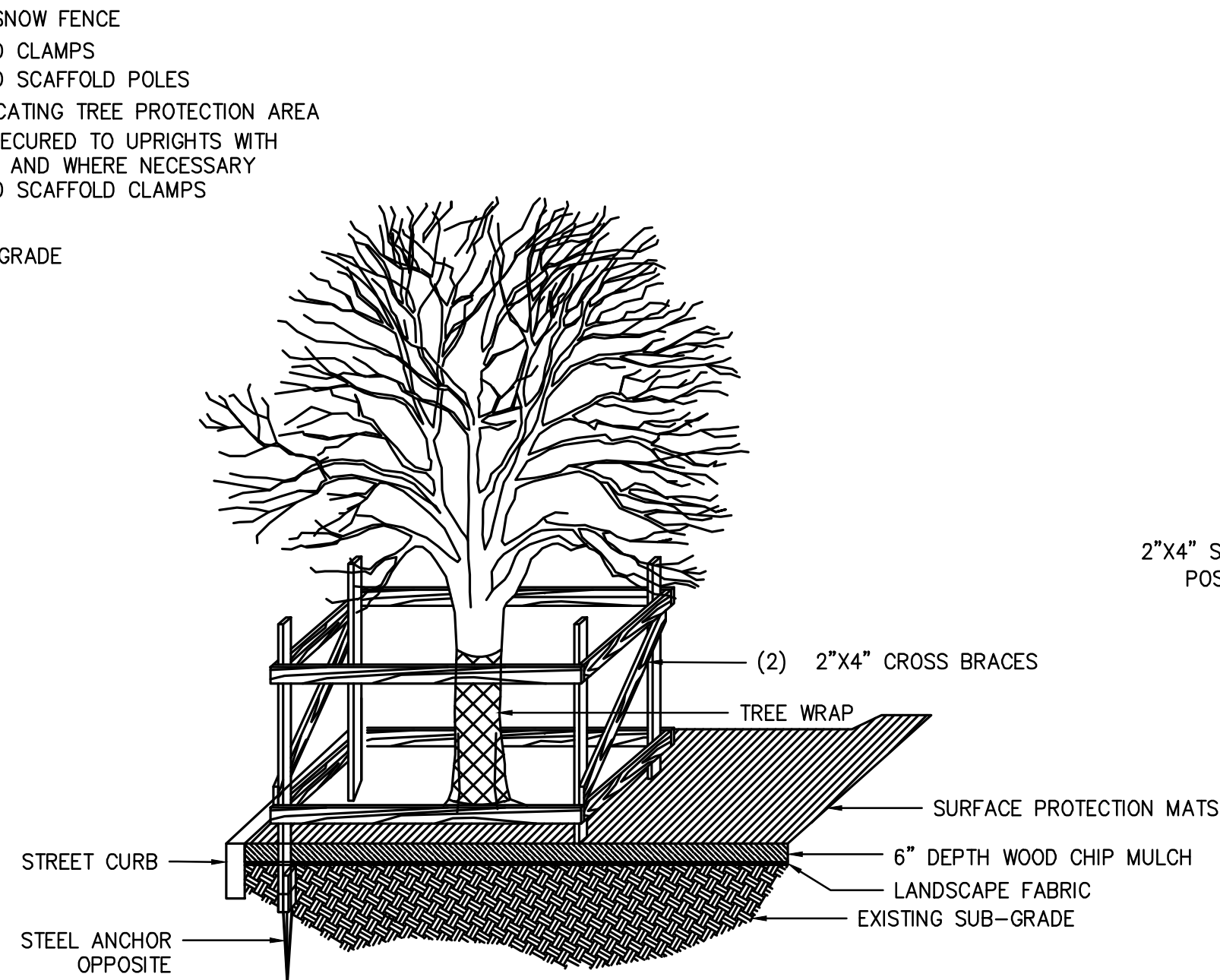


SECTION VIEW

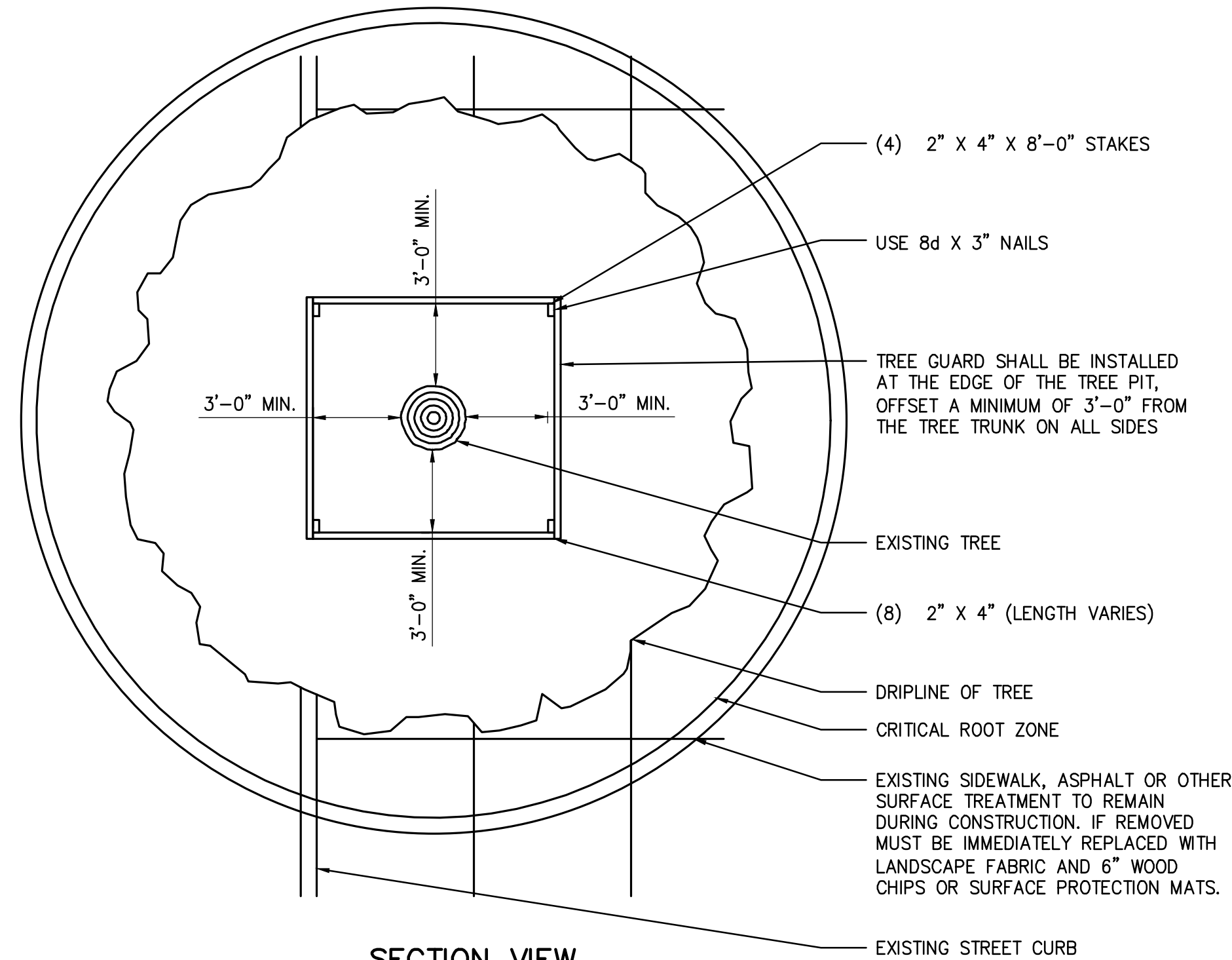


PLAN VIEW

2 TREE PROTECTIVE FENCE WITH GROUND PROTECTION
SCALE: N.T.S.

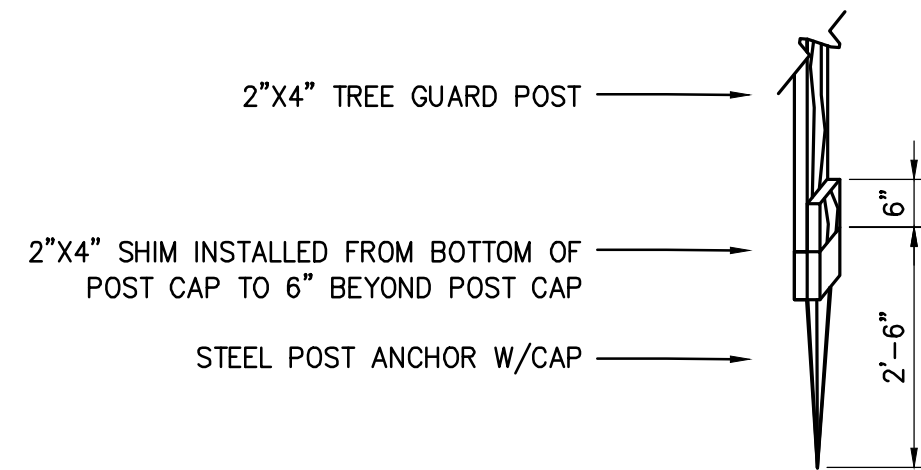


SECTION VIEW

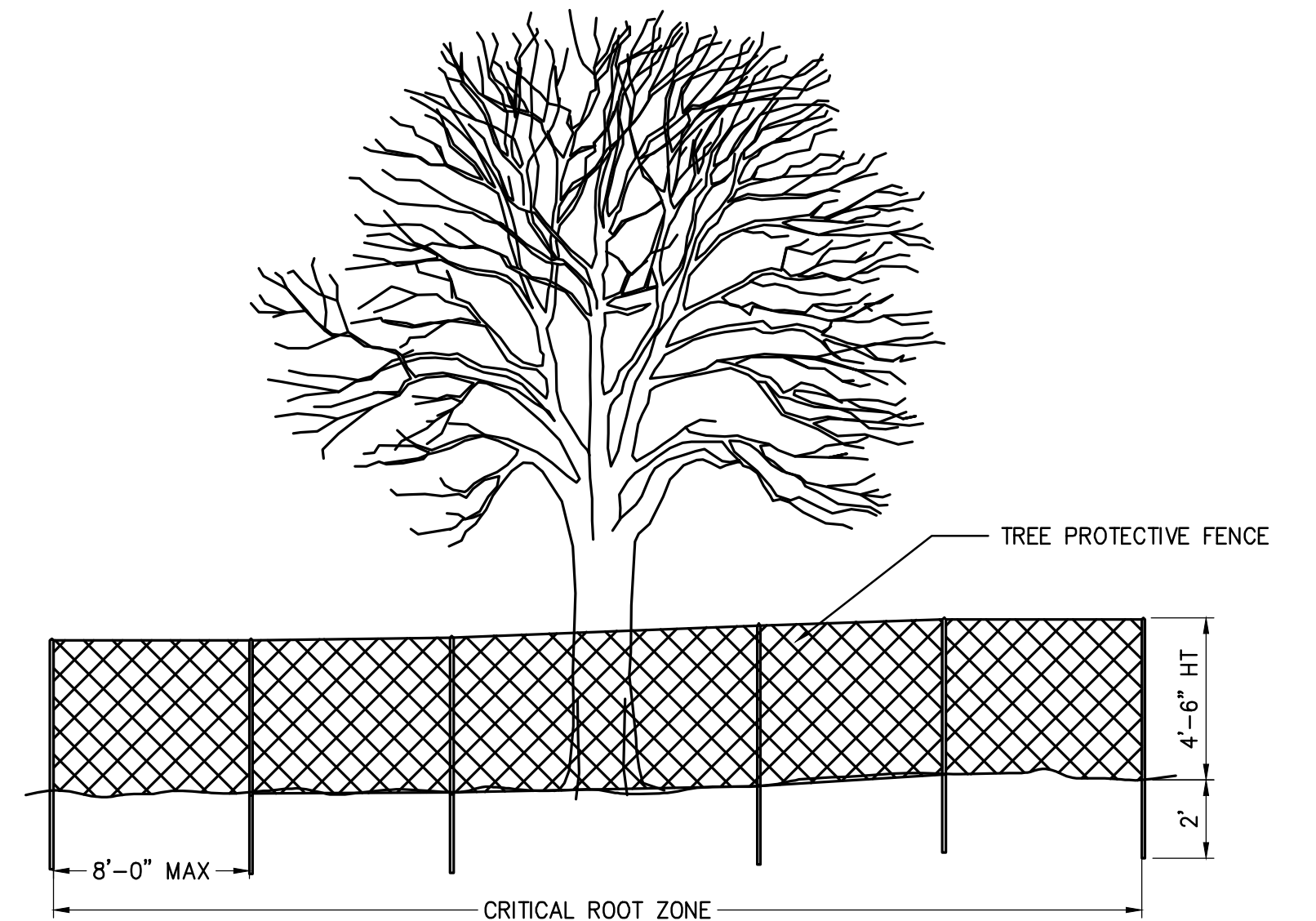


SECTION VIEW

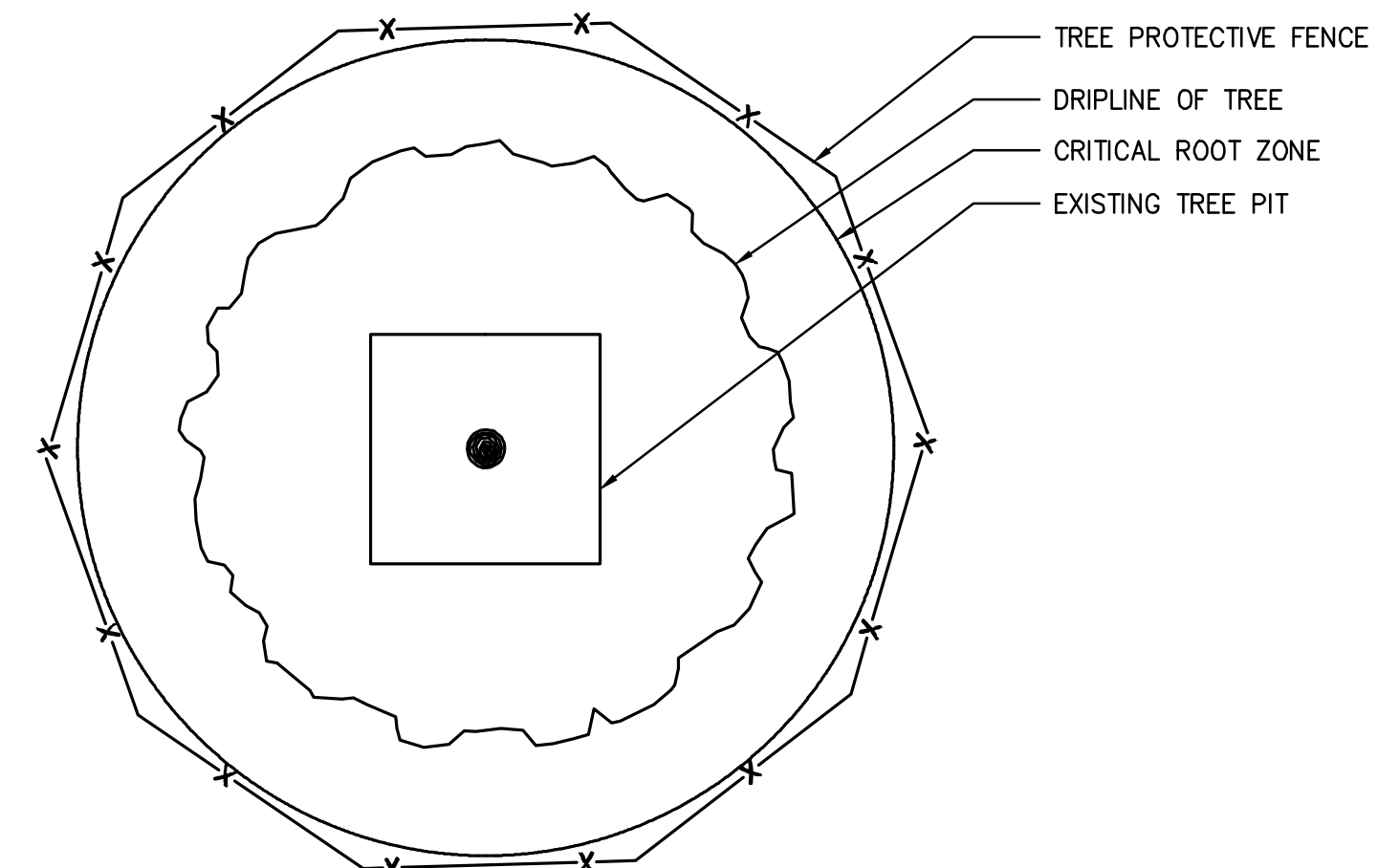
3 TREE GUARD WITH CRITICAL ROOT ZONE GROUND PROTECTION
SCALE: N.T.S.



ENLARGEMENT SECTION VIEW

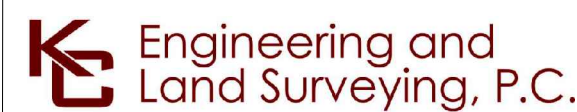


SECTION VIEW



PLAN VIEW

4 TREE PROTECTIVE FENCE AT CRITICAL ROOT ZONE
SCALE: N.T.S.



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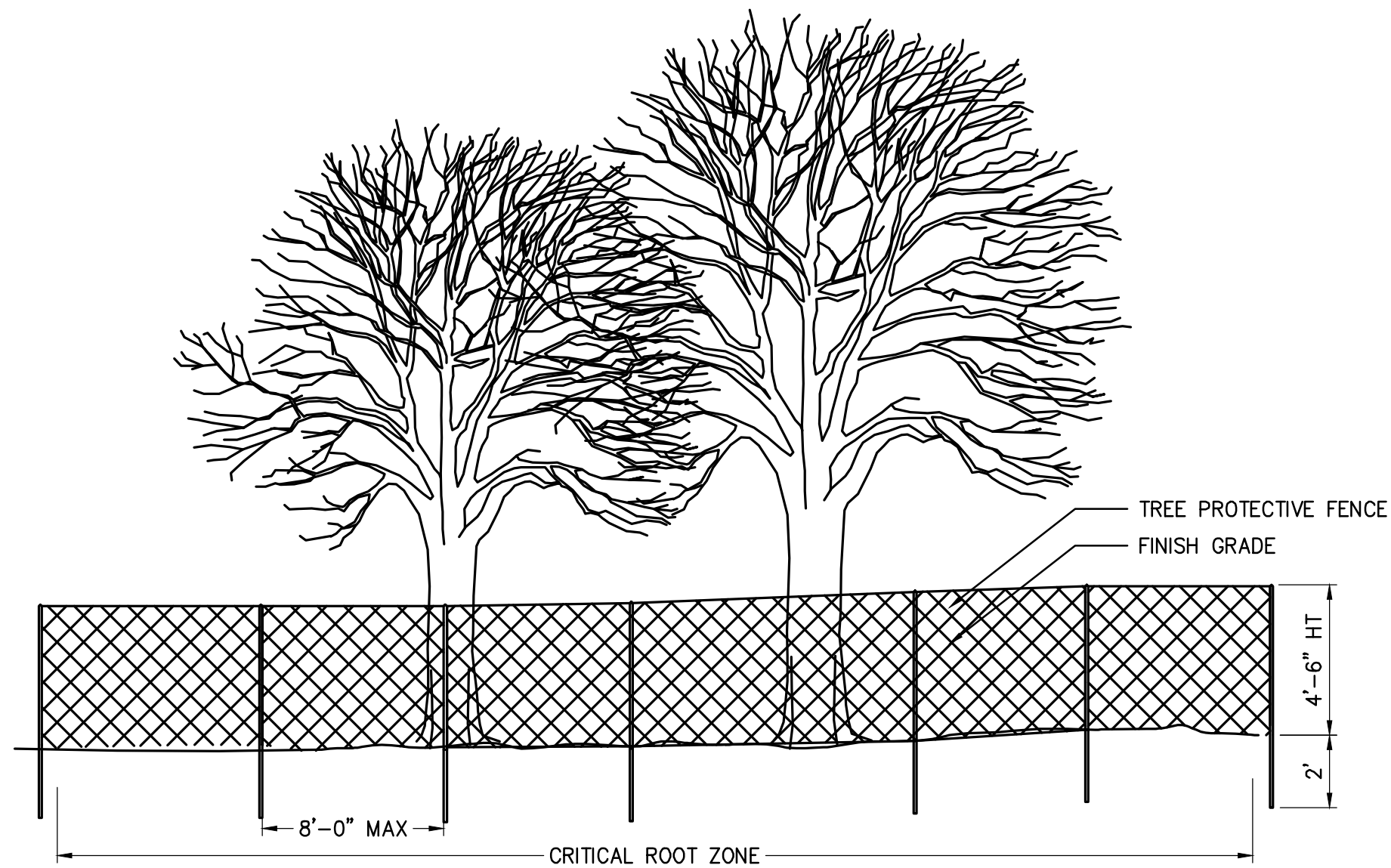
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
EROSION AND SEDIMENT CONTROL DETAILS

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KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-601

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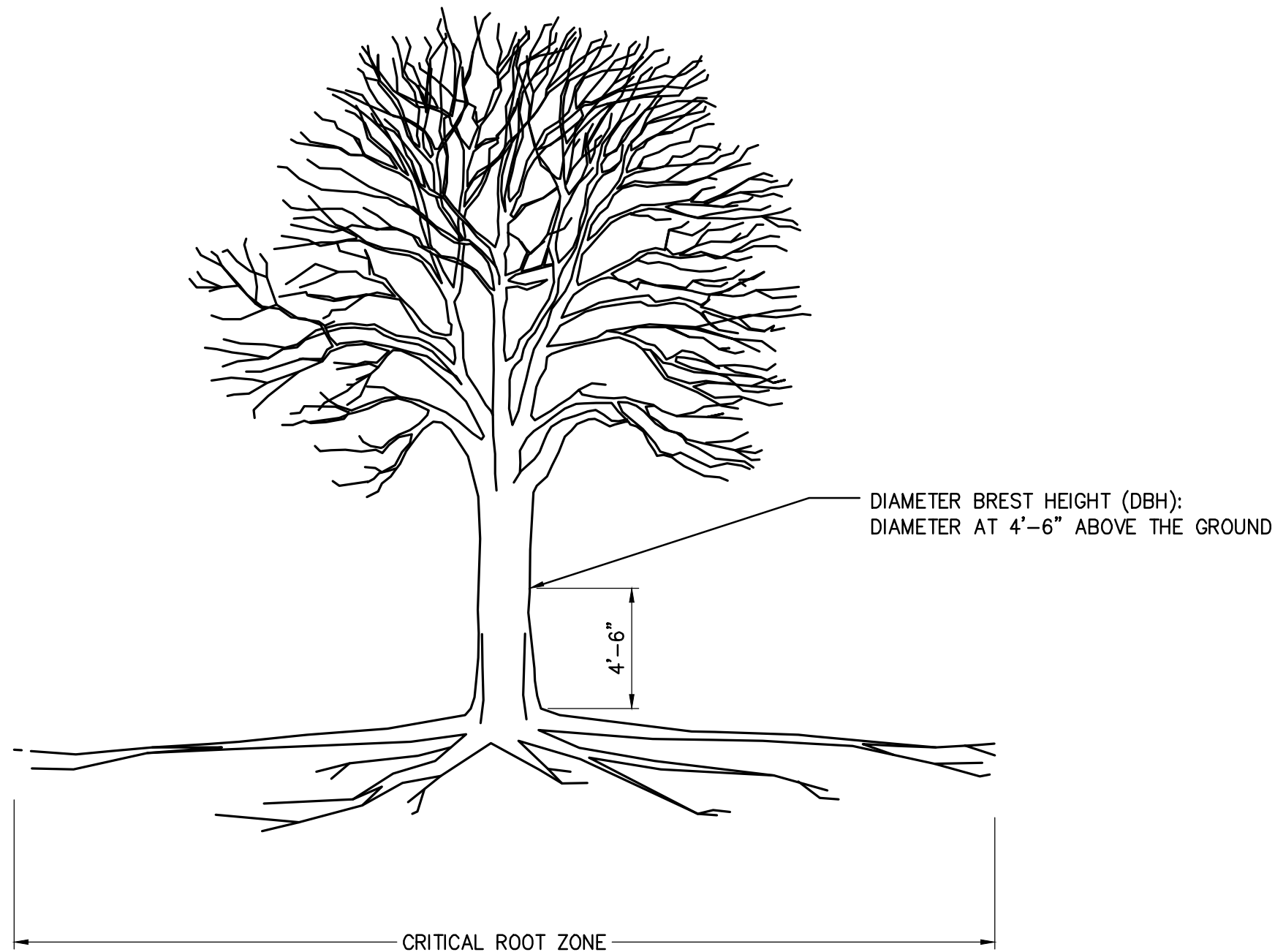


SECTION VIEW

1 TREE PROTECTIVE FENCE FOR GROVES

SCALE: N.T.S.

THE CRITICAL ROOT ZONE (CR2)



SECTION VIEW

NOTE: 12 X DBH(feet) = RADIUS OF CR2(feet)

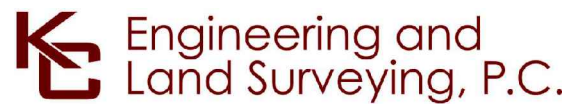
FOR EXAMPLE, IF A TREE'S DBH IS 10 INCHES THEN THE CRITICAL ROOT ZONE HAS A RADIUS OF 10 FEET FROM THE TRUNK IN ALL DIRECTIONS.

2 CALCULATING CRITICAL ROOT ZONE

SCALE: N.T.S.

NOTES:

- ANYONE PERFORMING ANY AND ALL WORK PERFORMED WITHIN 50 FEET OF A CITY TREE MUST POSSESS A PERMIT ISSUED BY THE NYC PARKS DEPARTMENT TO AVOID UNSAFE, HAZARDOUS AND OTHER CONDITIONS WHICH MAY BE DETRIMENTAL OR POTENTIALLY DETRIMENTAL TO ANY CITY TREE. IT IS INCUMBENT UPON THE APPLICANT TO ASCERTAIN AS TO WHETHER OR NOT THERE ARE ANY TREES OR TREE ROOTS SITUATED WITHIN THE CITY RIGHT OF WAY. ANY AND ALL TREES THAT FALL WITH THE JURISDICTION OF THE NYC PARKS DEPARTMENT ARE PROTECTED BY LAW FROM ANY AND ALL DAMAGE THERETO INCLUDING BUT NOT LIMITED TO ANY INCIDENTAL DAMAGES, DAMAGE TO THE CANOPY, OR DAMAGE TO THE TRUNK OR ROOT ZONE DURING AND IN THE COURSE OF ANY AND ALL CONSTRUCTION ACTIVITIES, AND ALSO THE AFTERMATH OF ANY AND ALL CONSTRUCTION ACTIVITIES. NO CUTTING OR OTHERWISE DAMAGING OF TREE ROOTS IS PERMITTED. ANY AND ALL TREE WORK MUST BE PERMITTED. TREE WORK PERFORMED ABSENT A PERMIT CAN PRECIPITATE SERIOUS FINANCIAL AND LEGAL REPERCUSSIONS. VIOLATIONS AND MISDEMEANORS ARE PUNISHABLE BY A FINE NOT TO EXCEED \$15,000 AND/OR IMPRISONMENT FOR UP TO ONE YEAR. ANY AND ALL APPLICATIONS RELATING TO CONSTRUCTION ACTIVITIES MUST BE ACCOMPANIED BY THE APPROPRIATE DOCUMENTATION AS REQUESTED PER P-A FORESTRY APPLICATION OR UPON FORESTER REQUEST.
- APPLICANT SHALL NOTIFY NYC PARKS/ FORESTRY AT LEAST 20 BUSINESS DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK REQUIRING A PERMIT.
- TEMPORARY WOODEN TREE GUARDS AND TEMPORARY SNOW FENCE BOUNDARY SHALL BE INSTALLED IMMEDIATELY AROUND EACH TREE IMPACTED BY DEMOLITION AND/OR CONSTRUCTION AND MAINTAINED THROUGHOUT THE COURSE OF THE ENTIRE DEMOLITION AND CONSTRUCTION PROCESS. ([HTTP://WWW.NYCGOVPARKS.ORG/PAGEFILES/84/TREE-PROTECTION-DETAILS.PDF](http://www.nycgovparks.org/pagefiles/84/tree-protection-details.pdf))
- THE CONTRACTOR SHALL TAKE EXTREME CARE TO PROTECT THE ROOT SYSTEMS OF THE EXISTING TREES. BULK MATERIAL, EQUIPMENT, SCAFFOLD FOOTINGS, OR VEHICLES SHALL NOT BE STOCKPILED OR PARKED WITHIN THE CRITICAL ROOT ZONE (CRZ) OF ANY TREE, OR WITHIN TEN (10) FT. OF THE TRUNK (WHICHEVER IS GREATER). THIS IS DONE TO MINIMIZE SURFACE AND SUBSURFACE ROOT AND SOIL COMPACTION. THIS APPLIES TO ALL CRZS WITHIN OR OUTSIDE THE PROJECT LIMIT LINE. EVERY INCH OF DBH(DIAMETER BREST HEIGHT) OF THE TREE REPRESENTS ONE REQUIRED RADIAL FOOT OF TREE PROTECTION.
- IF STOCKPIILING OCCURS WITHIN THE CRZ, A STOP WORK ORDER SHALL BE ISSUED IMMEDIATELY TO THE NYC DEPARTMENT OF BUILDINGS. ADDITIONAL VIOLATIONS MAY BE ISSUED AND MAY REQUIRE REMEDIAL WORK TO REMAIN WITHIN FORESTRY INSPECTOR'S PRESCRIBED TIMEFRAME. WORK SHALL NOT RE-COMMENCE UNTIL ALL STOCKPILED MATERIAL IS REMOVED FROM THE CRZ AND TREE REMEDIATION IS SATISFIED.
- IF ANY MACHINERY IS OPERATING WITHIN THE CRZ THE AFFECTED AREA SHALL BE COVERED WITH MULCH TO A DEPTH OF AT LEAST TWELVE (12)-INCHES AND COVERED WITH PLYWOOD OR METAL PLATES TO DISTRIBUTE WEIGHT IN ORDER TO PROTECT ROOTS FROM DAMAGE CAUSED BY HEAVY EQUIPMENT. SUCH COVERING SHALL BE MAINTAINED DURING THE COURSE OF CONSTRUCTION AND REMOVED BY HAND OR AS SPECIFIED BY THE CONTRACTED CERTIFIED ARBORIST OR FORESTRY INSPECTOR WITH ASSOCIATED PHOTOS REPORTED ACCORDINGLY. HEAT SOURCES, FLAMES, IGNITION SOURCES, AND SMOKING ARE PROHIBITED WITHIN THE CRZ AND WITHIN THE ABOVE MENTIONED MULCHED AREA.
- WHEN A DEFICIENCY IN TREE PROTECTION IS DETERMINED BY A FORESTRY INSPECTOR IT MUST BE REMEDIED IMMEDIATELY. FAILURE TO CORRECT THE DEFICIENCY IMMEDIATELY MAY RESULT IN VIOLATIONS AND SUMMONS.
- ANY DAMAGE TO EXISTING TREES DURING CONSTRUCTION SHALL BE THE CONTRACTOR'S RESPONSIBILITY. THE CONTRACTOR SHALL PERFORM REMEDIAL WORK TO DAMAGED TREES AT THE CONTRACTOR'S EXPENSE; THIS WORK SHALL MEET ALL NYC PARKS REGULATIONS. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS TO COMPLY WITH NYC PARKS REGULATIONS. ADDITIONAL RESOURCES ARE AVAILABLE AT THE NYC PARKS WEBSITE [HTTP://WWW.NYCGOVPARKS.ORG/SERVICES/FORESTRY/](http://www.nycgovparks.org/services/forestry/)
- CONTRACTOR WILL CONTACT NYC PARKS IF ANY UNDERGROUND INFRASTRUCTURE (GAS, WATER/ELECTRIC ETC.) AFFECTS ANY PROPOSED/EXISTING TREES ONSITE. PROJECT MANAGER IS AWARE THAT ANY WORK DONE ON OR WITHIN 50 FEET OF A CITY TREE REQUIRES A PERMIT FROM NYC PARKS. THIS INCLUDES UTILITY, SIDEWALK, PRUNING, OR ANY OTHER WORK WITHIN THE CRZ OF A TREE (WITHIN THE CITY RIGHT OF WAY) DONE BY THE GENERAL CONTRACTOR OR ANY SUBCONTRACTORS. CONTRACTOR WILL BE FAMILIAR WITH, AND FOLLOW NYC PARKS PLANTING AND FORESTRY SPECIFICATIONS. IN SOME INSTANCES, UTILITIES MAY NOT BE LABELED ON PROPOSED SITE PLAN. IF UTILITIES ARE UNKNOWN, THE PROJECT MANAGER MUST AMEND THE PLANS AND REQUEST THEIR PLANS BE REAPPROVED BY NYC PARKS.
- CONSTRUCTION ACCESS ROUTE IS TO BE DIAGRAMMED AND ROUTED TO MINIMALLY IMPACT ANY EXISTING TREES. FINAL ROUTE SHALL BE ESTABLISHED ON SITE AND APPROVED BY THE FORESTRY INSPECTOR. SITE PLANS ARE TO BE INCLUDED AND AMENDED ACCORDINGLY WHEN REQUESTING NYC PARKS APPROVAL.
- ROOTS OVER ONE (1)-INCH IN DIAMETER SHALL NOT BE CUT WITHOUT THE WRITTEN PERMISSION OF THE BOROUGH DIRECTOR OF FORESTRY.
- TO BEST PROTECT TREE ROOTS THE CONTRACTOR SHALL EXERCISE EXTREME CARE IN REMOVING CONCRETE OR ASPHALT WITHIN THE CRZ OF EXISTING TREES. PAVEMENT SHOULD BE LIFTED RATHER THAN DRAGGED. ANY EXCAVATION WITHIN THE CRZ, OR ELSEWHERE ON SITE, AS INDICATED ON TREE PROTECTION PLAN, SHALL BE DONE BY HAND OR PNEUMATIC EXCAVATION AND IN THE PRESENCE OF THE FORESTRY INSPECTOR OR CONTRACTED CERTIFIED ARBORIST WITH ASSOCIATED PHOTOS AND REPORT TO BE FILED WITH NYC PARKS FORESTRY INSPECTOR. CONTRACTOR IS TO SCHEDULE APPOINTMENT WITH FORESTRY INSPECTOR ACCORDINGLY.
- THE EXCAVATION AREA WITHIN THE CRZ SHALL BE BACKFILLED IMMEDIATELY AND/OR ROOTS SHALL BE KEPT CONSTANTLY MOIST WITH BURLAP COVERED WITH WHITE PLASTIC AND CHECKED A MINIMUM OF TWO (2) TIMES A DAY, ONCE IN THE MORNING AND ONCE IN THE AFTERNOON, FOR A MAXIMUM OF FORTY-EIGHT (48) HOURS, UNTIL BACKFILL IS COMPLETE AS DIRECTED BY THE DIRECTOR OF LANDSCAPE CONSTRUCTION AND THE RESIDENT ENGINEER. IF DIRECTED, SOAKER HOSES SHALL BE INSTALLED TO FACILITATE PROPERLY MOIST CONDITIONS. NO POOLING OF WATER OR CONTINUOUS RUNNING WATER SHALL OCCUR WITHIN THE DRIP LINE OF EXISTING TREES OR WITHIN THE TREE PROTECTION ZONES OTHER THAN THAT DURING THE IRRIGATION PROCESS.
- IF ROOTS ARE TO BE EXPOSED FOR A PERIOD GREATER THAN FORTY-EIGHT (48)-HOURS, THE EXPOSED AREA SHALL BE COVERED WITH AT LEAST SIX (6)-INCHES OF MULCH AND MAINTAINED MOIST DURING THE COURSE OF CONSTRUCTION UNTIL THE AREA CAN BE PROPERLY BACKFILLED. PHOTOS TO BE TAKEN PERIODICALLY AND REPORTED TO THE FORESTRY INSPECTOR BY LANDSCAPE CONTRACTOR OR CONTRACTED CERTIFIED ARBORIST.
- NO RUNOFF OR SPILLAGE OF NOXIOUS MATERIALS WHILE MIXING, PLACING, OR STORING CONSTRUCTION MATERIAL SHALL OCCUR WITHIN THE TREE PIT OR CRZ. NO PONDING, ERODING, OR EXCESSIVE WETTING CAUSED BY DEWATERING OPERATIONS SHALL OCCUR WITHIN TREE PIT OR CRITICAL ROOT ZONE.
- ALL EXISTING TREES BEING PROTECTED ON PROPOSED JOBSITE ARE TO BE WATERED 20 GALLONS ONCE WEEKLY BETWEEN MARCH 1 AND OCTOBER 30 ACCORDINGLY TO BEST PRESERVE EXISTING TREES DURING DEMOLITION AND CONSTRUCTION PROCESSES. WATERING SHALL BE DONE IN A MANNER THAT THERE SHOULD NOT BE STANDING WATER AROUND THE TREE.
- UNLESS OTHERWISE NOTED IT IS BEST TO KEEP EXISTING CONCRETE WITHIN TREE PROTECTION ZONE AS LONG AS POSSIBLE UNTIL REMOVAL AND REINSTALLATION OF NEW SIDEWALK. CONCRETE SHOULD BE LEFT INTACT THROUGHOUT THE DEMOLITION AND CONSTRUCTION PROCESS TO PREVENT FURTHER SOIL COMPACTION ON EXISTING TREE ROOTS. OTHER WORK MAY BE SPECIFIED BY FORESTRY INSPECTOR TO BE DONE WITHIN A PRESCRIBED TIMEFRAME. METAL GRATES ARE TO BE REMOVED IMMEDIATELY. COBBLESTONES ARE TO BE REMOVED IMMEDIATELY AND THE VOID CREATED IS TO BE AMENDED WITH SOIL LEVEL TO THE SIDEWALK. PIT EXPANSION MAY BE REQUIRED BY FORESTRY INSPECTOR.
- PREPARATORY PRUNING WORK SHALL BE PERFORMED ONLY WHEN DIRECTED BY A FORESTRY INSPECTOR. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH ANSI A300 STANDARDS AND BY A QUALIFIED, LICENSED & INSURED ARBORIST OR TREE SERVICE COMPANY. CONTRACTOR IS TO FOLLOW ALL NYC PARKS FORESTRY PERMIT & WORK ORDER REGULATIONS. CONTRACTOR IS RESPONSIBLE FOR SCHEDULING THE APPOINTMENT WITH FORESTRY INSPECTOR.
- ALL NEW INDICATED TREE PITS ARE TO BE FULLY EXCAVATED TO THE DIMENSIONS LABELED AND REPLACED WITH NEW QUALITY TOPSOIL TO NYC PARKS STANDARDS.



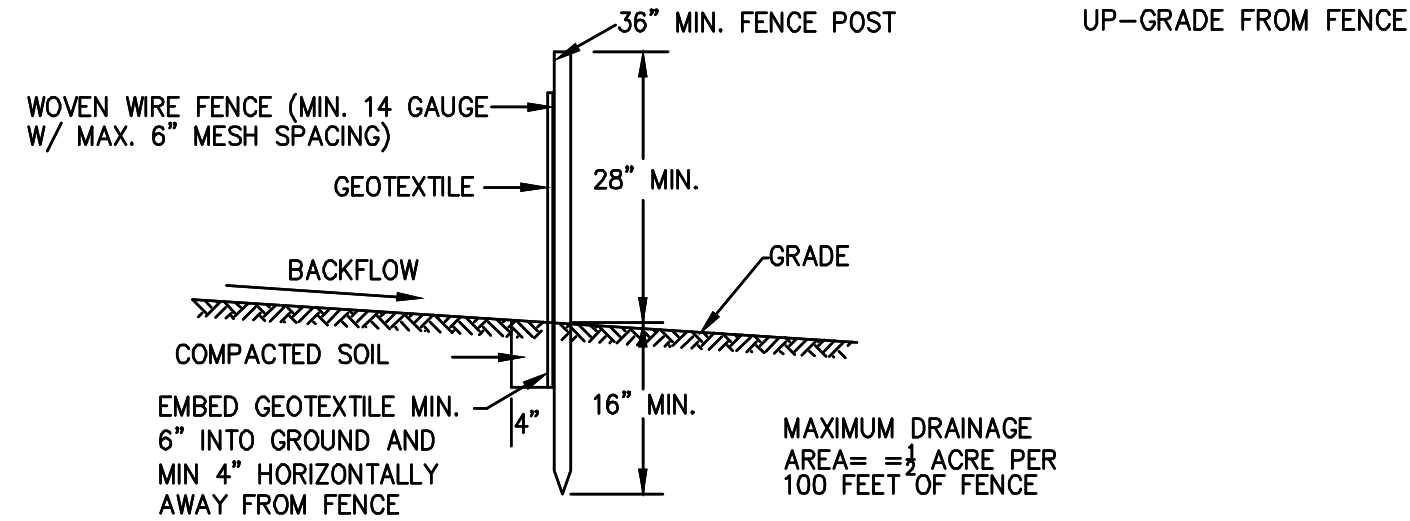
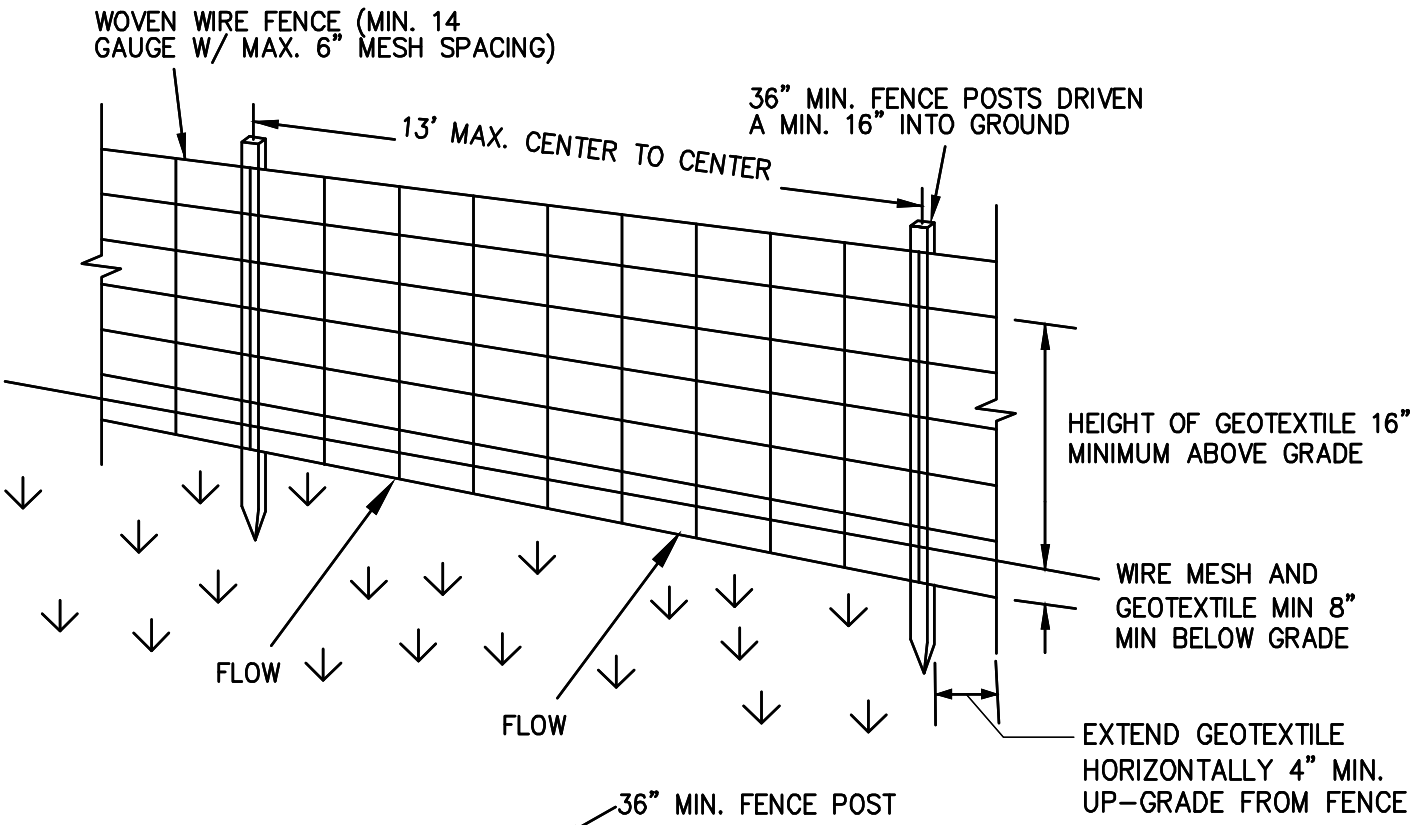
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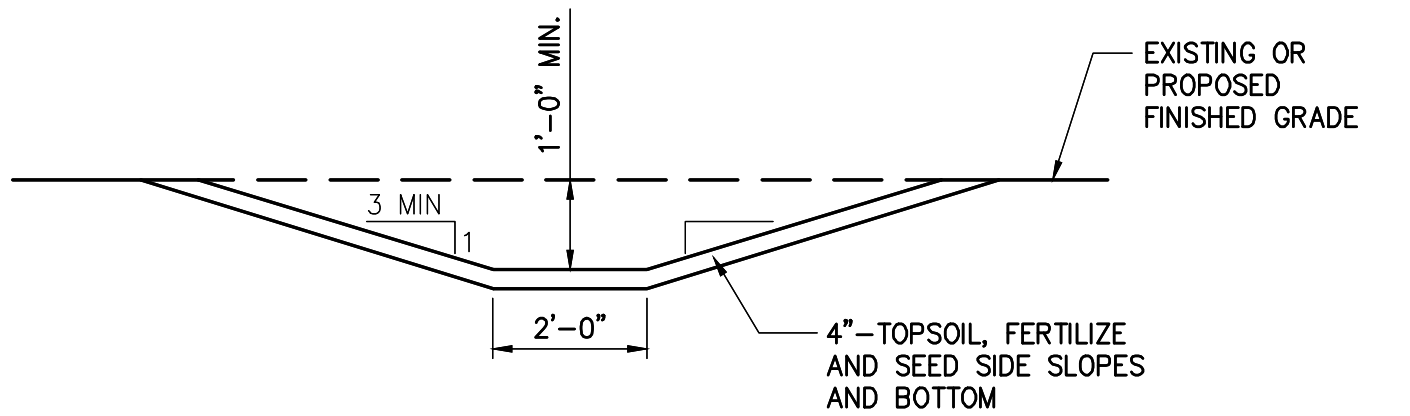
CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
EROSION AND SEDIMENT CONTROL DETAILS

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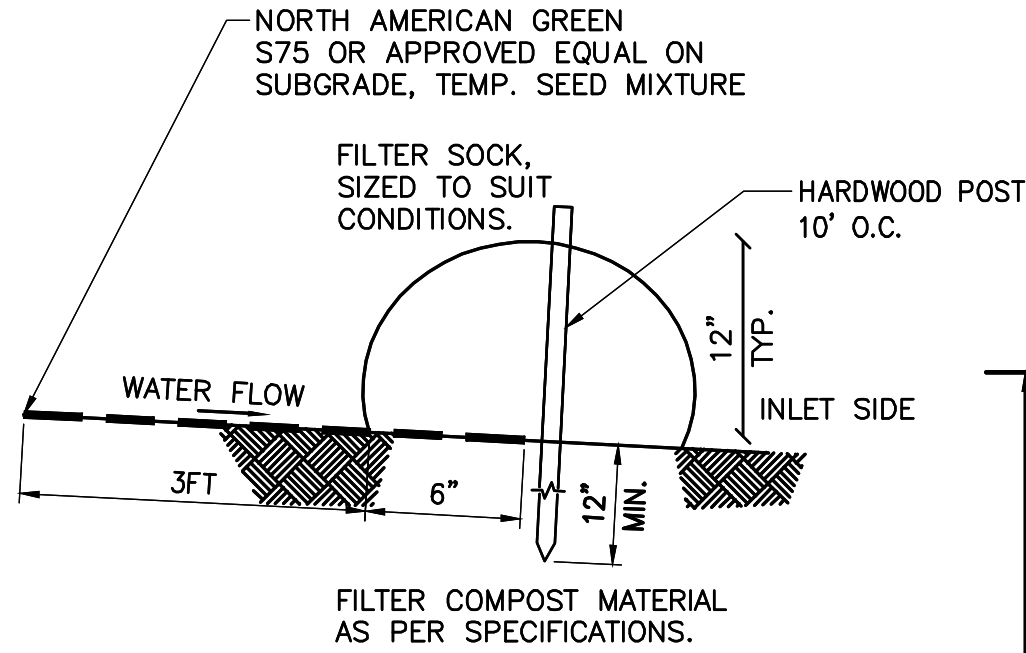
KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-602



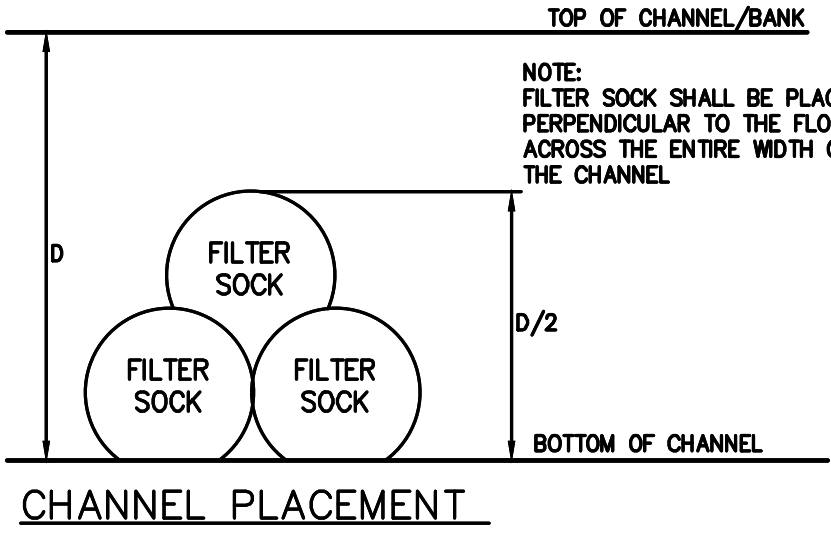
1 SILT FENCE
SCALE: N.T.S.



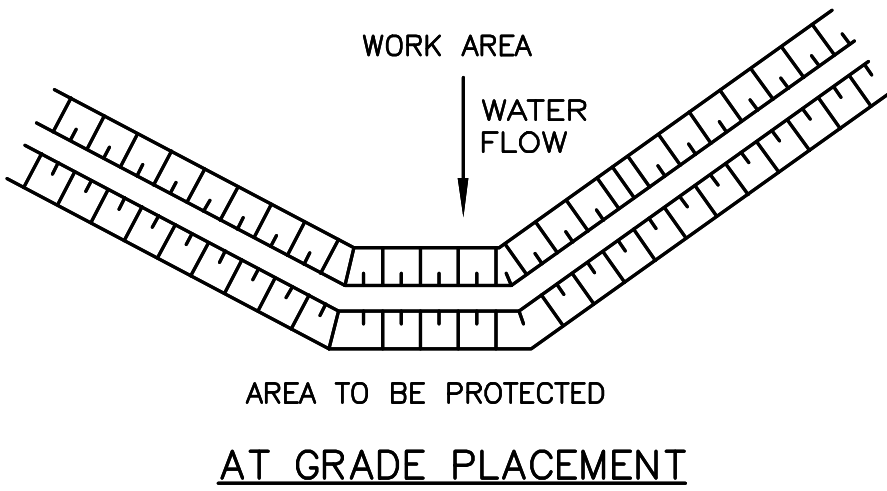
3 TYPICAL GRASS DRAINAGE SWALE
SCALE: N.T.S.



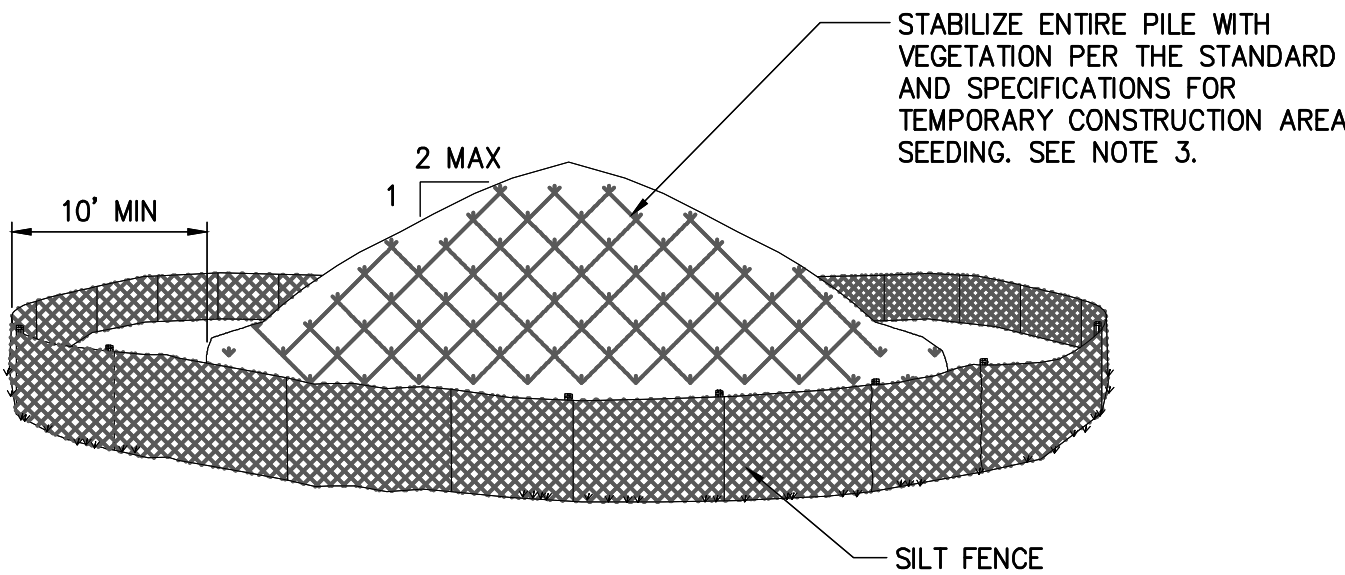
- NOTES:**
1. ALL MATERIAL TO MEET MANUFACTURER SPECIFICATIONS.
 2. ALL FILTER SOCKS SHALL BE 12" DIAMETER UNLESS INDICATED OTHERWISE.
 3. THE CONTRACTOR SHALL MAINTAIN THE COMPOST FILTER BERM IN A FUNCTIONAL CONDITION AT ALL TIMES AND IT SHALL BE ROUTINELY INSPECTED.
 4. WHERE THE BERM REQUIRES REPAIR, IT WILL BE ROUTINELY REPAIRED.
 5. THE CONTRACTOR SHALL REMOVE SEDIMENTS COLLECTED AT THE BASE OF THE BERM WHEN THEY REACH 1/3 OF THE EXPOSED HEIGHT OF THE BERM, OR AS DIRECTED BY THE OWNERS.
 6. THE COMPOST FILTER BERM WILL BE REMOVED ON SITE WHEN NO LONGER REQUIRED, AS DETERMINED BY THE OWNERS.
 7. INSTALL PERPENDICULAR TO FLOW.



- MAINTENANCE NOTES:**
1. TRAFFIC SHALL NOT BE PERMITTED TO CROSS FILTER SOCKS.
 2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES 1/3 OF THE EXPOSED HEIGHT OF THE PRACTICE AND DISPOSED OF IN ACCORDANCE WITH THE SWPPP.
 3. SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED IN THE MANNER REQUIRED BY THE MANUFACTURER OR REPLACED WITHIN 24 HOURS OF INSPECTION NOTIFICATION.
 4. BIODEGRADABLE FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTO-DEGRADABLE FILTER SOCKS AFTER 1 YEAR. POLY-PROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
 5. UPON STABILIZATION OF THE AREA CONTRIBUTORY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK SHALL BE REMOVED. FOR REMOVAL THE MESH CAN BE CUT AND COMPOST SPREAD AS AN ADDITIONAL MULCH TO ACT AS A SOIL SUPPLEMENT.

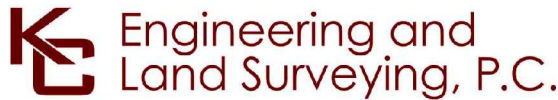


2 COMPOST FILTER SOCK DETAIL
SCALE: N.T.S.



- NOTES:**
1. AREA CHOSEN FOR STOCKPILING OPERATIONS SHALL BE DRY, STABILIZED AND LOCATED AWAY FROM KNOWN WORK AREAS TO PREVENT RELOCATION.
 2. MAXIMUM STOCKPILE HEIGHT SHALL BE 12 FEET.
 3. EACH PILE SHALL BE SURROUNDED WITH SILT FENCING, INSTALLED PER SILT FENCE DETAIL, THEN STABILIZED IN ACCORDANCE WITH THE NYSDEC STANDARD AND SPECIFICATIONS FOR TEMPORARY CONSTRUCTION AREA SEEDING WITHIN 7 DAYS OF COMPLETION.
 4. A PERIMETER DIKE/SWALE SHALL BE LOCATED UP-SLOPE OF THE TOPSOIL STOCKPILE TO DIVERT STORMWATER AROUND THE STOCKPILE.

4 STABILIZED SOIL STOCKPILE
SCALE: N.T.S.



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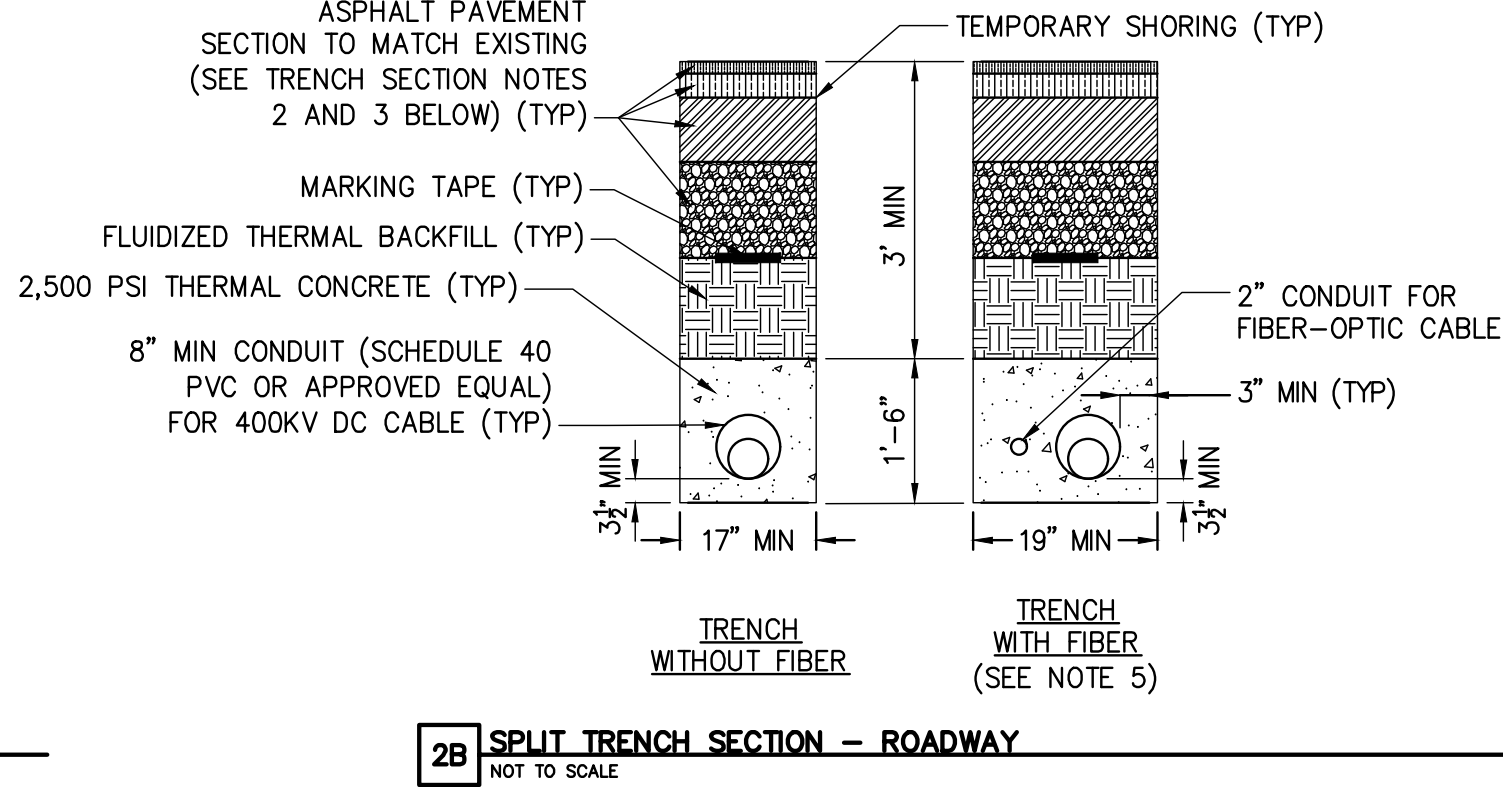
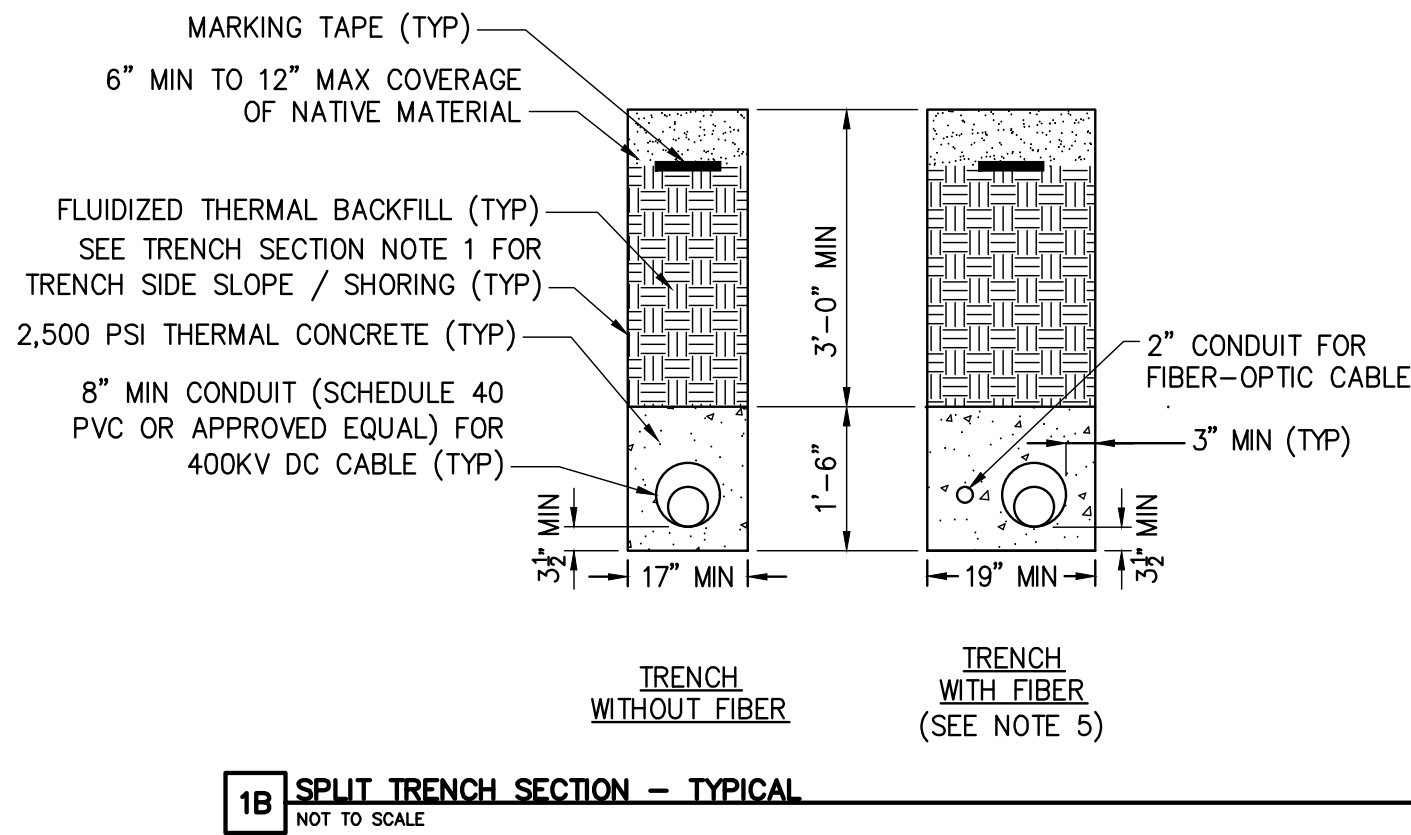
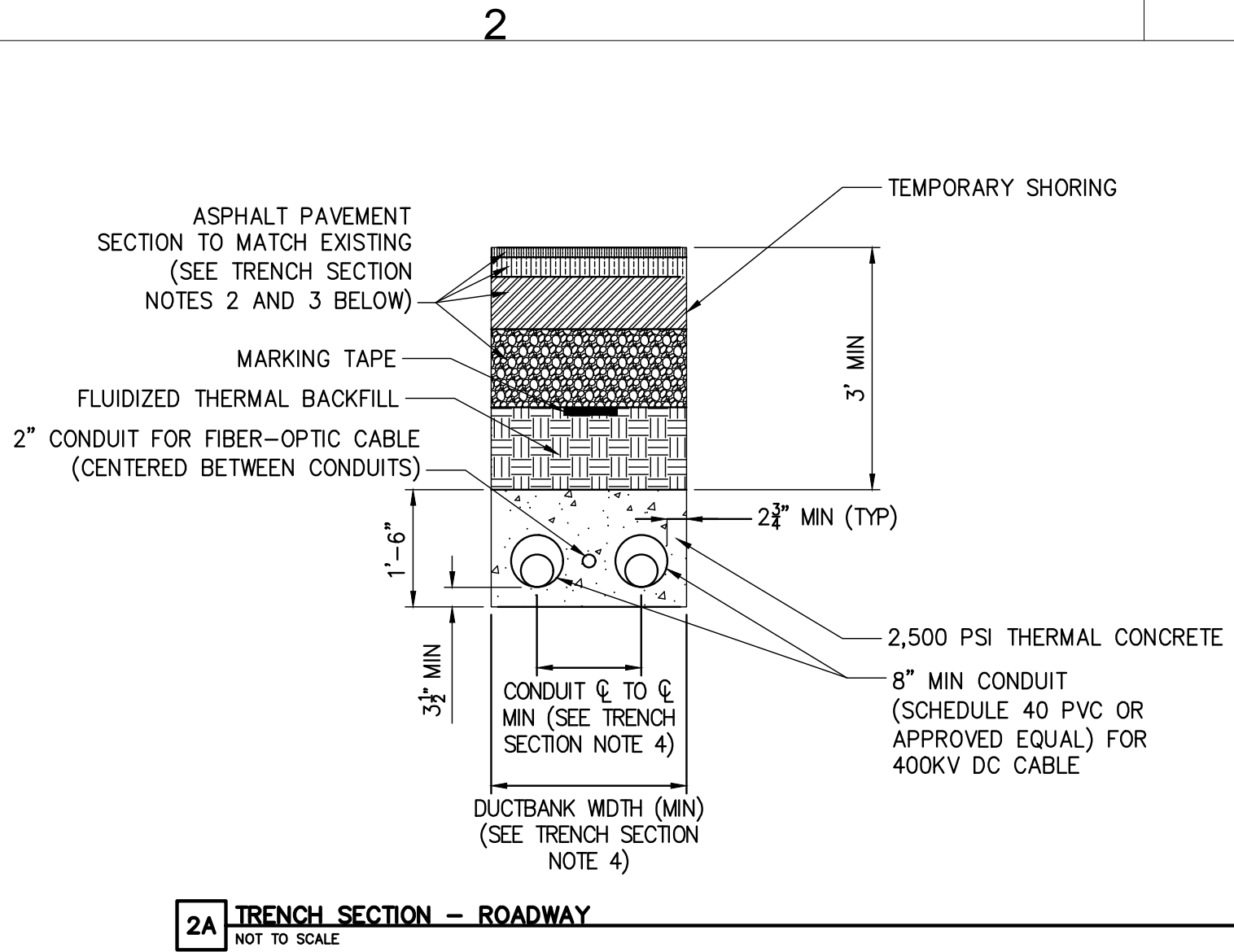
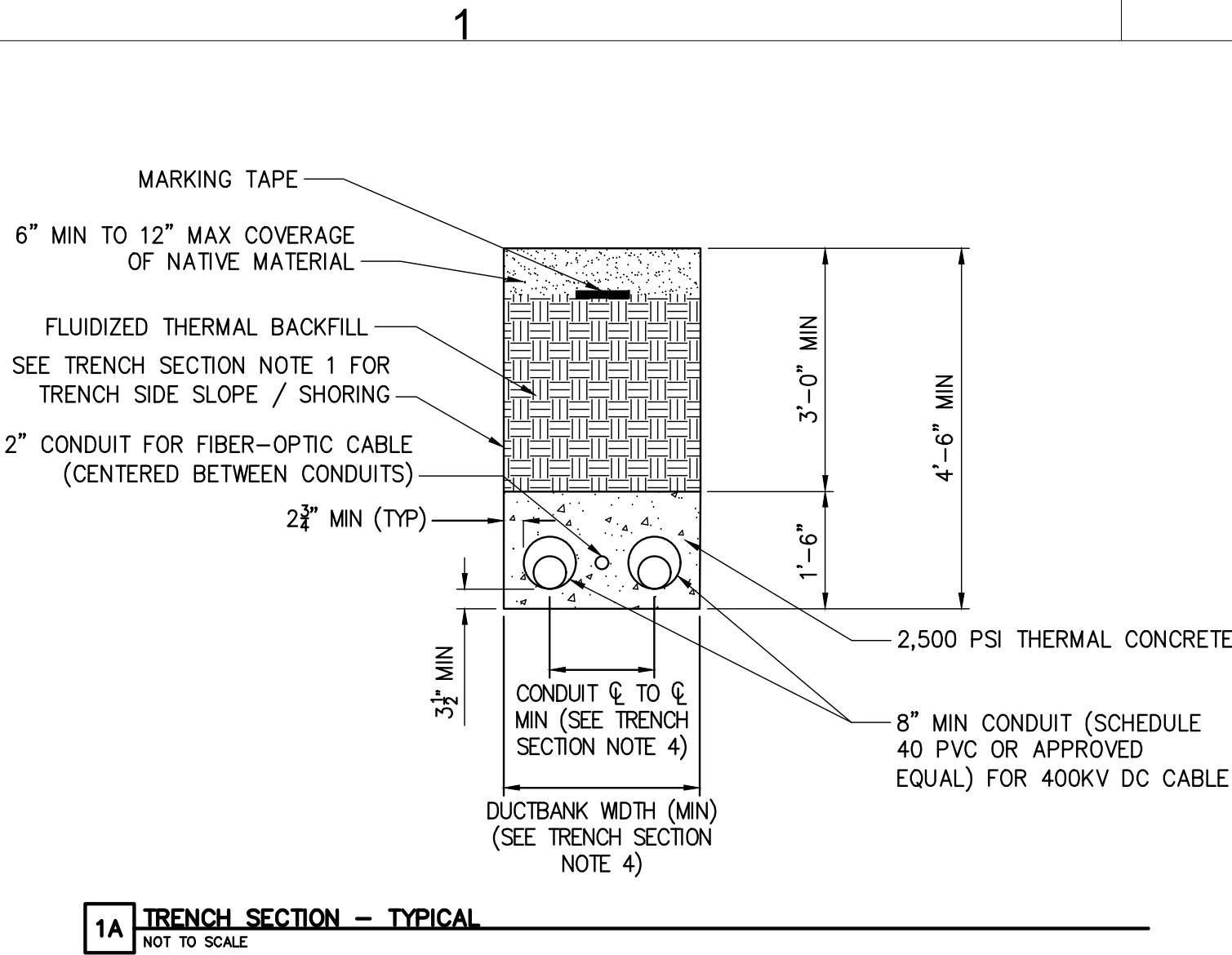
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CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
EROSION AND SEDIMENT CONTROL DETAILS

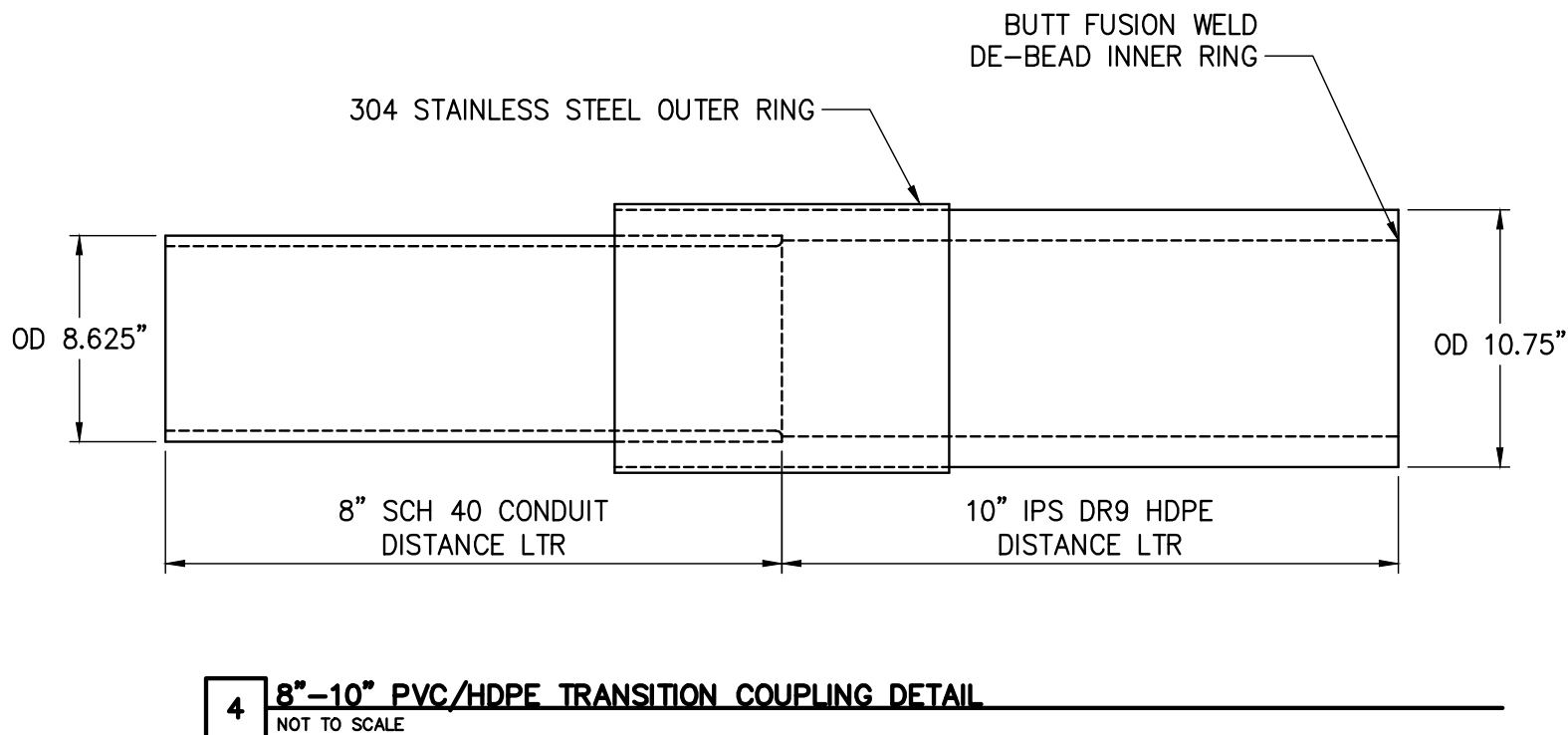
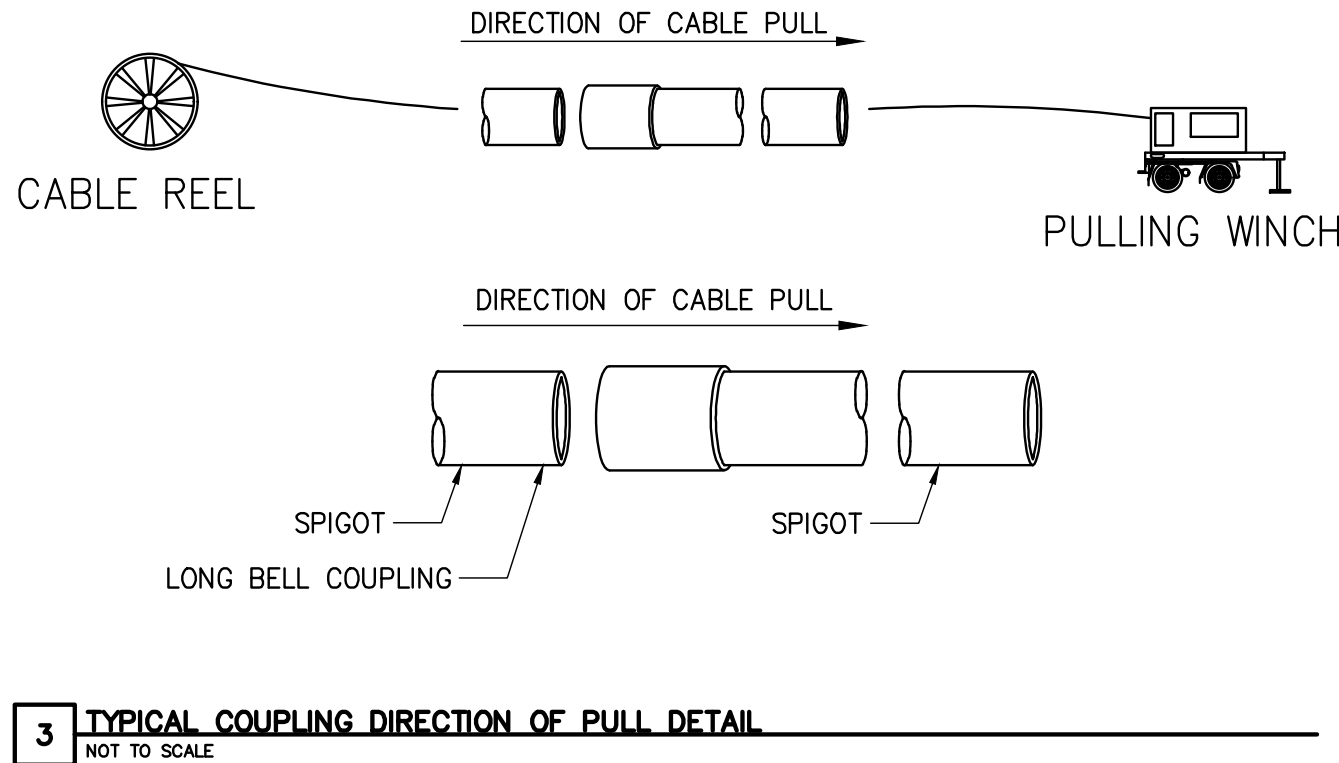
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KC PROJECT NO.	120174
DRAWING NO.	C-603
DATE	07/31/2023
SH.NO.	OF

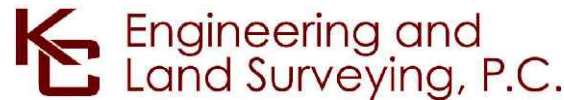
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- TRENCH SECTION NOTES:
1. SLOPING, BENCHING, OR SHORING SHALL BE IN ACCORDANCE WITH OSHA EXCAVATION STANDARDS, 29 CFR PART 1926, SUBPART P. AT LOCATIONS WHERE THE TRENCH IS NOT SHORED, SLOPING AND/OR BENCHING WILL DEPEND ON TYPE OF SOILS ENCOUNTERED ON SITE. SLOPE FROM EDGE OF ROADWAY TO BOTTOM OF EXCAVATIONS MAY BE FLATTER THAN 2:1 (H:V) FOR AASHTO HS-20 LOADING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EVALUATING SLOPE STABILITY BASED ON ACTUAL EQUIPMENT FOR SITE OPERATIONS AS DETERMINED BY A GEOTECHNICAL ENGINEER.
 2. SEE DETAIL 8 ON DETAIL SHEET C-631 FOR PAVEMENT TRANSITION DETAIL.
 3. SEE SHEET C-631 FOR SURFACE RESTORATION DETAILS.
 4. SEE PLAN AND PROFILE SHEETS FOR CONDUIT Ø TO Ø AND DUCTBANK WIDTH (NOTE ABOVE PROFILE VIEW).
 5. FIBER TO LEAD IN AND OUT OF (-) POLARITY HDD BORE, AS DENOTED ON PLAN AND PROFILE SHEETS.



NOTE:
THIS TRANSITION COUPLING COMES ASSEMBLED AS A UNIT. POLY-CAM, ISCO INDUSTRIES P/N: 737-1008PVC40PVI09 TRANSITION COUPLING SHALL ARRIVE FROM VENDOR WITH NO ROUGH EDGES OR PROTRUSIONS ON INTERIOR. INTEGRITY OF COUPLING TO BE FIELD-VERIFIED PRIOR TO INSTALLATION. IF UNSATISFACTORY, CONTRACTOR TO SHAPE OR SAND MINOR IRREGULARITIES PRIOR TO INSTALLATION.



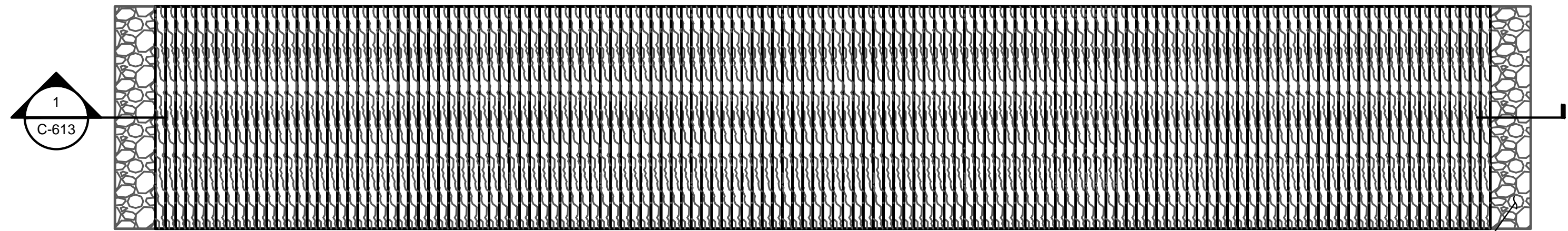
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CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
TRENCHING DETAILS

KIEWIT PROJECT NO.		21162
KC PROJECT NO.		120174
DRAWING NO.		C-621
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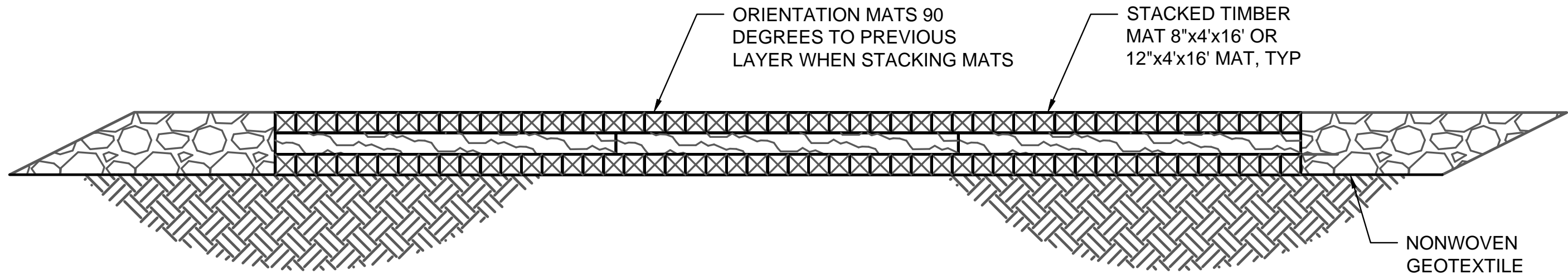
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NO. 3 STONE, NO. 4 STONE OR
GRADED AGGREGATE BASE

WETLAND WORKING SURFACE PLAN

NOT TO SCALE



ORIENTATION MATS 90
DEGREES TO PREVIOUS
LAYER WHEN STACKING MATS

STACKED TIMBER
MAT 8'x4'x16' OR
12'x4'x16' MAT, TYP

NONWOVEN
GEOTEXTILE

SECTION

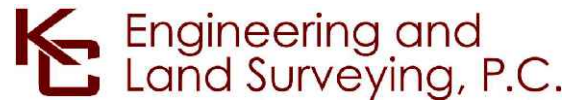
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NOTES:

1. TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO PREVENT RUTTING.
2. BASED ON ACTUAL SITE CONDITIONS, NUMBER OF TIMBER MAT LAYERS TO BE DETERMINED ON SITE.
3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING.
4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND AND AGRICULTURAL LAND DURING RAIN EVENTS. SEDIMENT SHOULD BE REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION.
5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.
6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.
7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.
8. REFER TO EM+CP PLANS FOR EROSION CONTROL DETAILS.
9. REFER TO EM+CP PLANS FOR RESTORATION OF WETLAND.

GENERAL NOTES:

1. TIMBER:
 - A. TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.
2. CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE ENGINEER IMMEDIATELY.
3. NONWOVEN GEOTEXTILE SHALL BE MIRAFI 180N OR EQUIVALENT APPROVED BY EOR.



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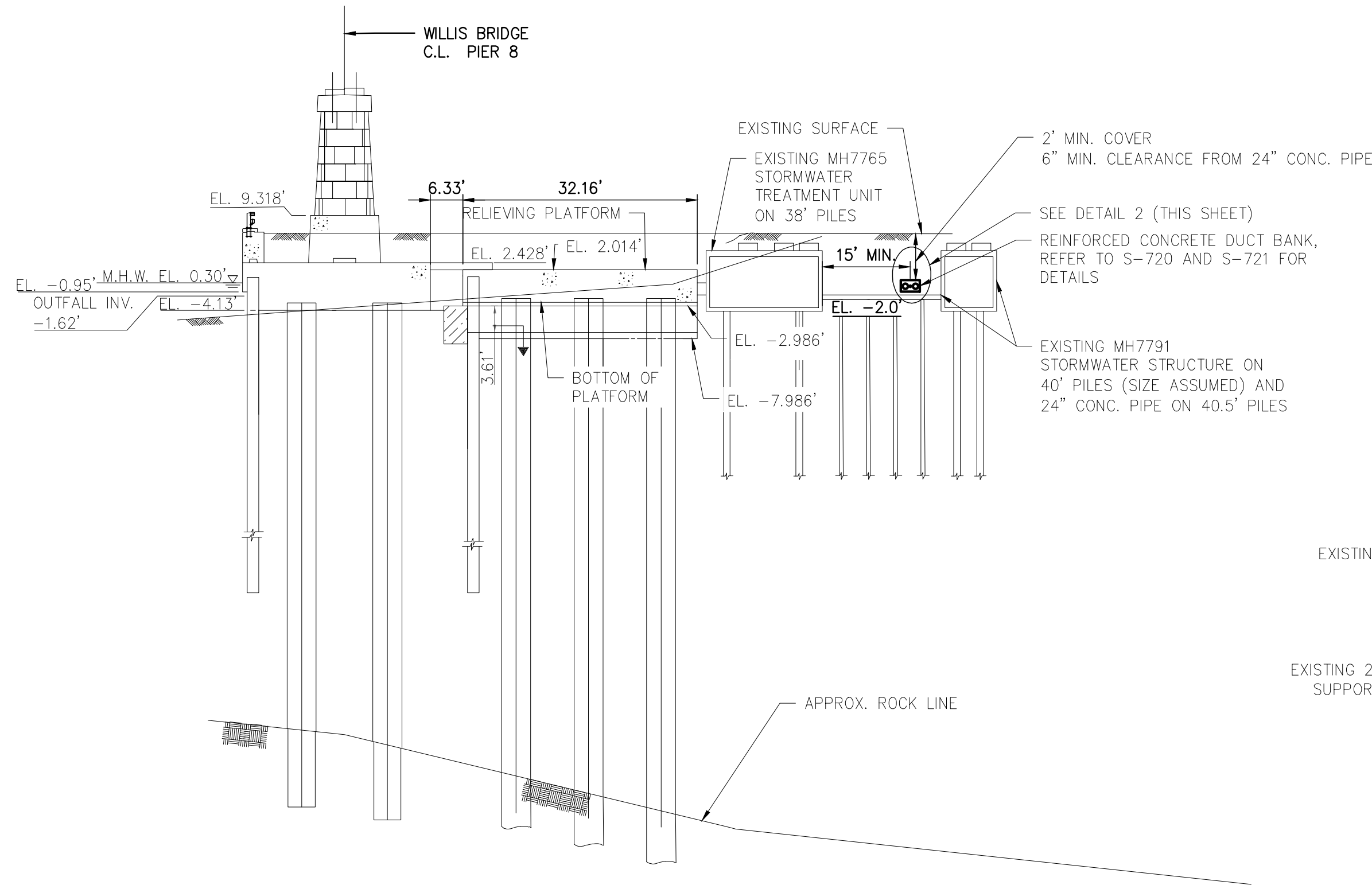
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CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
WETLAND WORKING SURFACE

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-613
DATE	07/31/2023
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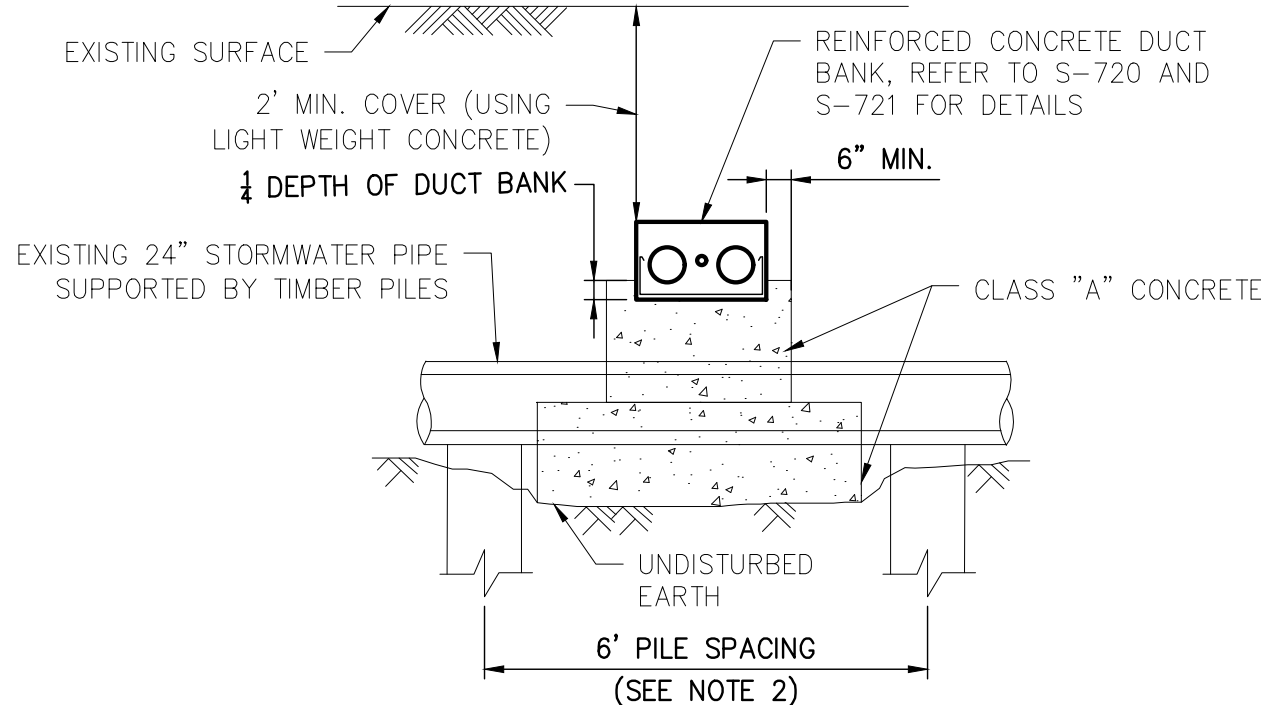
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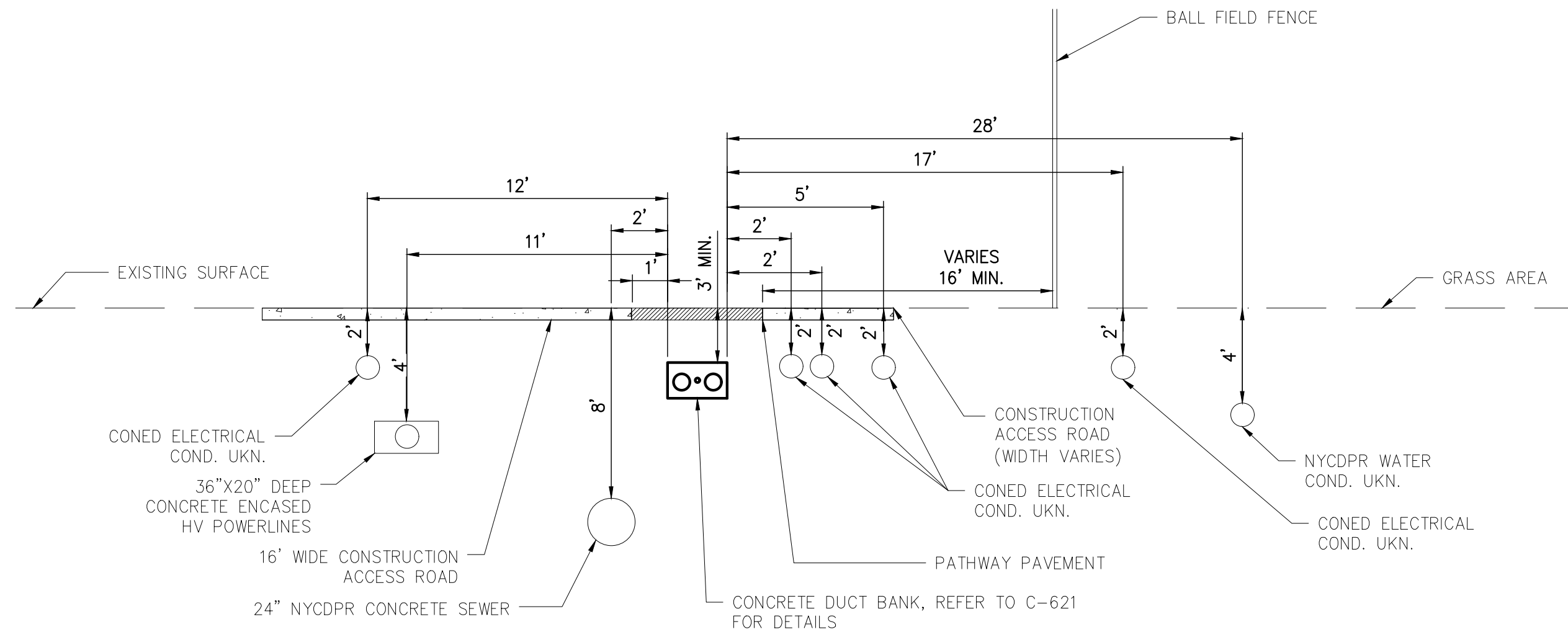
**1 WASTE MANAGEMENT FACILITY SECTION AT PIER 8
FROM APPROX. STA. 80000+50 TO STA. 80001+50**
NOT TO SCALE

NOTE: EXISTING STORMWATER TREATMENT UNIT STRUCTURES AND BRIDGE SUBSTRUCTURE INFORMATION INCLUDING SIZE, MATERIAL, HORIZONTAL AND VERTICAL LOCATION TO BE FIELD VERIFIED. EXISTING INFORMATION IS BASED ON AS-BUILT DRAWING PROVIDED BY MTA - CONTRACT NUMBER BRCR076 DWG NUMBER FE-14F



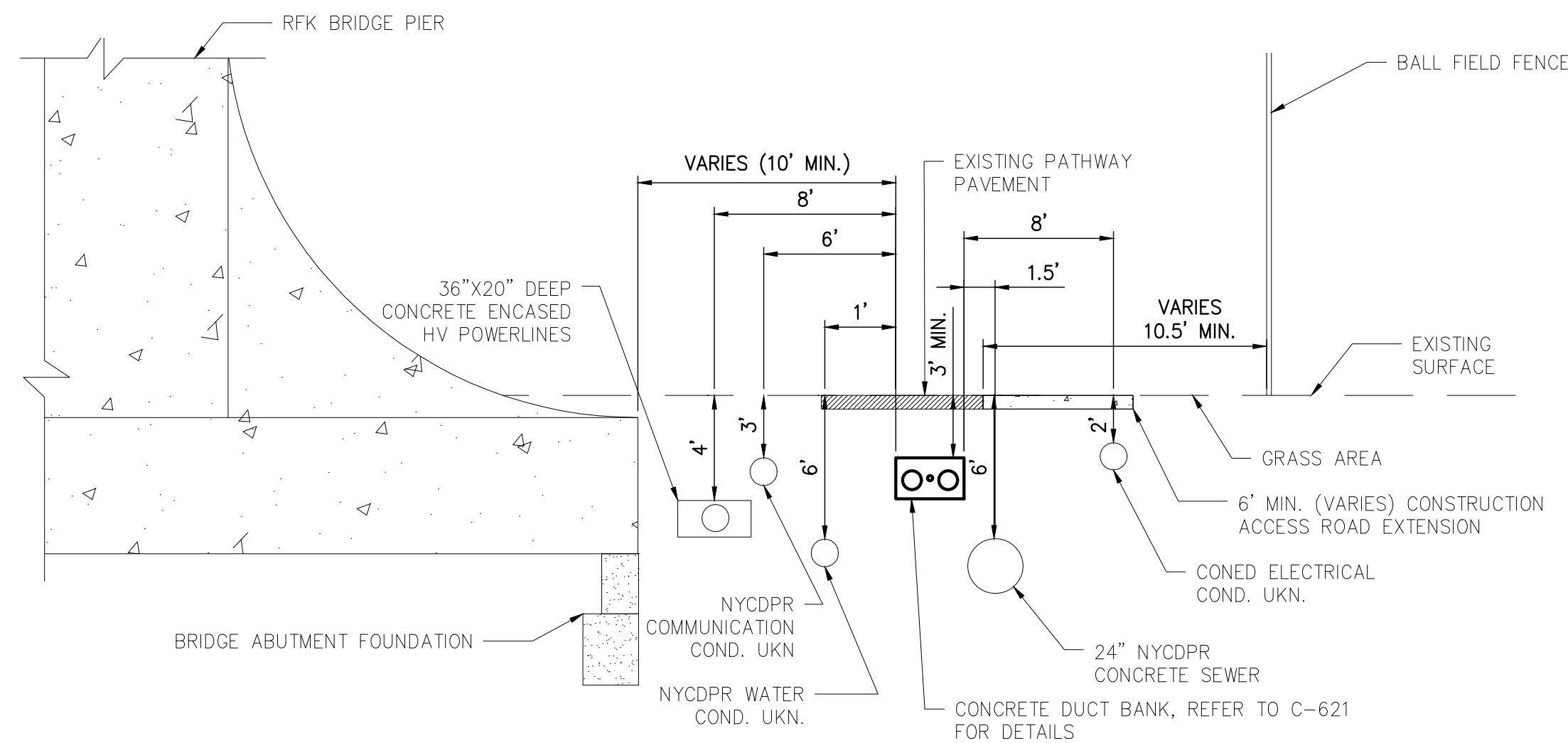
2 CONCRETE ENCASEMENT DETAIL AT APPROX STA. 80001+50
NOT TO SCALE

NOTES:
1. CONCRETE ENCASEMENT SHALL BE IN ACCORDANCE WITH NYSDOT STANDARD SHEET 663-04.
2. 6' PILE SPACING IS BASED ON NYCDP SEWER DESIGN STANDARD SHEET SE-04.
3. LIGHTWEIGHT CONCRETE BACKFILL CONFORMING TO SECTION 6.10 OF NYCDP GENERAL SPECIFICATIONS 11-CONCRETE.
4. MAXIMUM 75 PCF UNIT WEIGHT FOR LIGHTWEIGHT FILL
5. REDUCED THICKNESS OF LIGHTWEIGHT FILL IS ALLOWED FOR LOCATIONS WHERE UTILITIES ARE ENCOUNTERED.



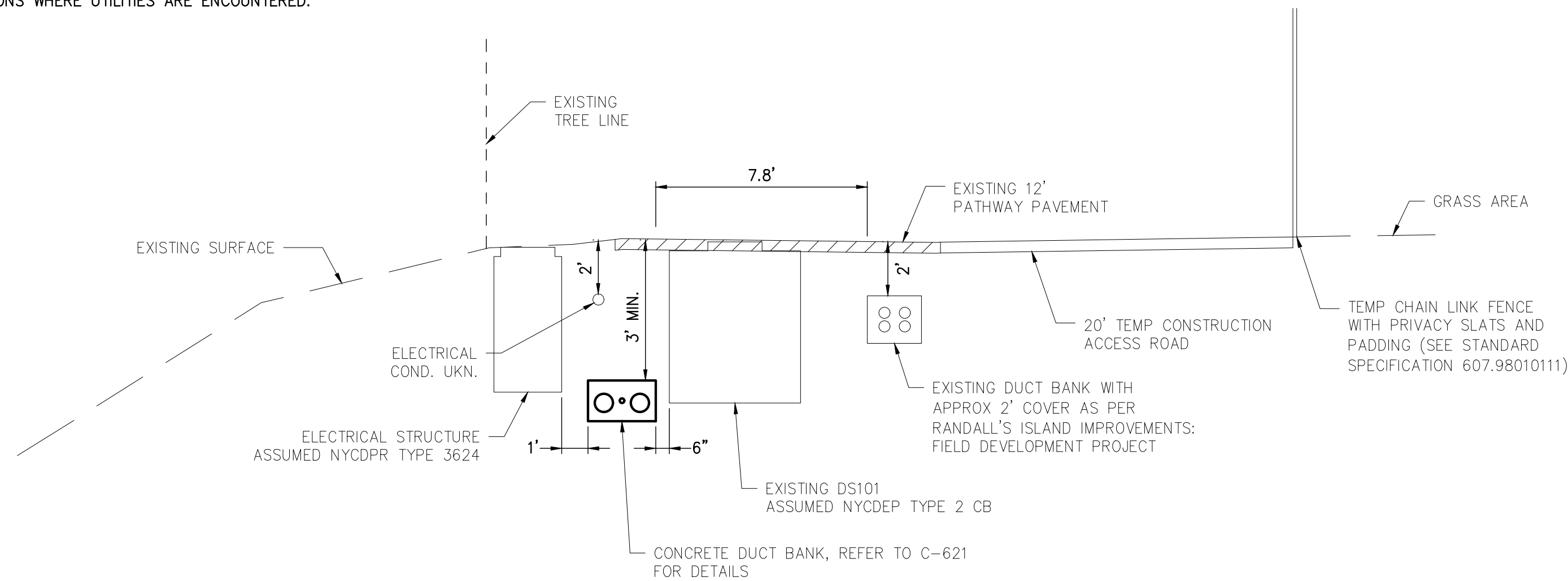
4 RANDALL'S ISLAND CROSS SECTION FROM APPROX. STA. 80036+50 TO STA. 80037+50
NOT TO SCALE

NOTE: EXISTING UTILITY INFORMATION SHOWN ARE OBTAINED FROM TEST PIT #4 PERFORMED ON 08/09/2022. ALL EXISTING UTILITY INFORMATION IS TO BE FIELD VERIFIED.



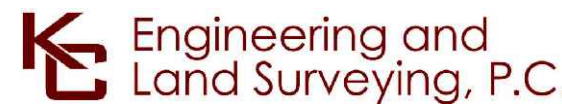
**3 RANDALL'S ISLAND CROSS SECTION
FROM APPROX. STA. 80035+50 TO STA. 80036+50**
NOT TO SCALE

NOTE: EXISTING UTILITY INFORMATION SHOWN ARE OBTAINED FROM TEST PIT #3 PERFORMED ON 08/09/2022. ALL EXISTING BRIDGE STRUCTURE LOCATION IS TO BE FIELD VERIFIED.



5 RANDALL'S ISLAND CROSS SECTION AT APPROX STA. 80041+15
NOT TO SCALE

NOTE: EXISTING ELECTRICAL INFORMATION SHOWN ARE OBTAINED FROM TEST PIT #5 PERFORMED ON 08/09/2022. SIZE OF EXISTING STRUCTURES ARE ASSUMED. ALL EXISTING UTILITY INFORMATION IS TO BE FIELD VERIFIED.



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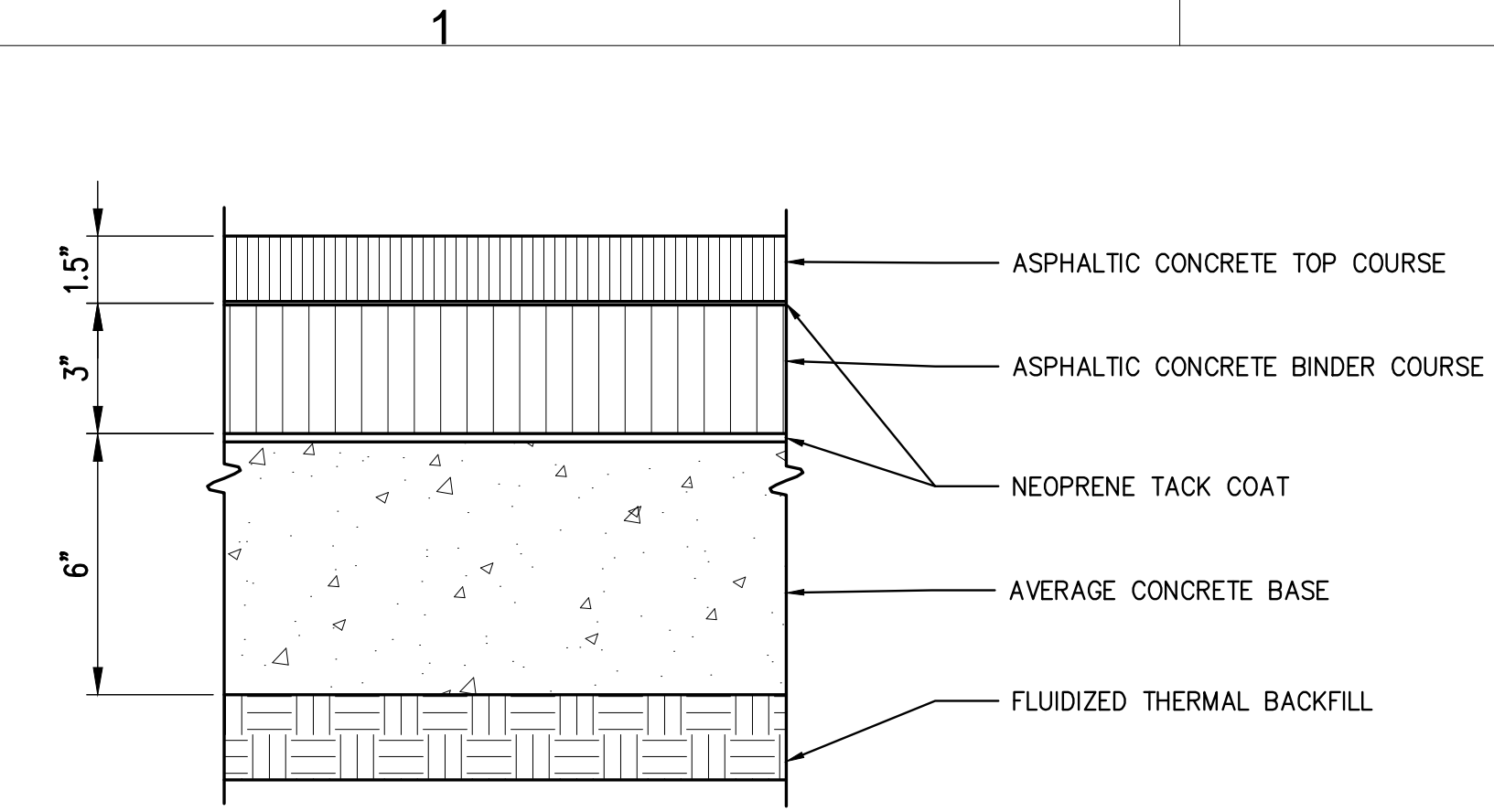
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0	07/31/2023	ISSUED FOR CONSTRUCTION SUBMISSION		MK CV
No.	DATE	SUBMITTAL / REVISION DESCRIPTION		DB APP

**CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
DETAILS**

DRAWN BY:	MK	DESIGNED BY:	MK	APPROVED BY:	CV	SCALE	AS SHOWN	DATE	07/31/2023
						REV. NO.	0	SH. NO.	OF

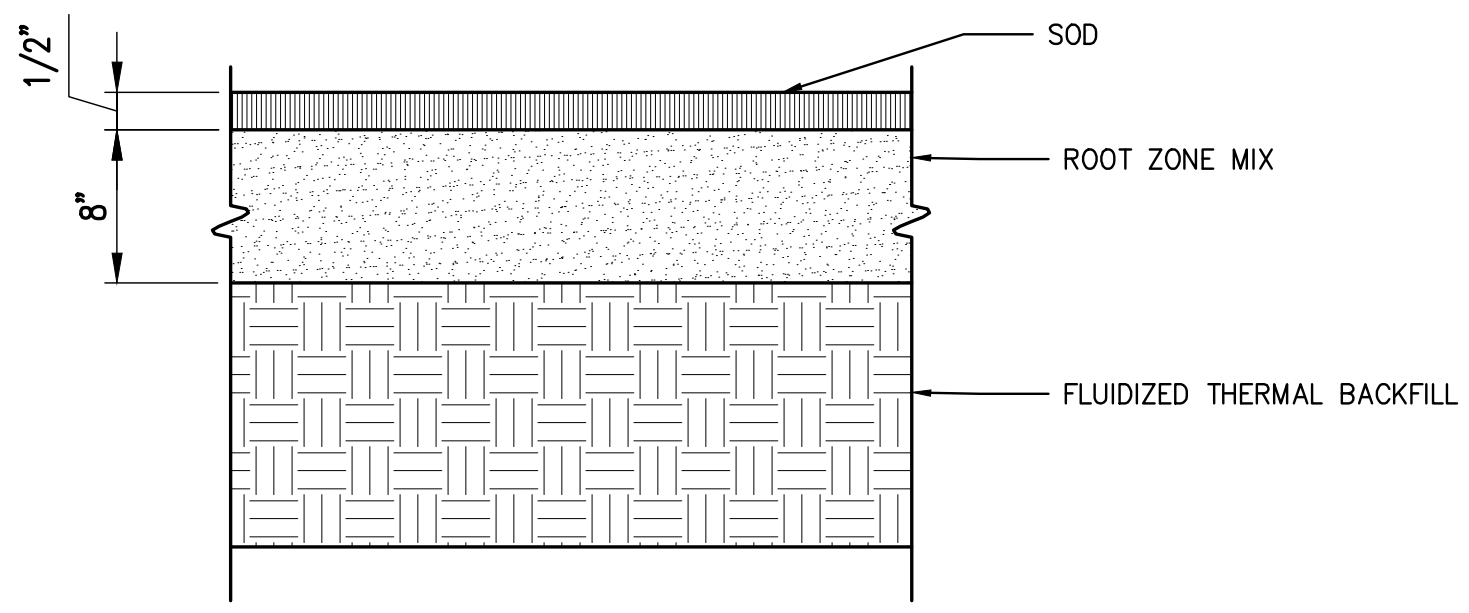
KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-622
DATE	07/31/2023
SH. NO.	OF

File: P:\120174-CHPE-INSTALL-KIEWIT-KIEWIT\60_CAD\20_ENGINEERING_CAD_FILES\PACKAGE_8\NY_HIGHWAY_DESIGN\01_KCE\SHEET_FILES\21162_8_C631-DWG Saved: 7/25/2023 4:14:56 PM Plotted: 7/25/2023 4:24:14 PM Current User: Melinda Kwok LastSavedBy: mkwok



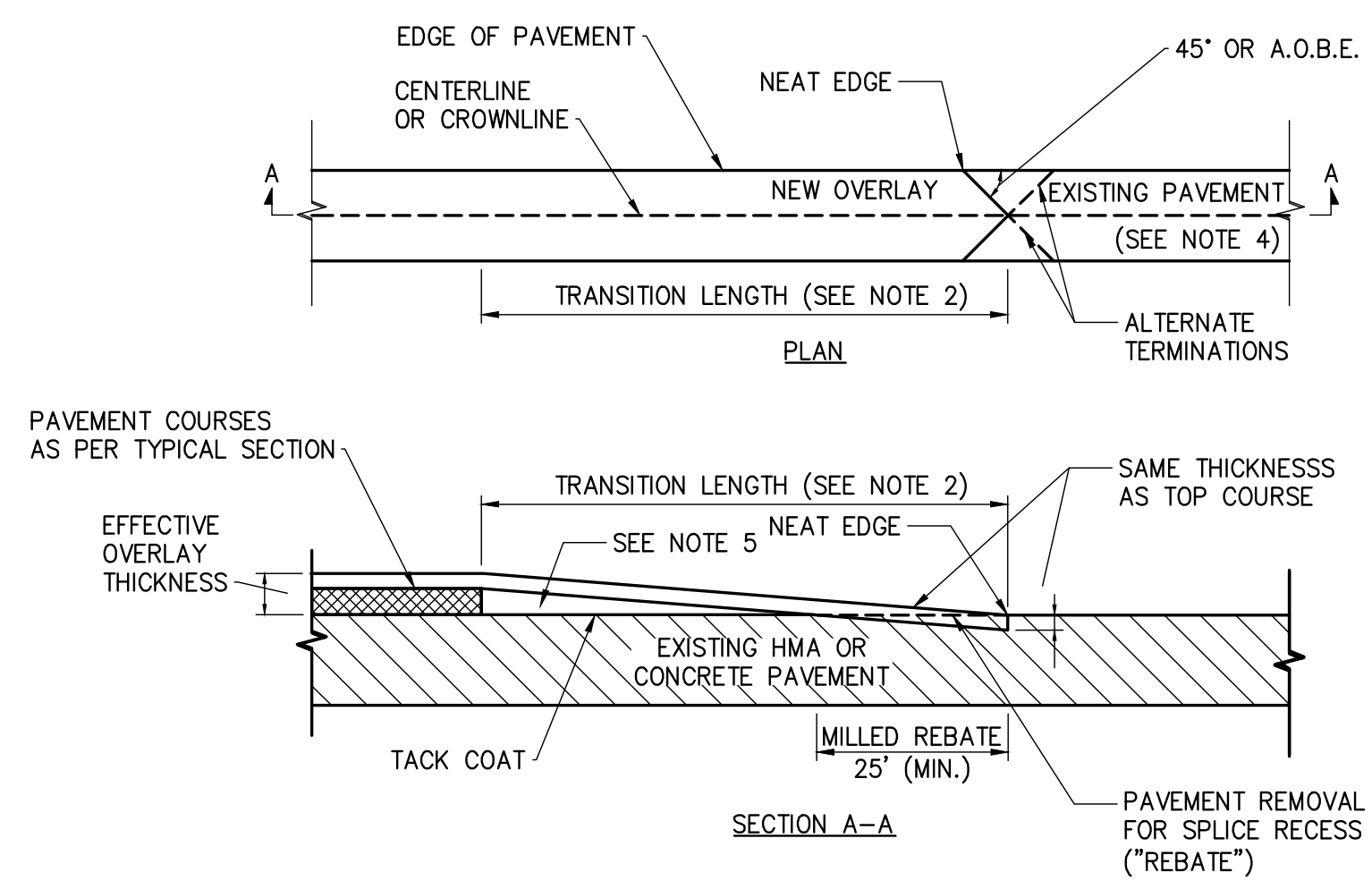
1 ASPHALT PAVEMENT FOR PARK ROADS
NOT TO SCALE

NOTE: DETAILS AND DIMENSIONS BASED ON CITY OF NEW YORK PARKS & RECREATION STANDARD DETAILS - 15, DRAWING TITLE: PAVEMENT DETAILS NO. 1

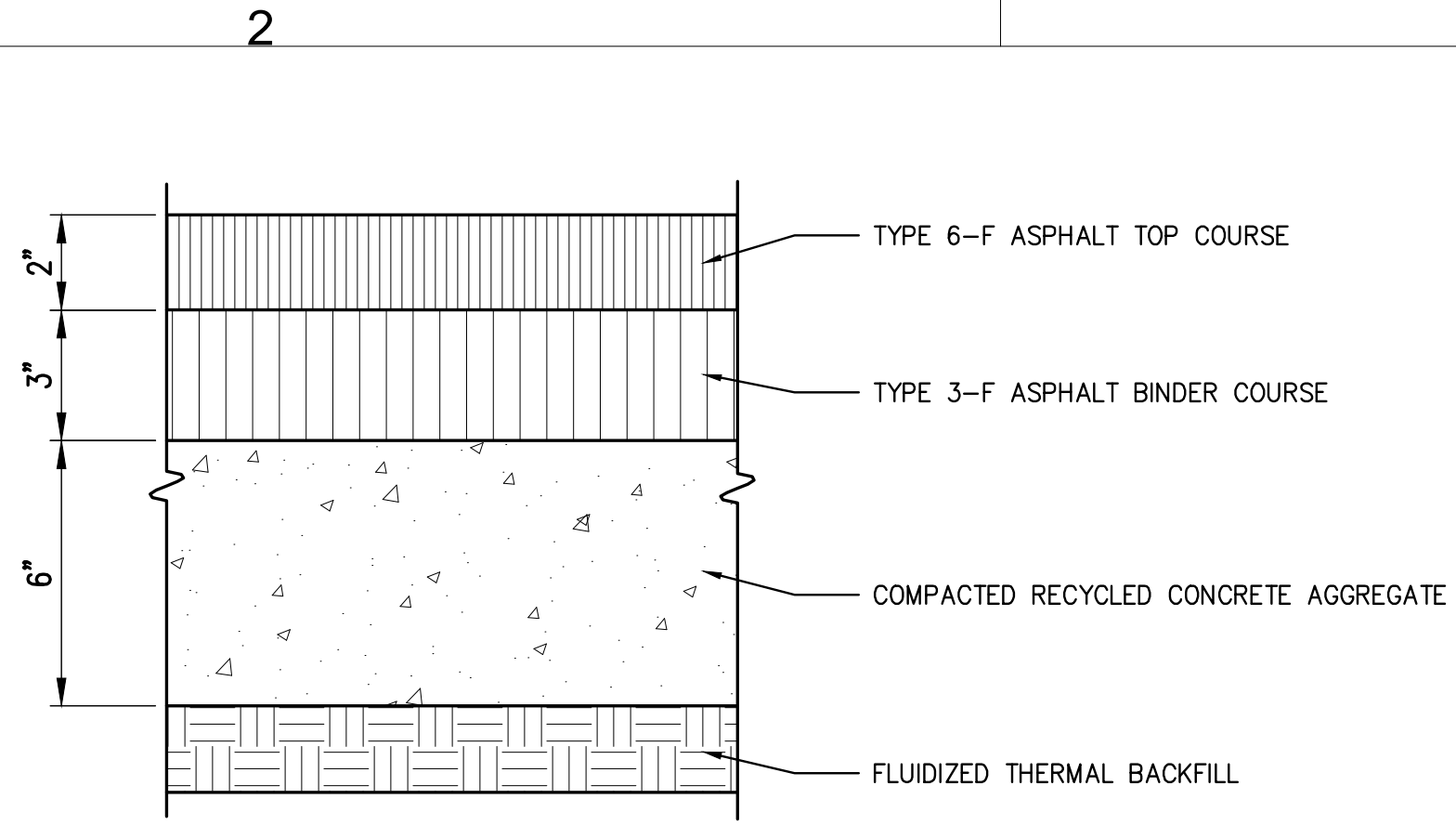


4 SOD
NOT TO SCALE

NOTE: DETAILS AND DIMENSIONS BASED ON BRONX SHORE PATHWAYS PROJECT NYCEDC CONTRACT NO. 52370001, DWG. NO: L5.0

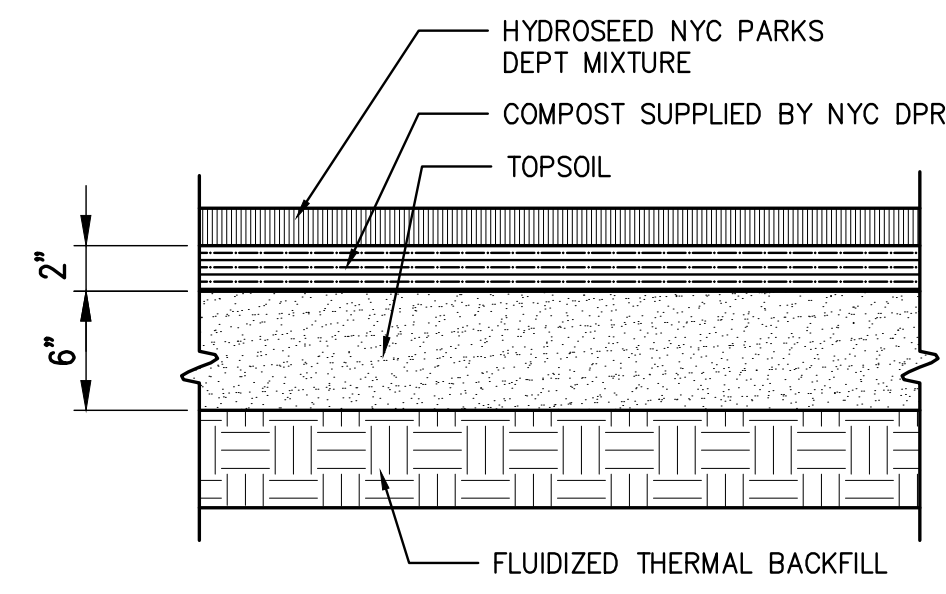


8 HOT MIX ASPHALT OVERLAY SPLICE
N.T.S.



2 PEDESTRIAN WALKWAY ASPHALT PAVEMENT
NOT TO SCALE

NOTE: DETAILS AND DIMENSIONS BASED ON BRONX SHORE PATHWAYS PROJECT NYCEDC CONTRACT NO. 52370001, DWG. NO: L5.1



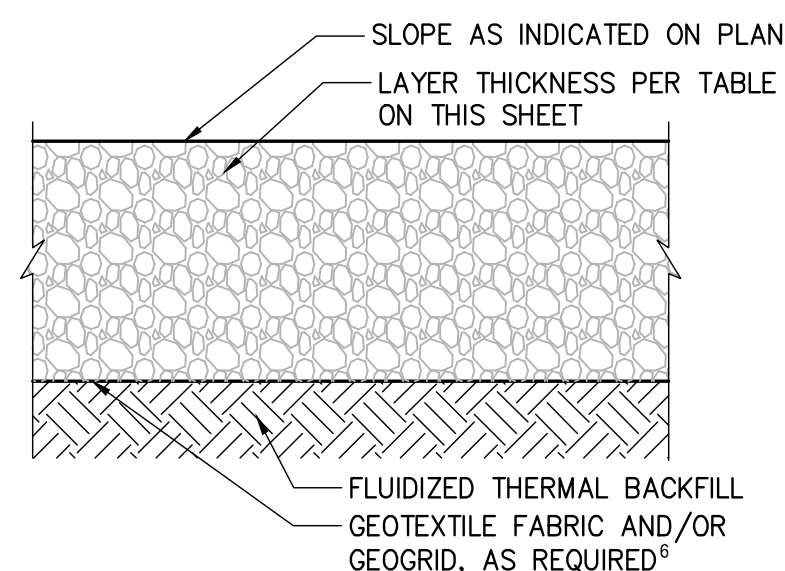
5 HYDROSEED
NOT TO SCALE

NOTE: DETAILS AND DIMENSIONS BASED ON BRONX SHORE PATHWAYS PROJECT NYCEDC CONTRACT NO. 52370001, DWG. NO: L5.0

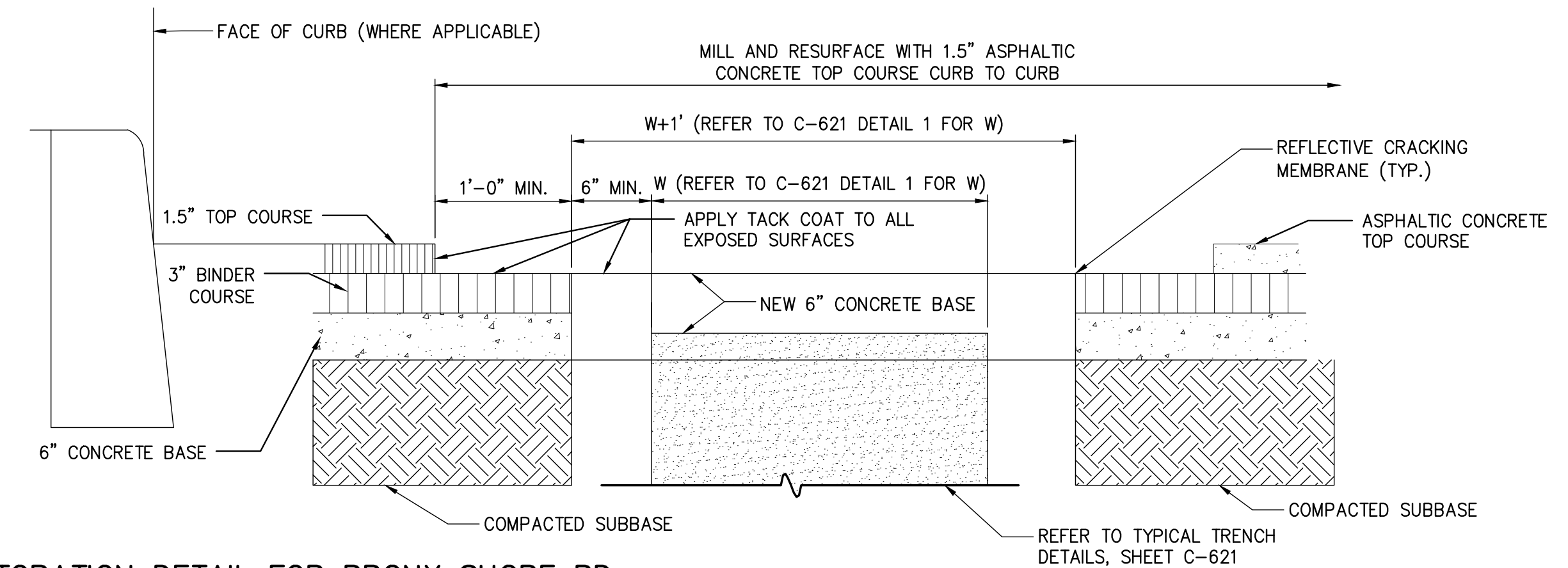
NOTES:

- HOT MIX ASPHALT (HMA) OVERLAY SPLICES SHALL BE USED AT: ENDS OF HMA OVERLAY HIGHWAY SECTIONS, MAJOR INTERSECTIONS, AND OTHER LOCATIONS INDICATED IN THE PLANS.
- THE TRANSITION LENGTH IN FEET SHALL NOT BE LESS THAN THE VALUE OBTAINED BY MULTIPLYING THE EFFECTIVE OVERLAY THICKNESS IN INCHES (DIFFERENCE BETWEEN THE EXISTING AND THE OVERLAD ELEVATIONS) BY THE K VALUE FROM THE TABLE FOR THE POSTED SPEED OF THE HIGHWAY. THE MINIMUM TRANSITION LENGTH IS 30'.
- ALL SURFACES OF THE HMA OVERLAY SPLICE TRANSITION AREA SHALL BE CLEANED AND TACK-COATED PRIOR TO HMA PLACEMENT. THE COST OF MILLING REBATES, AND TACK COAT IN THE HMA OVERLAY SPLICE TRANSITION AREA SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS. COST OF LEAVING A NEAT EDGE SHALL BE INCLUDED IN THE MILLING ITEM.
- SAW CUTS SHALL BE MADE SO THAT SURFACE RUNOFF IS DIRECTED TO THE EDGE OF PAVEMENT.
- IN THE TRANSITION AREA, PAVEMENT COURSES OTHER THAN THE TOP COURSE SHALL BE FEATHERED OUT USING TOP COURSE OR OTHER APPROPRIATE MATERIAL. PAYMENT SHALL BE MADE UNDER THE APPROPRIATE ITEM.

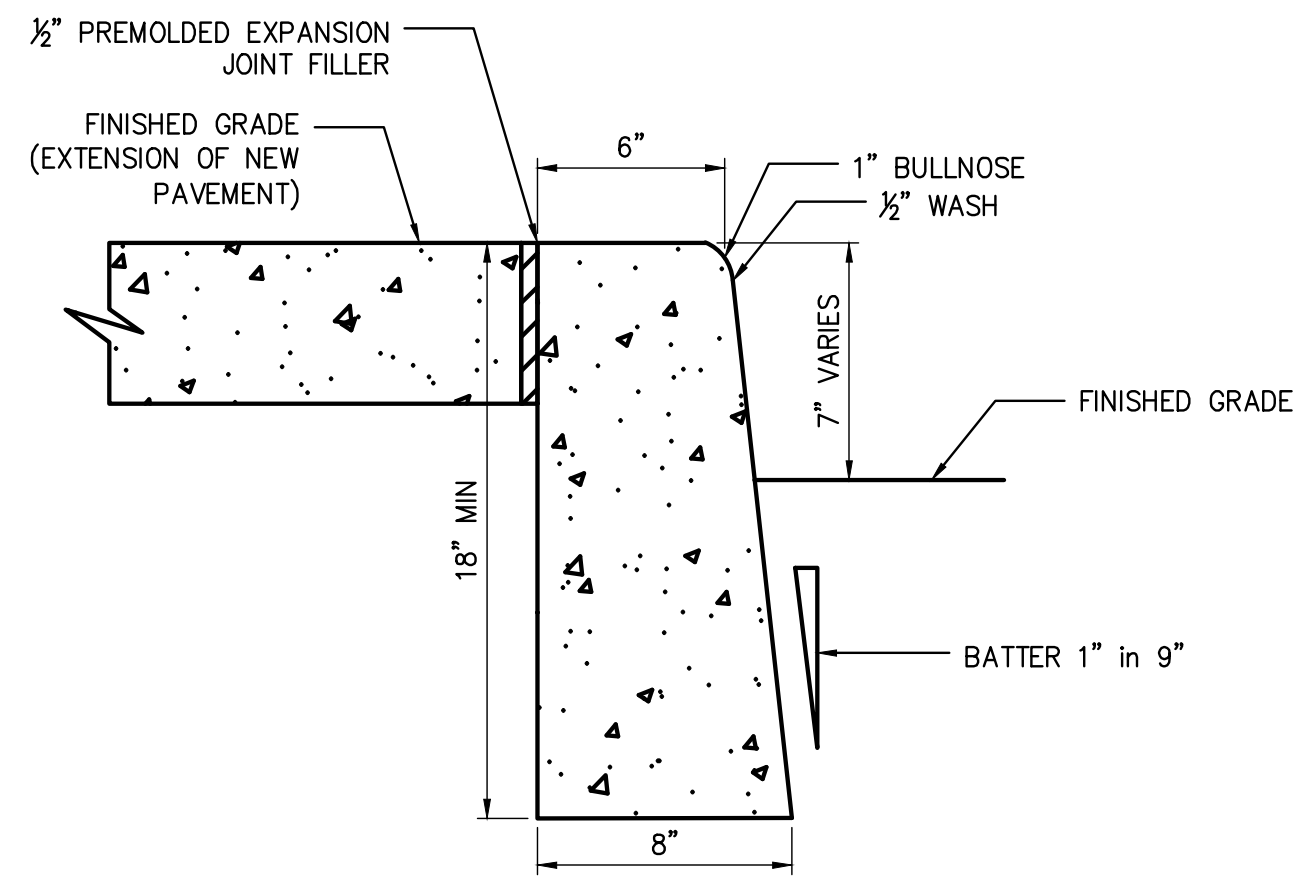
K VALUE TABLE FT/IN										
POSTED SPEED MPH	30	35	40	45	50	55	60	65		
K	16.67	19.17	21.67	24.17	27.50	30.00	32.50	35.00		



6 TEMPORARY ACCESS ROAD
N.T.S.



7 RESTORATION DETAIL FOR BRONX SHORE RD
NOT TO SCALE

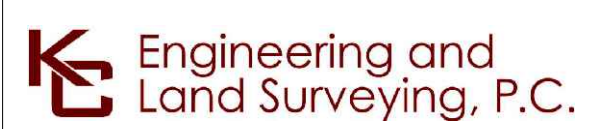


3 CONCRETE STREET CURB
NOT TO SCALE

NOTE: DETAILS AND DIMENSIONS BASED ON CITY OF NEW YORK PARKS & RECREATION STANDARD DETAILS - 18, DRAWING TITLE: CURBS, RAMPS, AND MISCELLANEOUS DETAILS

TEMPORARY ACCESS ROAD SECTION ^{1,2,3,4}				
CBR ⁵	UNSTABILIZED	MIRAFI 180N GEOTEXTILE ⁶	TENSAR BX1200 GEOGRID ⁶	MIRAFI RSI SERIES GEOTEXTILE ⁶
0.5	--	20 INCH RIP RAP ⁷ + 6 INCH AGGREGATE	--	20 INCH RIP RAP ⁷ + 4 INCH AGGREGATE (RS580I)
1.0	--	18 INCH AGGREGATE	12 INCH AGGREGATE	15 INCH AGGREGATE (RS280I)
1.5	--	12 INCH AGGREGATE	9 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
2.0	18 INCH AGGREGATE	11 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
3.0+	15 INCH AGGREGATE	8 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)

- NOTES:
- TEMPORARY ACCESS ROAD SECTIONS PER KIEWIT ENGINEERING (NY) CORP.
 - AGGREGATE SHALL BE NYSDOT TYPE 2 CRUSHED AGGREGATE OR APPROVED ALTERNATIVE.
 - DESIGN CONSIDERS 1,000 PASSES OF MAXIMUM 22-KIP AXLE LOAD AND A DESIGN RUT DEPTH OF 3 INCHES. ADDITIONAL AXLE PASSES, HEAVIER AXLE LOADS, AND DETERIORATED SUBGRADE CONDITIONS MAY REQUIRE THICKER AGGREGATE SECTIONS OR ADDITIONAL MAINTENANCE.
 - ALTERNATE TEMPORARY ACCESS ROAD DESIGNS MAY BE PROVIDED BY KIEWIT ENGINEERING, AS REQUIRED, BASED ON FIELD CONDITIONS AND TRAFFIC LOADING.
 - ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER OR ALTERNATIVE METHOD APPROVED BY GEOTECHNICAL ENGINEER OF RECORD (EOR). CBR OF IN-SITU SOIL MAY VARY SEASONALLY DUE TO FREEZE/THAW AND BASED ON MOISTURE CONDITIONS.
 - GEOGRID AND GEOTEXTILE
 - GEOGRID AND GEOTEXTILES SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS.
 - SPECIFIED GEOTEXTILE OR GEOGRID MAY BE REPLACED BY EQUIVALENT MATERIAL APPROVED BY EOR.
 - GEOTEXTILE IS REQUIRED IN REGULATED WETLANDS AND AGRICULTURAL LANDS.
 - GEOTEXTILE SEPARATOR FABRIC IS REQUIRED BENEATH GEOGRID ON COHESIVE SUBGRADE
 - RIP RAP
 - RIP RAP SHALL BE NYSDOT LIGHT STONE FILL OR APPROVED ALTERNATIVE.
 - A LAYER OF #57 STONE IS RECOMMENDED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE GEOTEXTILE FABRIC WHERE RIP RAP IS USED.



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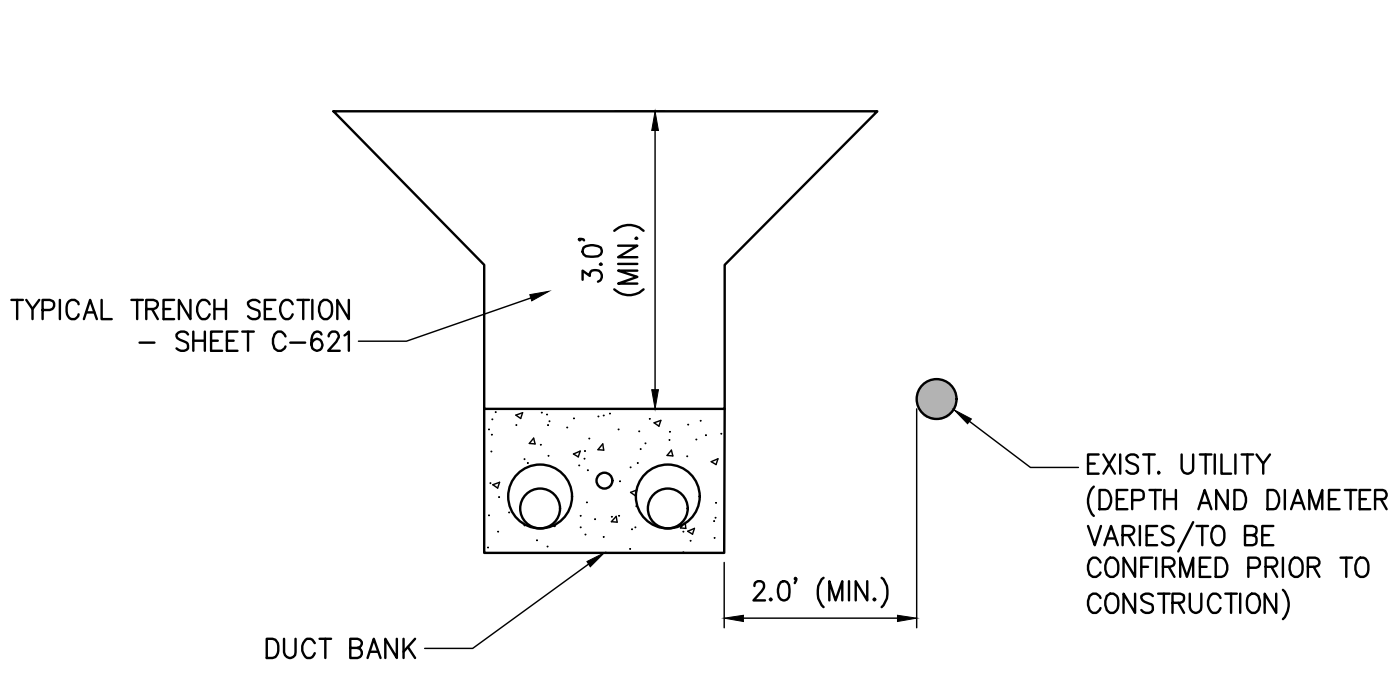
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
SURFACE RESTORATION DETAILS

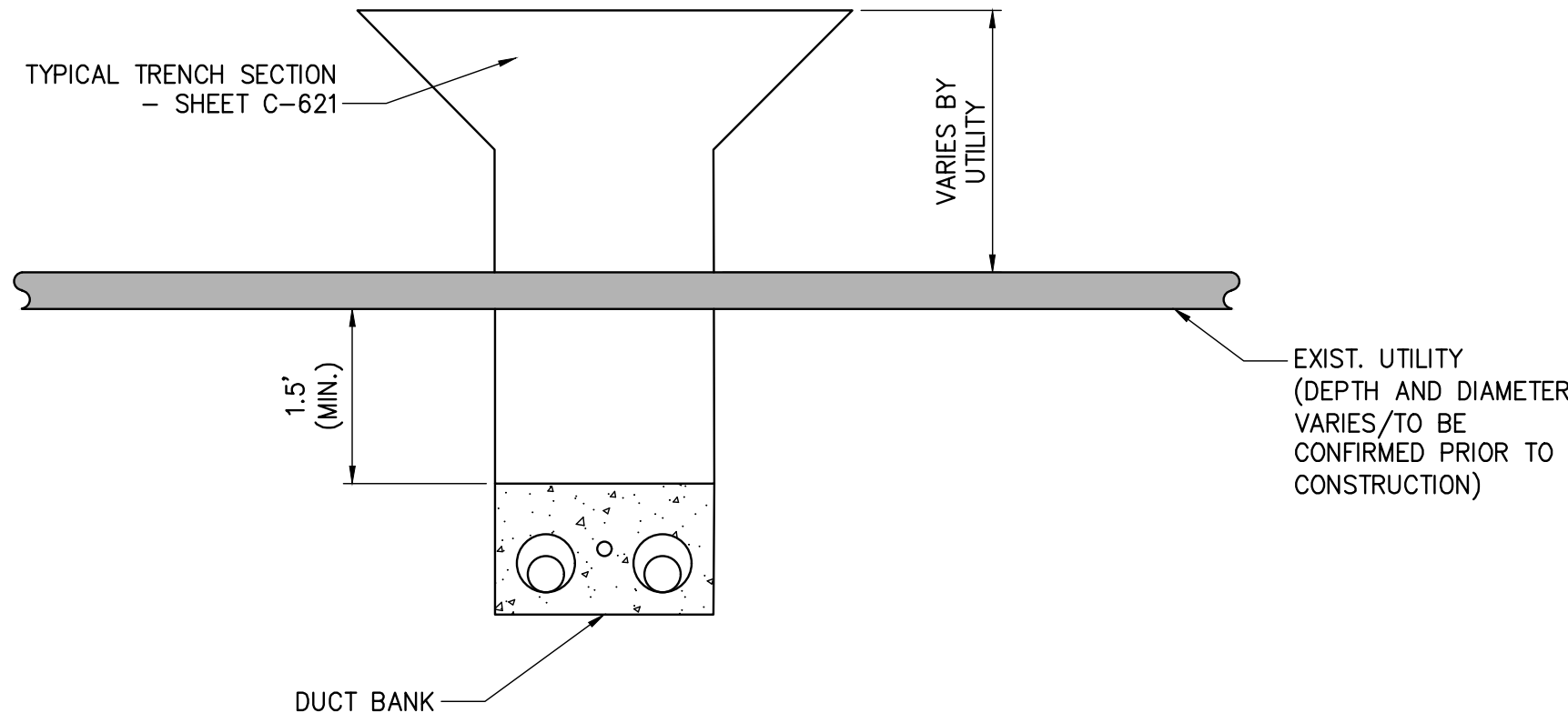
DRAWN BY:	SC	DESIGNED BY:	MK	APPROVED BY:	MS	SCALE	AS SHOWN	DATE	07/31/2023
						REV. NO.	0	SH.NO.	OF

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-631
DATE	07/31/2023
SH.NO.	OF

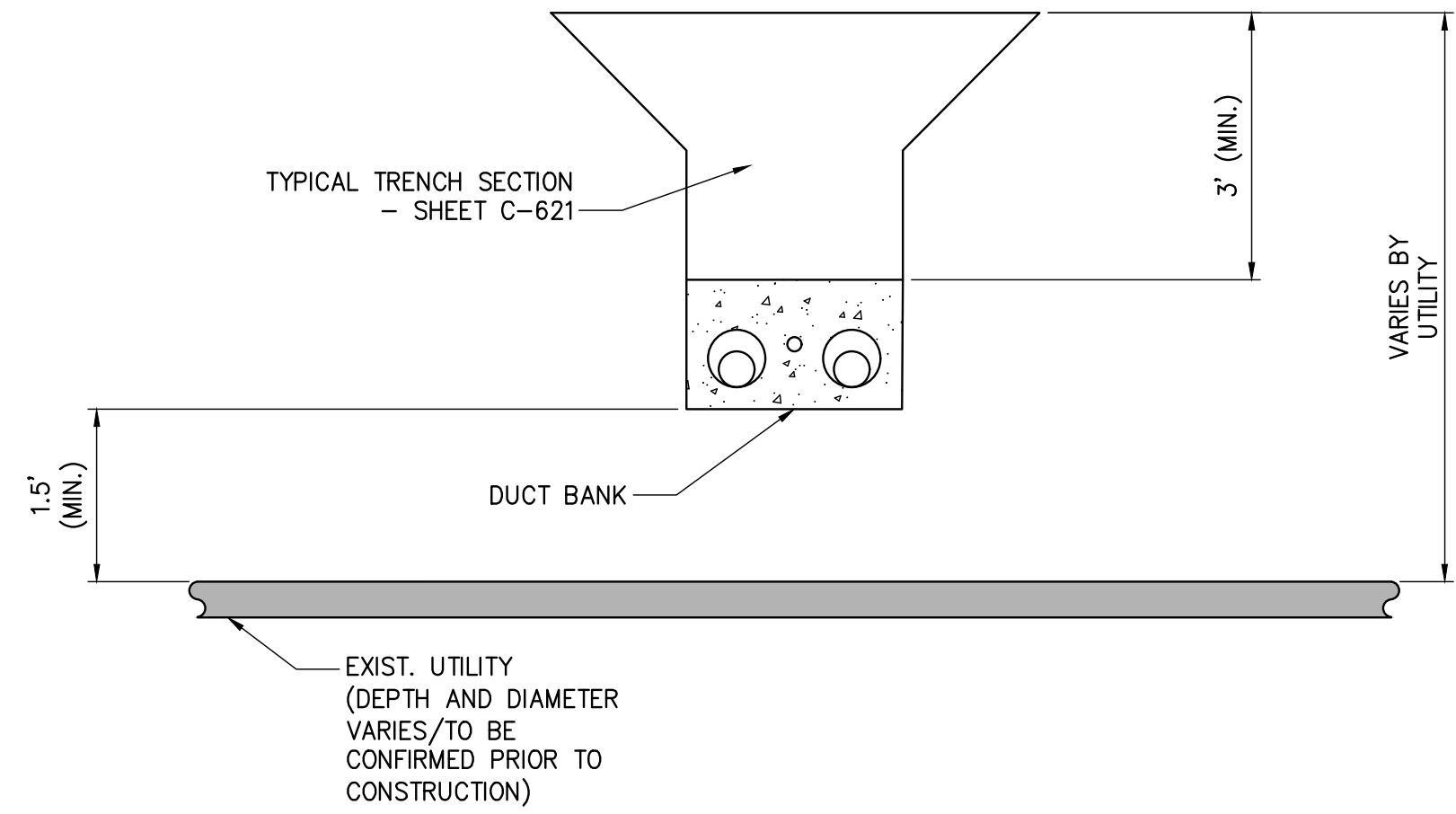
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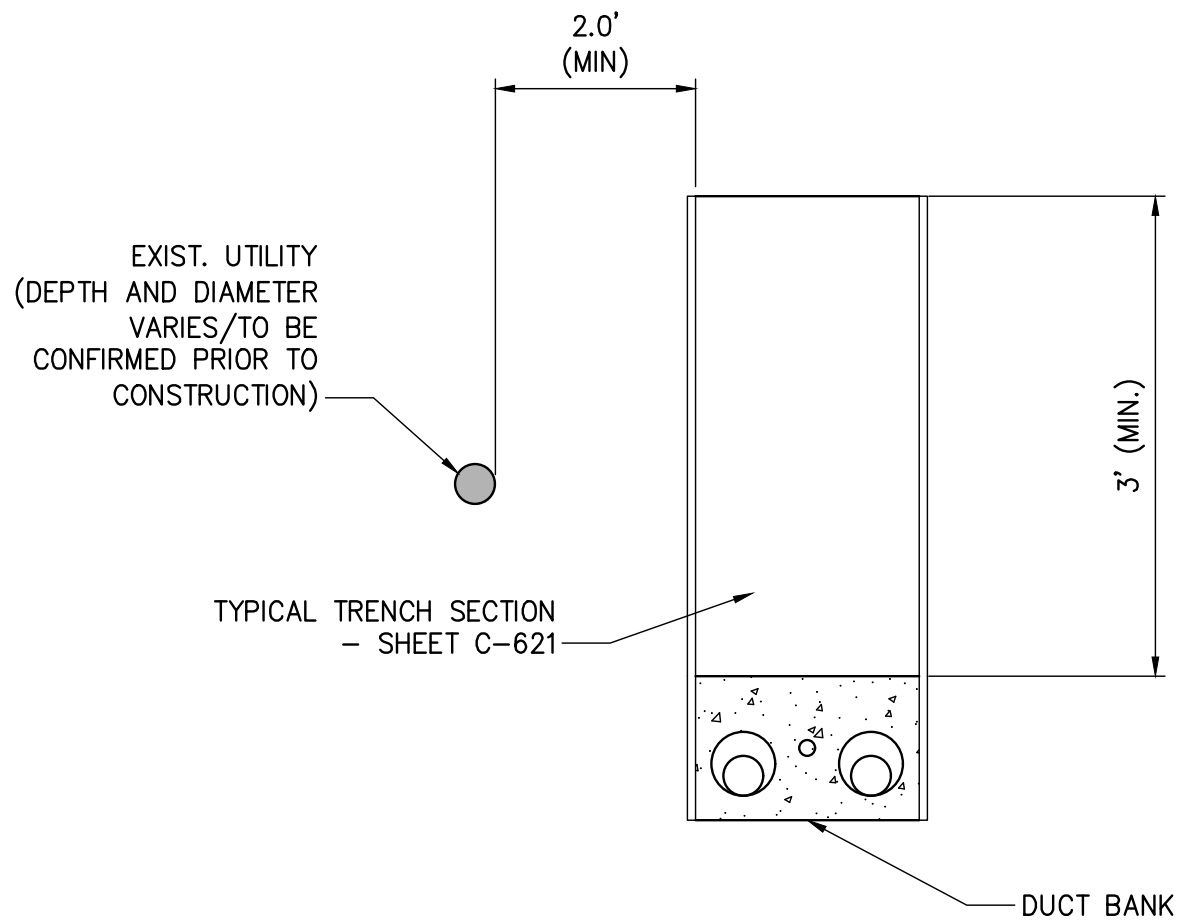
1 OPEN TRENCH - HORIZONTAL SEPARATION
NOT TO SCALE



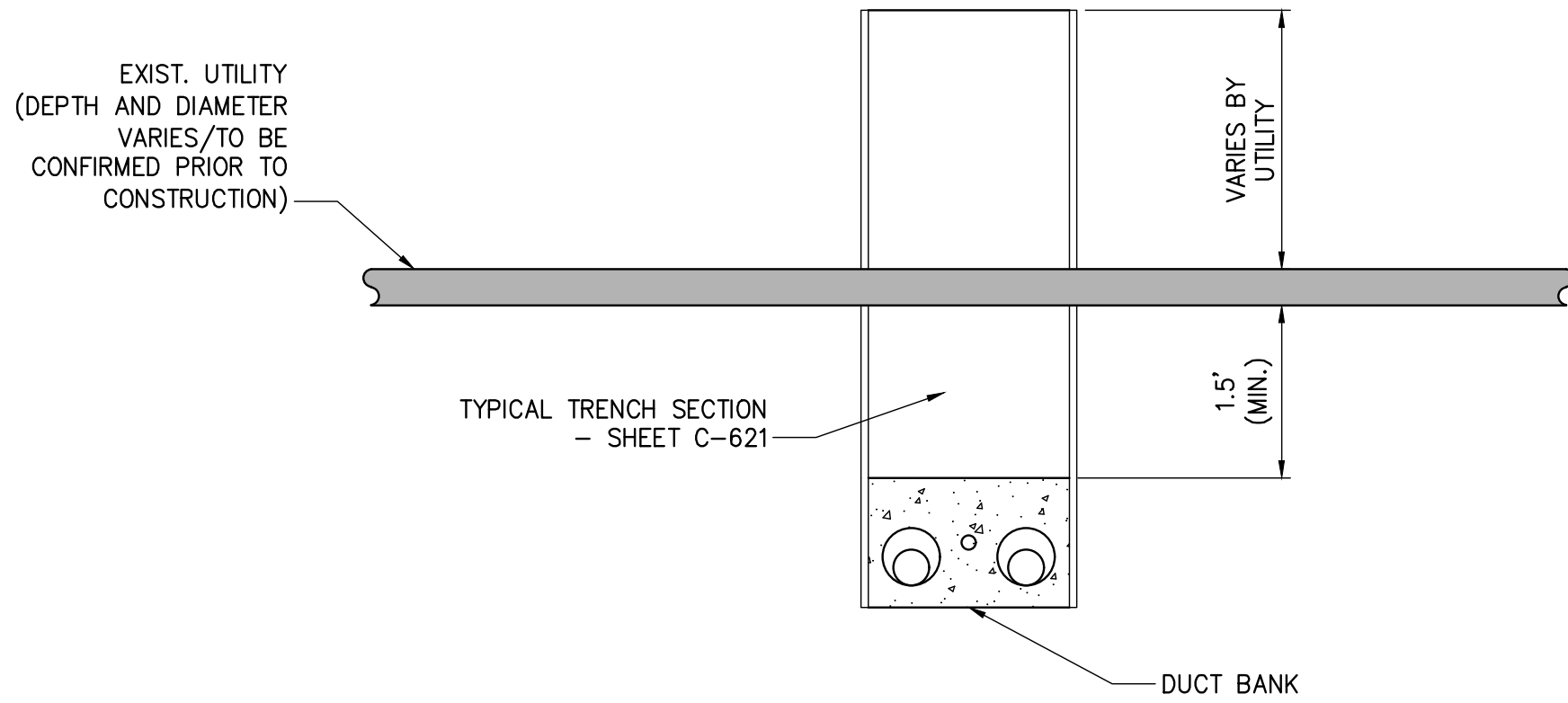
3 OPEN TRENCH - VERTICAL SEPARATION (UNDER)
NOT TO SCALE



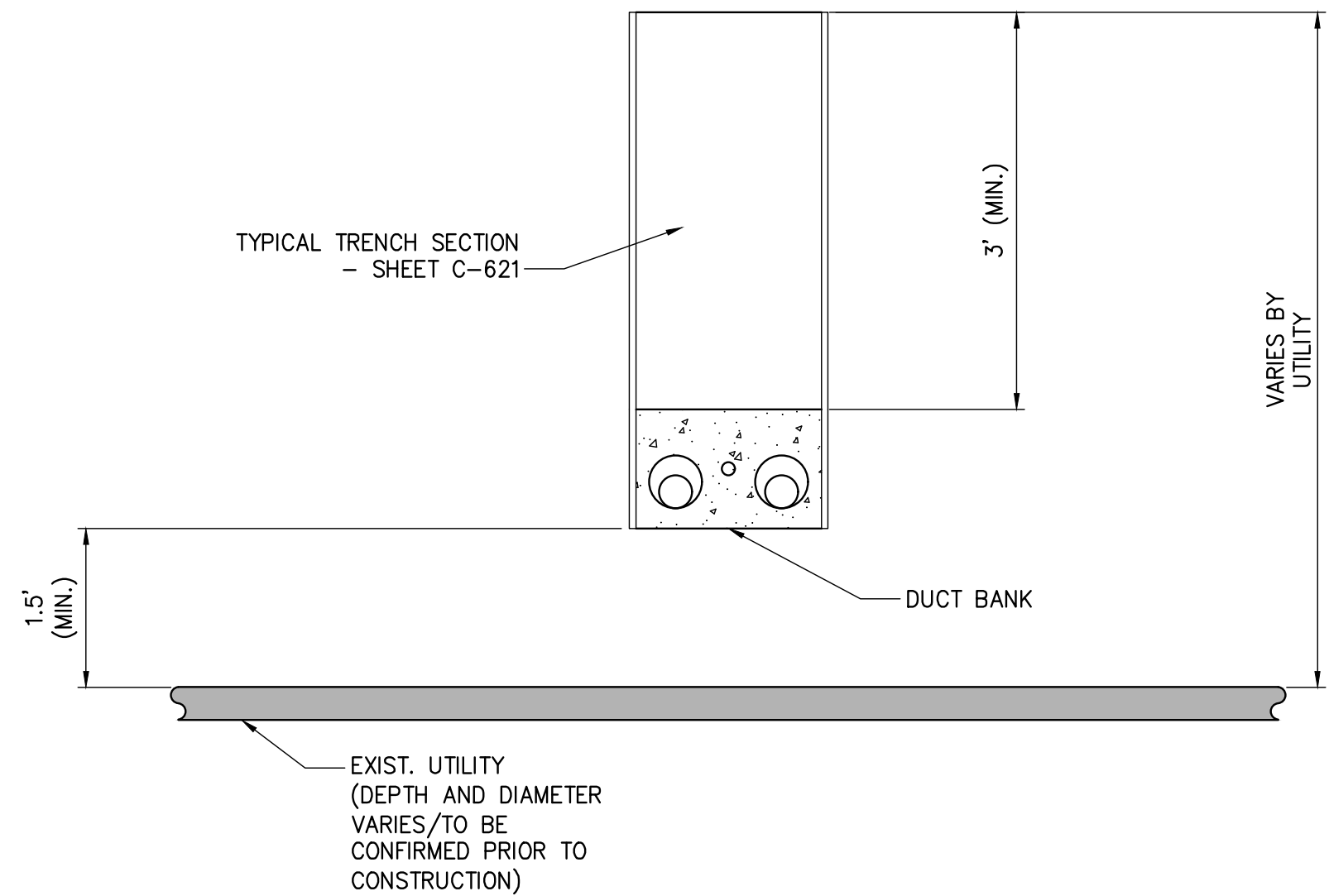
5 OPEN TRENCH - VERTICAL SEPARATION (OVER)
NOT TO SCALE



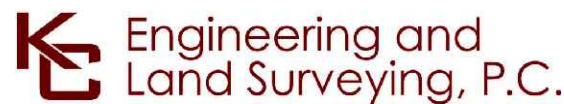
2 TYPICAL DUCTBANK IN ROADWAY - HORIZONTAL SEPARATION
NOT TO SCALE



4 TYPICAL DUCTBANK IN ROADWAY - VERTICAL SEPARATION (UNDER)
NOT TO SCALE



6 TYPICAL DUCTBANK IN ROADWAY - VERTICAL SEPARATION (OVER)
NOT TO SCALE



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION		DB APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 13 TO 15 - PACKAGE 8
TRANSITION VAULT 5 TO ASTORIA CONVERTER STATION
UTILITY TYPICAL SEPARATION DETAILS

KIEWIT PROJECT NO.		21162
KC PROJECT NO.		120174
DRAWING NO.		C-901
DATE	07/31/2023	
SH.NO.	OF	

DRAWN BY:	DESIGNED BY:	APPROVED BY: MW	SCALE	AS NOTED	DATE	07/31/2023
			REV. NO.		SH.NO.	OF

APPENDIX K – FEMA FIRM MAPS