







IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY
ARE ACTING UNDER THE DIRECTION OF A LICENSED
PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT
OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN
ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS
ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE
ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT
AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY
THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A
SPECIFIC DESCRIPTION OF THE ALTERATION.

04/05/2022	FINAL FARCE CLIPATICCION	115	IDD		
				SEDIMENT CONTROL PLAN	
				STA. 31380+00 TO STA. 31384+30 EROSION AND	
				CTA 24200,00 TO CTA 24204,20 EDOCION AND	
				SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON	
				CHAMPLAIN HUDSON POWER EXPRESS	

DATE SUBMITTAL / REVISION DESCRIPTION

DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

KIEWIT PROJECT NO.					
21162					
CHA PROJECT NO.					
066076					
DRAWING NO.					
CAA7					

C-44/ AS NOTED DATE

X SH.NO.

- 2. ALL TEMPORARY TRAFFIC CONTROL AND WORK AREA PROTECTION DEVICES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED ON THE CONTRACT DRAWINGS AND APPROVED BY THE ENGINEER PRIOR TO COMMENCING WORK AND WILL BE INCLUDED IN THIS EM&CP.
- 3. ALL MAINTENANCE AND PROTECTIONS OF TRAFFIC WORK SHALL CONFORM TO THE CONTRACT DRAWINGS. MAINTENANCE AND PROTECTION OF TRAFFIC SCHEMES SHOWN ON THE CONTRACT DRAWINGS SHALL NOT BE CHANGED BY THE CONTRACTOR WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER AND WILL BE INCLUDED IN THIS EM&CP.
- 4. THE TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION DEVICE LOCATIONS SHOWN ON THE CONTRACT DRAWINGS ARE SCHEMATIC EXCEPT AS NOTED. LOCATION OF TRAFFIC CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- 5. ALL CLOSURES SHALL BE COORDINATED WITH THE ENGINEER. A SCHEDULE OF WORK SHALL BE SUBMITTED AT LEAST TWO WEEKS IN ADVANCE FOR APPROVAL BY THE ENGINEER.
- 6. THERE MAY BE ONGOING CONSTRUCTION CONTRACTS WITHIN THE VICINITY OF THE WORK AREA. DO NOT MOVE, MODIFY, OR RELOCATE ANY ITEM ASSOCIATED WITH THESE CONTRACTS WITHOUT PROPER APPROVAL OF AND COORDINATION WITH THE ENGINEER.
- 7. PERFORM WORK IN SUCH A MANNER AND SEQUENCE AS TO INTERFERE AS LITTLE AS POSSIBLE WITH THE PASSAGE OF VEHICLES. PEDESTRIANS, AND OTHER KINDS OF PUBLIC TRAFFIC.
- 8. ALL TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION SIGN SUPPORTS AND MOUNTING SHALL BE IN CONFORMANCE WITH NYSDOT STANDARD SHEETS AND STANDARD SPECIFICATIONS.
- ALL CONSTRUCTION SIGNS SHALL BE COVERED WITH THICK PLASTIC WHEN THE WORK THEY ARE INTENDED FOR IS NOT IN PROGRESS.
- 10. ALL TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF THE WORK TO WHICH THEY APPLY, AND SHALL BE MAINTAINED BY THE CONTRACTOR THEREAFTER. THE DEVICES SHALL REMAIN IN PLACE AS LONG AS THEY ARE APPLICABLE.
- 11. TRAFFIC CONTROL DEVICES SHALL NOT BE PLACED AT ANY LOCATIONS WHERE THEY MAY OBSCURE OR INTERFERE WITH THE MOTORIST VIEW OF APPROACHING, MERGING, OR INTERSECTING TRAFFIC: OBSTRUCT OTHER TEMPORARY OR PERMANENT TRAFFIC CONTROL DEVICES WHICH ARE STILL APPLICABLE TO ROADWAY CONDITIONS; MISLEAD OR MISDIRECT MOTORISTS, OR ARE BLOCKED BY OTHER TEMPORARY OR PERMANENT OBJECTS.
- 12. ALL EXISTING ROADWAY ITEMS SUCH AS GUIDE RAILS, PAVEMENT MARKINGS, CURBS, SIGNALS AND SIGNS DAMAGED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.
- 13. TRAFFIC LANES AND OTHER AREAS CLOSED BY THE CONTRACTOR DURING PERMITTED WORK HOURS SHALL BE CLEARED OF ALL MATERIAL, EQUIPMENT, AND DEBRIS, AND SAFELY REOPENED TO TRAFFIC BY THE END OF THE WORK PERIOD UNLESS OTHERWISE INDICATED ON THE CONTRACT DRAWINGS.
- 14. IF THE CONTRACTOR REQUIRES A LANE CLOSURE TO PERFORM OPERATIONS, THEY MAY DO SO WITH THE APPROVAL OF THE ENGINEER. ANY LANE CLOSURES WILL BE SHORT TERM AND IN ACCORDANCE WITH NYS STANDARD SHEETS UNLESS OTHERWISE SHOWN IN CONTRACT PLANS.
- 15. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.
- 16. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
- 17. WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK IS 2 MILES TO 5 MILES, A SUPPLEMENTAL DISTANCE PLAQUE (W7-3a) SHOULD BE USED WITH THE SHOULDER WORK SIGN (W21-5).
- 18. THE ROAD WORK NEXT XX MILES SIGN (G20-1) MAY BE USED INSTEAD OF THE ROAD WORK AHEAD SIGN (W20-1) IF WORK LOCATIONS OCCUR OVER A DISTANCE OF MORE THAN 2 MILES.
- 19. ALL ADDITIONAL TEMPORARY SIGNAGE, NOT COVERED IN FHWA MUTCD, SHALL COMPLY WITH NYS SUPPLEMENT TO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

- 20. DURING NON-WORK HOURS, LEAVE DRUMS AND TCB IN PLACE, OR AS DIRECTED BY THE ENGINEER.
- 21. DURING WINTER MORATORIUM, COVER EXCAVATION WITH TEMPORARY ASPHALT AND OPEN ALL LANES TO TRAFFIC.
- 22. THE WZTC DETAILS CONTAINED IN THE CONTRACT PLANS SUPPLEMENT THE CURRENT NYSDOT STANDARD SHEETS. REFERENCE SHALL BE MADE TO THE APPLICABLE NYSDOT STANDARD SHEETS FOR ALL NOTES AND TABLES. THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYSDOT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
- 23. MAINTAIN SAFE AND ADEQUATE ACCESS FOR INTERSECTING ROADWAYS, HOMES, AND BUSINESSES, AT ALL TIMES, TO THE SATISFACTION OF THE ENGINEER.
- 24. ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES. UNLESS OTHER AGREEMENTS SUITABLE TO THE PROPERTY OWNERS CAN BE MADE, PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ANY ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE, OR AS DIRECTED BY THE ENGINEER.
- 25. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING HOLIDAY WORK RESTRICTIONS ARE APPLICABLE TO THIS PROJECT. DURING THESE HOLIDAY PERIODS, THE CONTRACTOR WILL NOT BE ALLOWED TO PERFORM ANY WORK THAT WILL BE DISRUPTIVE TO TRAFFIC, INCLUDING BUT NOT LIMITED TO LANE CLOSURES. LANE CLOSURES WILL NOT BE PERMITTED DURING THE FOLLOWING STATE RECOGNIZED HOLIDAYS:

1) MEMORIAL DAY 3) LABOR DAY 5) CHRISTMAS DAY

- MONDAY, MAY 29, 2023 2) INDEPENDENCE DAY - TUESDAY, JULY 4, 2023 MONDAY, SEPTEMBER 4, 2023 4) THANKSGIVING DAY - THURSDAY, NOVEMBER 23, 2023 - MONDAY, DECEMBER 25, 2023

6) NEW YEAR'S DAY 7) MEMORIAL DAY 9) LABOR DAY

MONDAY, MAY 27, 2024 8) INDEPENDENCE DAY - THURSDAY, JULY 4, 2024 MONDAY, SEPTEMBER 2, 2024 10) THANKSGIVING DAY - THURSDAY, NOVEMBER 28, 2024 11) CHRISTMAS DAY - WEDNESDAY, DECEMBER 25, 2024

MONDAY. JANUARY 1, 2024

12) NEW YEAR'S DAY - WEDNESDAY, JANUARY 1, 2025 13) MEMORIAL DAY - MONDAY, MAY 26, 2025 14) INDEPENDENCE DAY - FRIDAY, JULY 4, 2025 15) LABOR DAY - MONDAY, SEPTEMBER 1, 2025

16) THANKSGIVING DAY - THURSDAY, NOVEMBER 27, 2025 17) CHRISTMAS DAY - THURSDAY, DECEMBER 25, 2025

26. LANE CLOSURES SHALL BE SUSPENDED AS FOLLOWS:

- BEGINNING 6AM, FRIDAY MAY 26, 2023 AND ENDING 6AM, TUESDAY MAY 30. 2023 - BEGINNING 6AM, SATURDAY JULY 1, 2023 AND ENDING 6AM, WEDNESDAY JULY 5, 2023 - BEGINNING 6AM, FRIDAY SEPTEMBER 1, 2023 AND ENDING 6AM, TUESDAY SEPTEMBER 5, 2023

- BEGINNING 6AM, WEDNESDAY NOVEMBER 22, 2023 AND ENDING 6AM, MONDAY NOVEMBER 27, 2023 - BEGINNING 6AM. FRIDAY DECEMBER 22, 2023 AND ENDING 6AM, TUESDAY DECEMBER 26, 2023

- BEGINNING 6AM, SUNDAY DECEMBER 30, 2023 AND ENDING 6AM, WEDNESDAY JANUARY 3, 2024 - BEGINNING 6AM, FRIDAY MAY 24, 2024 AND ENDING 6AM, TUESDAY MAY 28, 2024

- BEGINNING 6AM, MONDAY JULY 1, 2024 AND ENDING 6AM, FRIDAY JULY 5, 2024 - BEGINNING 6AM, FRIDAY AUGUST 30, 2024 AND ENDING 6AM, TUESDAY SEPTEMBER 3, 2024

- BEGINNING 6AM, WEDNESDAY NOVEMBER 27, 2024 AND ENDING 6AM, MONDAY DECEMBER 2, 2024 - BEGINNING 6AM, FRIDAY DECEMBER 20, 2024 AND ENDING 6AM, THURSDAY DECEMBER 26, 2024

- BEGINNING 6AM. TUESDAY DECEMBER 30. 2024 AND ENDING 6AM. FRIDAY JANUARY 3. 2025 - BEGINNING 6AM, FRIDAY MAY 23, 2025 AND ENDING 6AM, TUESDAY MAY 27, 2025

- BEGINNING 6AM, THURSDAY JULY 3, 2025 AND ENDING 6AM, MONDAY JULY 7, 2025 - BEGINNING 6AM, FRIDAY AUGUST 29, 2025 AND ENDING 6AM, TUESDAY SEPTEMBER 2, 2025

- BEGINNING 6AM, WEDNESDAY NOVEMBER 26, 2025 AND ENDING 6AM, MONDAY DECEMBER 1, 2025 - BEGINNING 6AM, WEDNESDAY DECEMBER 24, 2025 AND ENDING 6AM, MONDAY DECEMBER 29, 2025

- 27. DURING WINTER STORM EVENTS. NO WORK WITHIN THE WORK AREA OR RIGHT-OF-WAY IS PERMITTED TO ALLOW FOR SNOW REMOVAL, PLOWING ACTIVITIES.
- 28. ANY EARTHWORK DONE BETWEEN NOVEMBER 1ST AND APRIL 1ST MUST CONFORM TO NYSDOT STANDARD SPECIFICATION SECTION 203-1.01 P.
- 29. ALL UNPROTECTED OPEN TRENCH EXCAVATIONS SHALL BE BACKFILLED OR COVERED BY A STEEL PLATE (HS-20 LOAD RATED) AT THE END OF EACH WORK DAY, OR AS DIRECTED BY THE ENGINEER.
- 30. PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR SHALL COORDINATE WITH CP RAILROAD FOR RAILROAD FLAGGING FOR ALL WORK IN THE VICINITY OF ANY RAILROAD GRADE CROSSING AND WHENEVER WITHIN THE RAILROAD ROW.

LEGEND

TEMPORARY SIGN

TEMPORARY TRAFFIC SIGNAL UNIT

WORK ZONE

TEMPORARY CONCRETE BARRIER (TCB)

WORK VEHICLE WITH TRUCK

FLAGGER

MOUNTED ATTENUATOR

FLASH UNIT (LEFT ARROW)

TYPE III BARRICADE

FLASH UNIT (CAUTION MODE)

TRAILER UNIT

WARNING LIGHT ON SIGN

TEMPORARY TRAFFIC FLOW ARROW

ABBREVIATIONS

AVERAGE ANNUAL DAILY TRAFFIC

COUNTY ROUTE

DIRECTIONAL HOURLY VOLUME

MILE POST

FLASHING ARROW SIGN UNIT

FEDERAL HIGHWAY ADMINISTRATION

MAX. MAXIMUM

> MIN. MINIMUM

MILES PER HOUR

MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES

NYS NEW YORK STATE

NEW YORK STATE DEPARTMENT OF TRANSPORTATION

RTE ROUTE

TEMPORARY CONCRETE BARRIER

STA. STATION

Champlain Hudson **Power Express**





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0	04/05/2023	FINAL EM&CP SUBMISSION	МН	JS	_	LEGEND AN	DADDNEVIAT	IONS
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV. NO.

CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON

WORK ZONE TRAFFIC CONTROL NOTES LEGEND AND ABBREVIATIONS

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

04/05/2023

OF XXX

SH.NO.

C-501

WORK NOTES

TRENCHING & CONDUIT INSTALLATION

SPLICE BOX INSTALLATION

SPLICE BOX INSTALLATION

HORIZONTAL DIRECTIONAL DRILL

TRENCHING & CONDUIT INSTALLATION

SPLICE BOX INSTALLATION

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HORIZONTAL DIRECTIONAL DRILL

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SPLICE BOX INSTALLATION

RENCHING & CONDUIT INSTALLATION

RENCHING & CONDUIT INSTALLATION

PLAN SHEET

C-506

C-506

C-506

C-506

C-506

C-508, C-509

C-508, C-509

C-508, C-509

C-503

CHPE	
Champlain Hudson Power Express	

MAIN STAGE

STA. START

30019+60

30052+05

30058+85

30068+30

A-P3 0+00

A-P3 18+35

A-P3 32+95

A-P3 35+00

A-P3 35+95

A-P3 36+15

A-P3 39+00

A-P3 42+85

A-P3 43+10

A-P3 47+30

Δ-P3 47+75

A-P3 53+70

A-P3 54+00

A-P3 73+80

A-P3 76+00

A-P3 77+00

A-P3 84+15

A-P3 92+45

A-P3 97+60

30181+80

30204+40

30208+05

30213+90

30221+50

30237+85

30240+55

30261+00

30268+90

30280+70

30291+60

30301+20

30313+00

30313+39

30318+70

30325+1

30327+90

30355+00

30358+40

30379+25

30379+60

30381+40

30392+45

30411+60

30413+55

30425+60

30451+70

30452+30

30455+15

30473+05

30473+30

30516+55

30542+15

30545+45

30556+15

30574+40

30595+40

30595+7

30606+80

30619+15

30627+90

30630+50

30632+80

30639+20

30651+05

30651+40

30654+05

30344+70

STA. END

30058+85

30083+50

A-P3 20+75

A-P3 32+95

A-P3 35+00

A-P3 36+15

A-P3 42+85

A-P3 43+10

A-P3 47+30

A-P3 47+75

A-P3 53+70

A-P3 54+00

A-P3 73+80

A-P3 76+00

A-P3 84+15

A-P3 97+60

30204+40

30213+90

30240+55

30261+00

30280+70

30291+60

30313+00

30318+70

30325+15

30344+70

30355+00

30379+25

30392+45

30451+70

30473+05

30545+45

30556+15

30595+40

30619+15

30632+80

30639+20

30651+05

30651+40

30656+60

ROUTE(S)

CP RAIL R.O.W.

CP RAIL R.O.W. / EAST S

CP RAIL R.O.W.

P RAIL R.O.W. / CENTER

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W. / NYS RTE 4 (BROADWAY)

CP RAIL R.O.W.

CP RAIL R.O.W. / ROGERS ISLAND DR

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W. / MILL SITE RD

CP RAIL R.O.W. / MILL SITE RD

CP RAIL R.O.W.

CP RAIL R.O.W. / GURN SPRINGS RD

CP RAIL R.O.W.

CP RAIL.R.O.W. / PETTIS RD

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W. / CR 33 (BALLARD RD)

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W.

CP RAIL R.O.W. / SCOUT RD

CP RAIL R.O.W.

CP RAIL R.O.W.

P RAIL / NYS RTE 32 (SCHUYLERVILLE RD)

P RAIL R.O.W. / MOTT ROAD

RAIL R.O.W. / CLARK ROAD

P RAIL R.O.W. / CR 29 (W RIVER RD)

CP RAIL R.O.W. / CANAL





SEGMENTS 4 & 5 - PACKAGE 3 WORK ZONE TRAFFIC CONTROL MAIN STAGES

CLOSURE TYPE

WORK BEYOND SHOULDER

CLOSURE AT THE END OF CENTER STREET

WORK BEYOND SHOULDER

WORK BEYOND SHOULDER

WORK BEYOND SHOULDER

ROAD CLOSURE

ROAD CLOSURE

DETOUR/ TEMPORARY TRAFFIC SIGNAL

LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC

WORK BEYOND SHOULDER

LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC

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No.	DATE	SUBMITTAL / REVISION DESCRIPTION
0	04/05/2023	FINAL EM&CP SUBMISSION

CHAMPLAIN HUDSON POWER EXPRESS

066076 DRAWING NO.

|PLAN SHEET|

C-503

C-506

C-506

C-505

C-506

C-506

C-506

C-506

C-504

WORK NOTES

HORIZONTAL DIRECTIONAL DRILL FRENCHING & CONDUIT INSTALLATION

SPLICE BOX INSTALLATION

SPLICE BOX INSTALLATION

TRENCHING & CONDUIT INSTALLATION

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FRENCHING & CONDUIT INSTALLATION

SPLICE BOX INSTALLATION

HORIZONTAL DIRECTIONAL DRILL

HORIZONTAL DIRECTIONAL DRILL

TRENCHING & CONDUIT INSTALLATION

C-502

0	04/05/2023	FINAL EM&CP SUBMISSION	МН	JS		
					WORK ZONE TRAFFIC CONTROL NOTES	
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STA. START MAIN STAGE STA. END 30656+60 30675+20 CP RAIL R.O.W. 30675+20 30716+55 CP RAIL R.O.W. 30679+65 CP RAIL R.O.W. 30715+15 CP RAIL R.O.W. 30717+00 30716+55 CP RAIL R.O.W. / EDIE I 30717+00 30719+80 CP RAIL R.O.W. 30745+35 CP RAIL R.O.W. 30745+35 30796+25 CP RAIL R.O.W. CP RAIL R.O.W. 30748+15 CP RAIL R.O.W. 30796+25 CP RAIL R.O.W / JONES RD 30802+55 30839+30 CP RAIL R.O.W. CP RAIL R.O.W. 30804+65 30829+70 CP RAIL R.O.W. 30839+30 CP RAIL R.O.W. / I-8 30847+05 30870+80 CP RAIL R.O.W. CP RAIL R.O.W. 30849+60 30871+20 CP RAIL R.O.W / JONES RD 30870+80 30871+20 30895+50 CP RAIL R.O.W. 30880+60 CP RAIL R.O.W. CP RAIL R.O.W. 30895+50 CP RAIL R.O.W. 30914+40 30910+60 30911+65 CP RAIL R.O.W. CP RAIL R.O.W. / NYS RTE 9 (MAPLE AVE: 30914+40 30920+35 30920+35 30989+20 CP RAIL R.O.W. CP RAIL R.O.W. CP RAIL R.O.W. 30966+95 CP RAIL R.O.W / CLINTON S 30989+20 30989+45 30989+45 31000+50 CP RAIL R.O.W. CP RAIL R.O.W. 30998+80 31014+65 CP RAIL R.O.W. 31000+50 CP RAIL R.O.W. 31014+65 31026+05 31023+45 CP RAIL R.O.W. 31026+05 31026+75 DENTON RD/BLOOMFIELD S 31026+75 31043+10 CP RAIL R.O.W. CP RAIL R.O.W. 31043+10 31050+20 31059+75 CP RAIL R.O.W. 31050+20 31055+55 CP RAIL R.O.W. P RAIL R.O.W. / NYS RTE 9N (CHURCH S 31059+75 31064+85 31080+90 CP RAIL R.O.W. CP RAIL R.O.W. 31066+85 31080+90 CP RAIL / NYS RTE 29 (WASHINGTON ST 31091+40 31100+35 CP RAIL R.O.W. CP RAIL R.O.W. 31094+60 CP RAIL R.O.W. / GRAND AVE 31100+35 31106+10 31156+80 CP RAIL R.O.W. 31106+10 CP RAIL R.O.W. 31115+55 CP RAIL R.O.W. 31163+10 CP RAIL R.O.W. / CR 43 (GEYSER RD) 31156+80 CP RAIL R.O.W. 31163+10 31176+50 31174+45 CP RAIL R.O.W. CP RAIL R.O.W. 31176+50 CP RAIL R.O.W. 31208+00 31221+35 31210+05 CP RAIL R.O.W. / NYS RTE 50 (BALLSTON AVE) 31227+15 31312+95 CP RAIL R.O.W. CP RAIL R.O.W. 31240+05 31255+00 CP RAIL R.O.W. CP RAIL R.O.W. 31282+90 CP RAIL R.O.W. 31310+75 CP RAIL R.O.W. / CR 45 (NORTHLINE RD) 31312+95 31313+30 313340+20 CP RAIL R.O.W. CP RAIL R.O.W. CP RAIL R.O.W. / CR 63 (MALTA AVE) 31340+20 31359+50 31359+50 31369+20 CP RAIL R.O.W. 31366+45 CP RAIL R.O.W. 31369+20 31376+40 CP RAIL R.O.W. 31384+30 CP RAIL R.O.W. 31383+20

TRENCHING & CONDUIT INSTALLATION CP RAIL R.O.W. SPLICE BOX INSTALLATION

SEGMENTS 4 & 5 - PACKAGE 3 WORK ZONE TRAFFIC CONTROL MAIN STAGES

CLOSURE TYPE

LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC

WORK BEYOND SHOULDER

LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC

WORK BEYOND SHOULDER

LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC

ANE CLOSURE WITH ALTERNATING ONE-WAY TRAFFIC WITH TEMPORARY CONCRETE BARRIER

LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC AT OR NEAR INTERSECTION

WORK BEYOND SHOULDER

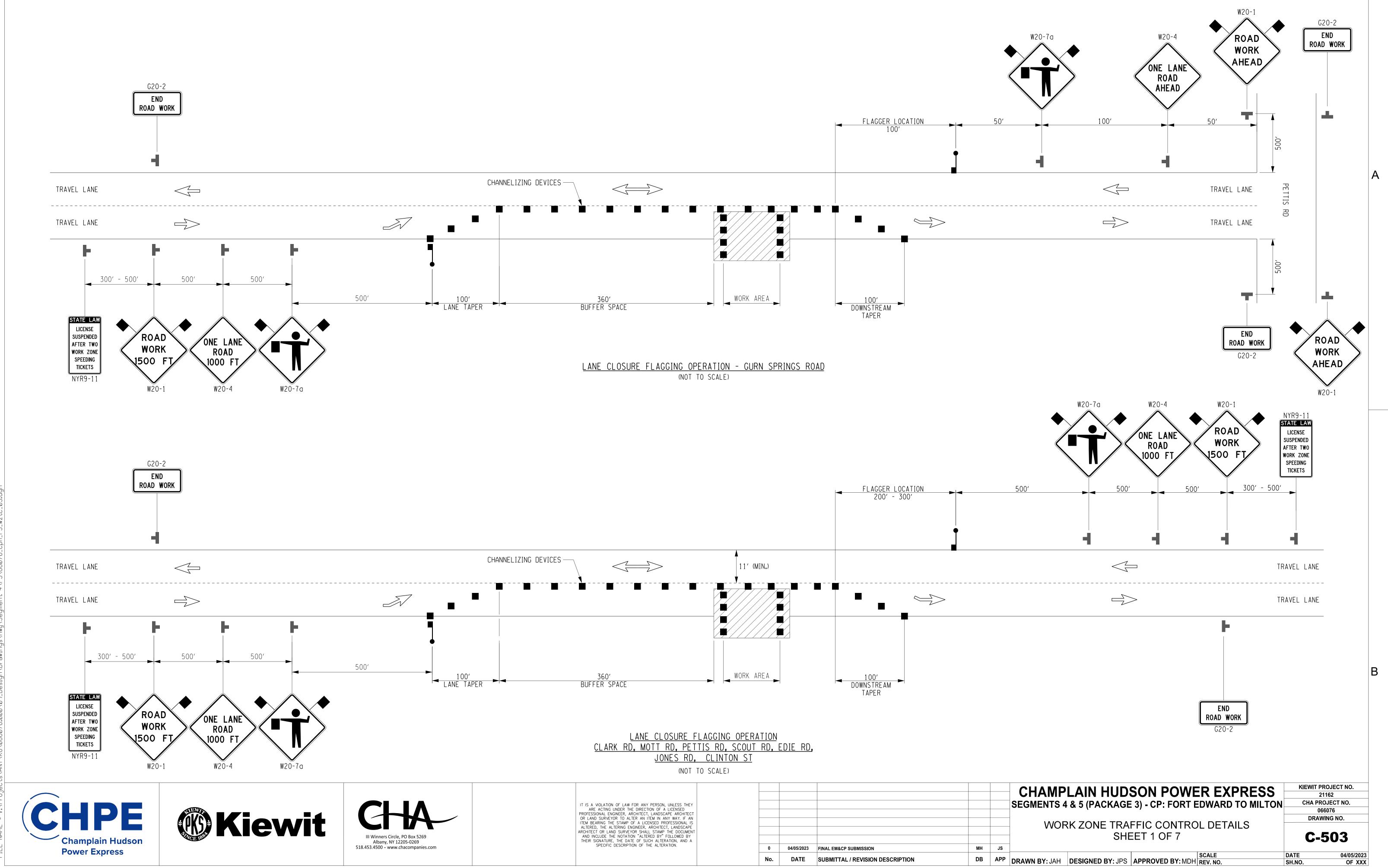
LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC

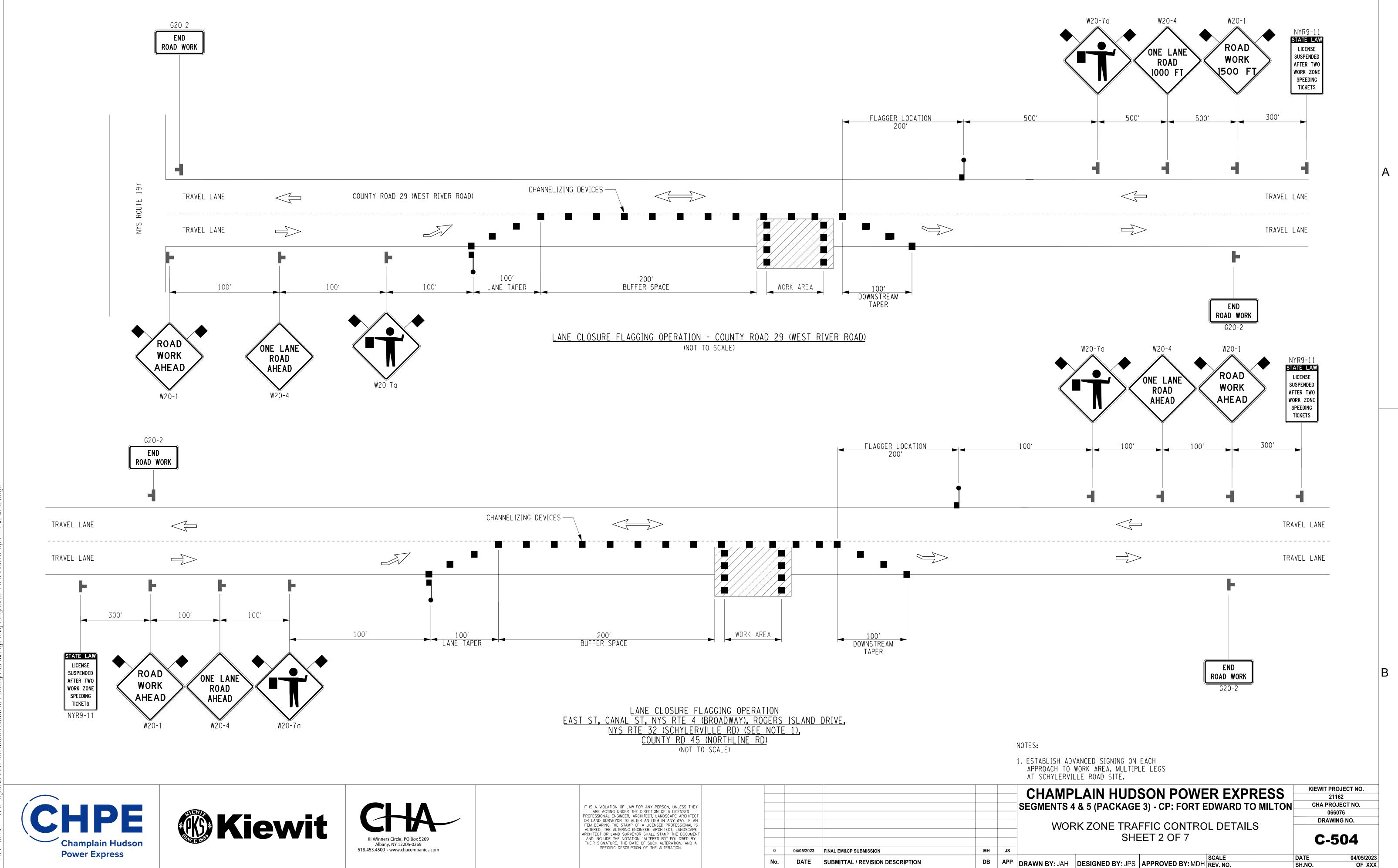
WORK BEYOND SHOULDER

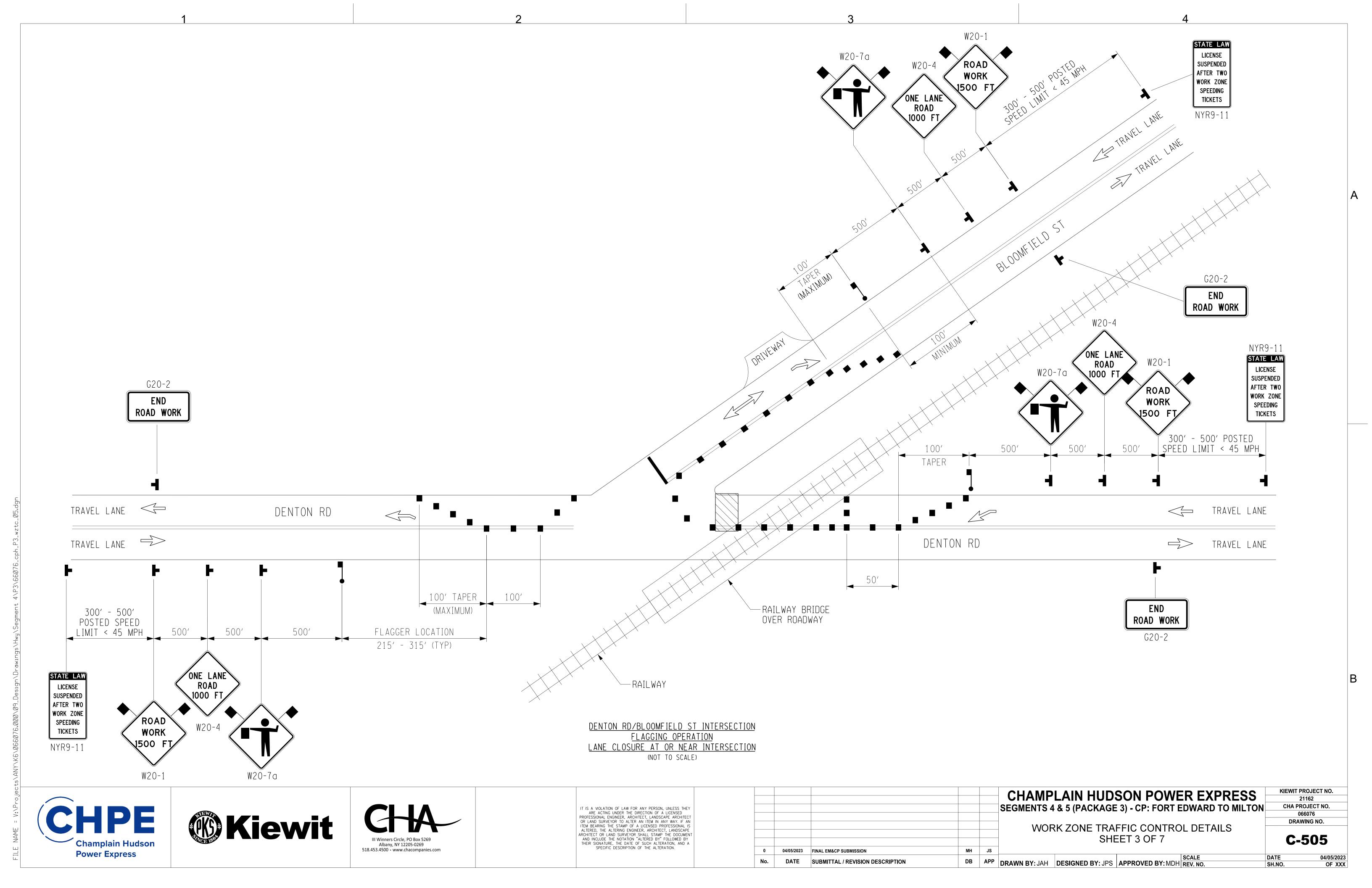
KIEWIT PROJECT NO. 21162 CHA PROJECT NO.

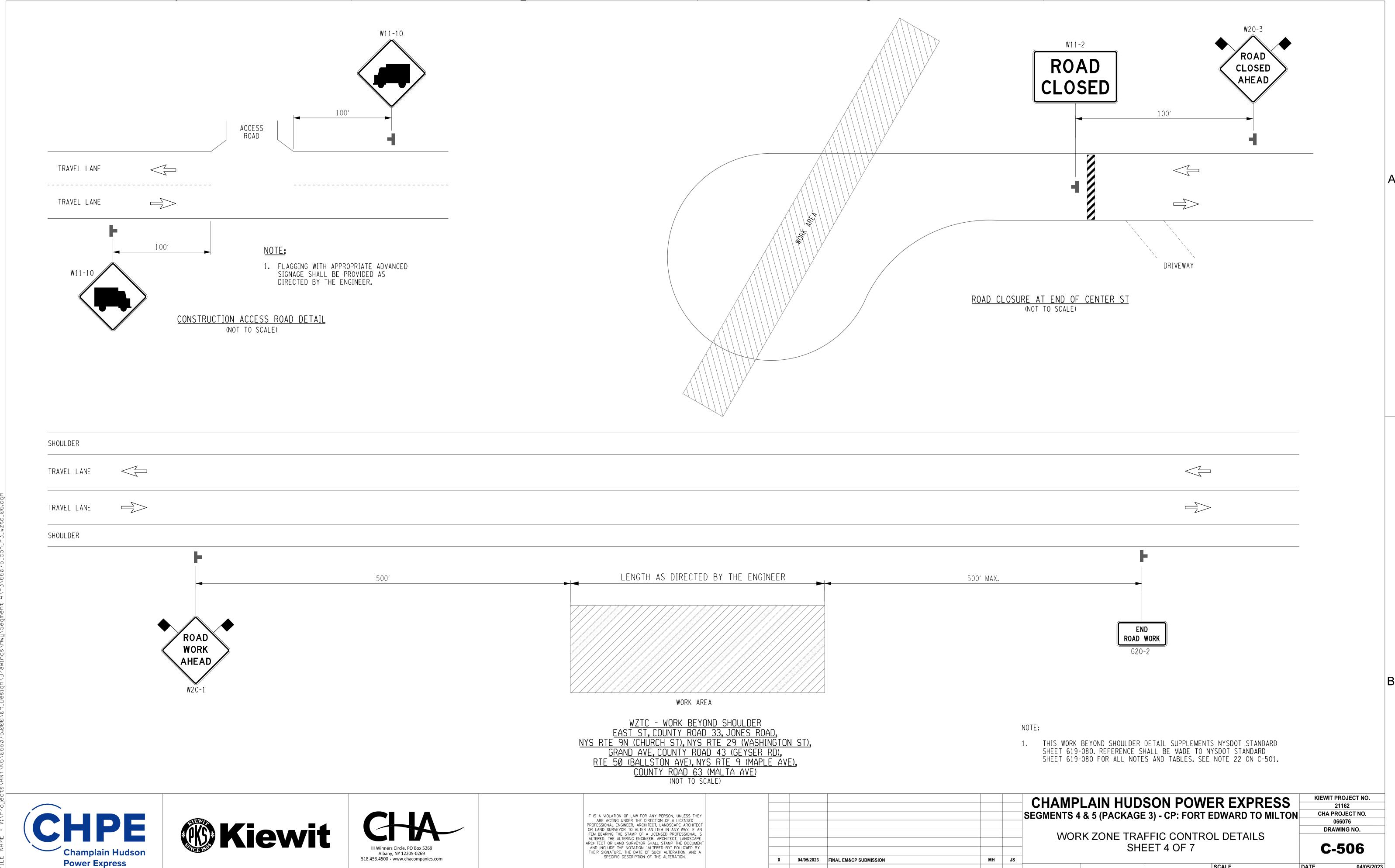
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DB | APP | DRAWN BY: JAH | DESIGNED BY: JPS | APPROVED BY: MDH | REV. NO. SH.NO.









DB APP DRAWN BY: JAH DESIGNED BY: JPS APPROVED BY: MDH REV. NO.

OF XXX

DATE SUBMITTAL / REVISION DESCRIPTION

TERMAN TINE TO THE TOTAL TO THE TERMAN TERMAN THE TERMA

G20-2

END ROAD WORK

SHOULDER

SHOULDER

TRAVEL LANE

TRAVEL LANE

DETECTION

500′

ROAD 1000 FT

MAXIMUM

- 1. FOR LONG DURATION LANE CLOSURES, TEMPORARY / INTERIM PAVEMENT MARKINGS SHALL BE USED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.
- 2. THE TEMPORARY CONCRETE BARRIER SHALL NOT BE PLACED ALONG THE MERGING TAPER. THE LANE SHALL BE CLOSED USING CHANNELIZING DEVICES AND PAVEMENT MARKINGS.
- 3. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.
- 4. REFER TO MUTCD FOR ADDITIONAL TRAFFIC SIGNAL REQUIREMENTS.
- 5. DETECTORS SHALL BE INSTALLED ON ALL APPROACH LANES AND ALL PHASES SHALL BE ACTUATED. DURATION OF RED CLEARANCE INTERVALS SHALL BE ADEQUATE TO CLEAR THE ONE-LANE SECTION OF CONFLICTING TRAFFIC.
- 6. THE TRAFFIC SIGNAL CYCLE SHALL REST IN RED. WHEN THE SIGNAL IS CHANGED TO THE FLASHING MODE, EITHER MANUALLY OR AUTOMATICALLY, RED SIGNAL INDICATIONS SHALL BE FLASHED TO ALL APPROACHES.
- 7. STOP LINES SHALL BE INSTALLED WITH TEMPORARY TRAFFIC SIGNALS FOR INTERMEDIATE AND LONG-TERM CLOSURES. EXISTING, CONFLICTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKINGS BETWEEN THE ACTIVITY AREA AND THE STOP LINE SHALL BE REMOVED. AFTER THE TEMPORARY TRAFFIC SIGNAL IS REMOVED, THE STOP LINES AND ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND PERMANENT PAVEMENT MARKINGS RESTORED.
- 8. WHERE NO-PASSING LINES ARE NOT ALREADY IN PLACE. THEY SHALL BE ADDED FOR A DISTANCE OF 500'-600' FROM THE STOP BAR. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.
- 9. ADJUSTMENTS IN THE LOCATION OF ADVANCED WARNING SIGNS SHOULD BE MADE TO ACCOMMODATE THE HORIZONTAL AND VERTICAL ALIGNMENT OF THE ROADWAY, RECOGNIZING THAT THE DISTANCES SHOWN FOR SIGN SPACING ARE MINIMUMS. ADJUSTMENTS IN THE HEIGHT OF THE SIGNAL HEADS SHALL BE MADE AS NEEDED TO CONFORM TO THE VERTICAL ALIGNMENT.

TEMPORARY / INTERIM YELLOW PAVEMENT MARKING

500' - 600' FROM STOP BAR (SEE NOTES 1 AND 8)

STOP BAR PLACEMENT 40' FROM THE

TEMPORARY TRAFFIC

— 24" STOP LINE (SEE NOTE 7)

(SEE NOTE 3)

SIGNAL UNIT

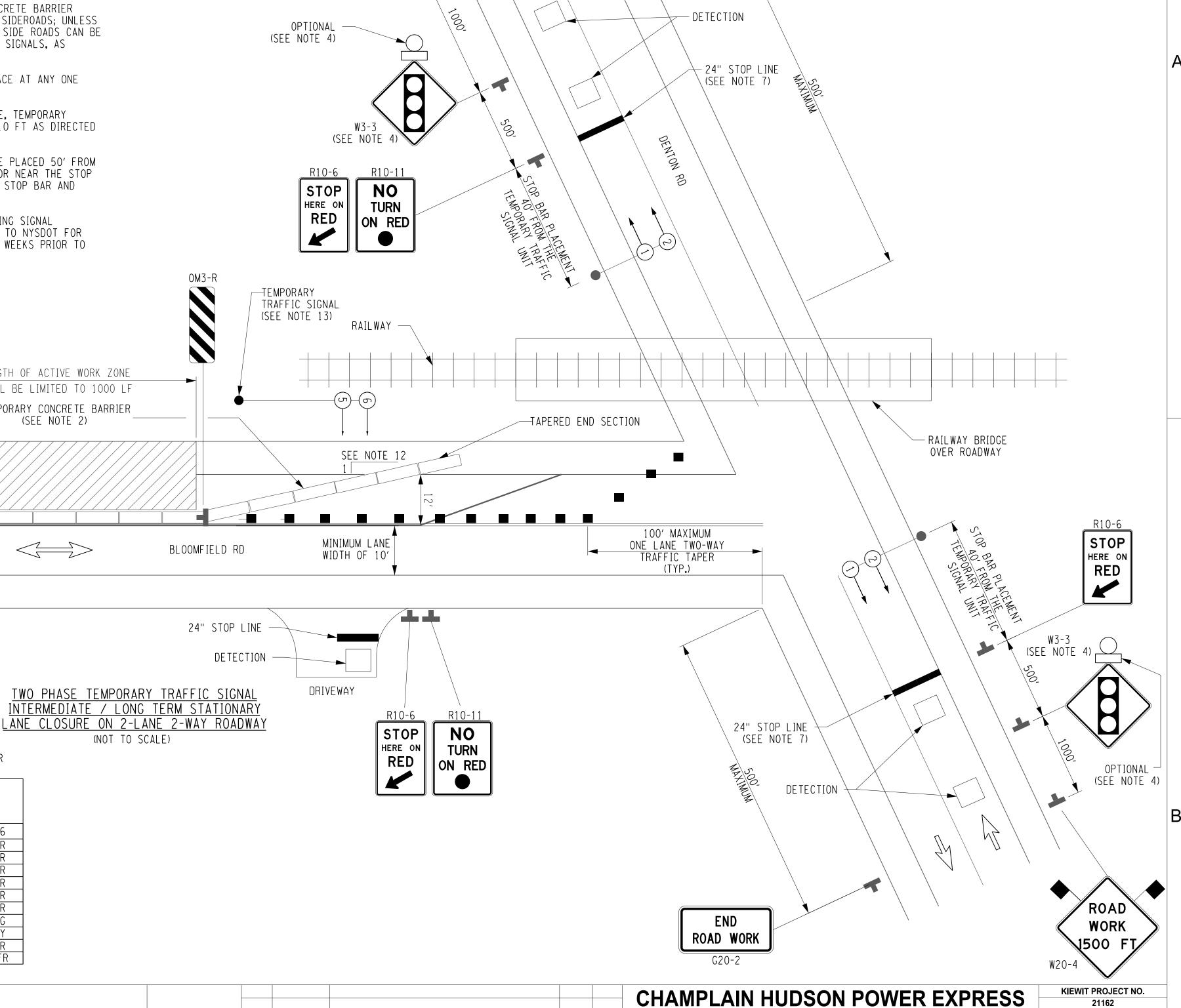
- 10. THE SUPPORTS FOR TEMPORARY TRAFFIC CONTROL SIGNALS SHALL NOT ENCROACH INTO THE MINIMUM REQUIRED WIDTH OF A "PEDESTRIAN ACCESS ROUTE" OF 4' OR AN "ALTERNATE CIRCULATION PATH" OF 3'.
- 11. THE ENGINEER MAY REQUIRE THE USE OF AN ADVISORY SPEED PLAQUE (W13-1) TO SUPPLEMENT A WARNING SIGN. THE PLAQUE WILL BE USED TO INDICATE AN ADVISORY SPEED FOR THE WORK ZONE CONDITION. (IE. NARROW LANES, BUMPS, POOR ROADWAY SURFACE, LOW OR NO SHOULDER, DROP-OFFS, GEOMETRIC CONSTRAINTS, AND/OR POOR SIGHT CONDITIONS).
- 12. TEMPORARY CONCRETE BARRIER TAPER SHALL BE 8:1 (30 MPH), 11:1 (40 MPH), 14:1 (50 MPH), AND 16:1 (55 MPH),
- 13. LIMITS OF LONG-TERM LANE CLOSURE WITH TEMPORARY CONCRETE BARRIER SHALL BE ESTABLISHED BETWEEN EXISTING DRIVEWAYS AND SIDEROADS; UNLESS TEMPORARY SIGNALIZATION OF EXISTING DRIVEWAYS AND/OR SIDE ROADS CAN BE PROVIDED AND COORDINATED WITH THE MAINLINE TEMPORARY SIGNALS, AS DIRECTED BY THE ENGINEER.
- 14. LONG-TERM LANE CLOSURES ARE ANTICIPATED TO BE IN PLACE AT ANY ONE LOCATION FOR APPROXIMATELY ONE WEEK (7 DAYS).
- 15. IF EXISTING PAVEMENT CANNOT ACCOMMODATE A 10 FT LANE, TEMPORARY PAVEMENT MUST BE PLACED TO MAINTAIN THE MINIMUM OF 10 FT AS DIRECTED BY THE ENGINEER.
- 16. WHEN LOOP DETECTORS ARE USED, A POINT LOOP SHOULD BE PLACED 50' FROM THE STOP BAR, AND CALLING LOOP SHOULD BE PLACED AT OR NEAR THE STOP BAR FOR WHEN A SIDE ROAD OR DRIVEWAY IS BETWEEN THE STOP BAR AND POINT LOOP.
- 17. SITE SPECIFIC WORK ZONE TRAFFIC CONTROL PLANS INCLUDING SIGNAL CLEARANCE AND DETECTION PLACEMENT SHALL BE PROVIDED TO NYSDOT FOR REVIEW BY THE REGIONAL TRAFFIC OFFICE A MINIMUM OF 2 WEEKS PRIOR TO THE START OF OPERATIONS.

TEMPORARY SIGNAL TIMING SHALL BE PROVIDED TO NYSDOT

FOR REVIEW BY THE REGIONAL TRAFFIC OFFICE, COUNTY, OR

MUNICIPALITY AT LEAST 2 WEEKS PRIOR TO INSTALLATION

OPERATIONAL SCHEDULE FOR TEMPORARY TRAFFIC SIGNALS



END

ROAD WORK

G20-2

CHA PROJECT NO.

066076

DRAWING NO.

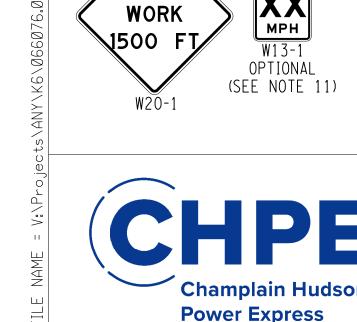
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OF XXX

W20-1

WORK

1500 F





HERE ON



CLEARANCE ALL RED

CLEARANCE

ALL RED

CLEARANCE

EMERGENCY FLASH | FR | FR |

SEE NOTE 12

TEMPORARY / INTERM WHITE PAVEMENT MARKING___

(SEE NOTE 1)

100' MAXIMUM ONE LANE TWO-WAY

TRAFFIC TAPER

(TYP.)

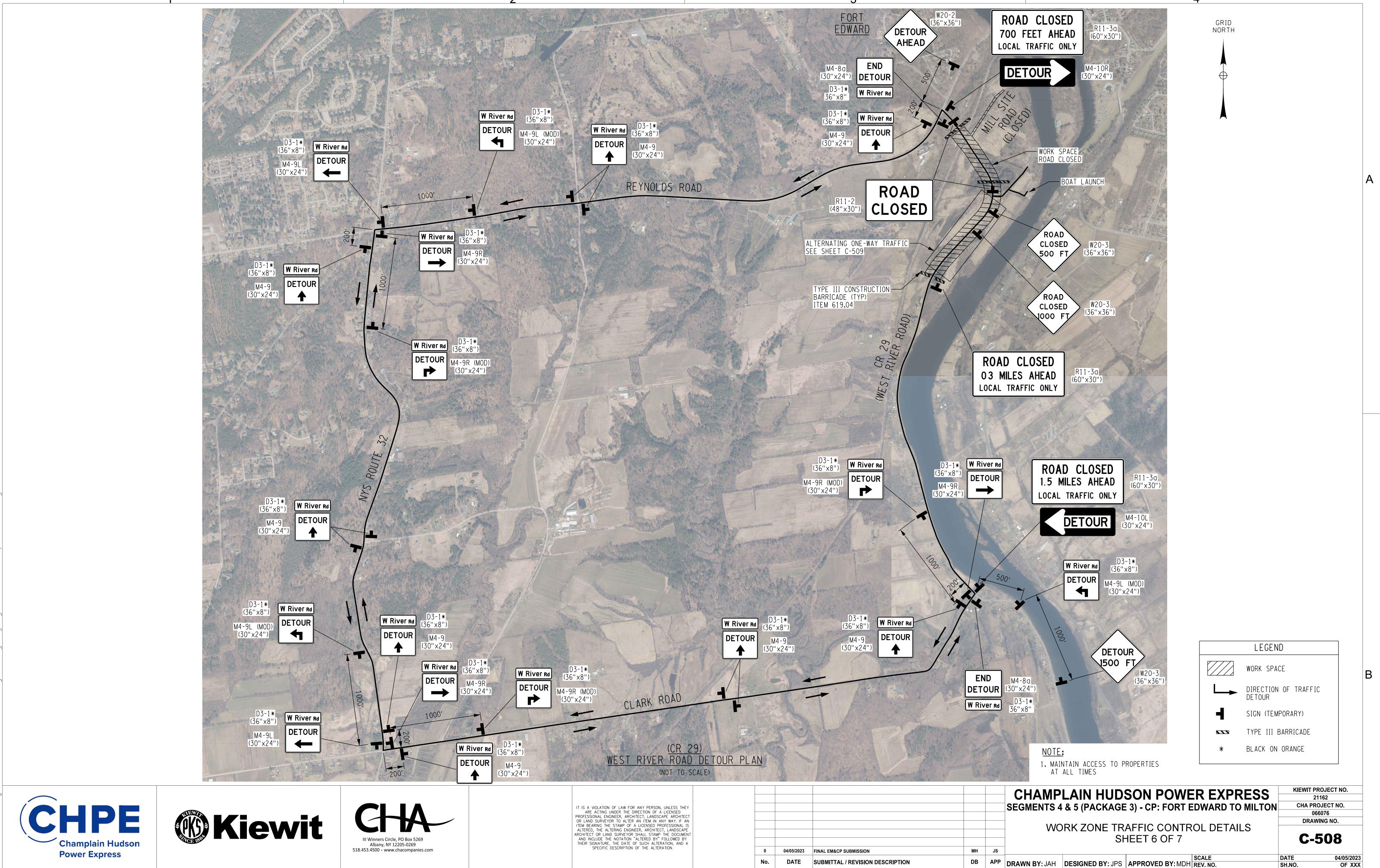
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, IHE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

LENGTH OF ACTIVE WORK ZONE

SHALL BE LIMITED TO 1000 LF

TEMPORARY CONCRETE BARRIER (SEE NOTE 2)

> SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON WORK ZONE TRAFFIC CONTROL DETAILS SHEET 5 OF 7 04/05/2023 FINAL EM&CP SUBMISSION DB | APP | DRAWN BY: JAH | DESIGNED BY: JPS | APPROVED BY: MDH | REV. NO. SUBMITTAL / REVISION DESCRIPTION



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MINIMUM LANE

WIDTH OF 10'

48" X30" ROAD R11-2 LENGTH OF ACTIVE WORK ZONE 48" X30" **CLOSED** TEMPORARY / INTERM WHITE PAVEMENT MARKING (SEE NOTE 1) HERE ON R10-6 100, ROAD AHEAD ROAD WORK **AHEAD**

- 1. FOR LONG DURATION LANE CLOSURES, TEMPORARY / INTERIM PAVEMENT MARKINGS SHALL
- 2. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE
- 4. DETECTORS SHALL BE INSTALLED ON ALL APPROACH LANES AND ALL PHASES SHALL BE ACTUATED. DURATION OF RED CLEARANCE INTERVALS SHALL BE ADEQUATE TO CLEAR THE
- 5. THE TRAFFIC SIGNAL CYCLE SHALL REST IN RED. WHEN THE SIGNAL IS CHANGED TO THE FLASHING MODE, EITHER MANUALLY OR AUTOMATICALLY, RED SIGNAL INDICATIONS SHALL
- 6. STOP LINES SHALL BE INSTALLED WITH TEMPORARY TRAFFIC SIGNALS FOR INTERMEDIATE AND LONG-TERM CLOSURES. EXISTING, CONFLICTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKINGS, BETWEEN THE ÁCTIVITY AREA AND THE STOP LINE, SHALL BE REMOVED. AFTER THE TEMPORARY TRAFFIC SIGNAL IS REMOVED, THE STOP LINES AND ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND PERMANENT PAVEMENT

- 7. WHERE NO-PASSING LINES ARE NOT ALREADY IN PLACE, THEY SHALL BE ADDED FOR A DISTANCE OF 500'-600' FROM THE STOP BAR. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.
- ADJUSTMENTS IN THE LOCATION OF ADVANCED WARNING SIGNS SHOULD BE MADE TO ACCOMMODATE THE HORIZONTAL AND VERTICAL ALIGNMENT OF THE ROADWAY, RECOGNIZING THAT THE DISTANCES SHOWN FOR SIGN SPACING ARE MINIMUMS. ADJUSTMENTS IN THE HEIGHT OF THE SIGNAL HEADS SHALL BE MADE AS NEEDED TO CONFORM TO THE VERTICAL ALIGNMENT.
- 9. THE SUPPORTS FOR TEMPORARY TRAFFIC CONTROL SIGNALS SHALL NOT ENCROACH INTO THE MINIMUM REQUIRED WIDTH OF A "PEDESTRIAN ACCESS" ROUTE" OF 4' OR AN "ALTERNATE CIRCULATION PATH" OF 3'.
- 10. THE ENGINEER MAY REQUIRE THE USE OF AN ADVISORY SPEED PLAQUE (W13-1) TO SUPPLEMENT A WARNING SIGN. THE PLAQUE WILL BE USED TO INDICATE AN ADVISORY SPEED FOR THE WORK ZONE CONDITION. (IE. NARROW LANES, BUMPS, POOR ROADWAY SURFACE, LOW OR NO SHOULDER, DROP-OFFS, GEOMETRIC CONSTRAINTS, AND/OR POOR SIGHT CONDITIONS).
- 11. IF EXISTING PAVEMENT CANNOT ACCOMMODATE A 10 FT LANE, TEMPORARY PAVEMENT MUST BE PLACED TO MAINTAIN THE MINIMUM OF 10 FT AS DIRECTED BY THE ENGINEER.





G20-2

END

ROAD WORK

SHOULDER

CR 29 S.B.

CR 29 N.B.

SHOULDER

500′ MAXIMUM ►

TEMPORARY / INTERIM

YELLOW PAVEMENT MARKING 500' - 600' FROM STOP BAR (SEE NOTES 1 AND 8)

24" STOP LINE (SEE NOTE 7)



STOP BAR PLACEMENT 40' FROM THE

TEMPORARY TRAFFIC SIGNAL UNIT

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> THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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					SEGMENTS 4	& 5 (PACKAGE	: 3) - CP: FORT E	EDWAR
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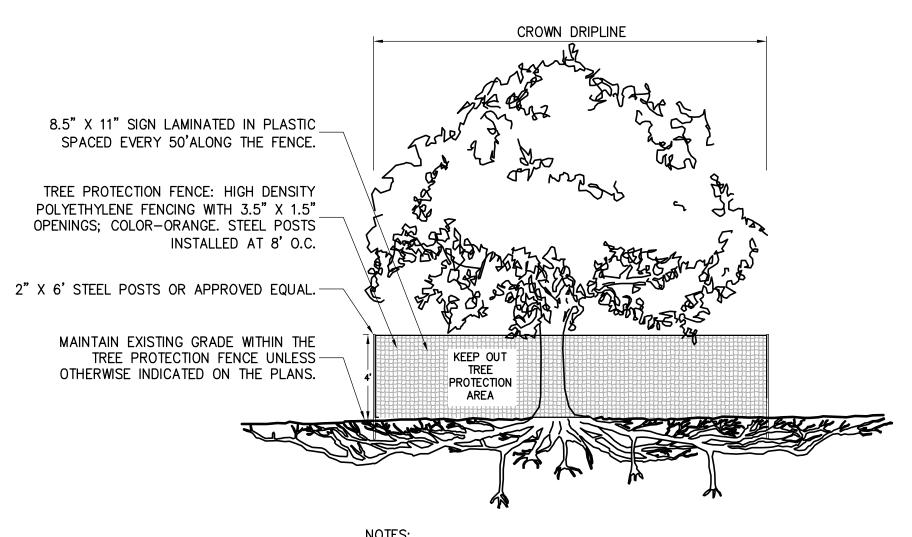
CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON

WORK ZONE TRAFFIC CONTROL DETAILS SHEET 7 OF 7

C-509

SH.NO. OF XXX

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.



1. CONTRACTOR TO MAINTAIN INTEGRITY OF CONSTRUCTION FENCE FOR DURATION OF PROJECT.

- 2. NO PRUNING SHALL BE PERFORMED EXCEPT BY APPROVED ARBORIST. 3. NO EQUIPMENT SHALL OPERATE INSIDE THE PROTECTIVE FENCING INCLUDING DURING FENCE
- INSTALLATION AND REMOVAL.

4. SEE EROSION CONTROL PLANS FOR LOCATIONS OF TREE PROTECTION AREAS.

REE PROTECTION

POST 36" LONG 11/2"x11/2" HARDWOOD SPACING 10' O.C. (SEE NOTE 2) 36" WIRE FENCING-14 GA. 6" SQ. STAPLED TO POST MIRAFI 100X (OR EQUIVALENT) FABRIC (FILTER CLOTH) 30#/IN. WRAP IN TRENCH ÁS SHÓWN COMPACTED FLOW BACKFILL -

- 1. TIE FABRIC TO WIRE FENCE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- 2. IF EXTRA STRENGTH FABRIC (GREATER THAN 50#/INCH) IS USED, WIRE CAN BE DELETED IF POST SPACING IS REDUCED TO 6' O.C.
- 3. AT THE ENDS OF THE FENCING THE FIRST 20' SHALL BE TURNED UP THE SLOPE 2'.
- 4. POSTS SHOULD BE INCLINED TOWARD THE DIRECTION FLOW CAME
- 5. OVERLAP FABRIC A MINIMUM OF 6" AND FOLDED AT JOINTS. ATTACH FILTER FABRIC TO STAKES ALLOWING EXTENSION INTO TRENCH AS SHOWN; SECURE TO STAKES AS NOTED.
- 6. THE MAXIMUM AREA OF RUNOFF PER 100LF. OF FENCE SHALL NOT EXCEED 0.25 ACRES.
- 7. MAINTENANCE SHALL BE PERFORMED AS NECESSARY. THE FENCING SHALL BE CHECKED AFTER EVERY STORM TO ENSURE THEIR PROPER FUNCTIONING.
- 8. WHEN FENCE IS NO LONGER NEEDED, THE ACCUMULATED SILT, THE POSTS AND FABRIC SHALL BE REMOVED AND TRENCH BACK FILLED WITH TOPSOIL AND SEEDED.
- 9. FENCING SHOULD BE PLACED AS SHOWN ON THE DRAWING OR IF NOT SHOWN, 10' BEYOND THE TOE OF THE SLOPE AND AT A SPACING IN ACCORDANCE WITH THE TABLE.
- 10. EXCAVATE TRENCH AS PER DETAIL AND SET POSTS AT 10' O.C.
- 11. BACKFILL WITH COMPACTED, EXCAVATED SOIL FROM TRENCH.

-NORTH AMERICAN GREEN S75 OR APPROVED EQUAL ON SUBGRADE, TEMP. SEED MIXTURE FILTER SOCK, SIZED TO SUIT CONDITIONS. -HARDWOOD POST 10' O.C. WATER FLOW INLET SIDE

- 1. ALL MATERIAL TO MEET MANUFACTURER SPECIFICATIONS.
- 2. ALL FILTER SOCKS SHALL BE 12" DIAMETER OR LARGER. 3. THE CONTRACTOR SHALL MAINTAIN THE COMPOST FILTER BERM IN A FUNCTIONAL CONDITION AT ALL TIMES AND IT SHALL BE ROUTINELY INSPECTED.

FILTER COMPOST MATERIAL

AS PER SPECIFICATIONS.

- 4. WHERE THE BERM REQUIRES REPAIR, IT WILL BE ROUTINELY
- 5. THE CONTRACTOR SHALL REMOVE SEDIMENTS COLLECTED AT THE BASE OF THE BERM WHEN THEY REACH 1/3 OF THE EXPOSED HEIGHT OF THE BERM, OR AS DIRECTED BY THE
- 6. THE COMPOST FILTER BERM WILL BE REMOVED ON SITE WHEN NO LONGER REQUIRED, AS DETERMINED BY THE OWNERS.
- 7. INSTALL PERPENDICULAR TO FLOW.

MAINTENANCE NOTES:

TOP OF CHANNEL/BANK

PERPENDICULAR TO THE FLOW

THE CHANNEL

FILTER

SOCK

CHANNEL PLACEMENT

FILTER

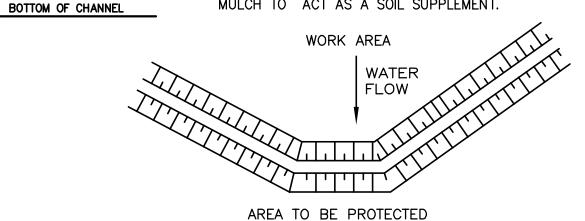
SOCK

FILTER

SOCK

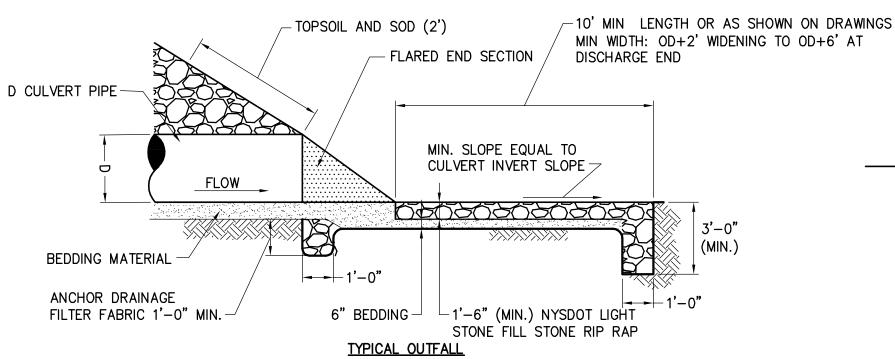
ACROSS THE ENTIRE WIDTH OF

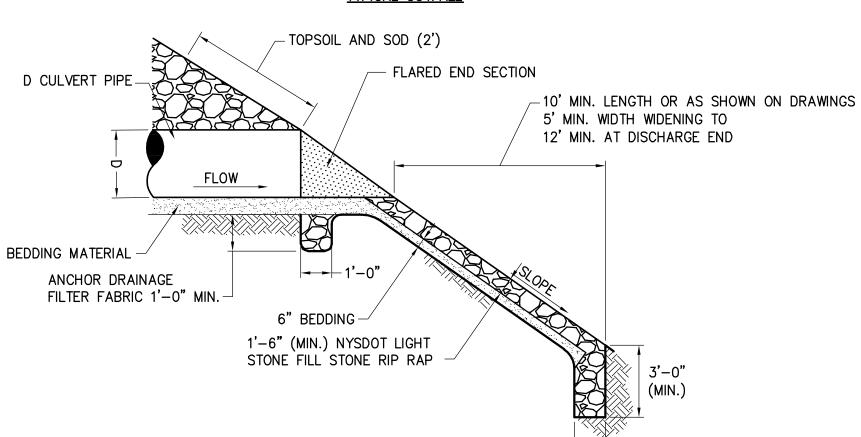
- 1. TRAFFIC SHALL NOT BE PERMITTED TO CROSS FILTER
- 2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES \(\frac{1}{3}\) OF THE EXPOSED HEIGHT OF THE PRACTICE AND DISPOSED OF IN ACCORDANCE WITH THE SWPPP.
- SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED IN THE MANNER REQUIRED BY THE MANUFACTURER OR REPLACED WITHIN 24 HOURS OF FILTER SOCK SHALL BE PLACED INSPECTION NOTIFICATION.
 - BIODEGRADABLE FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTO-DEGRADABLE FILTER SOCKS AFTER 1 YEAR. POLY-PROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
 - 5. UPON STABILIZATION OF THE AREA CONTRIBUTORY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK SHALL BE REMOVED. FOR REMOVAL THE MESH CAN BE CUT AND COMPOST SPREAD AS AN ADDITIONAL MULCH TO ACT AS A SOIL SUPPLEMENT.



AT GRADE PLACEMENT

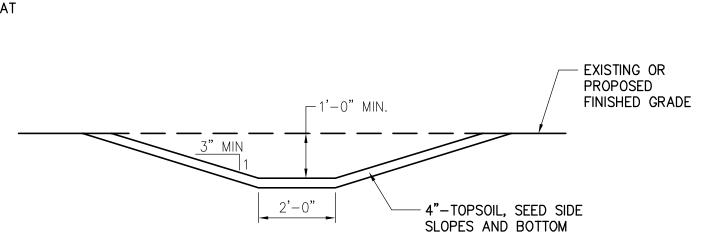
COMPOST FILTER SOCK DETAIL



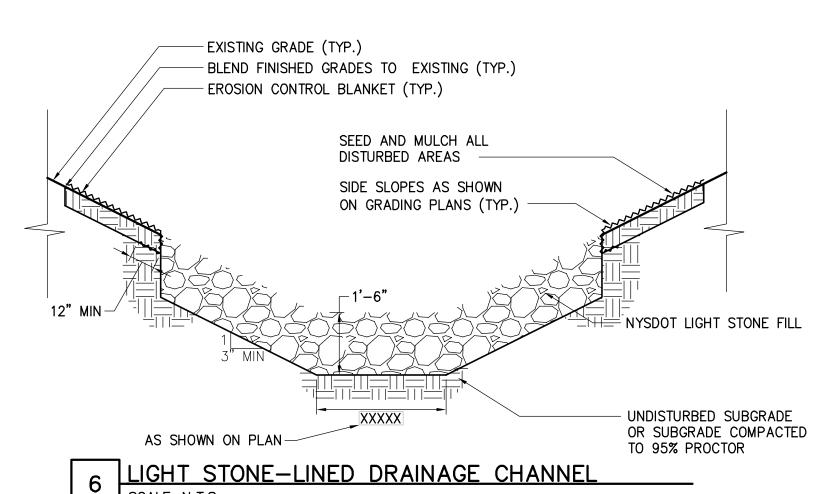


TYPICAL CULVERT OUTFALL RIP RAP

TYPICAL OUTFALL ON SLOPE



<u>TYPICAL GRASS DRAINAGE SWALE</u>



SILT FENCE





ELEVATION



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JJE JPR 04/05/2023 FINAL EM&CP SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION

CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON **EROSION AND SEDIMENT CONTROL DETAILS**

DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. DRAWING NO.

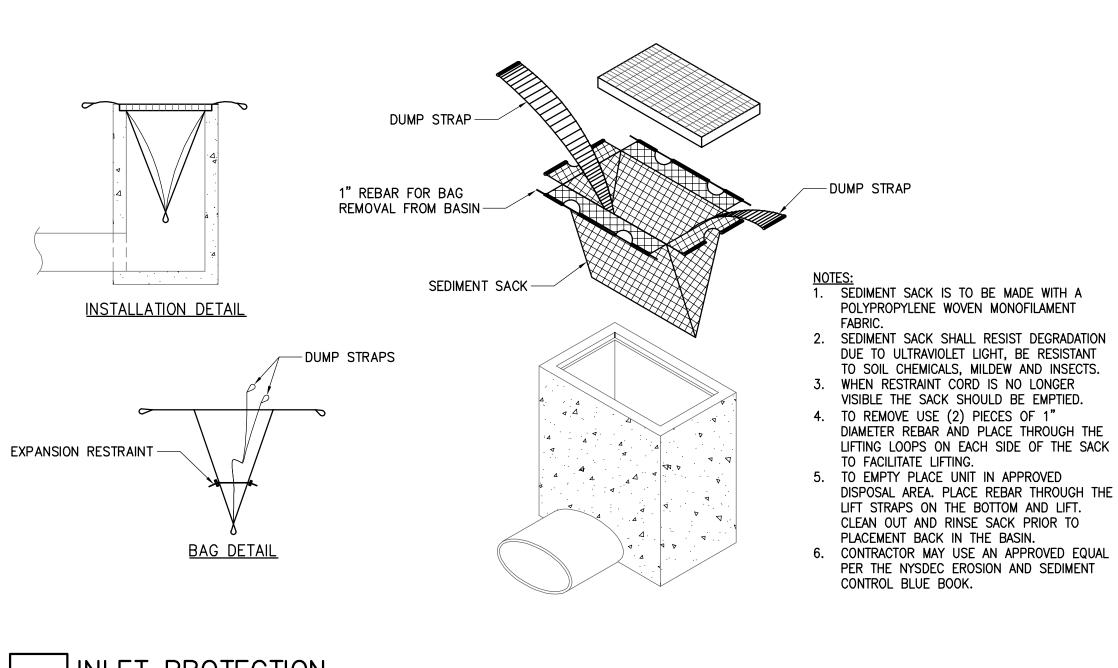
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AS NOTED DATE

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Power Express

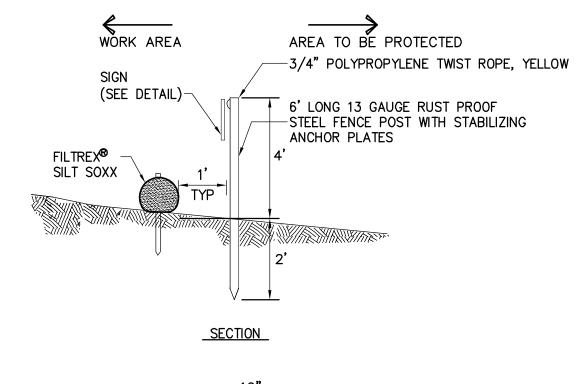
Champlain Hudson

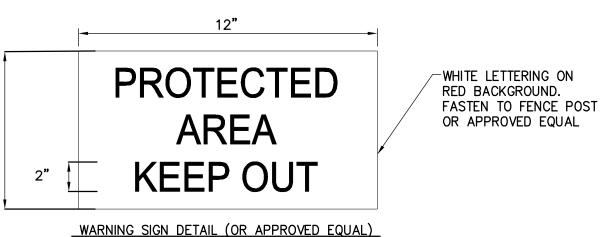


6' LONG 13 GAUGE RUST PROOF -STEEL FENCE POST WITH STABILIZING ANCHOR PLATES _CONSTRUCTION _3/4" POLYPROPYLENE TWIST ROPE, YELLOW BARRIER ROPE -WARNING SIGN (SEE ADJACENT DETAIL) 8' 0.C. 💄 MAX. (SIGNS SPACED AT 48' ON CENTER±) SEDIMENT SACK IS TO BE MADE WITH A POLYPROPYLENE WOVEN MONOFILAMENT 2. SEDIMENT SACK SHALL RESIST DEGRADATION -EXISTING GRADE DUE TO ULTRAVIOLET LIGHT, BE RESISTANT ELEVATION TO SOIL CHEMICALS, MILDEW AND INSECTS.

> CONSTRUCTION BARRIER FENCE SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PLANS PRIOR TO BEGINNING ANY WORK ADJACENT TO THESE AREAS.

2. THE CONTRACTOR SHALL INSTALL AT THE BEGINNING OF THE CONTRACT, AND MAINTAIN THROUGHOUT ITS DURATION.





STAPLE OR STAKE PER — MANUFACTURER'S RECOMMENDATIONS NORTH AMERICAN GREEN -ECB, MIRAFI MIRAMAT OR APPROVED EQUAL TOE TO BE BACKFILLED WITH COMPACTED EARTH

EROSION CONTROL BLANKETS TO BE INSTALLED ON SLOPES 3:1 OR GREATER (TYP.)

EROSION CONTROL BANK STABILIZATION DETAIL

INLET PROTECTION

SCALE: N.T.S. 50' MINIMUM FROM WETLANDS/WATERBODIES PUMP — - TIE DOWN DISCHARGE STRAP SPOUT WATER PUMP SEDIMENT. DEWATERING FILTERED — #2 STONE BERM (TYP)— -2' WIDE x 1' HIGH #2 STONE BERM

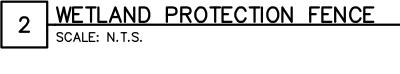
> AGGREGATE OR STRAW-SIDE VIEW UNDERLAY (FOR ADDED FLOW) NOTE: THE SEDIMENT DEWATERING BAG WILL BE MANUFACTURED IN THE U.S.A. FROM A NONWOVEN POLYPROPYLENE FABRIC THAT MEETS OR EXCEEDS THE FOLLOWING

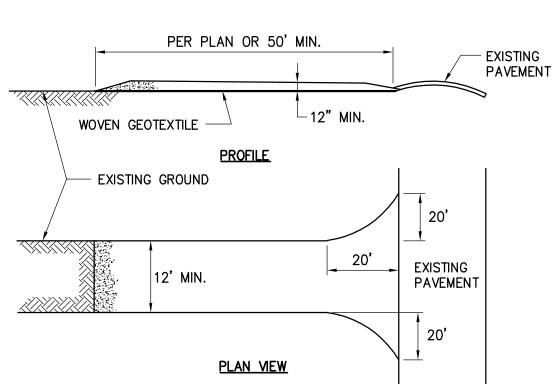
SPECIFICATIONS:

SEDIMENT DEWATERING BAG SPECIFICATIONS

		<u>-</u>	
Mechanical Properties	Test Method	Units	MARV
Grab Tensile Strength	ASTM D 4632	kN (lbs)	0.9 (205) × 0.9 (205)
Grab Tensile Elongation	ASTM D 4632	%	50 x 50
Puncture Strength	ASTM D 4833	kN (lbs)	0.58 (130)
Mullen Burst Strength	ASTM D 3786	kPa (psi)	2618 (380)
Trapezoid Tear Strength	ASTM D 4533	kN (lbs)	0.36 (80) X 0.36 (80)
UV Resistence	ASTM D 4355	%	70
Apparent Opening Size	ASTM D 4751	Mm (US Std Sieve)	0.180 (80)
Flow Rate	ASTM D 4491	1/min/m² (gal/min/ft²)	3866 (95)
Permittivity	ASTM D 4491	Sec ⁻¹	1.2

SEDIMENT DEWATERING BAG SCALE: N.T.S.



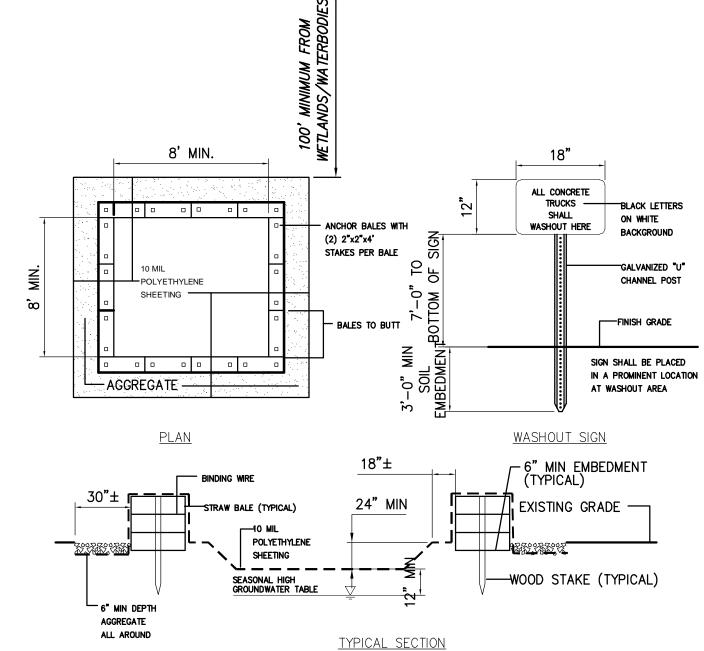


- 1. STONE SIZE-USE AASHTO M43 SIZE 3 COARSE AGGREGATE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
- 2. LENGTH NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY).
- 3. THICKNESS NOT LESS THAN 12".
- WIDTH TWELVE (12) FOOT MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. TWENTY-FOUR (24) FOOT IF SINGLE ACCESS TO SITE.
- 5. WOVEN GEOTEXTILE FABRIC WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE.
- 6. EXISTING ROAD SIDE DRAINAGE SHALL BE MAINTAINED.
- 7. SURFACE WATER ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- 8. MAINTENANCE-THE ACCESS SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT OR STONE SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
- 9. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON A AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.

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10. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.



MAINTENANCE NOTES:

- 1. ALL CONCRETE WASHOUT FACILITIES SHALL BE INSPECTED DAILY. DAMAGED OR LEAKING FACILITATES SHALL BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY. EXCESS RAINWATER THAT HAS ACCUMULATED OVER HARDENED CONCRETE SHALL BE PUMPED TO A STABILIZED AREA SUCH AS A GRASS FILTER STRIP.
- 2. ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE CAPACITY OF THE STRUCTURE IS FILLED. ANY EXCESS WASH WATER SHALL BE PUMPED INTO A CONTAINMENT VESSEL AND PROPERLY
- DISPOSED OF OFF SITE. 3. DISPOSAL OF THE HARDENED MATERIAL SHALL BE OFF-SITE IN A CONSTRUCTION/DEMOLITION LANDFILL.
- 4. THE PLASTIC LINER SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.
- 5. INSPECT THE PROJECT SITE FREQUENTLY TO ENSURE THAT NO CONCRETE DISCHARGES ARE TAKING PLACE IN NON-DESIGNATED AREAS. 6. LOCATION(S) TO BE DETERMINED IN THE FIELD
- BY THE OWNER'S REPRESENTATIVE 7. CONCRETE WASHOUTS SHALL NOT BE
- LOCATED WITHIN 200' OF ANY KNOWN WELL.

STABILIZED CONSTRUCTION ACCESS

SCALE: N.T.S.

Champlain Hudson Power Express





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		CHAMPLAIN HUDSON POWER EXPRESEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MEROSION AND SEDIMENT CONTROL DETAILS

DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

CONCRETE WASHOUT AREA

JJE JPR

SCALE: N.T.S.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. DRAWING NO.

C-602

AS NOTED DATE X SH.NO.

04/05/2023

DEWATERING PROCEDURES:

TRAPPED WATER WITHIN THE TRENCH SHALL BE DISCHARGED INTO A PORTABLE SEDIMENT TANK OR SEDIMENT FILTER BAGS LOCATED AWAY FROM THE WATERBODY TO PREVENT SILT-LADEN WATER FROM FLOWING INTO THE WATERBODY.

DAM AND PUMP CROSSING PROCEDURES:

BEFORE THE INITIATION OF ANY IN-STREAM ACTIVITIES, ALL MATERIAL ASSOCIATED WITH THE DAM AND PUMP SITE SET-UP MUST BE ON-HAND. THESE MATERIALS INCLUDE, BUT ARE NOT LIMITED TO THE FOLLOWING:

B)DOWNSTREAM SPLASH PLATE

C)PUMPS (PRIMARY AND SECONDARY) AND HOSES

D)FUEL FOR PUMPS (STORED AT LEAST ONE HUNDRED (100) FEET FROM WATERBODY) E)SPILL PREVENTION AND CONTROL MATERIALS (INCLUDING SECONDARY CONTAINMENT FOR PUMPS LOCATED WITHIN ONE HUNDRED (100) FEET OF WETLAND OR WATERBODY)

ONCE THE NECESSARY MATERIALS ARE ON-LOCATION, SITE SET-UP MAY BEGIN. THE FIRST STEP IS TO SELECT AN APPROPRIATE LOCATION FOR THE PUMP INTAKE HOSE(S) TO BE POSITIONED. DEPENDING UPON THE CHANNEL CHARACTERISTICS, EITHER A NATURALLY OCCURRING DEEP SPOT OR CHANNEL WILL BE SELECTED AS A "SUMP" OR A SUMP MAY NEED TO BE CREATED TO PROVIDE SUFFICIENT WATER DEPTH FOR THE SCREENED HOSE INTAKE(S). IF A NATURAL SUMP IS NOT AVAILABLE FOR THE INTAKE HOSE, AN IN-STREAM SUMP WILL BE CREATED BY EXCAVATING WITHIN THE STREAM CHANNEL AND SURROUNDING THE EXCAVATION USING SANDBAGS.

THE FOLLOWING BMPS SHALL BE IMPLEMENTED AT THE INTAKE OR SUMP SITE:

A)ALL EQUIPMENT, MATERIAL, AND CONSTRUCTION PERSONNEL NECESSARY FOR THE CROSSING SHALL BE ON-SITE BEFORE SET-UP BEGINS)UPON COMPLETION OF THE WATERBODY CROSSING ANY SANDBAGS UTILIZED FOR A SUMP SHALL BE REMOVED AND THE STREAM CHANNEL RESTORED TO PRE-CONSTRUCTION CONDITION C)THE SUMP SHALL BE OF SUFFICIENT DEPTH TO PREVENT THE ENTRAINMENT OF EXCESSIVE AMOUNTS OF SEDIMENT INTO THE SUMP INTAKE, HOSE AND PUMP

DURING THE ASSEMBLY OF THE UPSTREAM AND DOWNSTREAM WATER BARRIERS, THE PUMPING NETWORK SHALL BE SETUP TO BEGIN THE TRANSFER OF WATER AROUND THE CONSTRUCTION WORK AREA.

THE PUMP INTAKE AND DISCHARGE HOSES SHALL BE APPROPRIATELY PLACED AND OF SUFFICIENT LENGTH, BASED UPON SITE-SPECIFIC CONDITIONS. THE INTAKE HOSE SHALL BE SCREENED TO PREVENT THE ENTRAINMENT OF FISH. DISCHARGE HOSES SHALL BE PROVIDED WITH SUPPORT OVER THE DITCH-LINE AS NEEDED TO PREVENT EXCESSIVE SAGGING AND REDUCTION OF PUMPING CAPACITY.

THE NUMBER AND SIZES OF PUMPS TO BE USED AT ANY CROSSING SHALL BE DEPENDENT UPON THE VOLUME OF WATER FLOWING AT THE TIME THE CROSSING IS MADE.

BMPS TO BE IMPLEMENTED DURING PUMP SET-UP INCLUDE:

A)PUMPS SHALL BE FUELED PRIOR TO PLACING THEM IN POSITION

B)IF IT IS NECESSARY TO REFUEL DURING THE PUMP OPERATION, EXTRA CARE SHALL BE TAKEN TO AVOID

SPILLAGE AND SPILL CONTROL MATERIALS WILL BE READILY AVAILABLE ON SITE C)SECONDARY CONTAINMENT SHALL BE PLACED UNDER THE PUMPS AS AN ADDITIONAL PRECAUTIONARY

MEASURE TO PROTECT AGAINST ACCIDENTAL LEAKAGE OR SPILL

D)FUEL FOR FILLING THE PUMPS SHALL NOT BE STORED WITHIN ONE HUNDRED (100) FEET OF THE WATERBODY E)THE INTAKE HOSE SHALL BE SCREENED TO PREVENT THE ENTRAINMENT OF FISH

F)THE END OF THE DISCHARGE HOSE SHALL BE MOUNTED UPON A SPLASH PLATE OR SIMILAR DEVICE OR IN A MANNER THAT WILL DISSIPATE THE ENERGY OF THE DISCHARGING WATER AND REDUCE OR ELIMINATE STREAMBED SCOUR

G)IF HOSES CROSS THE TEMPORARY ACCESS ROAD, THEY SHALL BE PROTECTED FROM TRAVELING EQUIPMENT H)PUMP(S) SHALL BE OF SUFFICIENT CAPACITY TO TRANSFER TWICE THE CAPACITY OF THE ENTIRE STREAMFLOW AROUND THE CONSTRUCTION WORK AREA

I)RESERVE OR BACKUP PUMP(S) SHALL BE KEPT ON SITE AT ALL TIMES.

WATER BARRIER INSTALLATION

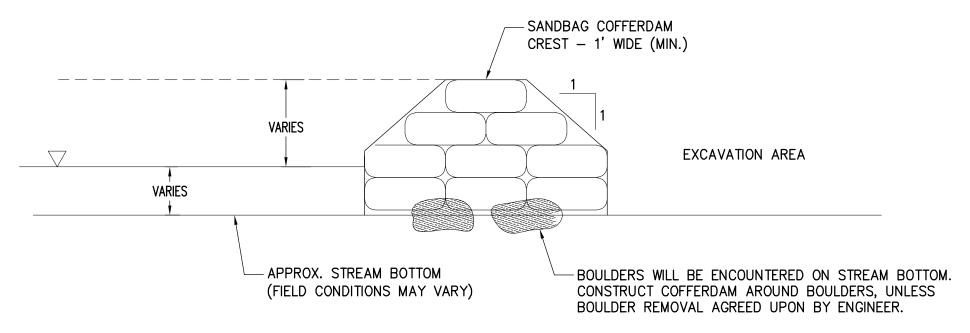
BETWEEN THE PUMP HOSE INTAKE OR SUMP HOLE AREA AND THE TRENCH, AS WELL AS DOWNSTREAM OF THE TRENCH, DAMS OF RELATIVELY IMPERVIOUS MATERIAL SHALL BE INSTALLED. THE UPSTREAM DAM SHALL BE COMPLETED FIRST. EVERY REASONABLE EFFORT SHALL BE MADE TO CONSTRUCT THE DAMS AS WATER TIGHT AS POSSIBLE.

THE FOLLOWING BMPS WILL BE IMPLEMENTED DURING WATER BARRIER INSTALLATION: A)DAMS SHALL BE CONSTRUCTED OF EITHER SANDBAGS, WATER BLADDERS, STEEL PLATES, PORTA-DAMS OR EQUIVALENT OR "JERSEY BARRIERS" AND PLASTIC SHEETING OR A COMBINATION THEREOF B)THE DAMS SHALL BE CONSTRUCTED OF SUFFICIENT HEIGHT TO ALLOW ADEQUATE FREEBOARD UNDER REASONABLY EXPECTED WATER LEVELS OR FLOWS AND PROVIDE FOR SOME IMPOUNDMENT OF WATER C)PRIOR TO COMPLETION OF THE DAMS, THE PUMP(S) MUST BE STARTED IN ORDER TO PROVIDE DOWNSTREAM FLOW OF WATER AROUND THE CONSTRUCTION WORK AREA

D)THE RATE OF PUMPING SHALL BE MONITORED TO MINIMIZE DRAINING OF THE INTAKE SUMP AND THE RESULTING CESSATION IN FLOW. ALTERNATIVELY, PUMPING SHALL BE MONITORED AND INCREASED AS NECESSARY TO PREVENT OVERTOPPING OF THE DAMS.

-FILTER SOCK OR HAY BALES (IF NECESSARY) SPOIL PILE 10' FROM TOP OF BANK - SANDBAG COFFERDAM OPEN PIPELINE TRENCH ---TRENCH PLUG--SPARE PUMP - FILTER SOCK - SPILL CONTAINMENT DEVICE EROSION AND SEDIMENTATION CONTROL TO BE PLACED ACROSS THE EQUIPMENT CROSSING AT KEEP EQUIPMEN THE END OF THE DAY CROSSING FREE OF MUD/SOIL EQUIPMENT CROSSING WOOD MAT BRIDGE _ _ _ _ _ STREA BANK STRAW BALES OR SAND BAGS-TO BE PLACED DURING NO CONSTRUCTION ACTIVITY - DISCHARGE HOSE **ENERGY DISSIPATER AT** THE END OF DISCHARGE ROW LIMIT

DAM AND PUMP AROUND STREAM CROSSING



1. SAND BAGS SHALL BE FILTER FABRIC TYPE AND BE DOUBLE BAGGED. 2. PORTADAM, BY PORTADAM, INC. SHALL BE CONSIDERED ACCEPTABLE SUBSTITUTE TO SAND BAGS

DATE SUBMITTAL / REVISION DESCRIPTION

SANDBAG COFFERDAM DETAIL

Albany, NY 12205-0269 518.453.4500 . www.chacompanies.com

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CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON **EROSION AND SEDIMENT CONTROL DETAILS**

GENERAL SEQUENCE:

WATER AT INTAKE.

1. SCHEDULE CONSTRUCTION DURING LOW FLOW PERIOD, IF POSSIBLE.

CAPACITY OR ANTICIPATED FLOW. HAVE STANDBY PUMP ON SITE.

DEPENDING ON STREAM FLOW, DIG SUMP HOLE TO CONCENTRATE

PLATING OR A COMBINATION OF BOTH. INSTALL DOWNSTREAM DAM,

2. SET UP PUMP AND HOSE AS SHOWN, OR USE PRACTICAL

ALTERNATIVES. PUMP SHOULD HAVE TWICE THE PUMPING

3. INSTALL UPSTREAM DAM COMPOSED OF SANDBAGS, METAL

4. AFTER DAMS ARE IN PLACE, IT MAY BE NECESSARY TO USE

7. RESTORE STREAM BANKS AND APPROACHES FOR A MINIMUM

5. EXCAVATE TRENCH AND LOWER IN PIPE UNDER HOSE. MOVE HOSE

6. DISMANTLE DOWNSTREAM DAM, THEN UPSTREAM DAM. KEEP PUMP

DISTANCE OF AT LEAST 50 FEET FROM THE STREAM EDGES AND

PERMANTENTLY STABLIZE WITHIN 1 DAY OF INITIAL RESTORATION.

AS REQUIRED OR DISCONNECT, IF TEMPORARY FLOW BLOCKAGE IS

IF REQUIRED, TO KEEP STREAM BED DRY.

ACCEPTABLE. BACKFILL TRENCH.

RUNNING TO MAINTAIN STREAM FLOW.

ADDITIONAL PUMPS TO HANDLE STREAM FLOW.

21162 CHA PROJECT NO. 066076 DRAWING NO.

KIEWIT PROJECT NO.

C-603

AS NOTED DATE

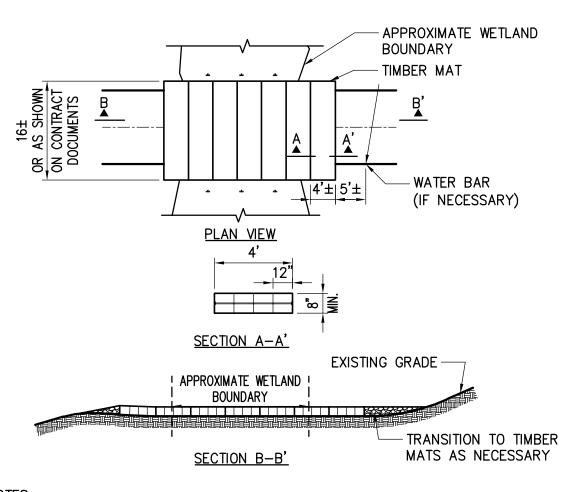
Champlain Hudson **Power Express**



AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

04/05/2023 FINAL EM&CP SUBMISSION JJE JPR

DB APP DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.



1. TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO PREVENT RUTTING.

2. BASED ON ACTUAL SITE CONDITIONS, MULTIPLE LAYERS OF TIMBER MATS MAY BE REQUIRED. 3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM

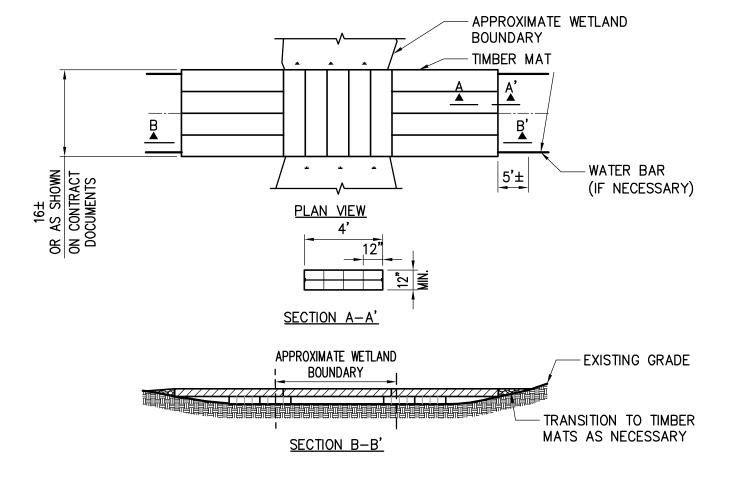
SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING. 4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND DURING RAIN EVENTS. SEDIMENT SHOULD BE

REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION. 5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.

6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.

7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF

EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.



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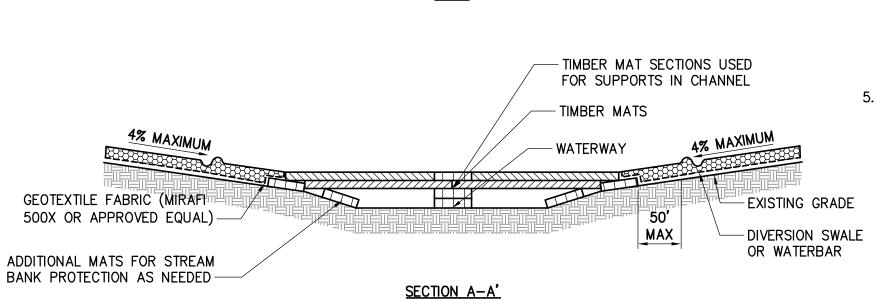
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-NYSDOT #703-0202 -#4 STONE -TIMBER MATS WATERWAY



1. IN-STREAM EXCAVATION SHOULD BE COMPLETED IN ACCORDANCE WITH "TEMPORARY ACCESS WATERWAY CROSSING" ON PAGE 2.32 OF THE 2016 NYSDEC STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (OR NEWEST VERSION) AND THE CERTIFICATE.

THE CONSTRUCTION OF ANY CROSSING SHOULD NOT CAUSE A SIGNIFICANT WATER LEVEL DIFFERENCE BETWEEN THE UPSTREAM AND DOWNSTREAM WATER SURFACE ELEVATIONS. IN-STREAM WORK WILL BE PROHIBITED WITHIN COLD WATER TROUT FISHERIES FROM

OCTOBER 1 TO MAY 31. 3. ALL FILL MATERIALS ASSOCIATED WITH THE ROADWAY APPROACH SHOULD BE LIMITED TO A MAXIMUM HEIGHT OF 2 FEET ABOVE THE

EXISTING FLOOD PLAIN ELEVATION. 4. A WATER DIVERTING STRUCTURE SUCH AS A SWALE OR WATER BAR SHOULD BE CONSTRUCTED (ACROSS THE ROADWAY ON BOTH ROADWAY APPROACHES) 50 FEET (MAXIMUM) ON EITHER SIDE OF THE WATERWAY CROSSING. THIS WILL PREVENT ROADWAY SURFACE RUNOFF FROM DIRECTLY ENTERING THE WATERWAY. THE 50 FEET MEASURED IS MEASURED FROM THE TOP OF THE WATERWAY BANK. IF THE ROADWAY APPROACH IS CONSTRUCTED WITH A REVERSE GRADE AWAY FROM THE WATERWAY, A SEPARATE DIVERTING

STRUCTURE IS NOT REQUIRED. 5. ALL CROSSINGS SHOULD HAVE ONE TRAFFIC LANE. THE MINIMUM WIDTH SHOULD BE 12 FEET WITH A MAXIMUM WIDTH OF 20 FEET.

NOT TO SCALE

TIMBER MATTING SCALE: N.T.S.







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	0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR	CHAMPLAIN HUDSON POWER E SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWA WETLAND CROSSING DETAILS
\vdash						SCALE
	No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO

CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON WETLAND CROSSING DETAILS

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

C-611

04/05/2023

AS NOTED DATE X SH.NO.

WATER SHALL BE DIVERTED OFF THE DISTURBED RIGHT-OF-WAY AT AN OUTSLOPE OF THREE TO FIVE PERCENT BY CONSTRUCTING DIVERSION DITCH ACCORDING TO THE FOLLOWING

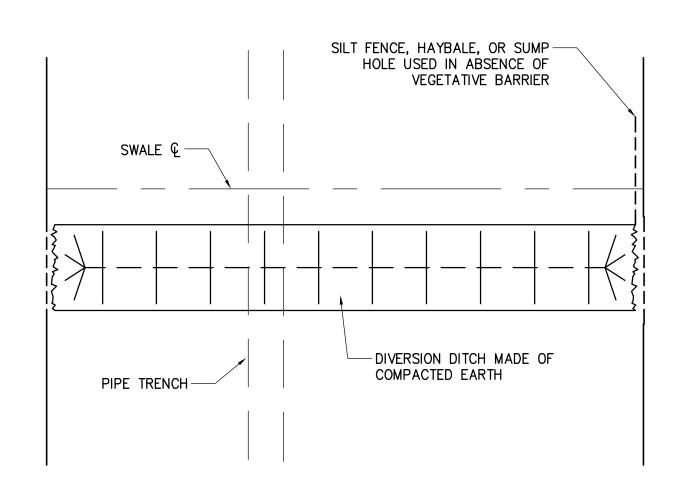
- 1. AT THE PROPOSED INTERCEPTOR DITCH LOCATION ESTABLISH A HORIZONTAL CONTOUR LINE (USING A POCKET TRANSIT OR HAND LEVEL) WHICH EXTENDS COMPLETELY ACROSS THE DISTURBED RIGHT-OF-WAY. THIS LINE WILL ALWAYS BE PERPENDICULAR TO THE DIRECTION OF WATER FLOW AND SHOULD BE PARALLEL TO THE MAP CONTOURS SHOWN ON THE PLAN DRAWINGS.
- 2. DETERMINE WHICH SIDE OF THE RIGHT-OF-WAY IS BEST SUITED FOR THE DITCH OUTLET (EVALUATE VEGETATION DENSITY, LOCAL TOPOGRAPHY, ETC.) AND DEVIATE DIKE AWAY FROM THE HORIZONTAL CONTOUR LINE SLIGHTLY DOWNWARD TOWARD THE SELECTED OUTLET SIDE MAINTAINING A THREE TO FIVE PERCENT SLOPE. AS AN EXAMPLE, THE CHART AT THE RIGHT SHOWS DIMENSIONS ASSUMING A FOUR PERCENT
- 3. WHEN OUTLETTING NEAR WATER BODIES, STREAMS, DITCHES, & CROP FIELDS, A FILTER FENCE OR STRAW BALE FENCE SHOULD BE PLACED ON OUTLET END OF THE DIVERSION

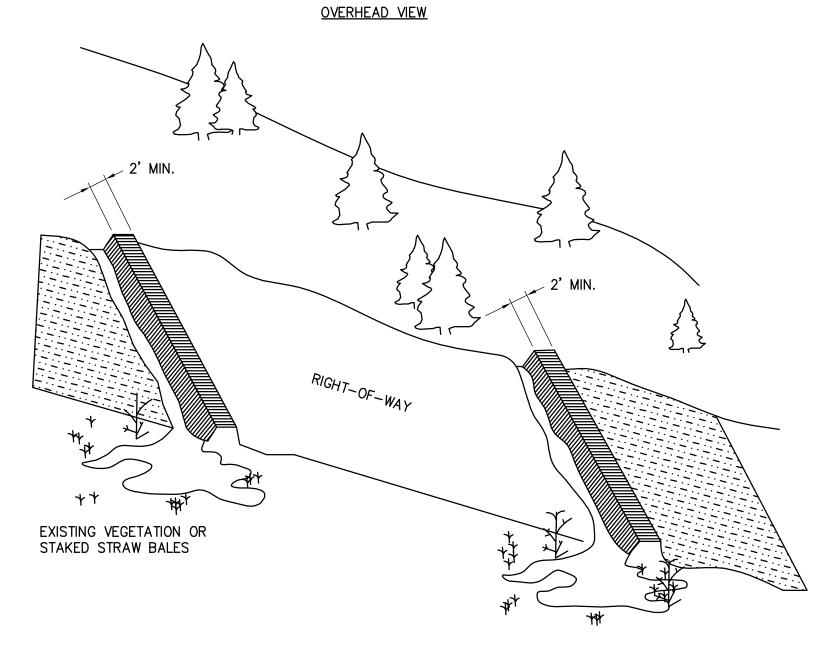
TEMPORARY DRAINAGE DITCH

- 1. TEMPORARY DIVERSION DITCH SHOULD BE BUILT SIMILAR TO THE PERMANENT DITCH CONFIGURATION BUT THE DIMENSION CAN BE SCALED BACK.
- 2. MAXIMUM HEIGHT SHOULD BE 12" AND SHOULD BE COMPACTED.
- 3. SPACING BETWEEN DIVERSION DITCHES AND SKEW OF THE DIVERSION DITCHES CAN VARY FROM THE PERMANENT DIVERSION DITCHES. 4. WHEN CONSTRUCTING TEMPORARY DIVERSION DITCHES THEY SHOULD BE FUNCTIONAL, WHILE

MAINLINE CONSTRUCTION IS PROCEEDING, UNTIL RESTORATION BEGINS AND PERMANENT

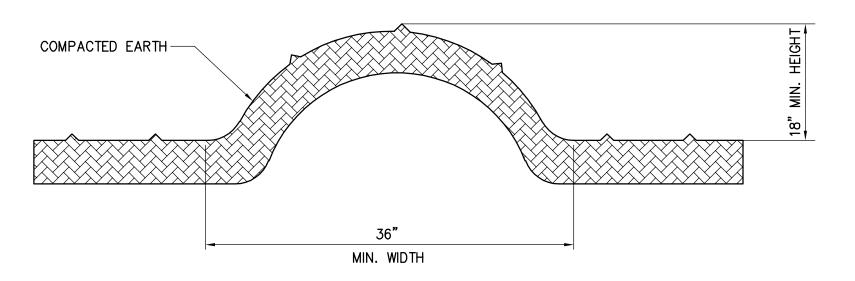
4% FLOW CHART						
HORIZONTAL DISTANCE BETWEEN WATERBAR INLET & OUTLET (FEET)	ELEVATION DISTANCE BETWEEN WATERBAR INLET AND OUTLET (FEET)					
75	3					
100	4					
125	5					
150	6					
175	7					





PERMANENT DIVERSION DITCH DETAIL SCALE: N.T.S.

DIVERSION DITCHES ARE THEN CONSTRUCTED.



04/05/2023 FINAL EM&CP SUBMISSION

SUBMITTAL / REVISION DESCRIPTION

2 MINIMAL HEIGHT & WIDTH DIMENSIONS FOR WATERBAR CONSTRUCTION SCALE: N.T.S.

Power Express





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ARE ACTING UNDER THE DIRECTION OF A LICENSED
PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITEC
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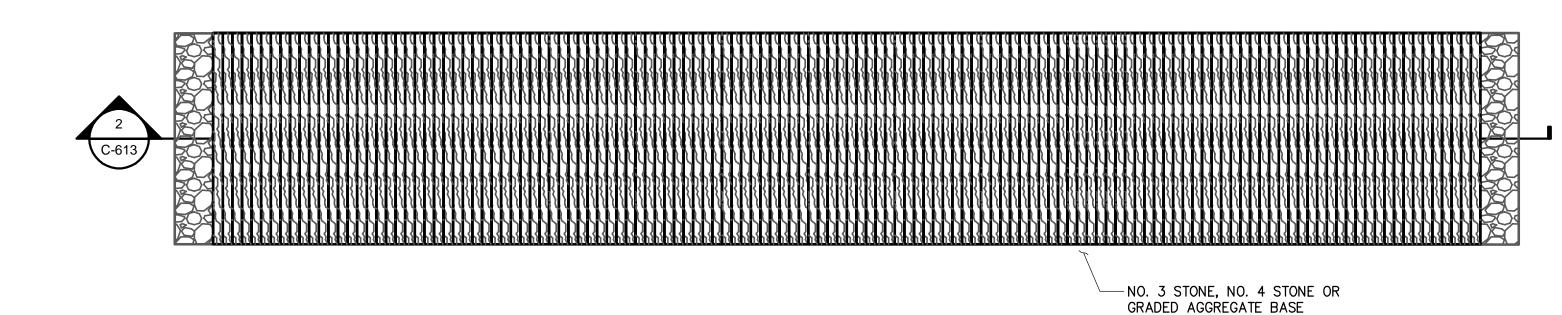
		CHAMPLAIN HUDSON POWER EXPRESS	
		SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON	
		WATERBAR DETAILS	

DB APP DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

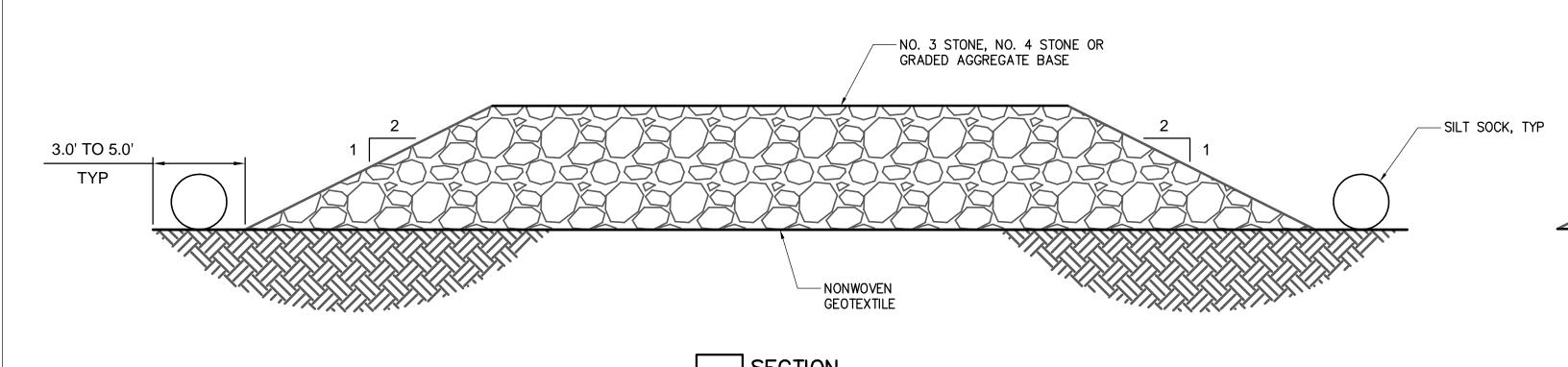
KIEWIT PROJECT NO. CHA PROJECT NO. DRAWING NO.

C-612

AS NOTED DATE



WETLAND WORKING SURFACE PLAN - OPTION B



NOTES:

- 1. UNDERCUT AND REMOVE TOP SOIL PRIOR TO PLACING GEOTEXTILE FABRIC.
- 2. A LAYER OF CLEAN CRUSHED STONE SHALL BE LAID ON TOP OF THE GEOTEXTILE FABRIC.

NOT TO SCALE

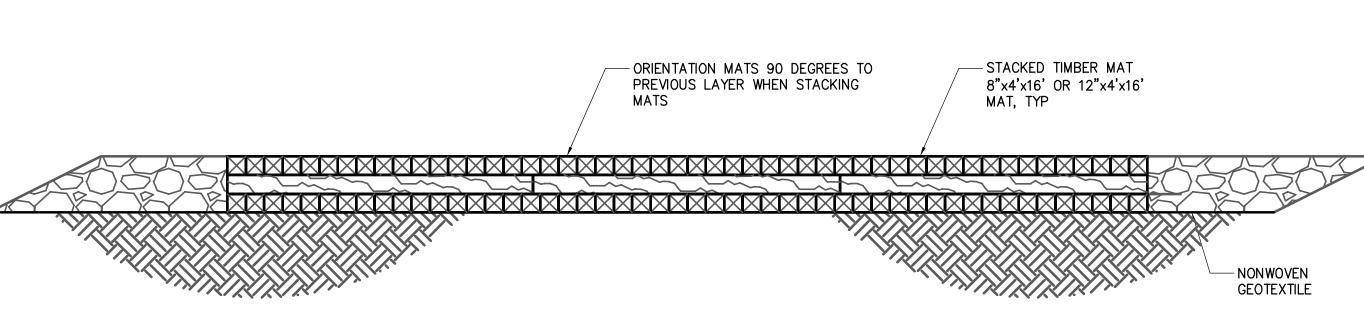
- 3. GEOTEXTILE FABRIC SHALL EXTEND AT LEAST 3 FT TO 5 FT BEYOND THE EDGE OF STONE PLACEMENT TO MINIMIZE STONE ENTERING THE WETLAND AND FACILITATE REMOVAL OF THE ROAD.
- 4. SUITABLE CROSS DRAINING SHALL BE PROVIDED ACROSS THE ROAD
- FOR STREAM CHANNELS AND SURFACE FLOW. 5. AREA TO BE RESTORED IN ACCORDANCE WITH THE EM&CP SECTION 14

GENERAL NOTES:

1. TIMBER:

A. TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.

2. CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER IMMEDIATELY.



NOTES:

- TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO
- PREVENT RUTTING. 2. BASED ON ACTUAL SITE CONDITIONS, NUMBER OF TIMBER MAT LAYERS TO BE
- DETERMINED ON SITE. 3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING.
- 4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND DURING RAIN EVENTS. SEDIMENT SHOULD BE REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION.
- 5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.
- 6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS
- MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF
- EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED. 8. IF DEEMED NECESSARY BY CONSTRUCTION IN THE FIELD, GEOTEXTILE FABRIC TO BE
- INSTALLED UNDER MATTING. (TYP) 9. AREA TO BE RESTORED IN ACCORDANCE WITH THE EM&CP SECTION 14.







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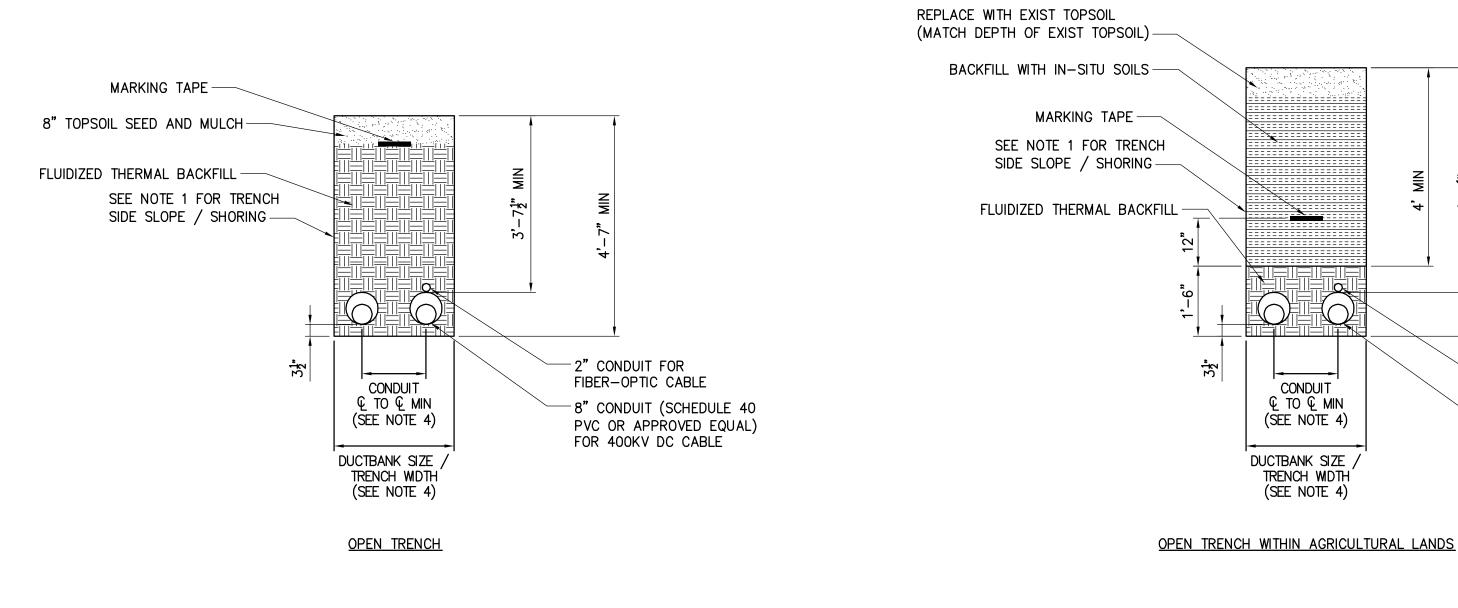
0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR	SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWAR WETLAND WORKING SURFACES PL
NI.	DATE			400	SCALE
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON WETLAND WORKING SURFACES PLAN

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

C-613

AS NOTED DATE X SH.NO.



ASPHALT PAVEMENT SECTION TO MACH EXISTING (SEE NOTES 2 AND 3 BELOW) -MARKING TAPE — TEMPORARY SHORING -FLUIDIZED THERMAL BACKFILL -2" CONDUIT FOR FIBER-OPTIC CABLE -- 2,500 PSI CONCRETE -8" CONDUIT (SCHEDULE 40 CONDUIT PVC OR APPROVED EQUAL) © TO € MIN FOR 400KV DC CABLE (SEE NOTE 4) DUCTBANK SIZE TRENCH WIDTH' (SEE NOTE 4)

DUCTBANK IN ROADWAY

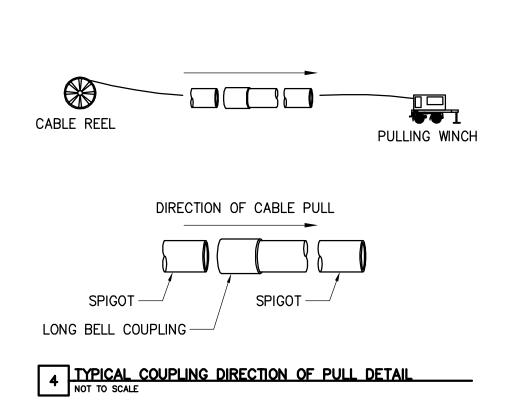
- 1. SLOPING, BENCHING, OR SHORING SHALL BE IN ACCORDANCE WITH OSHA EXCAVATION STANDARDS, 29 CFR PART 1926, SUBPART P. AT LOCATIONS WHERE THE TRENCH IS NOT SHORED, SLOPING AND/OR BENCHING WILL DEPEND ON TYPE OF SOILS ENCOUNTERED ON SITE. SLOPE FROM EDGE OF ROADWAY TO BOTTOM OF EXCAVATIONS MAY BE FLATTER THAN 2:1 (H: V) FOR AASHTO HS-20 LOADING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EVALUATING SLOPE STABILITY BASED ON ACTUAL EQUIPMENT FOR SITE OPERATIONS AS DETERMINED BY A GEOTECHNICAL ENGINEER.
- 2. SEE DETAIL 4 ON DETAIL SHEET C-631 FOR PAVEMENT TRANSITION DETAIL.

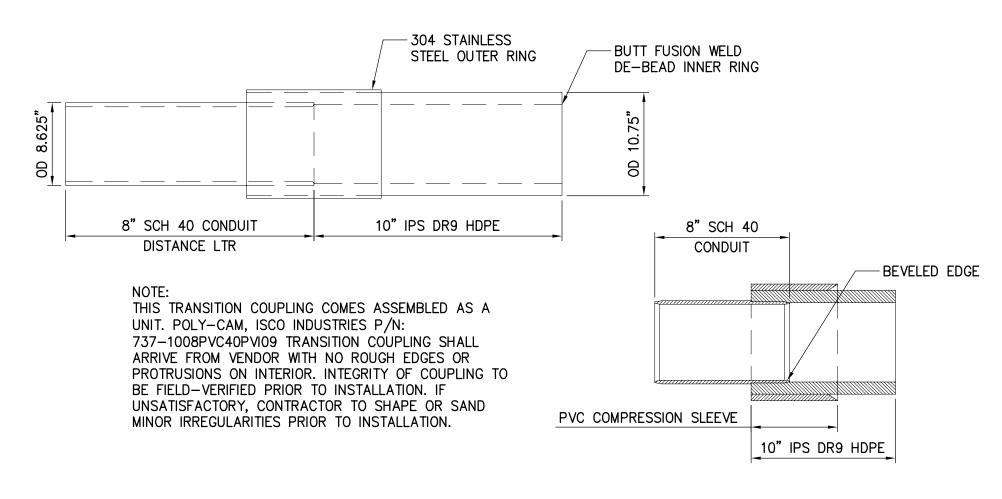
3. SEE SHEET C-631 FOR SURFACE RESTORATION DETAILS.

4. SEE PLAN AND PROFILE SHEETS FOR CONDUIT € TO € AND DUCTBANK SIZE TRENCH WIDTH (NOTE ABOVE PROFILE VIEW).

1 TYPICAL TRENCHING DETAILS

NOT TO SCALE





5 8"-10" PVC/HDPE TRANSITION COUPLING
NOT TO SCALE







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2" CONDUIT FOR

FIBER-OPTIC CABLE

-8" CONDUIT (SCHEDULE 40

PVC OR APPROVED EQUAL)

FOR 400KV DC CABLE

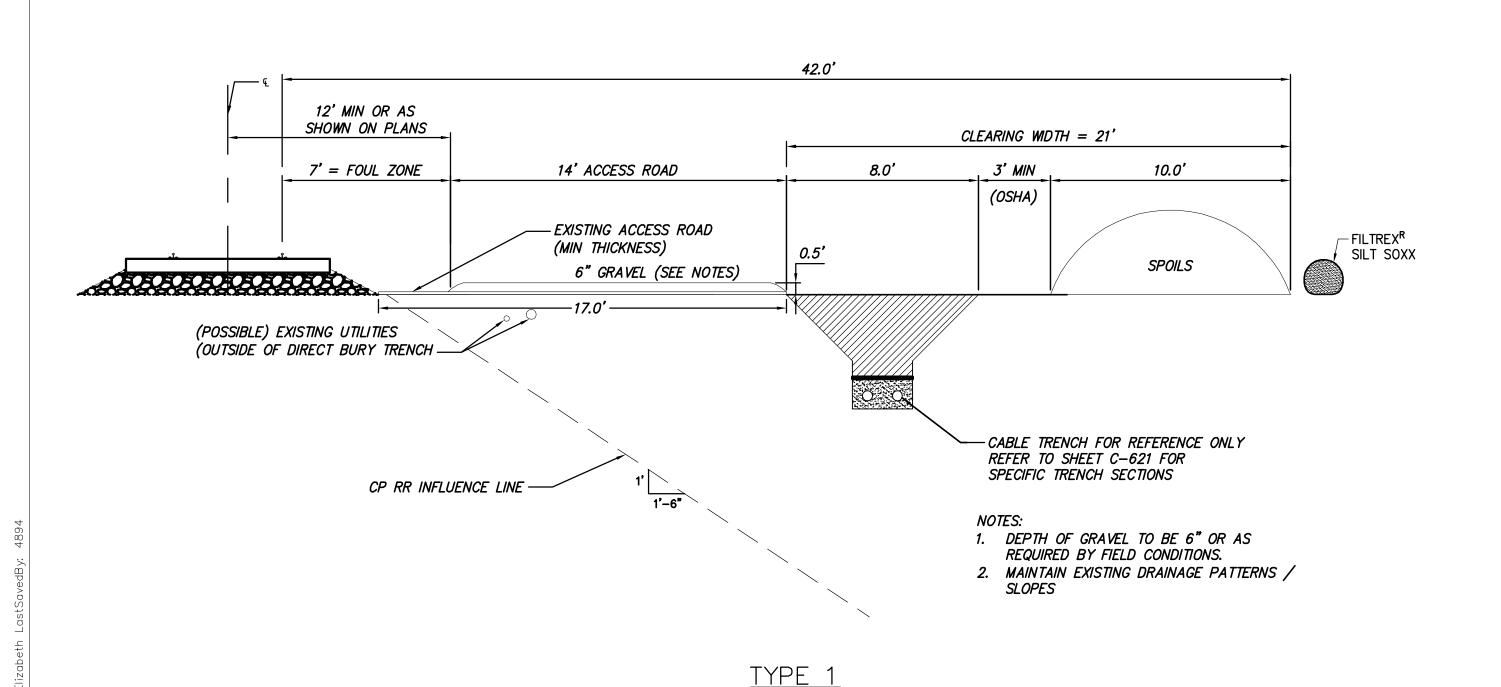
0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR	SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWAR TRENCHING DETAILS
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CHAMPLAIN HUDSON POWER EXPRESS MENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON TRENCHING DETAILS

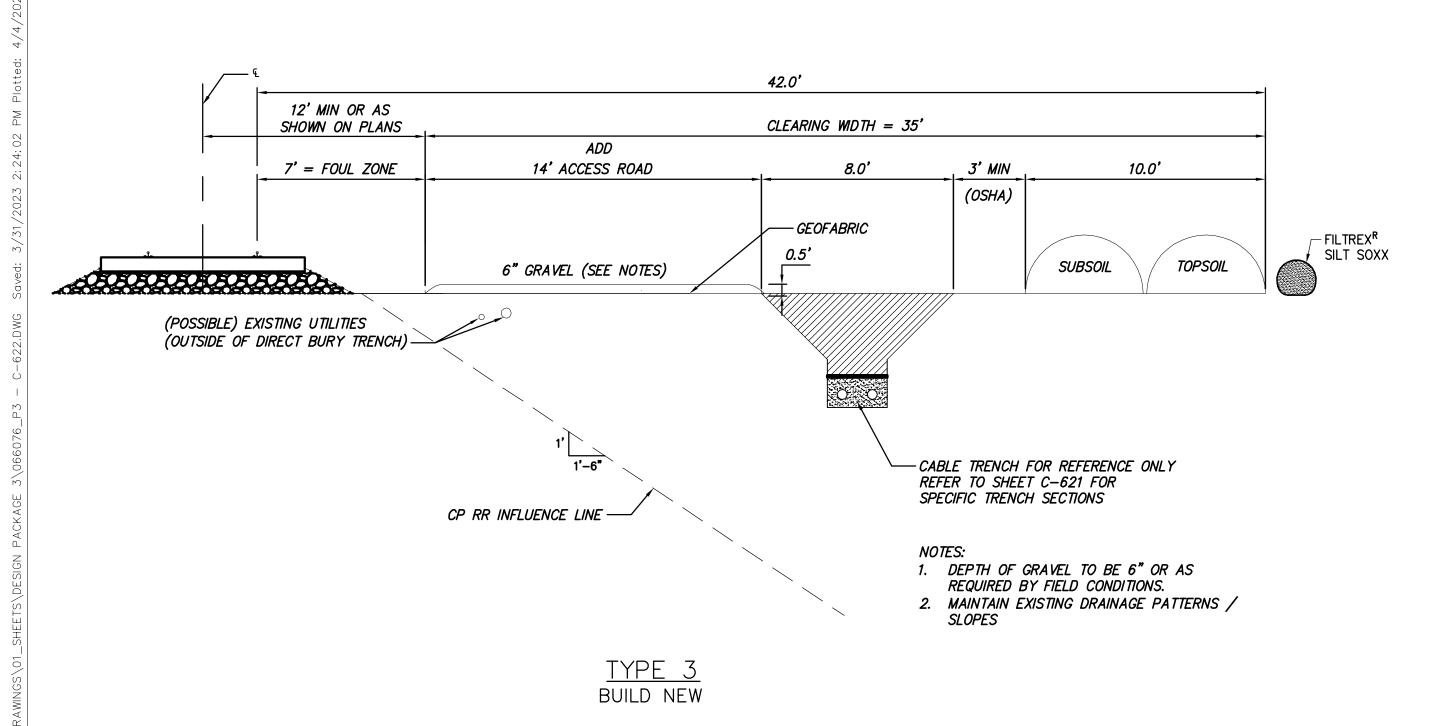
KIEWIT PROJECT NO. 21162 CHA PROJECT NO. DRAWING NO.

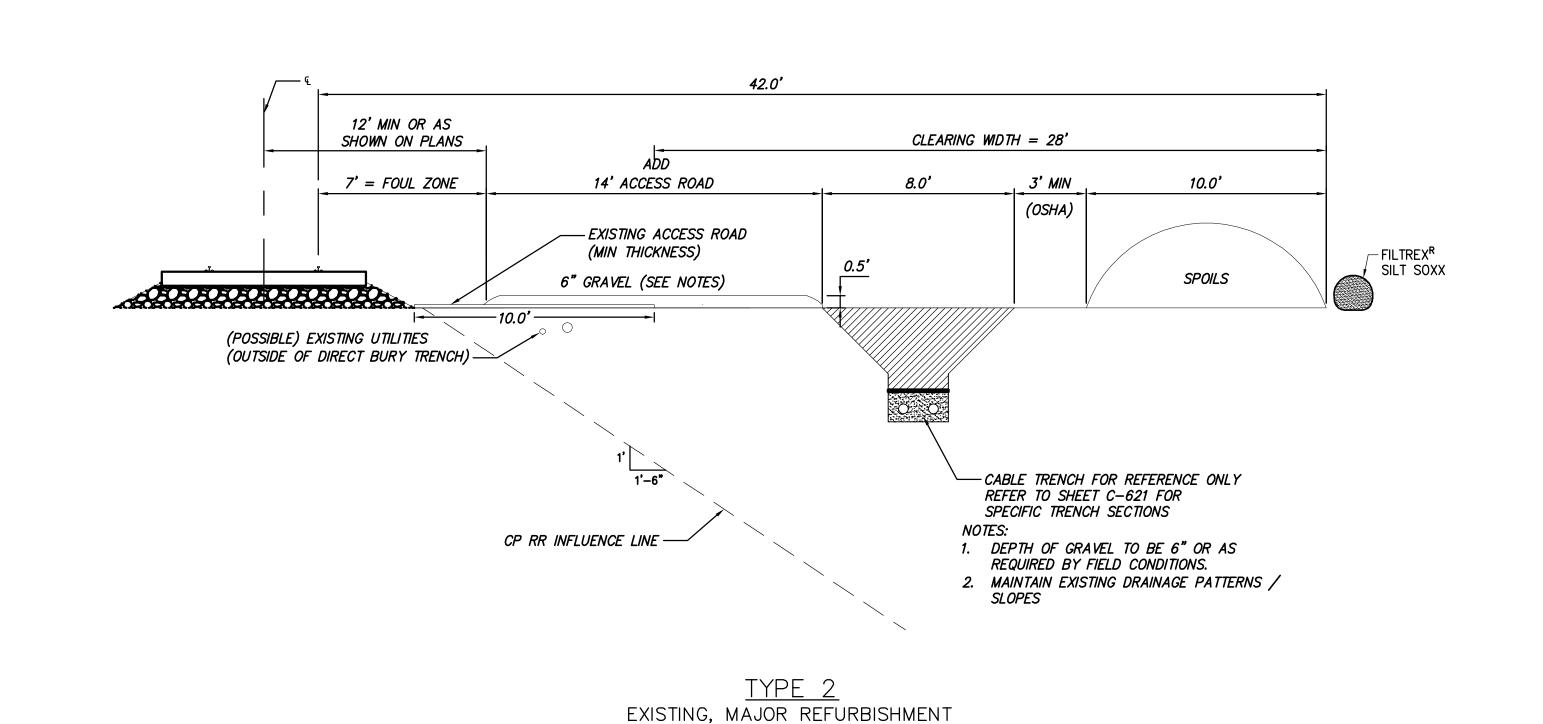
C-621

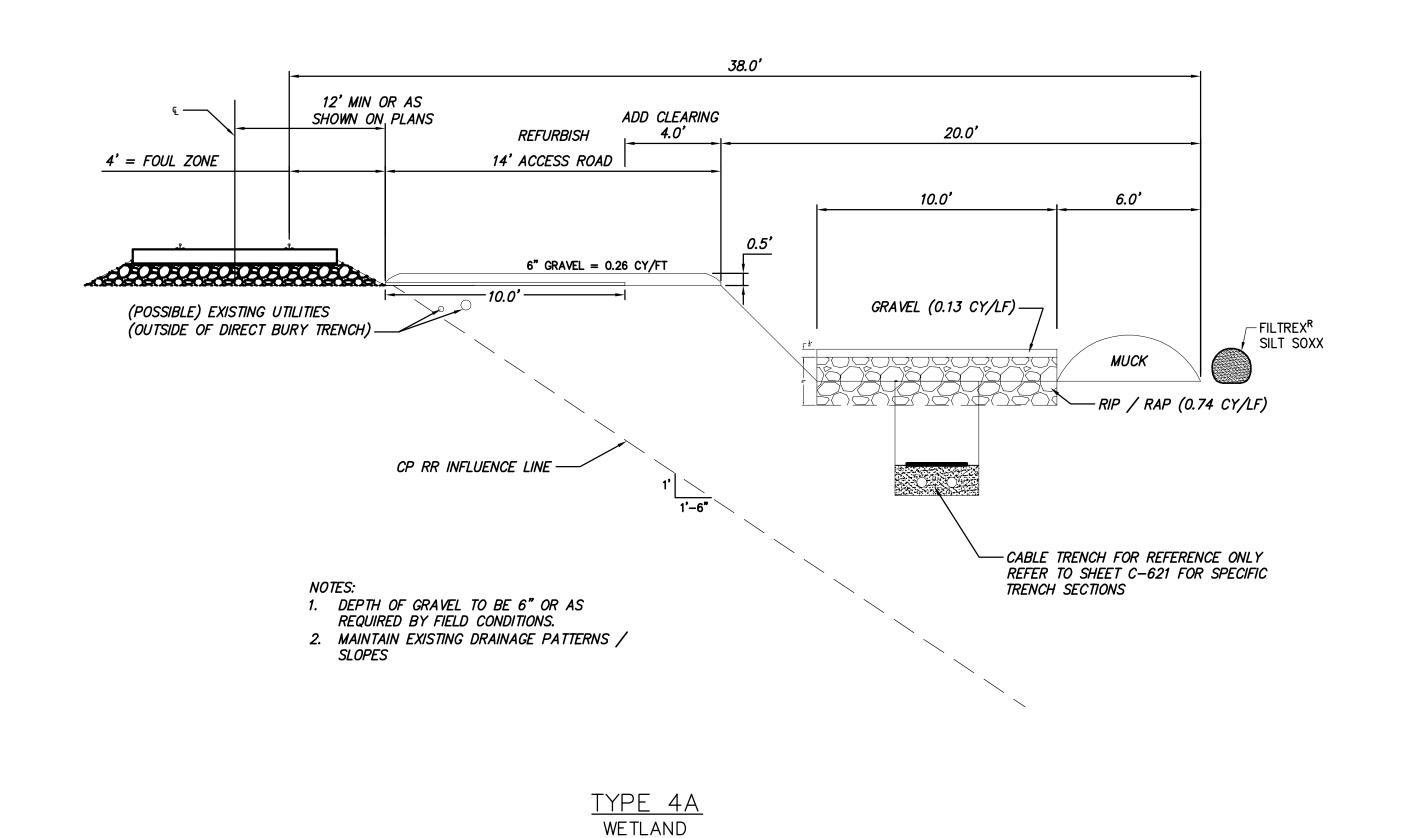
AS NOTED DATE X SH.NO.



EXISTING, MINOR REFURBISHMENT













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04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR	
				I TRICAL ACCESS ROAD CROSS SECTIONS
				TYPICAL ACCESS ROAD CROSS SECTIONS
				\mid SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
				CHAMPLAIN HUDSON POWER EXPRESS

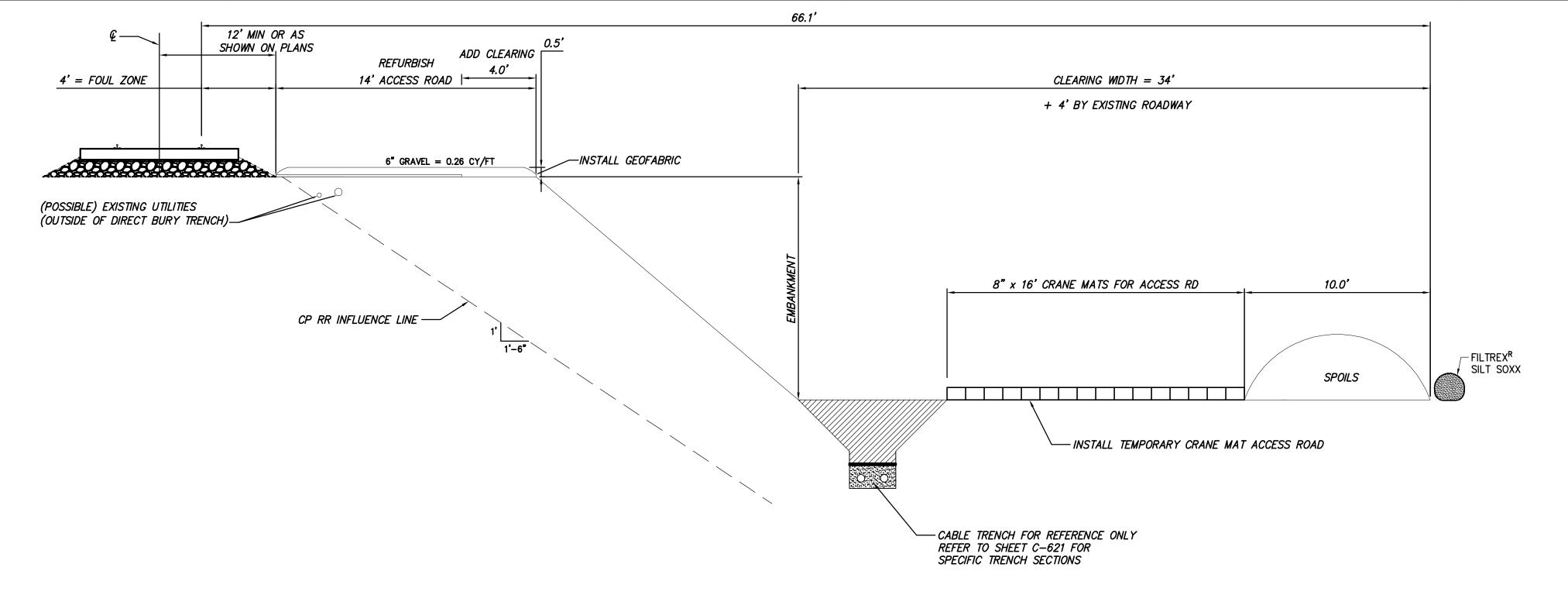
DATE SUBMITTAL / REVISION DESCRIPTION

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

C-622

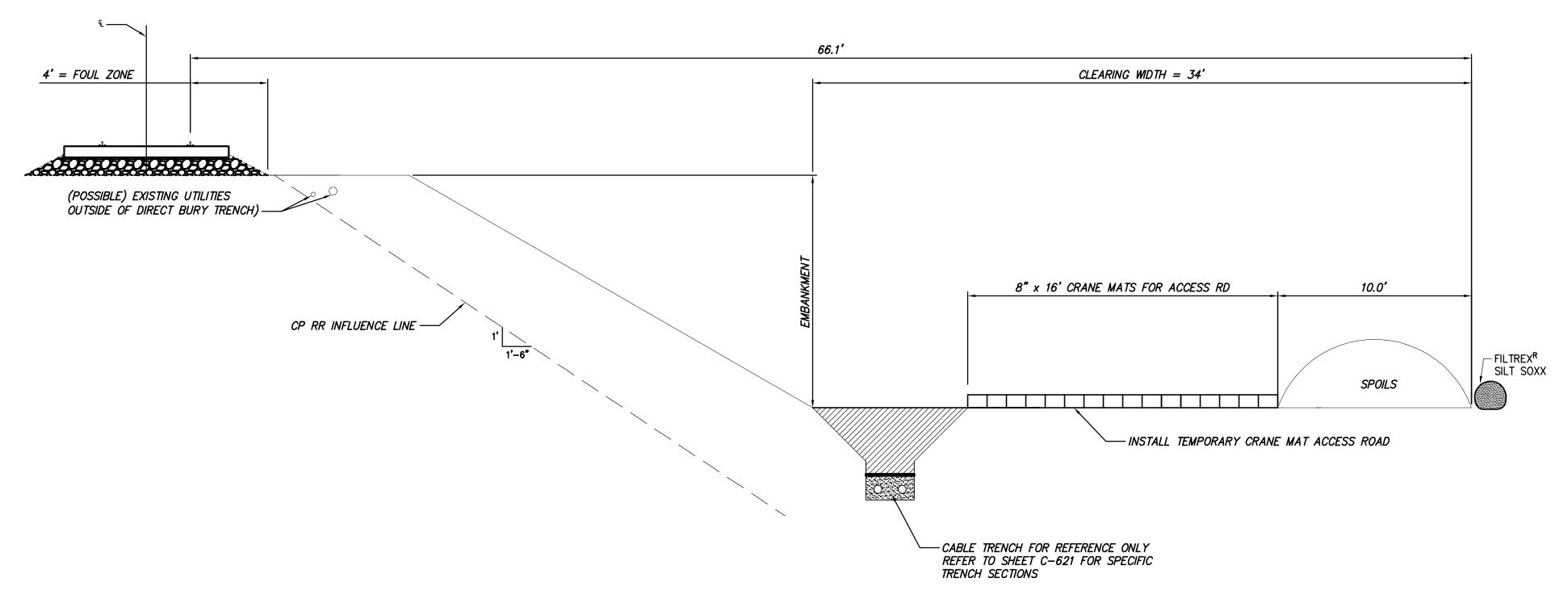
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AS NOTED DATE DB | APP | DRAWN BY: JJE | DESIGNED BY: JTM | APPROVED BY: JPR | REV. NO.



TYPE 4

REFURBISH ACCESS NEXT TO RAIL. BUILD TEMP ACCESS AT TOE OF SLOPE



TYPE 5 BUILD TEMP ACCESS AT TOE OF SLOPE







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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DF

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
TYPICAL ACCESS ROAD CROSS SECTIONS

KIEWIT PROJECT NO.
21162
CHA PROJECT NO.
066076
DRAWING NO.

C-623

AS NOTED DATE
X SH.NO.

O4/05/2023 FINAL EM&CP SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION

DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

TYPICAL DUCTBANK SECTION OVER EXISTING CP RAIL CULVERT OUTLET
NOT TO SCALE

NOTES:
1. DUCT BANK TO BE 2.0' MIN ABOVE OR BELOW EXISTING CULVERTS.

Package	Sheet Number	CP Rail Mile Post	Station	Utility	Size "d"	Exist. Ground Elev. (ft.)*	Invert (ft.)*	D (ft.)*	h (ft.)	L (ft.)
Package 3	C-175	36.12	31122+91 twin 1 31122+17 twin 2	Storm Drainage Pipe/Culvert	Twin 12'	319.7	272.9	46.7	26	33.9
Dackage 2	C 19E	22 AE	21265+00	Storm Drainage Dine/Culvert	60"	205.7	276.6	12.7	1 7	12

CHPE

Champlain Hudson

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	CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON	
	TYPICAL CULVERT SECTION (1 OF 3)	

KIEWIT PROJECT NO. CHA PROJECT NO. DRAWING NO.

C-624

AS NOTED DATE

X SH.NO.

JJE JPR DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

04/05/2023 FINAL EM&CP SUBMISSION DATE SUBMITTAL / REVISION DESCRIPTION TYPICAL DUCTBANK SECTION UNDER EXISTING CP RAIL CULVERT OUTLET
NOT TO SCALE

NOTES:
1. DUCT BANK TO BE 2.0' MIN ABOVE OR BELOW EXISTING CULVERTS.

Package	Sheet Number	CP Rail Mile Post	Station	Utility	Size "d"	Exist. Ground Elev. (ft.)*	Invert (ft.)*	D (ft.)*	h (ft.)*	L (ft.)
Package 3	C-105	56.45	30063+00	Storm Drainage Pipe/Culvert	60"	136.3	129.8	6.5	29.1	25
Package 3	C-114	53.92	30198+93	Storm Drainage Pipe/Culvert	28"	138.1	127.5	13.2	40.4	22.3
Package 3	C-120	52.27	30286+34	Storm Drainage Pipe/Culvert	144"	153.3	122.6	30.8	39.2	32
Package 3	C-122	51.65	30321+80	Storm Drainage Pipe/Culvert	36"	160.9	150.2	10.5	16.9	23
Package 3	C-127	50.11	30401+07	Storm Drainage Pipe/Culvert	144"	176.4	137.2	37.4	13.1	32
Package 3	C-145	45.08	30666+76	Storm Drainage Pipe/Culvert	48"	305.3	285.2	20.6	28.8	24
Package 3	C-161	40.27	30901+98	Storm Drainage Pipe/Culvert	120"	306.2	284.6	16.2	23.6	31.9
Package 3	C-170	37.56	31046+59	Storm Drainage Pipe/Culvert	52"	323	313.1	17	17.7	30
Package 3	C-184	33.55	31259+63	Storm Drainage Pipe/Culvert	36"X12"	299.9	292.2	3.7	2	23
Package 3	C-185	33.33	31271+40	Storm Drainage Pipe/Culvert	12"	313.6	311.3	3	2.7	21
Package 3	C-191	31.86	30354+81	Storm Drainage Pipe/Culvert	24"	279.8	271.5	8.3	43.9	31.5







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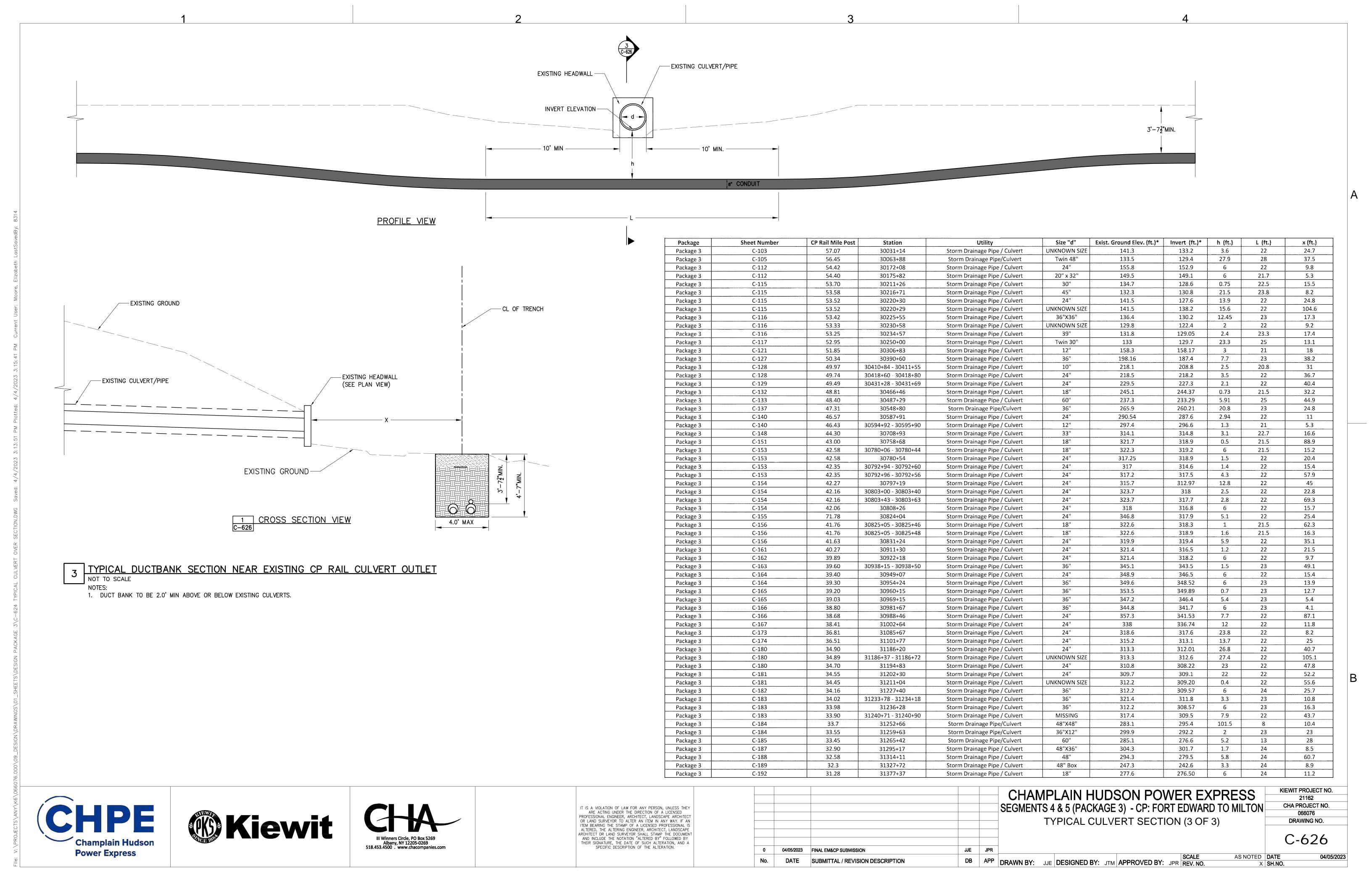
	ACKAGE 3) - CP: FORT EDWARD TO MILTON	
TYPICAL	CULVERT SECTION (2 OF 3)	

KIEWIT PROJECT NO. CHA PROJECT NO. DRAWING NO.

C-625

AS NOTED DATE
X SH.NO.

JJE JPR 0 04/05/2023 FINAL EM&CP SUBMISSION DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO. DATE SUBMITTAL / REVISION DESCRIPTION



ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION IF EXISTING THICKNESS IS GREATER

ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS.

APPLY STRAIGHT TACK COAT TO BETWEEN PAVEMENT SECTIONS NYSDOT ITEM407.0103 PRIVATE COMMERCIAL DRIVEWAYS TO UTILIZE COMMERCIAL DRIVEWAY SPECIFICATIONS AS SHOWN ON THE NYSDOT STANDARD SHEETS.

ASPHALT CONCRETE PAVEMENT DETAIL (PRIVATE DRIVEWAY)

SCALE: N.T.S.

2" ASPHALT CONCRETE TOP COURSE (NYSDOT ITEM NO. 404.096201) 2.5" ASPHALT CONCRETE BINDER COURSE (NYSDOT ITEM NO. 404.198901) -8" ASPHALT CONCRETE BASE COURSE (NYSDOT ITEM NO. 404.378901) -12" SUBBASE MATERIAL, MATCH EXISTING THICKNESS NYSDOT TYPE 2 (ITEM NO. 304.12)

-COMPACTED SUBGRADE 1. ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION IF

EXISTING THICKNESS IS GREATER 2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

3. APPLY STRAIGHT TACK COAT TO BETWEEN PAVEMENT SECTIONS NYSDOT ITEM407.0103

ASPHALT CONCRETE PAVEMENT (STATE/ROAD/COUNTY/TOWN)

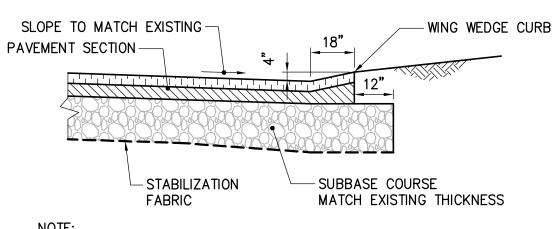
COAT AND INSTALL NEW TOP COURSE -— SAW CUT FULL DEPTH TOP COURSE PER — EXISTING PAVEMENT PAVEMENT DETAIL SUBBASE COLD MILL EXISTING PAVEMENT 24" MIN. BEYOND THE TRENCH WALL

1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

3 PAVEMENT TRANSITION DETAIL SCALE: N.T.S.

COLD MILL EXISTING PAVEMENT

TOP COURSE, TACK



1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

WING WEDGE CURB DETAIL

-SLOPE AS INDICATED ON PLAN -LAYER THICKNESS PER TABLE COMPACTED SUBGRADE -GEOTEXTILE FABRIC AND/OR GEOGRID, AS REQUIRED⁶

TEMPORARY ACCESS ROAD

TEMPORARY ACCESS ROAD SECTION^{1,2,3,4} MIRAFI RSI SERIES CBR⁵ TENSAR BX1200 GEOGRID⁶ UNSTABILIZED MIRAFI 180N GEOTEXTILE⁶ GEOTEXTILE⁶ 20 INCH RIP RAP⁷ 0.5 20 INCH RIP RAP⁷ + 6 INCH AGGREGATE + 4 INCH AGGREGATE (RS580I) 1.0 18 INCH AGGREGATE 12 INCH AGGREGATE 15 INCH AGGREGATE (RS280I) 12 INCH AGGREGATE 1.5 9 INCH AGGREGATE 9 INCH AGGREGATE (RS280I) 2.0 18 INCH AGGREGATE 11 INCH AGGREGATE 6 INCH AGGREGATE 9 INCH AGGREGATE (RS280I) 3.0+ 15 INCH AGGREGATE 8 INCH AGGREGATE 6 INCH AGGREGATE 9 INCH AGGREGATE (RS280I)

TEMPORARY ACCESS ROAD SECTIONS PER KIEWIT ENGINEERING (NY) CORP.

AGGREGATE SHALL BE NYSDOT TYPE 2 CRUSHED AGGREGATE OR APPROVED ALTERNATIVE.

SCALE: N.T.S.

DESIGN CONSIDERS 1,000 PASSES OF MAXIMUM 22-KIP AXLE LOAD AND A DESIGN RUT DEPTH OF 3 INCHES. ADDITIONAL AXLE PASSES, HEAVIER AXLE LOADS, AND DETERIORATED SUBGRADE CONDITIONS MAY REQUIRE THICKER AGGREGATE SECTIONS OR ADDITIONAL MAINTENANCE. ⁴ ALTERNATE TEMPORARY ACCESS ROAD DESIGNS MAY BE PROVIDED BY KIEWIT ENGINEERING, AS REQUIRED, BASED ON FIELD CONDITIONS AND TRAFFIC LOADING.

5 ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER OR ALTERNATIVE METHOD APPROVED BY GEOTECHNICAL ENGINEER OF RECORD (EOR). CBR OF IN-SITU SOIL MAY VARY SEASONALLY DUE TO FREEZE/THAW AND BASED ON MOISTURE CONDITIONS. GEOGRID AND GEOTEXTILE

A GEOGRID AND GEOTEXTILES SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS.

B SPECIFIED GEOTEXTILE OR GEOGRID MAY BE REPLACED BY EQUIVALENT MATERIAL APPROVED BY EOR.

^C GEOTEXTILE IS REQUIRED IN REGULATED WETLANDS AND AGRICULTURAL LANDS. D GEOTEXTILE SEPARATOR FABRIC IS REQUIRED BENEATH GEOGRID ON COHESIVE SUBGRADE

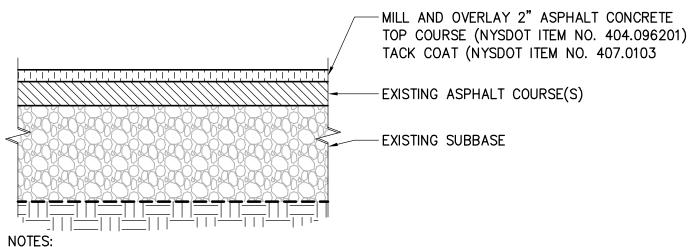
A RIP RAP SHALL BE NYSDOT LIGHT STONE FILL OR APPROVED ALTERNATIVE. B A LAYER OF #57 STONE IS RECOMMENDED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE GEOTEXTILE FABRIC WHERE RIP RAP IS USED.

-SLOPE AS INDICATED ON PLAN -8" SUBBASE — COMPACTED SUBGRADE - STABILIZATION FABRIC

6 GRAVEL PAVEMENT SCALE: N.T.S.

04/05/2023 FINAL EM&CP SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION



1. APPLY TACKCOAT TO MILLED SURFACE PRIOR TO PLACING ASPHALT

CONCRETE TOP COURSE.

2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

MILL AND OVERLAYASPHALT CONCRETE PAVEMENT DETAIL

-PIPE > 24" = 0.D. + 48"PIPE < 24" = 0.D. + 2DBACKFILL WITH SUITABLE MATERIAL (NYS DOT ITEM NO. 203.03) PIPE ZONE BACKFILL (NYS DOT SUBASE ITEM NO. 203.07) ✓ SLOPE TRENCH 1/2 0.D. + 12" MIN. AS REQ'D 6" PIPE ZONE BEDDING UNDISTURBED MATERIAL-

> 1. WHERE IDENTIFIED ON PLANS, CULVERT REPLACEMENTS AND INSTALLATION OF TEMPORARY CULVERTS TO BE COMPLETED IN ACCORDANCE WITH NYSDOT

2. TEMPORARY CULVERTS TO BE PLACED AT ELEVATION TO MAINTAIN EXISTING

CULVERT REPLACEMENT AND/OR TEMPORARY CULVERTS SCALE: N.T.S.







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CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON SURFACE RESTORATION DETAILS

DB APP DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

JJE JPR

KIEWIT PROJECT NO. CHA PROJECT NO. DRAWING NO.

C-631

AS NOTED **DATE**

X SH.NO.

21162

066076