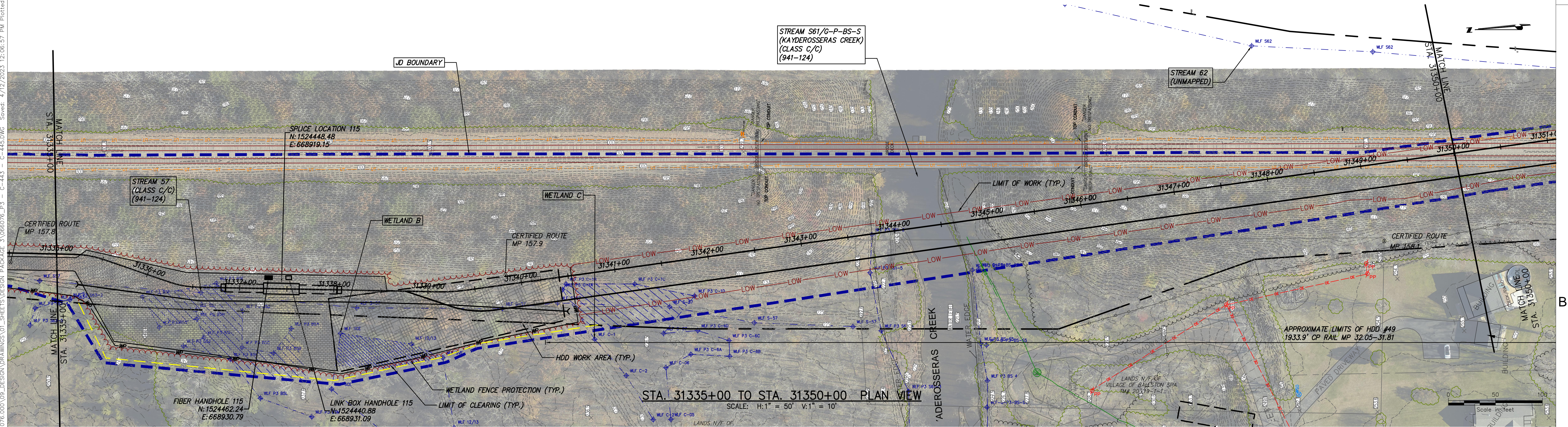




STA. 31320+00 TO STA. 31335+00 PLAN VIEW
SCALE: 1" = 50'




STA. 31335+00 TO STA. 31350+00 PLAN VIEW
SCALE: H: 1" = 50' V: 1" = 10'



Champlain Hudson
Power Express



Kiewit



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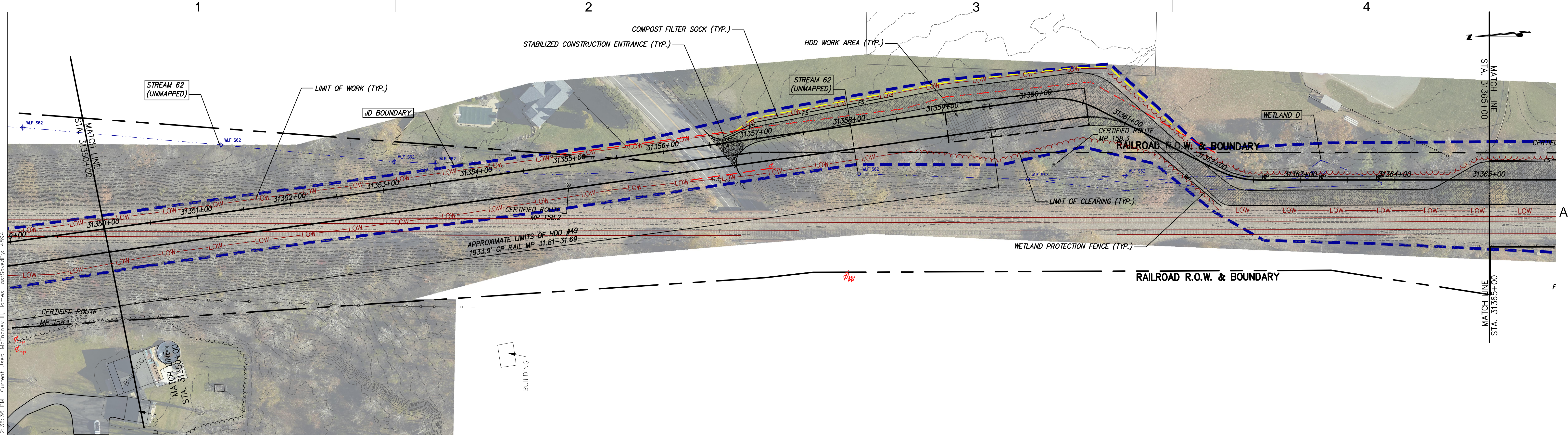
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CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
STA. 31320+00 TO STA. 31350+00 EROSION AND SEDIMENT CONTROL PLAN

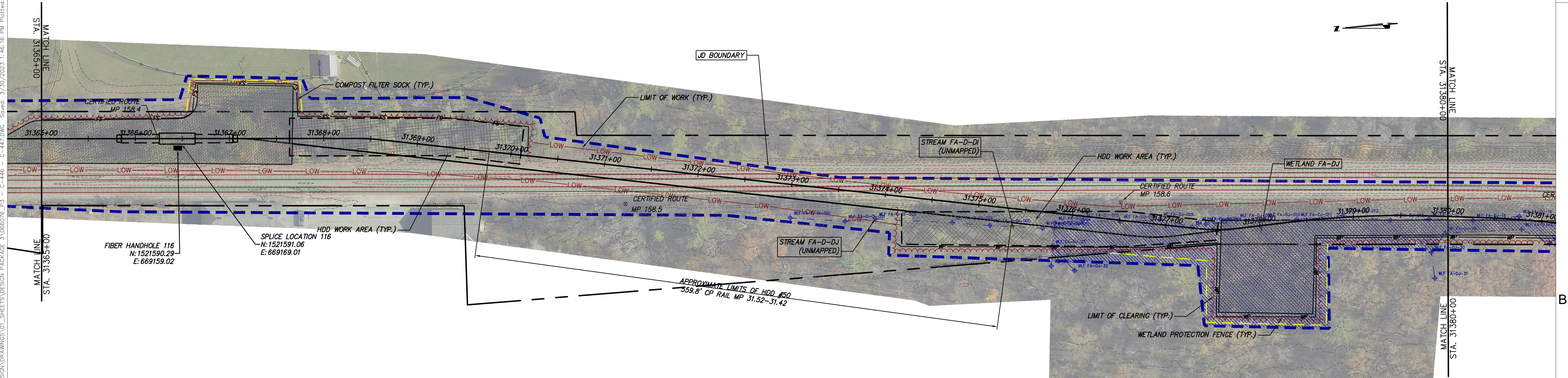
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REV. NO. X SH.NO.

KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 068076
DRAWING NO. C-445




DATE 04/05/2023



STA. 31350+00 TO STA. 31365+00 PLAN VIEW
SCALE: 1" = 50'



STA. 31365+00 TO STA. 31380+00 PLAN VIEW
SCALE: H: 1" = 50' V: 1" = 10'



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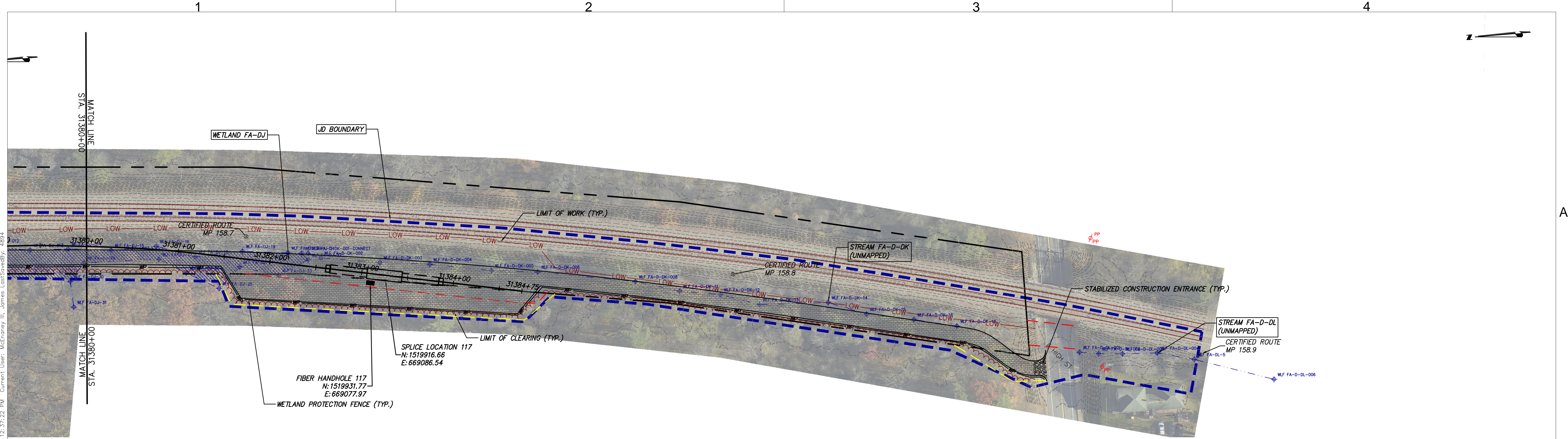
CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
STA. 31350+00 TO STA. 31380+00 EROSION AND
SEDIMENT CONTROL PLAN

KIEWIT PROJECT NO.
21162
CHA PROJECT NO.
066076
DRAWING NO.
C-446

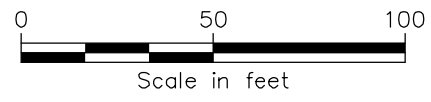
DRAWN BY: JJE
DESIGNED BY: JTM
APPROVED BY: JPR


SCALE AS NOTED
REV. NO. X

DATE 04/05/2023
SH.NO.




STA. 31380+00 TO STA. 31384+30 PLAN VIEW
SCALE: 1" = 50'






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Power Express



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0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
STA. 31380+00 TO STA. 31384+30 EROSION AND
SEDIMENT CONTROL PLAN

DRAWN BY: JJE

DESIGNED BY: JTM

APPROVED BY: JPR

SCALE AS NOTED
REV. NO. X

KIEWIT PROJECT NO.
21162

CHA PROJECT NO.
066076

DRAWING NO.
C-447

DATE
04/05/2023

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NOTES:

1. ALL TRAFFIC CONTROL AND WORK AREA PROTECTIONS DEVICES SHALL BE FABRICATED AND INSTALLED IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION (FHWA) MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS, LATEST EDITION, EXCEPT AS NOTED AND AS SPECIFIED IN THIS EM&CP.
2. ALL TEMPORARY TRAFFIC CONTROL AND WORK AREA PROTECTION DEVICES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED ON THE CONTRACT DRAWINGS AND APPROVED BY THE ENGINEER PRIOR TO COMMENCING WORK AND WILL BE INCLUDED IN THIS EM&CP.
3. ALL MAINTENANCE AND PROTECTIONS OF TRAFFIC WORK SHALL CONFORM TO THE CONTRACT DRAWINGS, MAINTENANCE AND PROTECTION OF TRAFFIC SCHEMES SHOWN ON THE CONTRACT DRAWINGS SHALL NOT BE CHANGED BY THE CONTRACTOR WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER AND WILL BE INCLUDED IN THIS EM&CP.
4. THE TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION DEVICE LOCATIONS SHOWN ON THE CONTRACT DRAWINGS ARE SCHEMATIC EXCEPT AS NOTED. LOCATION OF TRAFFIC CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
5. ALL CLOSURES SHALL BE COORDINATED WITH THE ENGINEER. A SCHEDULE OF WORK SHALL BE SUBMITTED AT LEAST TWO WEEKS IN ADVANCE FOR APPROVAL BY THE ENGINEER.
6. THERE MAY BE ONGOING CONSTRUCTION CONTRACTS WITHIN THE VICINITY OF THE WORK AREA. DO NOT MOVE, MODIFY, OR RELOCATE ANY ITEM ASSOCIATED WITH THESE CONTRACTS WITHOUT PROPER APPROVAL OF AND COORDINATION WITH THE ENGINEER.
7. PERFORM WORK IN SUCH A MANNER AND SEQUENCE AS TO INTERFERE AS LITTLE AS POSSIBLE WITH THE PASSAGE OF VEHICLES, PEDESTRIANS, AND OTHER KINDS OF PUBLIC TRAFFIC.
8. ALL TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION SIGN SUPPORTS AND MOUNTING SHALL BE IN CONFORMANCE WITH NYSDOT STANDARD SHEETS AND STANDARD SPECIFICATIONS.
9. ALL CONSTRUCTION SIGNS SHALL BE COVERED WITH THICK PLASTIC WHEN THE WORK THEY ARE INTENDED FOR IS NOT IN PROGRESS.
10. ALL TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF THE WORK TO WHICH THEY APPLY, AND SHALL BE MAINTAINED BY THE CONTRACTOR THEREAFTER. THE DEVICES SHALL REMAIN IN PLACE AS LONG AS THEY ARE APPLICABLE.
11. TRAFFIC CONTROL DEVICES SHALL NOT BE PLACED AT ANY LOCATIONS WHERE THEY MAY OBSCURE OR INTERFERE WITH THE MOTORIST VIEW OF APPROACHING, MERGING, OR INTERSECTING TRAFFIC; OBSTRUCT OTHER TEMPORARY OR PERMANENT TRAFFIC CONTROL DEVICES WHICH ARE STILL APPLICABLE TO ROADWAY CONDITIONS; MISLEAD OR MISDIRECT MOTORISTS, OR ARE BLOCKED BY OTHER TEMPORARY OR PERMANENT OBJECTS.
12. ALL EXISTING ROADWAY ITEMS SUCH AS GUIDE RAILS, PAVEMENT MARKINGS, CURBS, SIGNALS AND SIGNS DAMAGED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.
13. TRAFFIC LANES AND OTHER AREAS CLOSED BY THE CONTRACTOR DURING PERMITTED WORK HOURS SHALL BE CLEARED OF ALL MATERIAL, EQUIPMENT, AND DEBRIS, AND SAFELY REOPENED TO TRAFFIC BY THE END OF THE WORK PERIOD UNLESS OTHERWISE INDICATED ON THE CONTRACT DRAWINGS.
14. IF THE CONTRACTOR REQUIRES A LANE CLOSURE TO PERFORM OPERATIONS, THEY MAY DO SO WITH THE APPROVAL OF THE ENGINEER. ANY LANE CLOSURES WILL BE SHORT TERM AND IN ACCORDANCE WITH NYS STANDARD SHEETS UNLESS OTHERWISE SHOWN IN CONTRACT PLANS.
15. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.
16. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
17. WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK IS 2 MILES TO 5 MILES, A SUPPLEMENTAL DISTANCE PLAQUE (W7-3a) SHOULD BE USED WITH THE SHOULDER WORK SIGN (W21-5).
18. THE ROAD WORK NEXT XX MILES SIGN (G20-1) MAY BE USED INSTEAD OF THE ROAD WORK AHEAD SIGN (W20-1) IF WORK LOCATIONS OCCUR OVER A DISTANCE OF MORE THAN 2 MILES.
19. ALL ADDITIONAL TEMPORARY SIGNAGE, NOT COVERED IN FHWA MUTCD, SHALL COMPLY WITH NYS SUPPLEMENT TO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

20. DURING NON-WORK HOURS, LEAVE DRUMS AND TCB IN PLACE, OR AS DIRECTED BY THE ENGINEER.
21. DURING WINTER MORATORIUM, COVER EXCAVATION WITH TEMPORARY ASPHALT AND OPEN ALL LANES TO TRAFFIC.
22. THE WZTC DETAILS CONTAINED IN THE CONTRACT PLANS SUPPLEMENT THE CURRENT NYSDOT STANDARD SHEETS. REFERENCE SHALL BE MADE TO THE APPLICABLE NYSDOT STANDARD SHEETS FOR ALL NOTES AND TABLES. THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYSDOT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
23. MAINTAIN SAFE AND ADEQUATE ACCESS FOR INTERSECTING ROADWAYS, HOMES, AND BUSINESSES, AT ALL TIMES, TO THE SATISFACTION OF THE ENGINEER.
24. ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES. UNLESS OTHER AGREEMENTS SUITABLE TO THE PROPERTY OWNERS CAN BE MADE, PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ANY ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE, OR AS DIRECTED BY THE ENGINEER.
25. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING HOLIDAY WORK RESTRICTIONS ARE APPLICABLE TO THIS PROJECT. DURING THESE HOLIDAY PERIODS, THE CONTRACTOR WILL NOT BE ALLOWED TO PERFORM ANY WORK THAT WILL BE DISRUPTIVE TO TRAFFIC, INCLUDING BUT NOT LIMITED TO LANE CLOSURES. LANE CLOSURES WILL NOT BE PERMITTED DURING THE FOLLOWING STATE RECOGNIZED HOLIDAYS:

- 1) MEMORIAL DAY

2) INDEPENDENCE DAY

3) LABOR DAY

4) THANKSGIVING DAY

5) CHRISTMAS DAY
- MONDAY, MAY 29, 2023

- TUESDAY, JULY 4, 2023

- MONDAY, SEPTEMBER 4, 2023

- THURSDAY, NOVEMBER 23, 2023

- MONDAY, DECEMBER 25, 2023
- 6) NEW YEAR'S DAY

7) MEMORIAL DAY

8) INDEPENDENCE DAY

9) LABOR DAY

10) THANKSGIVING DAY

11) CHRISTMAS DAY
- MONDAY, JANUARY 1, 2024

- MONDAY, MAY 27, 2024

- THURSDAY, JULY 4, 2024

- MONDAY, SEPTEMBER 2, 2024

- THURSDAY, NOVEMBER 28, 2024

- WEDNESDAY, DECEMBER 25, 2024
- 12) NEW YEAR'S DAY

13) MEMORIAL DAY

14) INDEPENDENCE DAY

15) LABOR DAY

16) THANKSGIVING DAY

17) CHRISTMAS DAY
- WEDNESDAY, JANUARY 1, 2025

- MONDAY, MAY 26, 2025

- FRIDAY, JULY 4, 2025

- MONDAY, SEPTEMBER 1, 2025

- THURSDAY, NOVEMBER 27, 2025

- THURSDAY, DECEMBER 25, 2025

26. LANE CLOSURES SHALL BE SUSPENDED AS FOLLOWS:

- BEGINNING 6AM, FRIDAY MAY 26, 2023 AND ENDING 6AM, TUESDAY MAY 30, 2023

- BEGINNING 6AM, SATURDAY JULY 1, 2023 AND ENDING 6AM, WEDNESDAY JULY 5, 2023

- BEGINNING 6AM, FRIDAY SEPTEMBER 1, 2023 AND ENDING 6AM, TUESDAY SEPTEMBER 5, 2023

- BEGINNING 6AM, WEDNESDAY NOVEMBER 22, 2023 AND ENDING 6AM, MONDAY NOVEMBER 27, 2023

- BEGINNING 6AM, FRIDAY DECEMBER 22, 2023 AND ENDING 6AM, TUESDAY DECEMBER 26, 2023
- BEGINNING 6AM, SUNDAY DECEMBER 30, 2023 AND ENDING 6AM, WEDNESDAY JANUARY 3, 2024

- BEGINNING 6AM, FRIDAY MAY 24, 2024 AND ENDING 6AM, TUESDAY MAY 28, 2024

- BEGINNING 6AM, MONDAY JULY 1, 2024 AND ENDING 6AM, FRIDAY JULY 5, 2024

- BEGINNING 6AM, FRIDAY AUGUST 30, 2024 AND ENDING 6AM, TUESDAY SEPTEMBER 3, 2024

- BEGINNING 6AM, WEDNESDAY NOVEMBER 27, 2024 AND ENDING 6AM, MONDAY DECEMBER 2, 2024

- BEGINNING 6AM, FRIDAY DECEMBER 20, 2024 AND ENDING 6AM, THURSDAY DECEMBER 26, 2024
- BEGINNING 6AM, TUESDAY DECEMBER 30, 2024 AND ENDING 6AM, FRIDAY JANUARY 3, 2025

- BEGINNING 6AM, FRIDAY MAY 23, 2025 AND ENDING 6AM, TUESDAY MAY 27, 2025

- BEGINNING 6AM, THURSDAY JULY 3, 2025 AND ENDING 6AM, MONDAY JULY 7, 2025


- BEGINNING 6AM, FRIDAY AUGUST 29, 2025 AND ENDING 6AM, TUESDAY SEPTEMBER 2, 2025

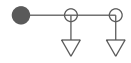
- BEGINNING 6AM, WEDNESDAY NOVEMBER 26, 2025 AND ENDING 6AM, MONDAY DECEMBER 1, 2025


- BEGINNING 6AM, WEDNESDAY DECEMBER 24, 2025 AND ENDING 6AM, MONDAY DECEMBER 29, 2025


27. DURING WINTER STORM EVENTS, NO WORK WITHIN THE WORK AREA OR RIGHT-OF-WAY IS PERMITTED TO ALLOW FOR SNOW REMOVAL, PLOWING ACTIVITIES.
28. ANY EARTHWORK DONE BETWEEN NOVEMBER 1ST AND APRIL 1ST MUST CONFORM TO NYSDOT STANDARD SPECIFICATION SECTION 203-1.01 P.
29. ALL UNPROTECTED OPEN TRENCH EXCAVATIONS SHALL BE BACKFILLED OR COVERED BY A STEEL PLATE (HS-20 LOAD RATED) AT THE END OF EACH WORK DAY, OR AS DIRECTED BY THE ENGINEER.
30. PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR SHALL COORDINATE WITH CP RAILROAD FOR RAILROAD FLAGGING FOR ALL WORK IN THE VICINITY OF ANY RAILROAD GRADE CROSSING AND WHENEVER WITHIN THE RAILROAD ROW.

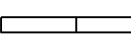
LEGEND


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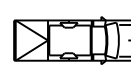
TEMPORARY SIGN
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TEMPORARY TRAFFIC SIGNAL UNIT
- 

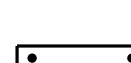
WORK ZONE
- 


DRUM
- 


TEMPORARY CONCRETE BARRIER (TCB)
- 


FLAGGER
- 

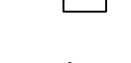
WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR
- 

FLASH UNIT (LEFT ARROW)
- 

FLASH UNIT (CAUTION MODE)
- 

TYPE III BARRICADE
- 

TRAILER UNIT
- 

WARNING LIGHT ON SIGN
- 

TEMPORARY TRAFFIC FLOW ARROW

ABBREVIATIONS

AADT	AVERAGE ANNUAL DAILY TRAFFIC
CR	COUNTY ROUTE
DHV	DIRECTIONAL HOURLY VOLUME
FASU	FLASHING ARROW SIGN UNIT
FHWA	FEDERAL HIGHWAY ADMINISTRATION
M.P.	MILE POST
MAX.	MAXIMUM
MIN.	MINIMUM
MPH	MILES PER HOUR
MUTCD	MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
NYS	NEW YORK STATE
NYSDOT	NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RTE	ROUTE
TCB	TEMPORARY CONCRETE BARRIER
STA.	STATION

A

B



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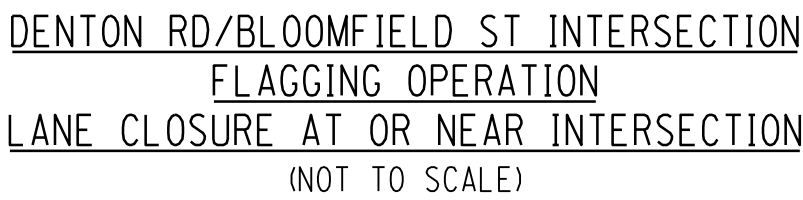
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LEGEND AND ABBREVIATIONS

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DRAWING NO.

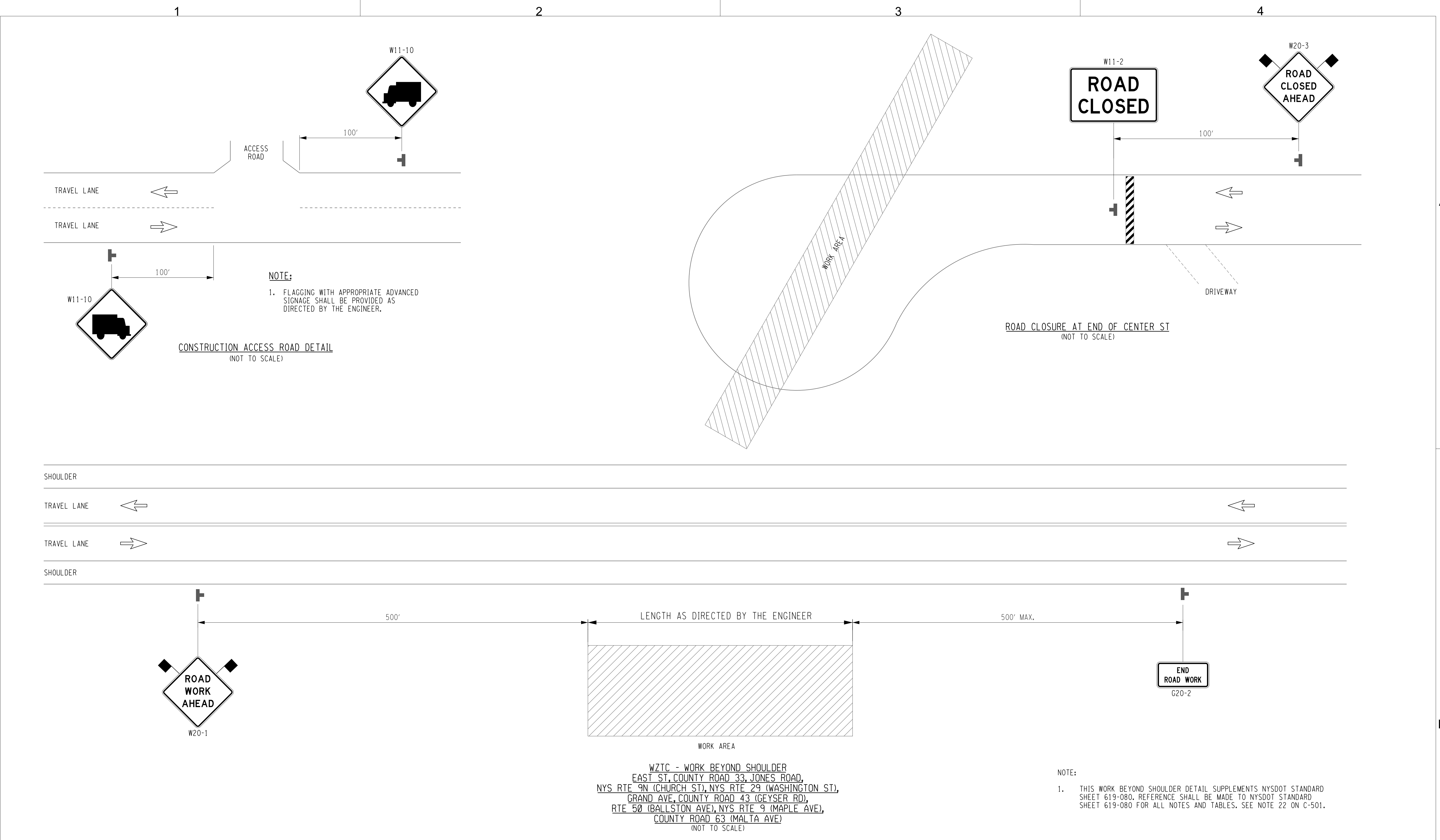
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DRAWN BY: JAH
DESIGNED BY: JPS
APPROVED BY: MDH
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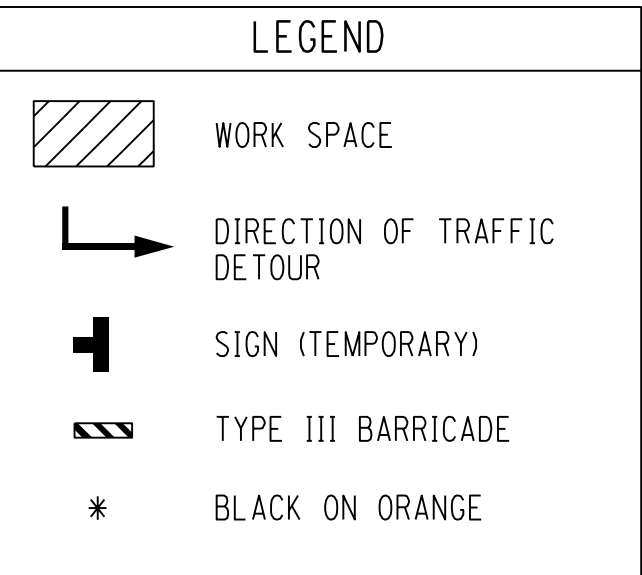
CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON

WORK ZONE TRAFFIC CONTROL DETAILS
SHEET 4 OF 7

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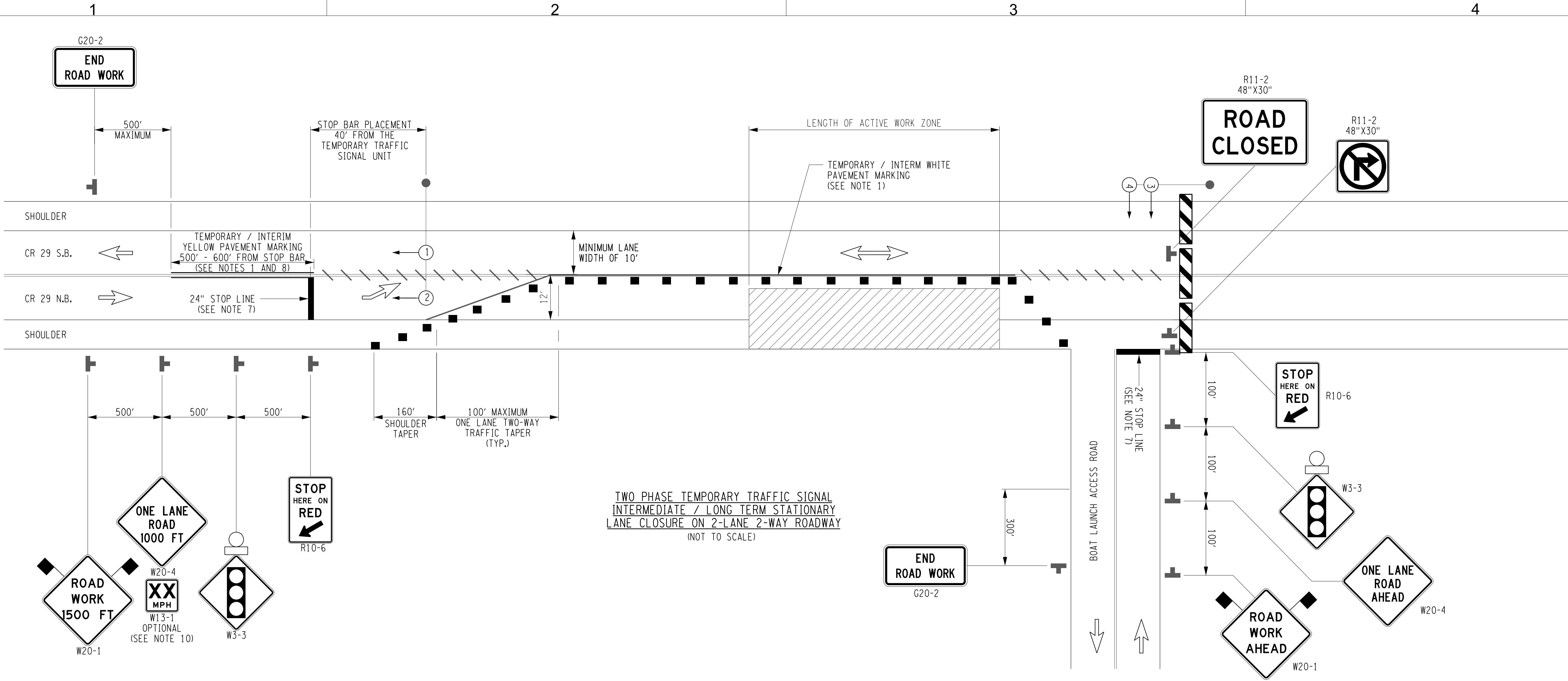
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DRAWING NO. C-506		DATE 04/05/2023	
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DATE	04/05/2023
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NOTE: TEMPORARY SIGNAL TIMING SHALL BE DETERMINED BY THE REGIONAL TRAFFIC OFFICE, COUNTY, OR MUNICIPALITY

OPERATIONAL SCHEDULE FOR TEMPORARY TRAFFIC SIGNALS				
PHASE	FACE	1	2	3
1		G	G	R
CLEARANCE		Y	Y	R
ALL RED		R	R	R
2		R	R	G
CLEARANCE		R	R	Y
ALL RED		R	R	R
EMERGENCY FLASH		FR	FR	FR

NOTES:

1. FOR LONG DURATION LANE CLOSURES, TEMPORARY / INTERIM PAVEMENT MARKINGS SHALL BE USED IN ACCORDANCE TO THE CONTRACT DOCUMENTS. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.

2. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.

3. REFER TO MUTCD FOR ADDITIONAL TRAFFIC SIGNAL REQUIREMENTS.

4. DETECTORS SHALL BE INSTALLED ON ALL APPROACH LANES AND ALL PHASES SHALL BE ACTUATED. DURATION OF RED CLEARANCE INTERVALS SHALL BE ADEQUATE TO CLEAR THE ONE-LANE SECTION OF CONFLICTING TRAFFIC.

5. THE TRAFFIC SIGNAL CYCLE SHALL REST IN RED. WHEN THE SIGNAL IS CHANGED TO THE FLASHING MODE, EITHER MANUALLY OR AUTOMATICALLY, RED SIGNAL INDICATIONS SHALL BE FLASHED TO ALL APPROACHES.

6. STOP LINES SHALL BE INSTALLED WITH TEMPORARY TRAFFIC SIGNALS FOR INTERMEDIATE AND LONG-TERM CLOSURES. EXISTING, CONFLICTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKINGS, BETWEEN THE ACTIVITY AREA AND THE STOP LINE, SHALL BE REMOVED. AFTER THE TEMPORARY TRAFFIC SIGNAL IS REMOVED, THE STOP LINES AND ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND PERMANENT PAVEMENT MARKINGS RESTORED.
7. WHERE NO-PASSING LINES ARE NOT ALREADY IN PLACE, THEY SHALL BE ADDED FOR A DISTANCE OF 500'-600' FROM THE STOP BAR. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.

8. ADJUSTMENTS IN THE LOCATION OF ADVANCED WARNING SIGNS SHOULD BE MADE TO ACCOMMODATE THE HORIZONTAL AND VERTICAL ALIGNMENT OF THE ROADWAY, RECOGNIZING THAT THE DISTANCES SHOWN FOR SIGN SPACING ARE MINIMUMS. ADJUSTMENTS IN THE HEIGHT OF THE SIGNAL HEADS SHALL BE MADE AS NEEDED TO CONFORM TO THE VERTICAL ALIGNMENT.

9. THE SUPPORTS FOR TEMPORARY TRAFFIC CONTROL SIGNALS SHALL NOT ENCROACH INTO THE MINIMUM REQUIRED WIDTH OF A "PEDESTRIAN ACCESS ROUTE" OF 4' OR AN "ALTERNATE CIRCULATION PATH" OF 3'.

10. THE ENGINEER MAY REQUIRE THE USE OF AN ADVISORY SPEED PLAQUE (W13-1) TO SUPPLEMENT A WARNING SIGN. THE PLAQUE WILL BE USED TO INDICATE AN ADVISORY SPEED FOR THE WORK ZONE CONDITION, (IE. NARROW LANES, BUMPS, POOR ROADWAY SURFACE, LOW OR NO SHOULDER, DROP-OFFS, GEOMETRIC CONSTRAINTS, AND/OR POOR SIGHT CONDITIONS).

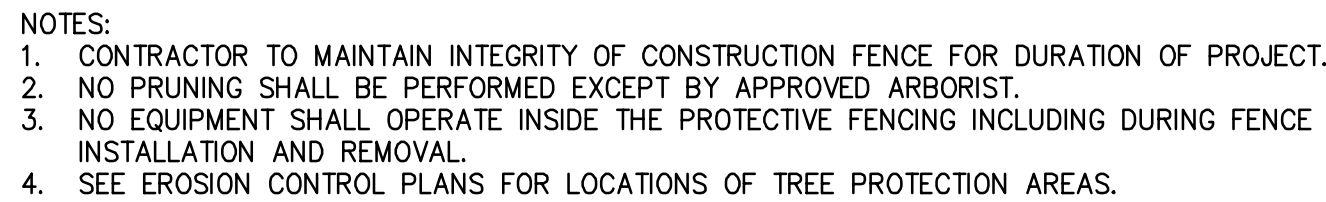
11. IF EXISTING PAVEMENT CANNOT ACCOMMODATE A 10 FT LANE, TEMPORARY PAVEMENT MUST BE PLACED TO MAINTAIN THE MINIMUM OF 10 FT AS DIRECTED BY THE ENGINEER.



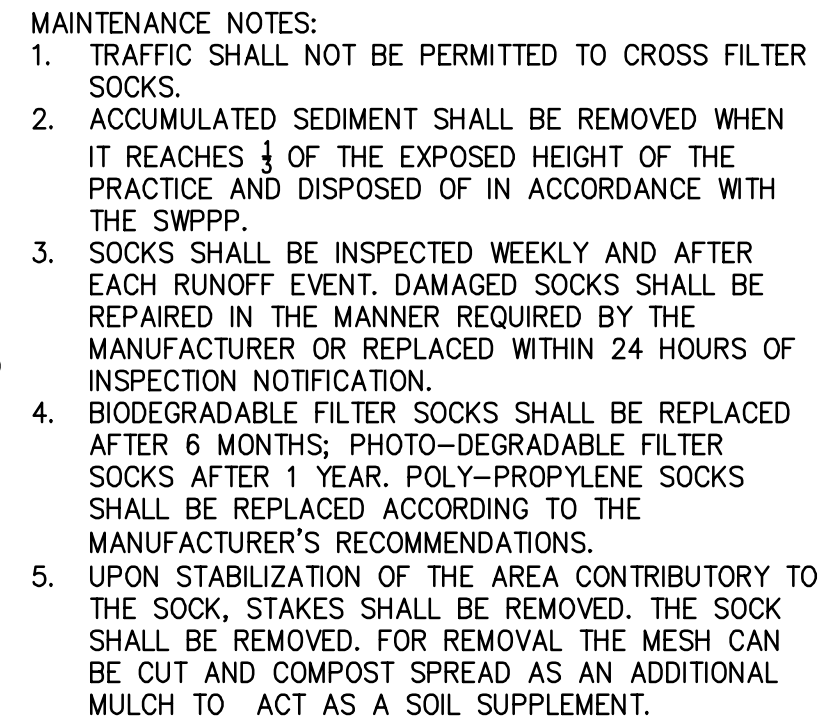
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0	04/05/2023	FINAL EM&CP SUBMISSION	MH	JS	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

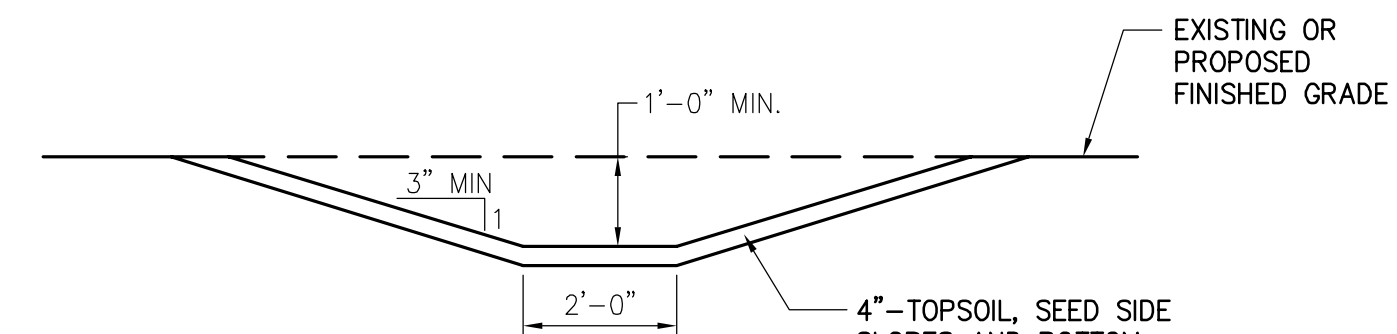
CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON				KIEWIT PROJECT NO. 21162	
				CHA PROJECT NO. 066076	
				DRAWING NO. C-509	
WORK ZONE TRAFFIC CONTROL DETAILS SHEET 7 OF 7					
DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV. NO.	DATE SH.NO.	04/05/2023 OF XXX



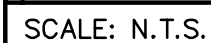
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SCALE: N.T.S.



SCALE: N.T.S.

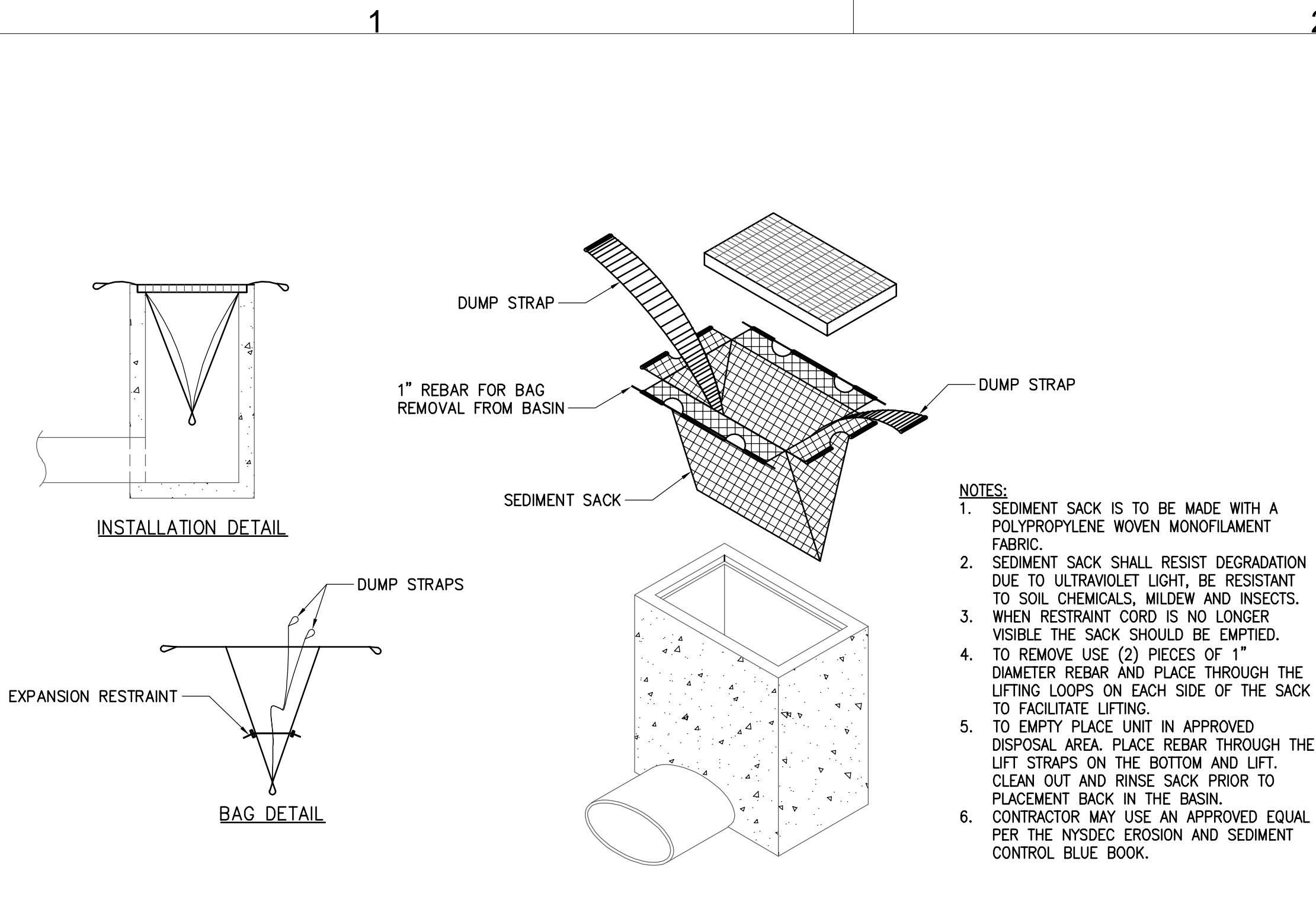


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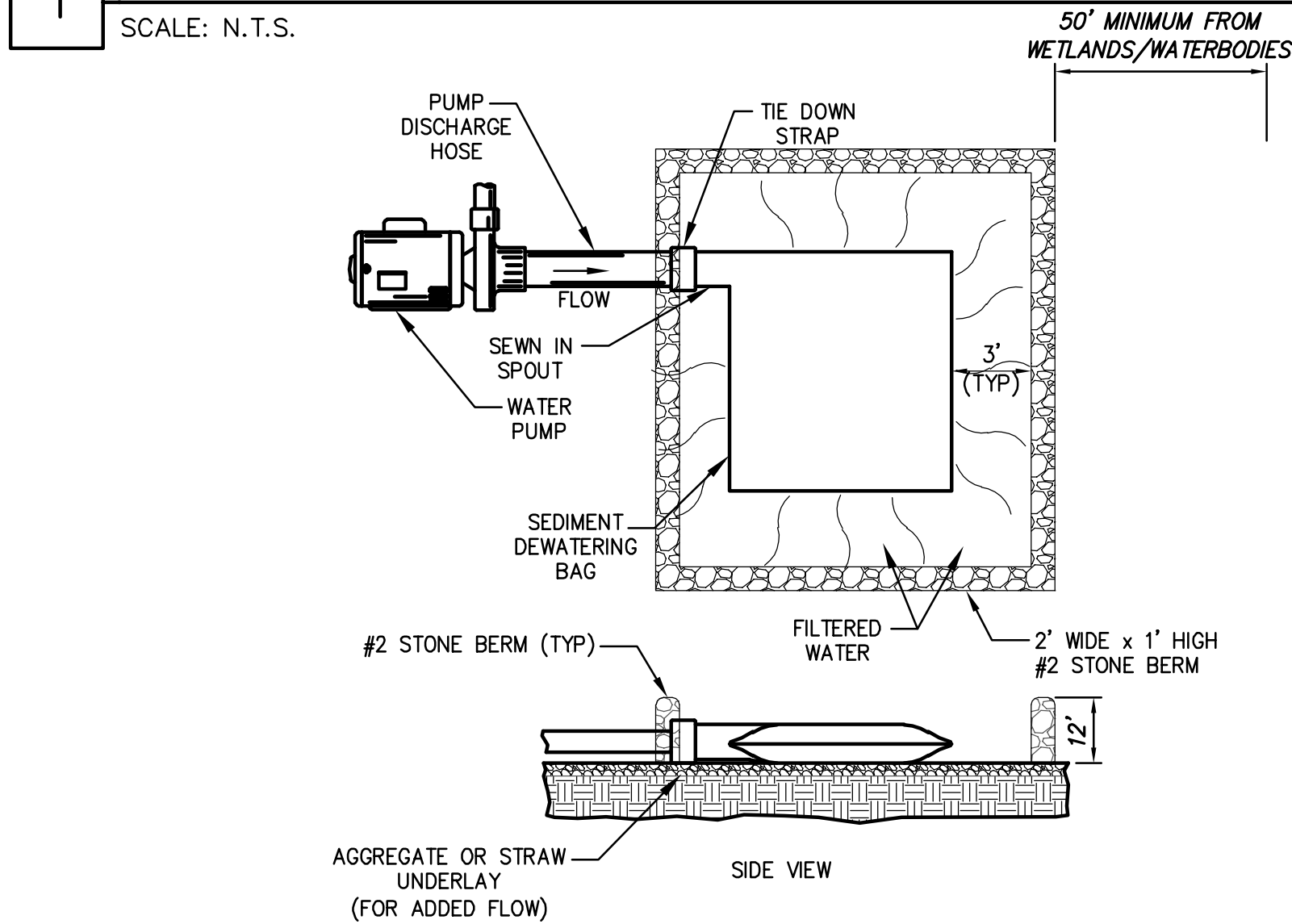
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File: V:\PROJECTS\ANY\K6\066076_000\09_DRAWINGS\DESIGN PACKAGE 3\066076_P3 - C-602.DWG Saved: 3/31/2023 10:17:35 AM Plotted: 4/4/2023 9:59:32 AM Current User: Zoyer, Tork Last Saved By: 7895



1 INLET PROTECTION

SCALE: N.T.S.



4 SEDIMENT DEWATERING BAG

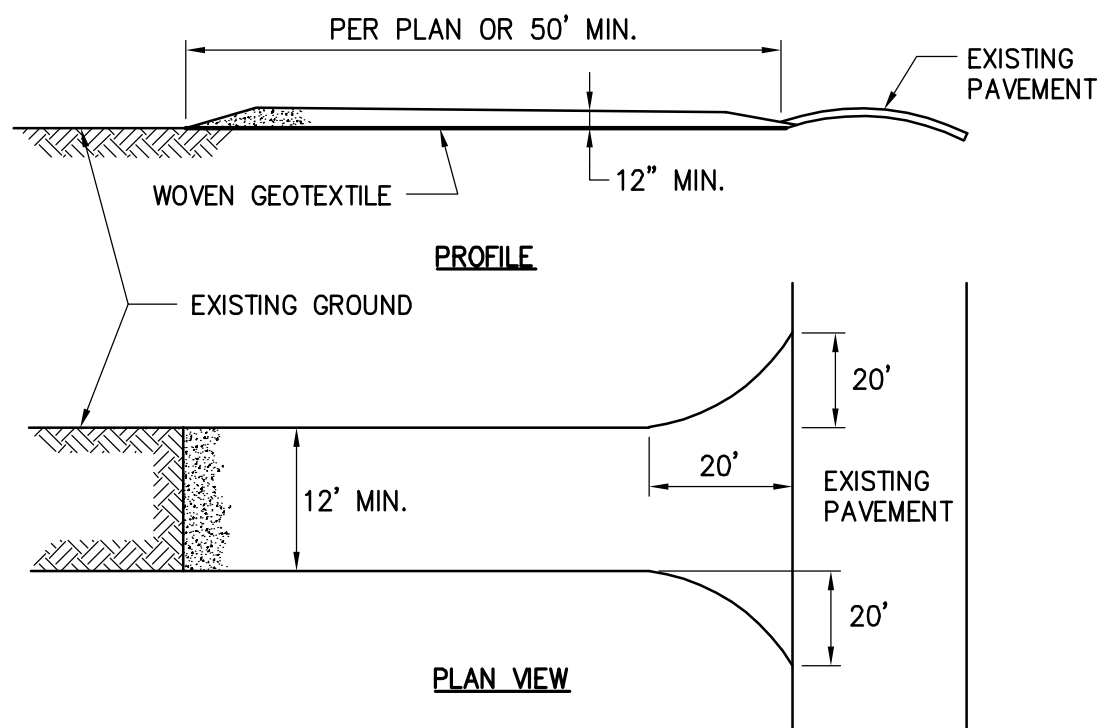
SCALE: N.T.S.

SEDIMENT DEWATERING BAG SPECIFICATIONS

Mechanical Properties	Test Method	Units	MARV
Grab Tensile Strength	ASTM D 4632	kN (lbs)	0.9 (205) x 0.9 (205)
Grab Tensile Elongation	ASTM D 4632	%	50 x 50
Puncture Strength	ASTM D 4833	kN (lbs)	0.58 (130)
Mullen Burst Strength	ASTM D 3786	kPa (psi)	2618 (380)
Trapezoid Tear Strength	ASTM D 4533	kN (lbs)	0.36 (80) X 0.36 (80)
UV Resistance	ASTM D 4355	%	70
Apparent Opening Size	ASTM D 4751	Mm (US Std Sieve)	0.180 (80)
Flow Rate	ASTM D 4491	1/min/m ² (gal/min/ft ²)	3866 (95)
Permittivity	ASTM D 4491	Sec ⁻¹	1.2

5 STABILIZED CONSTRUCTION ACCESS

SCALE: N.T.S.



6 CONCRETE WASHOUT AREA

SCALE: N.T.S.



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CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
EROSION AND SEDIMENT CONTROL DETAILS

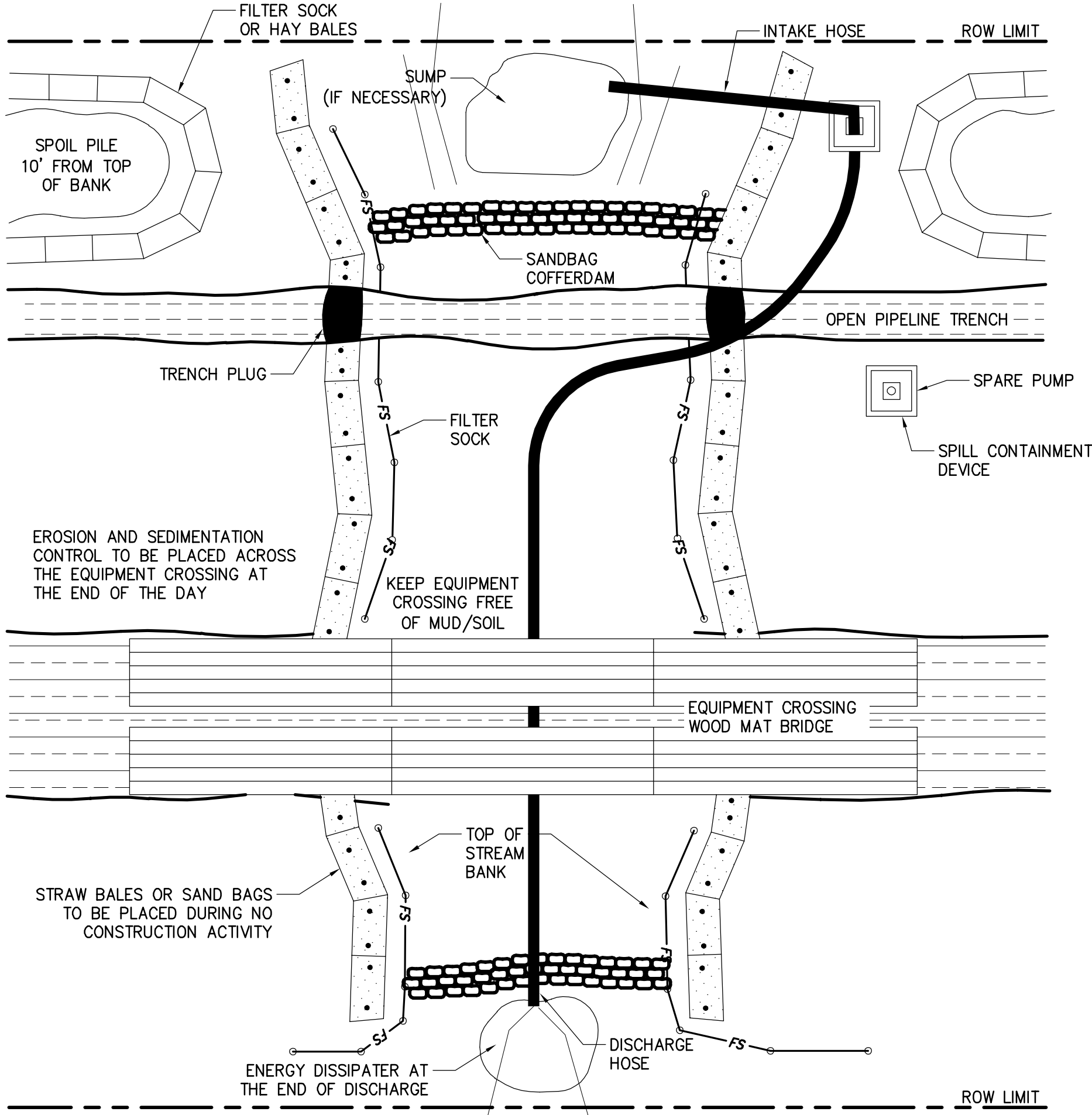
KIEWIT PROJECT NO.
21162
CHA PROJECT NO.
066076
DRAWING NO.

C-602

0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR					
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JJE	DESIGNED BY: JTM	APPROVED BY: JPR	SCALE AS NOTED X	DATE 04/05/2023
								REV. NO.	SH.NO.

THE FOLLOWING BMPs WILL BE IMPLEMENTED DURING WATER BARRIER INSTALLATION:

- A) DAMS SHALL BE CONSTRUCTED OF EITHER SANDBAGS, WATER BLADDERS, STEEL PLATES, PORTA-DAMS OR EQUIVALENT OR "JERSEY BARRIERS" AND PLASTIC SHEETING OR A COMBINATION THEREOF
- B) THE DAMS SHALL BE CONSTRUCTED OF SUFFICIENT HEIGHT TO ALLOW ADEQUATE FREEBOARD UNDER REASONABLY EXPECTED WATER LEVELS OR FLOWS AND PROVIDE FOR SOME IMPOUNDMENT OF WATER
- C) PRIOR TO COMPLETION OF THE DAMS, THE PUMP(S) MUST BE STARTED IN ORDER TO PROVIDE DOWNSTREAM FLOW OF WATER AROUND THE CONSTRUCTION WORK AREA
- D) THE RATE OF PUMPING SHALL BE MONITORED TO MINIMIZE DRAINING OF THE INTAKE SUMP AND THE RESULTING CESSATION IN FLOW. ALTERNATIVELY, PUMPING SHALL BE MONITORED AND INCREASED AS NECESSARY TO PREVENT OVERTOPPING OF THE DAMS.



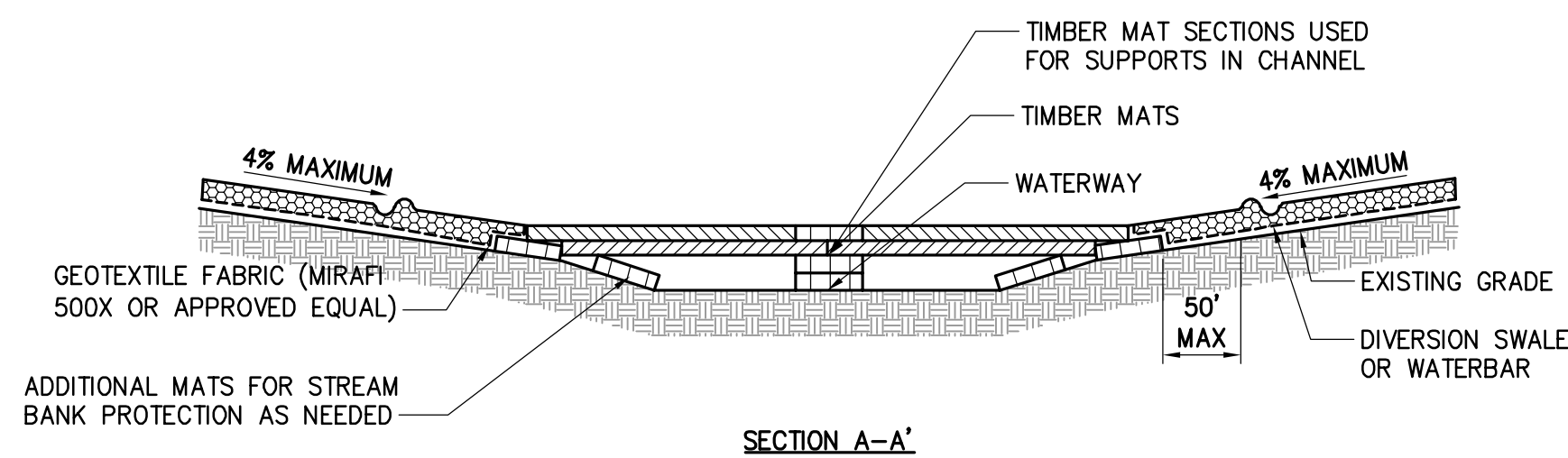
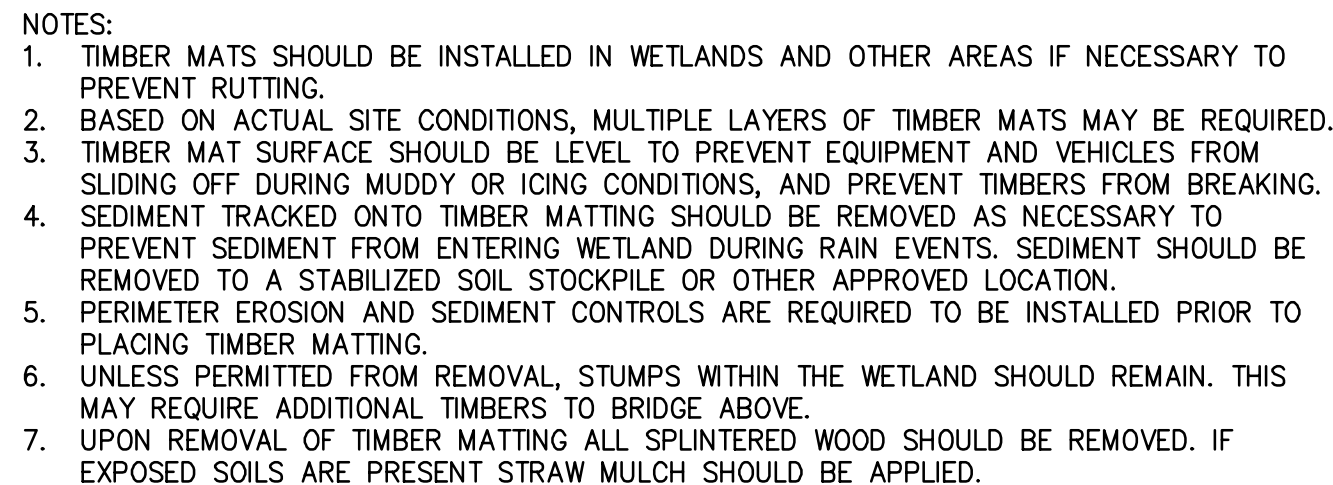
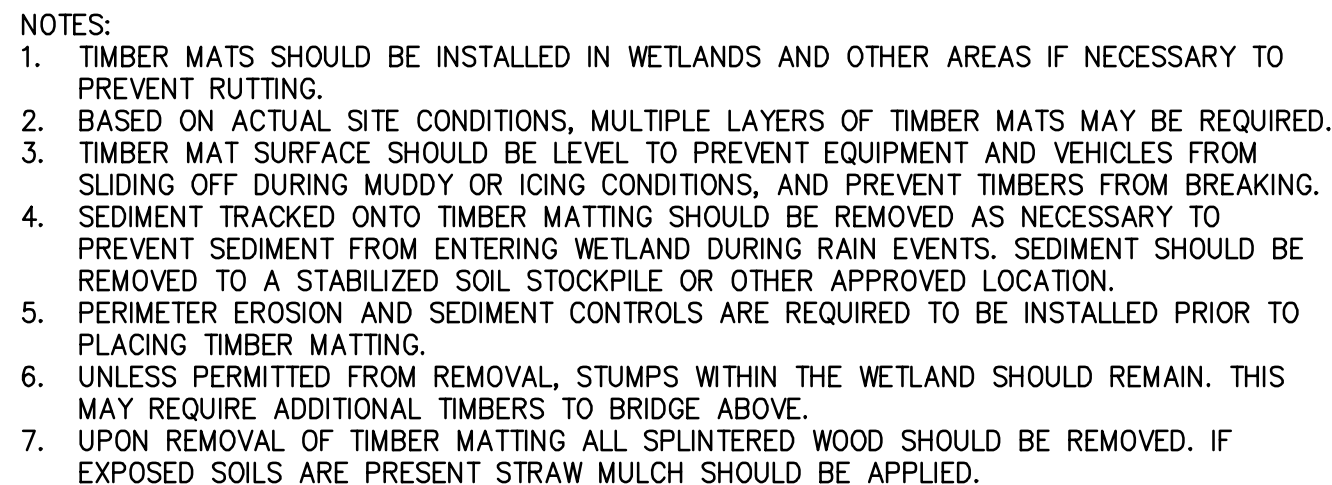
A cross-sectional diagram of a sandbag cofferdam. The cofferdam is a trapezoidal structure made of stacked sandbags, with a crest width of at least 1 foot. The height of the cofferdam is indicated as 'VARIES'. The top surface is labeled 'SANDBAG COFFERDAM CREST - 1" WIDE (MIN.)'. The area inside the cofferdam is labeled 'EXCAVATION AREA'. The ground surface is shown with a dashed line, and the stream bottom is indicated by a solid line with a triangle symbol. The distance from the stream bottom to the cofferdam base is labeled 'VARIES'. Below the cofferdam, two boulders are shown, with a note stating 'BOULDERS WILL BE ENCOUNTERED ON STREAM BOTTOM. CONSTRUCT COFFERDAM AROUND BOULDERS, UNLESS BOULDER REMOVAL AGREED UPON BY ENGINEER.' The overall width of the cofferdam at its base is labeled 'APPROX. STREAM BOTTOM (FIELD CONDITIONS MAY VARY)'.

NOTES:

1. SAND BAGS SHALL BE FILTER FABRIC TYPE AND BE DOUBLE BAGGED.
2. PORTADAM, BY PORTADAM, INC. SHALL BE CONSIDERED ACCEPTABLE SUBSTITUTE TO SAND BAGS

[illegible]

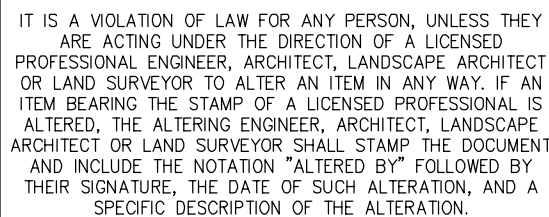
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- NOTES:
1. IN-STREAM EXCAVATION SHOULD BE COMPLETED IN ACCORDANCE WITH "TEMPORARY ACCESS WATERWAY CROSSING" ON PAGE 2.32 OF THE 2016 NYSDEC STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (OR NEWEST VERSION) AND THE CERTIFICATE.
 2. THE CONSTRUCTION OF ANY CROSSING SHOULD NOT CAUSE A SIGNIFICANT WATER LEVEL DIFFERENCE BETWEEN THE UPSTREAM AND DOWNSTREAM WATER SURFACE ELEVATIONS. IN-STREAM WORK WILL BE PROHIBITED WITHIN COLD WATER TROUT FISHERIES FROM OCTOBER 1 TO MAY 31.
 3. ALL FILL MATERIALS ASSOCIATED WITH THE ROADWAY APPROACH SHOULD BE LIMITED TO A MAXIMUM HEIGHT OF 2 FEET ABOVE THE EXISTING FLOOD PLAIN ELEVATION.
 4. A WATER DIVERTING STRUCTURE SUCH AS A SWALE OR WATER BAR SHOULD BE CONSTRUCTED (ACROSS THE ROADWAY ON BOTH ROADWAY APPROACHES) 50 FEET (MAXIMUM) ON EITHER SIDE OF THE WATERWAY CROSSING. THIS WILL PREVENT ROADWAY SURFACE RUNOFF FROM DIRECTLY ENTERING THE WATERWAY. THE 50 FEET MEASURED IS MEASURED FROM THE TOP OF THE WATERWAY BANK. IF THE ROADWAY APPROACH IS CONSTRUCTED WITH A REVERSE GRADE AWAY FROM THE WATERWAY, A SEPARATE DIVERTING STRUCTURE IS NOT REQUIRED.
 5. ALL CROSSINGS SHOULD HAVE ONE TRAFFIC LANE. THE MINIMUM WIDTH SHOULD BE 12 FEET WITH A MAXIMUM WIDTH OF 20 FEET.

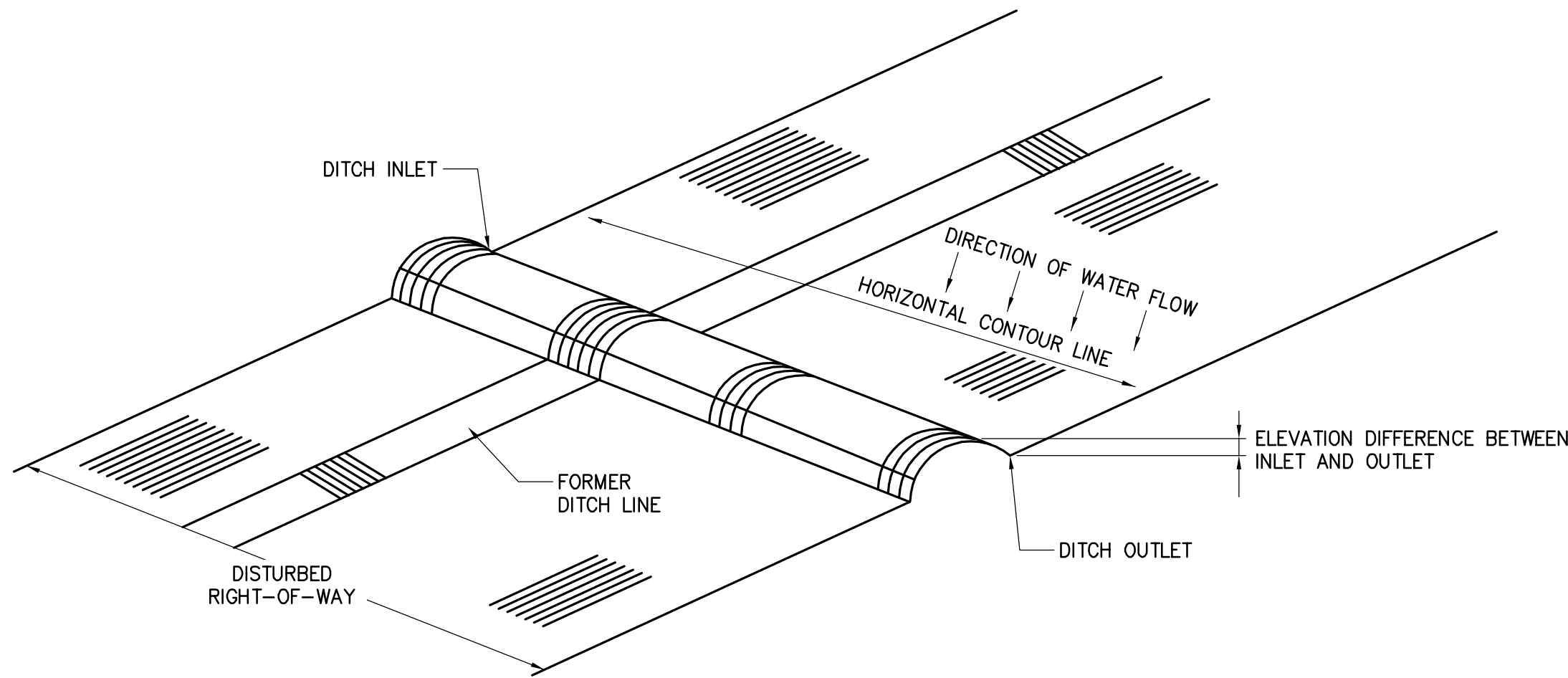
☐ OPTION "C"
NOT TO SCALE

1	TIMBER MATTING
	SCALE: N.T.S.



						CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON WETLAND CROSSING DETAILS					KIEWIT PROJECT NO. 21162	
											CHA PROJECT NO. 066076	
											DRAWING NO.	
											C-611	
0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR								
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JJE	DESIGNED BY: JTM	APPROVED BY: JPR	SCALE REV. NO.	AS NOTED X	DATE SH.NO.	04/05/2023	

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WATER SHALL BE DIVERTED OFF THE DISTURBED RIGHT-OF-WAY AT AN OUTSLOPE OF THREE TO FIVE PERCENT BY CONSTRUCTING DIVERSION DITCH ACCORDING TO THE FOLLOWING PROCEDURES:

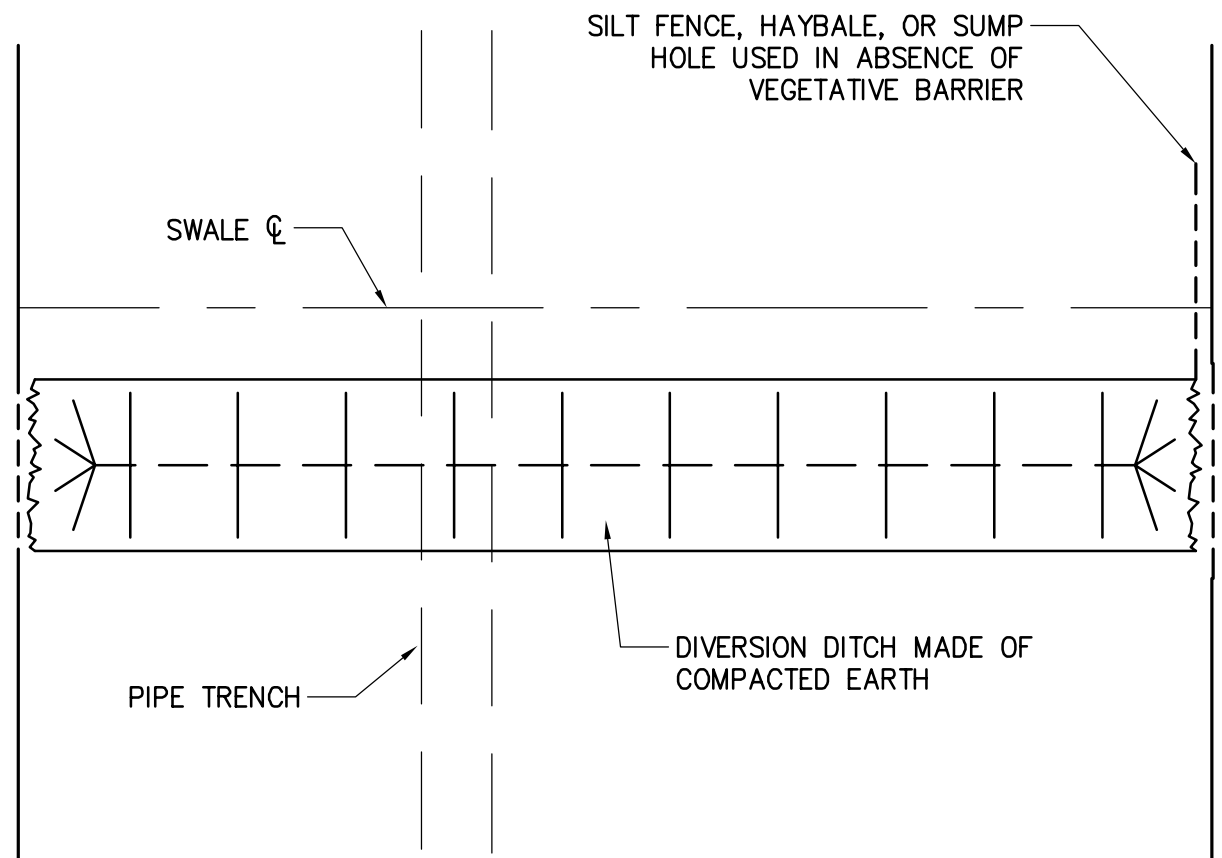
1. AT THE PROPOSED INTERCEPTOR DITCH LOCATION ESTABLISH A HORIZONTAL CONTOUR LINE (USING A POCKET TRANSIT OR HAND LEVEL) WHICH EXTENDS COMPLETELY ACROSS THE DISTURBED RIGHT-OF-WAY. THIS LINE WILL ALWAYS BE PERPENDICULAR TO THE DIRECTION OF WATER FLOW AND SHOULD BE PARALLEL TO THE MAP CONTOURS SHOWN ON THE PLAN DRAWINGS.
2. DETERMINE WHICH SIDE OF THE RIGHT-OF-WAY IS BEST SUITED FOR THE DITCH OUTLET (EVALUATE VEGETATION DENSITY, LOCAL TOPOGRAPHY, ETC.) AND DEVIATE DIKE AWAY FROM THE HORIZONTAL CONTOUR LINE SLIGHTLY DOWNWARD TOWARD THE SELECTED OUTLET SIDE MAINTAINING A THREE TO FIVE PERCENT SLOPE. AS AN EXAMPLE, THE CHART AT THE RIGHT SHOWS DIMENSIONS ASSUMING A FOUR PERCENT SLOPE.
3. WHEN OUTLETTING NEAR WATER BODIES, STREAMS, DITCHES, & CROP FIELDS, A FILTER FENCE OR STRAW BALE FENCE SHOULD BE PLACED ON OUTLET END OF THE DIVERSION DITCH.

TEMPORARY DRAINAGE DITCH

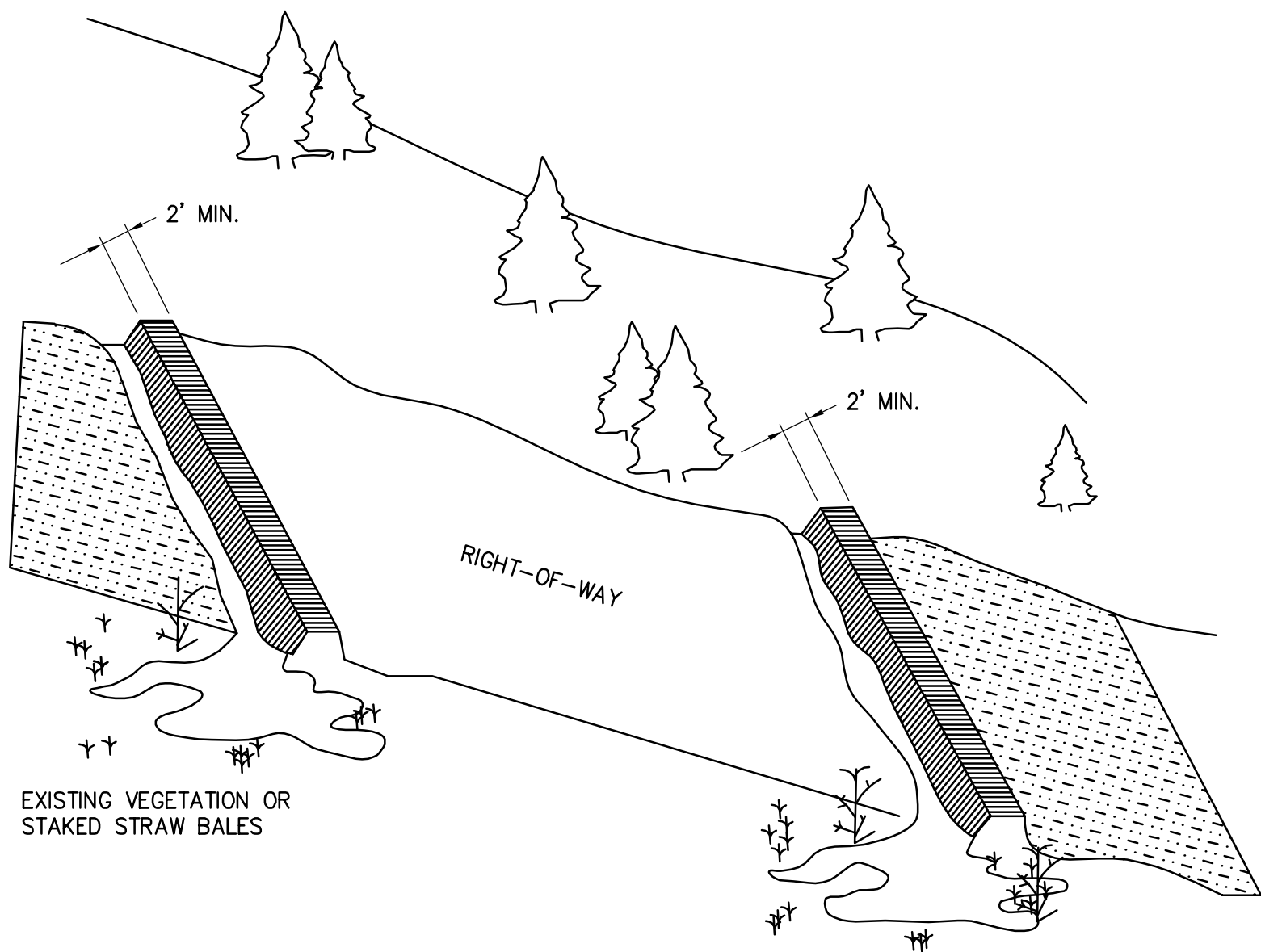
NOTES:

1. TEMPORARY DIVERSION DITCH SHOULD BE BUILT SIMILAR TO THE PERMANENT DITCH CONFIGURATION BUT THE DIMENSION CAN BE SCALED BACK.
2. MAXIMUM HEIGHT SHOULD BE 12" AND SHOULD BE COMPACTED.
3. SPACING BETWEEN DIVERSION DITCHES AND SKEW OF THE DIVERSION DITCHES CAN VARY FROM THE PERMANENT DIVERSION DITCHES.
4. WHEN CONSTRUCTING TEMPORARY DIVERSION DITCHES THEY SHOULD BE FUNCTIONAL, WHILE MAINLINE CONSTRUCTION IS PROCEEDING, UNTIL RESTORATION BEGINS AND PERMANENT DIVERSION DITCHES ARE THEN CONSTRUCTED.

4% FLOW CHART	
HORIZONTAL DISTANCE BETWEEN WATERBAR INLET & OUTLET (FEET)	ELEVATION DISTANCE BETWEEN WATERBAR INLET AND OUTLET (FEET)
75	3
100	4
125	5
150	6
175	7

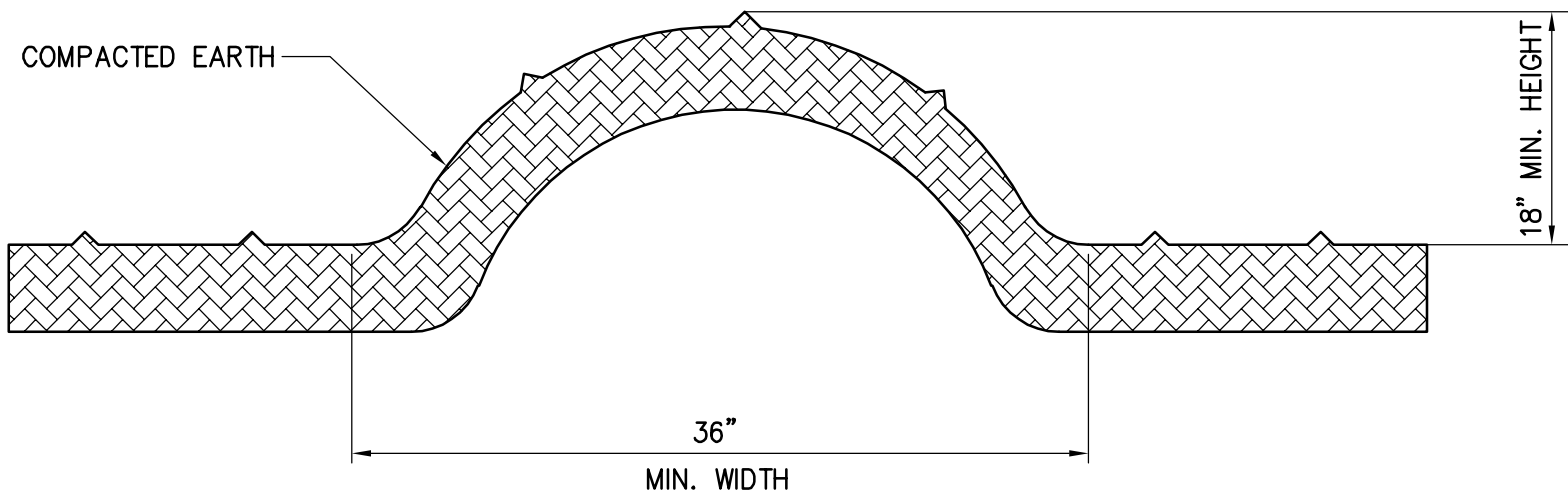


OVERHEAD VIEW



1 PERMANENT DIVERSION DITCH DETAIL

SCALE: N.T.S.



2 MINIMAL HEIGHT & WIDTH DIMENSIONS FOR WATERBAR CONSTRUCTION

SCALE: N.T.S.



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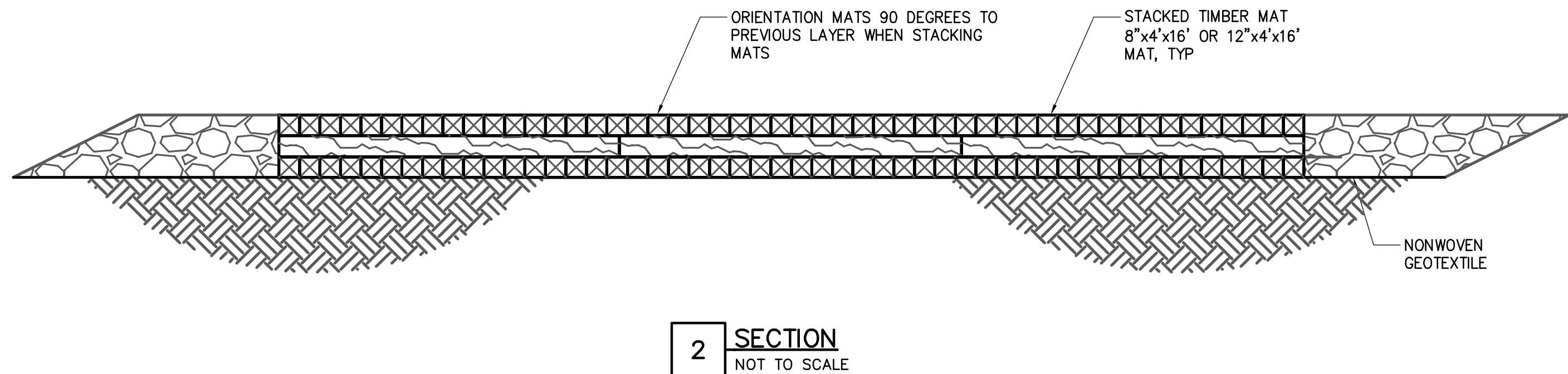
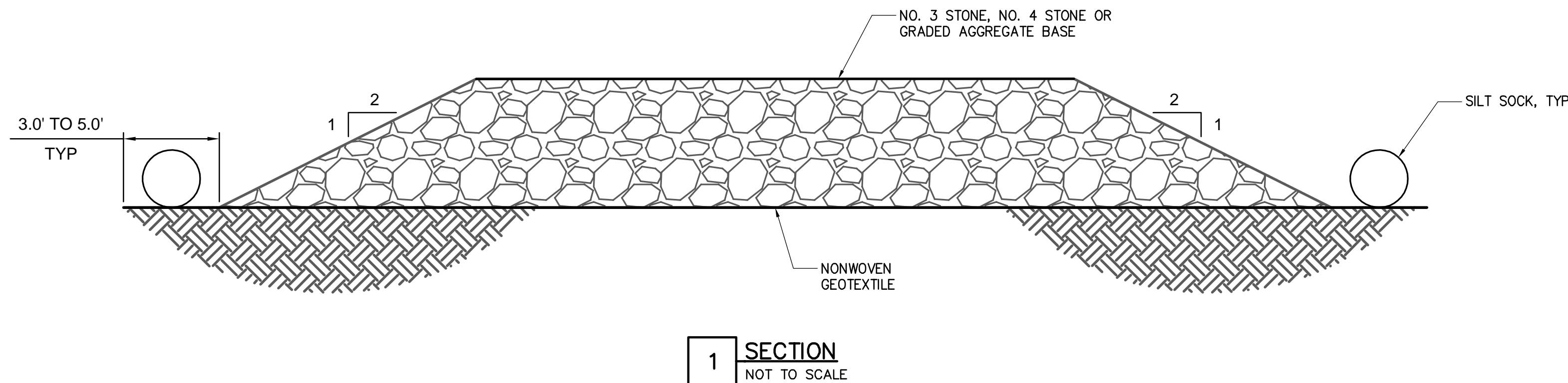
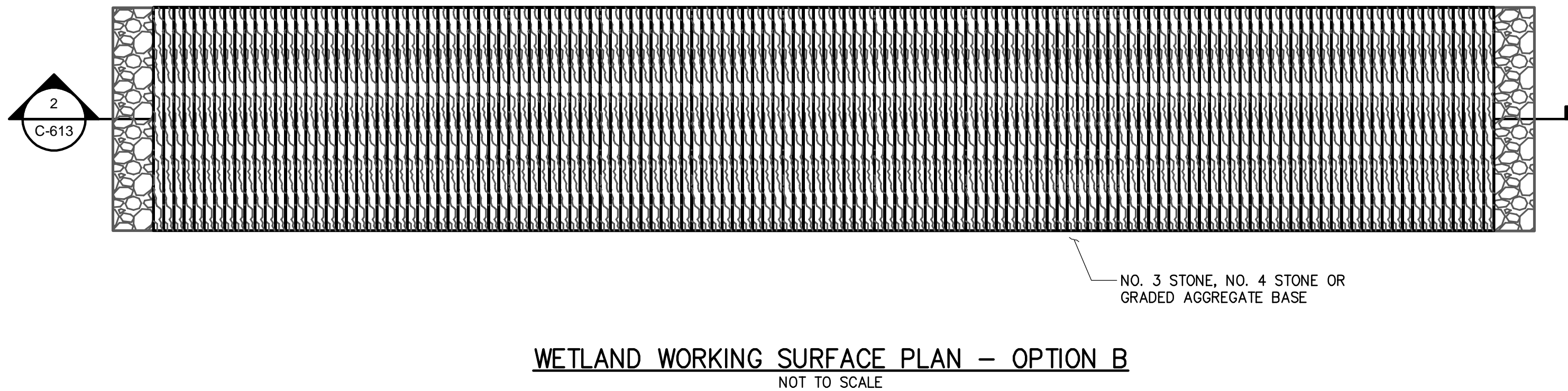
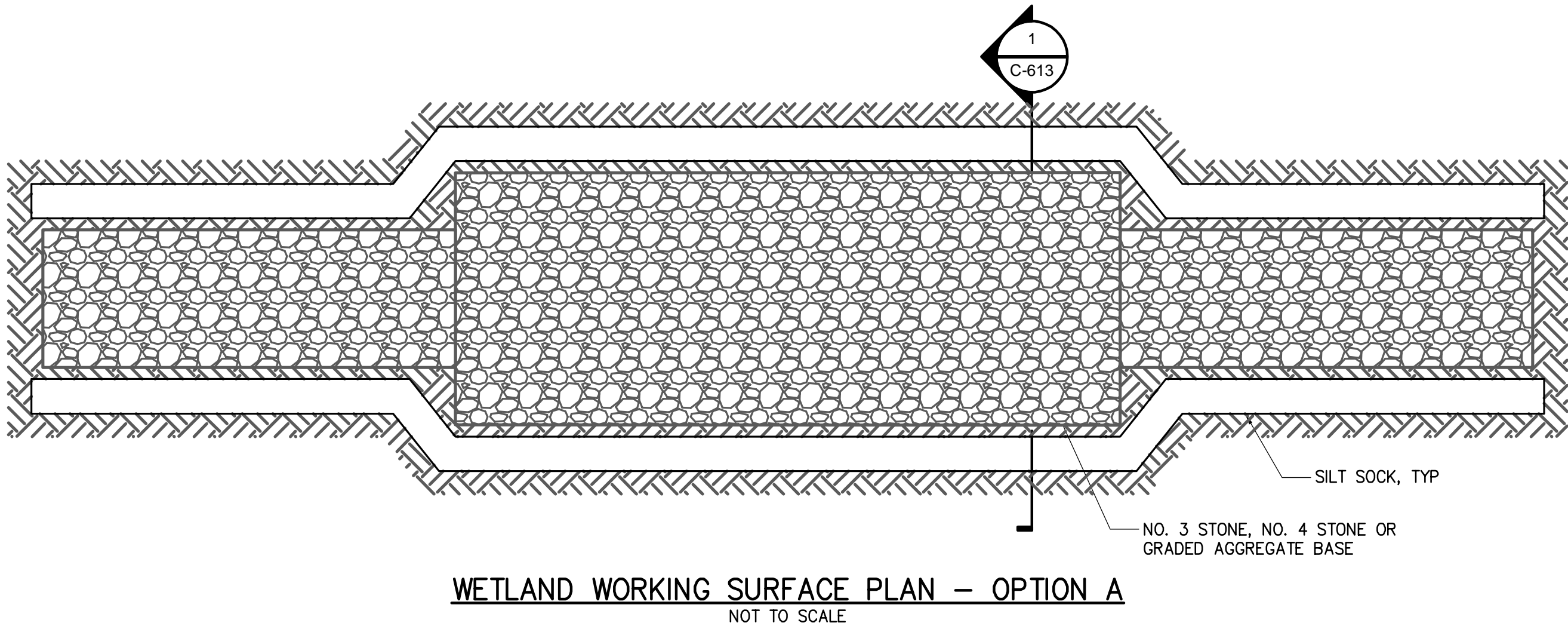
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP					

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
WATERBAR DETAILS

DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR SCALE AS NOTED REV. NO.

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-612
DATE	04/05/2023
SH.NO.	

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NOTES:

1. UNDERCUT AND REMOVE TOP SOIL PRIOR TO PLACING GEOTEXTILE FABRIC.
2. A LAYER OF CLEAN CRUSHED STONE SHALL BE LAID ON TOP OF THE GEOTEXTILE FABRIC.
3. GEOTEXTILE FABRIC SHALL EXTEND AT LEAST 3 FT TO 5 FT BEYOND THE EDGE OF STONE PLACEMENT TO MINIMIZE STONE ENTERING THE WETLAND AND FACILITATE REMOVAL OF THE ROAD.
4. SUITABLE CROSS DRAINAGE SHALL BE PROVIDED ACROSS THE ROAD FOR STREAM CHANNELS AND SURFACE FLOW.
5. AREA TO BE RESTORED IN ACCORDANCE WITH THE EM&CP SECTION 14

GENERAL NOTES:

1. TIMBER:
 - A. TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.
2. CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER IMMEDIATELY.

NOTES:

1. TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO PREVENT RUTTING.
2. BASED ON ACTUAL SITE CONDITIONS, NUMBER OF TIMBER MAT LAYERS TO BE DETERMINED ON SITE.
3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING.
4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND DURING RAIN EVENTS. SEDIMENT SHOULD BE REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION.
5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.
6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.
7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.
8. IF DEEMED NECESSARY BY CONSTRUCTION IN THE FIELD, GEOTEXTILE FABRIC TO BE INSTALLED UNDER MATTING. (TYP)
9. AREA TO BE RESTORED IN ACCORDANCE WITH THE EM&CP SECTION 14.



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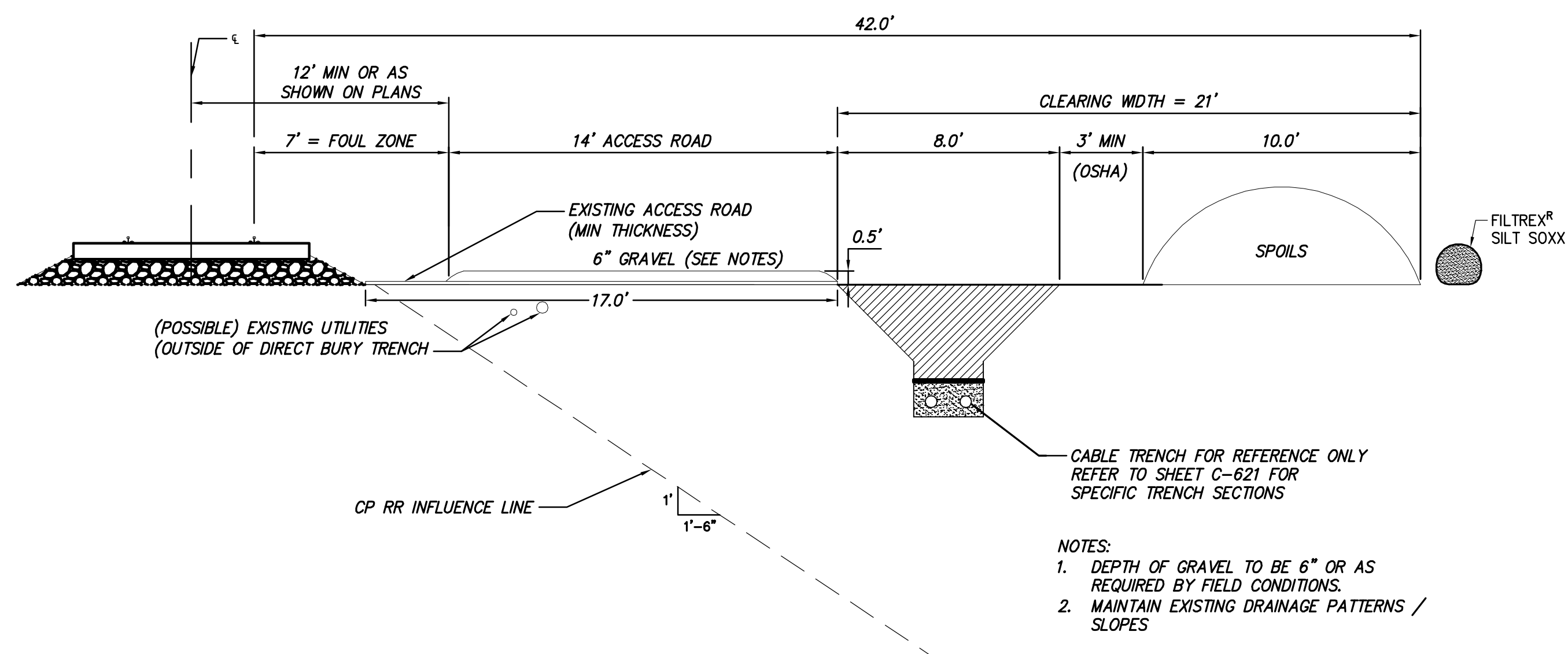
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION					DB	APP	

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SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
WETLAND WORKING SURFACES PLAN

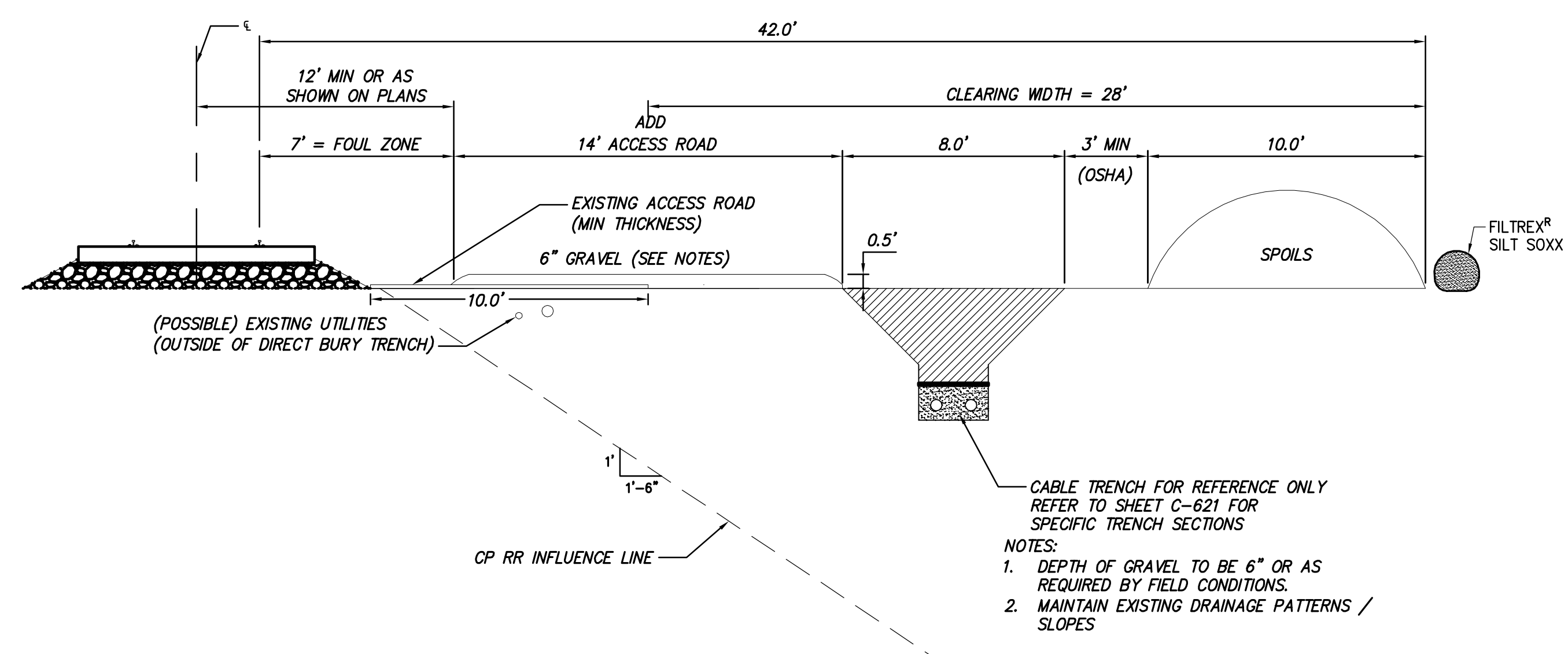
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REV. NO. X

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CHA PROJECT NO.	066076
DRAWING NO.	C-613
DATE	04/05/2023
SH.NO.	

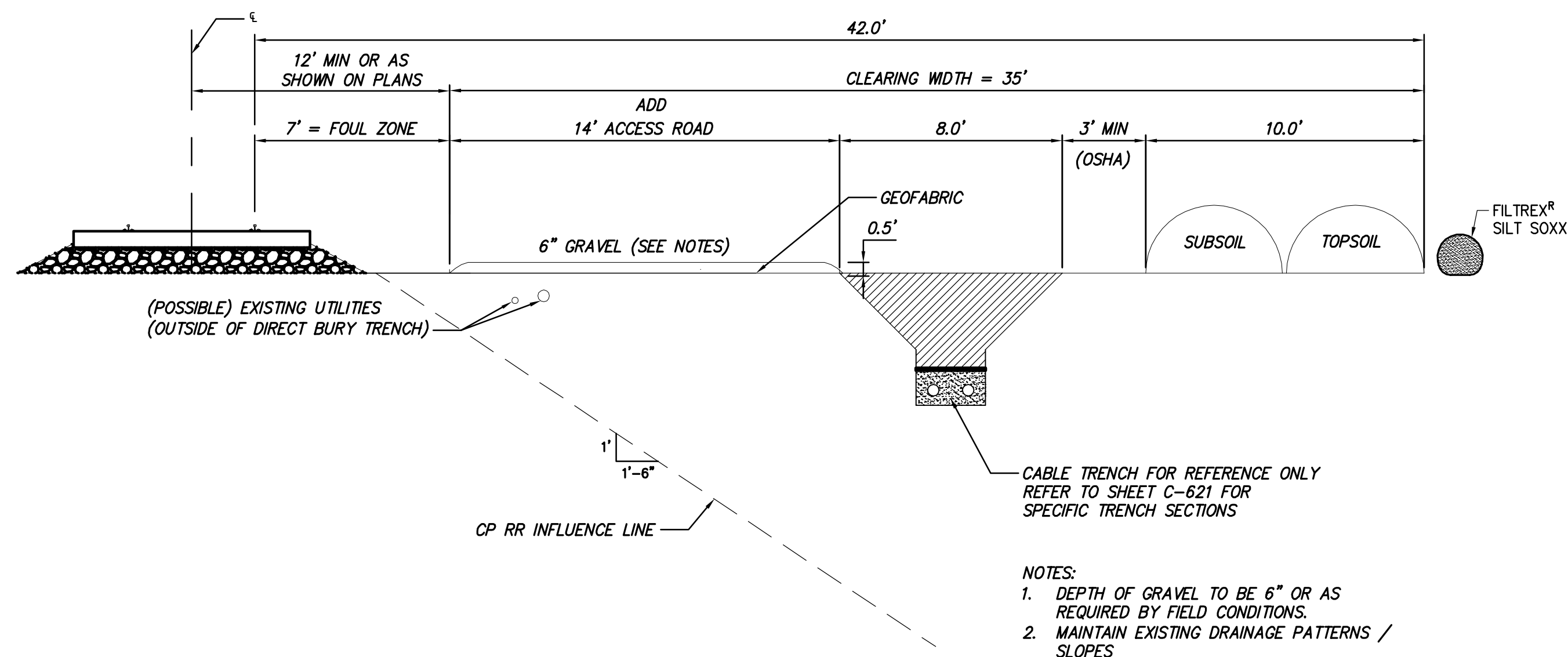
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SH.NO.	



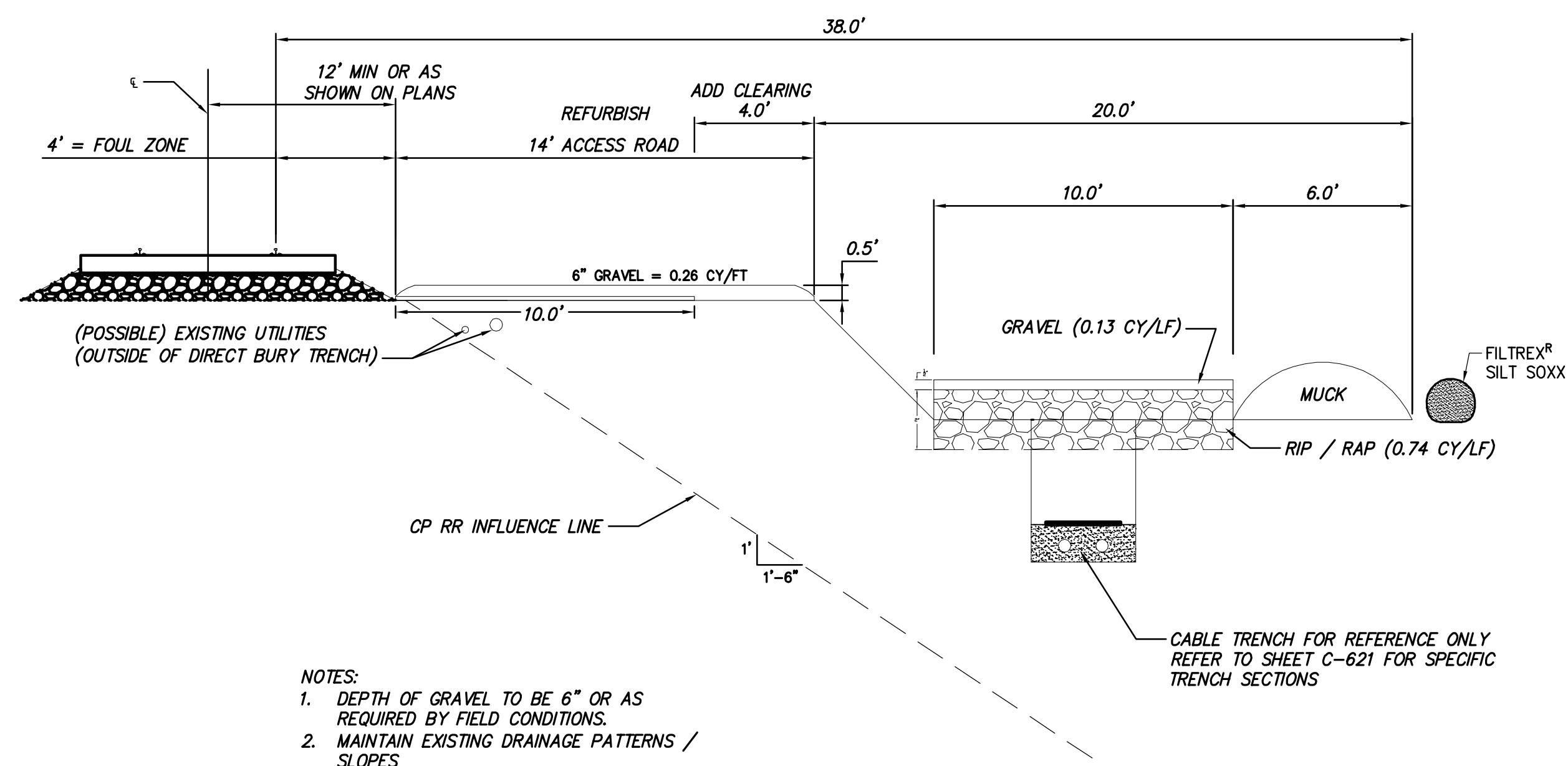
TYPE 1
EXISTING, MINOR REFURBISHMENT



TYPE 2
EXISTING, MAJOR REFURBISHMENT



TYPE 3
BUILD NEW



TYPE 4A
WETLAND



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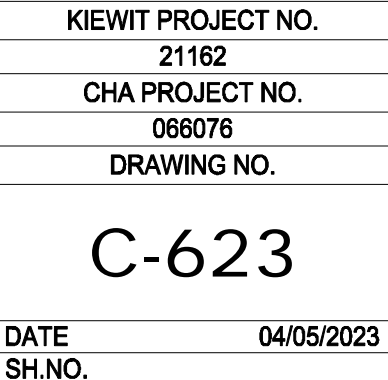
						CHAMPLAIN HUDSON POWER EXPRESS SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON TYPICAL ACCESS ROAD CROSS SECTIONS						KIEWIT PROJECT NO. 21162			
												CHA PROJECT NO. 068076			
												DRAWING NO.			
												C-622			
0	04/05/2023	FINAL EM&CP SUBMISSION			JJE	JPR							DATE 04/05/2023		
No.	DATE	SUBMITTAL / REVISION DESCRIPTION			DB	APP	DRAWN BY:	JJE	DESIGNED BY:	JTM	APPROVED BY:	JPR	SCALE REV. NO.	AS NOTED X	SH NO.



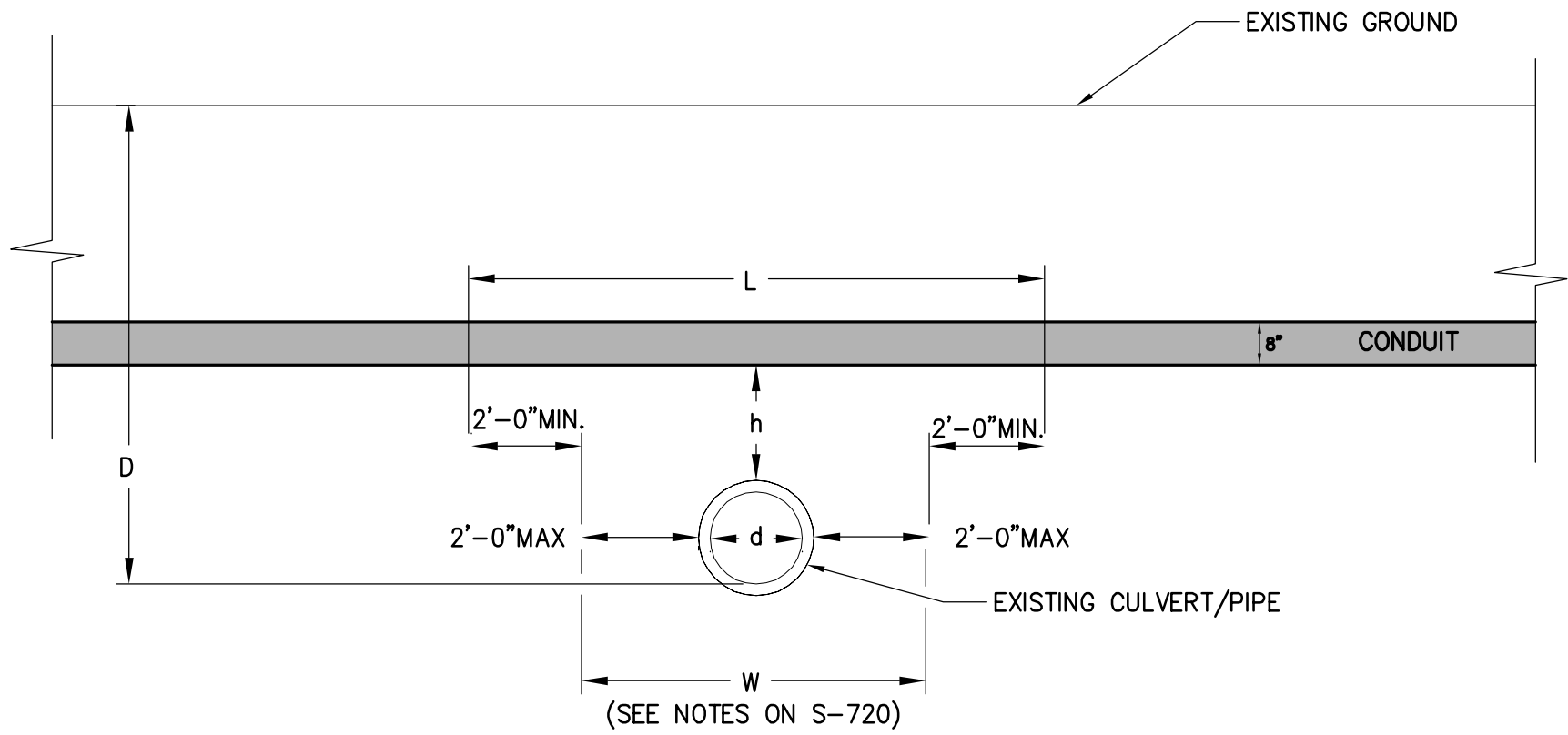
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1 **TYPICAL DUCTBANK SECTION OVER EXISTING CP RAIL CULVERT OUTLET**
NOT TO SCALE
NOTES:
1. DUCT BANK TO BE 2.0' MIN ABOVE OR BELOW EXISTING CULVERTS.

Package	Sheet Number	CP Rail Mile Post	Station	Utility	Size "d"	Exist. Ground Elev. (ft.)*	Invert (ft.)*	D (ft.)*	h (ft.)	L (ft.)
Package 3	C-175	36.12	31122+91 twin 1	Storm Drainage Pipe/Culvert	Twin 12'	319.7	272.9	46.7	26	33.9
Package 3	C-185	33.45	31122+17 twin 2	Storm Drainage Pipe/Culvert	60"	295.7	276.6	12.7	1.7	13



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0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
TYPICAL CULVERT SECTION (1 OF 3)

DRAWN BY: JJEDESIGNED BY: JTMAPPROVED BY: JPRSCALE AS NOTEDREV. NO. X

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-624
DATE	04/05/2023
SH.NO.	

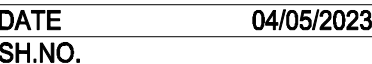


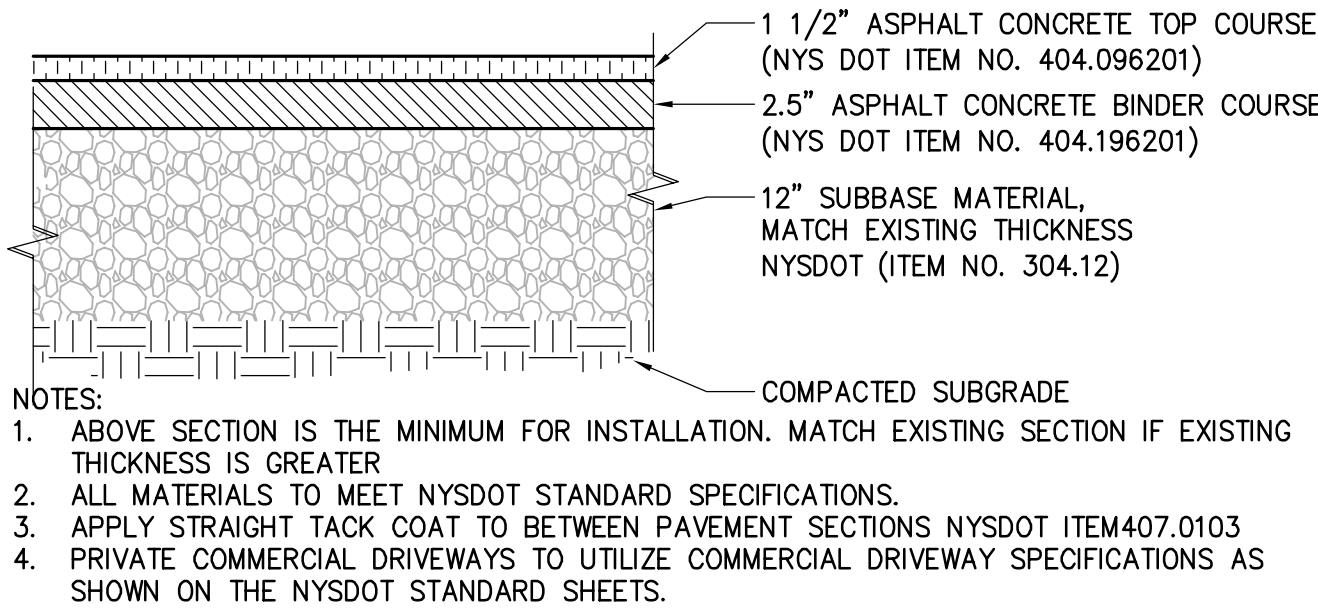
NOT TO SCALE

NOTES:

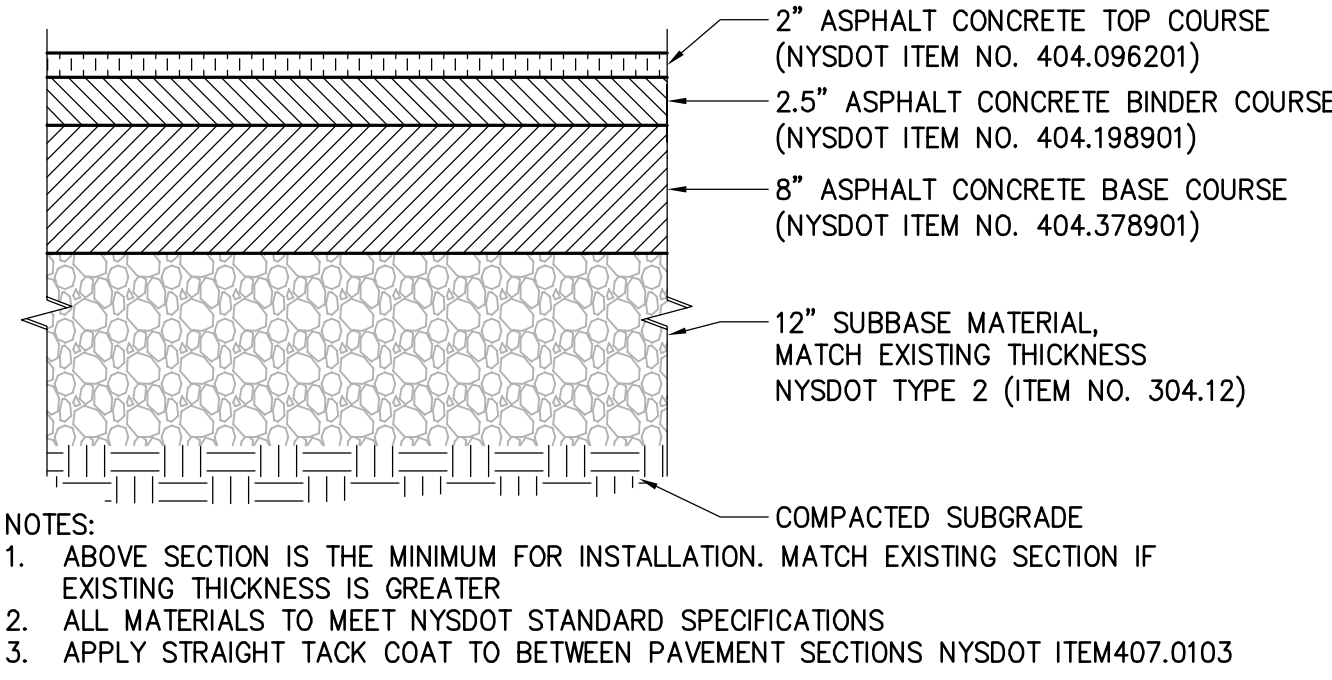
1. DUCT BANK TO BE 2.0' MIN ABOVE OR BELOW EXISTING CULVERTS.

B

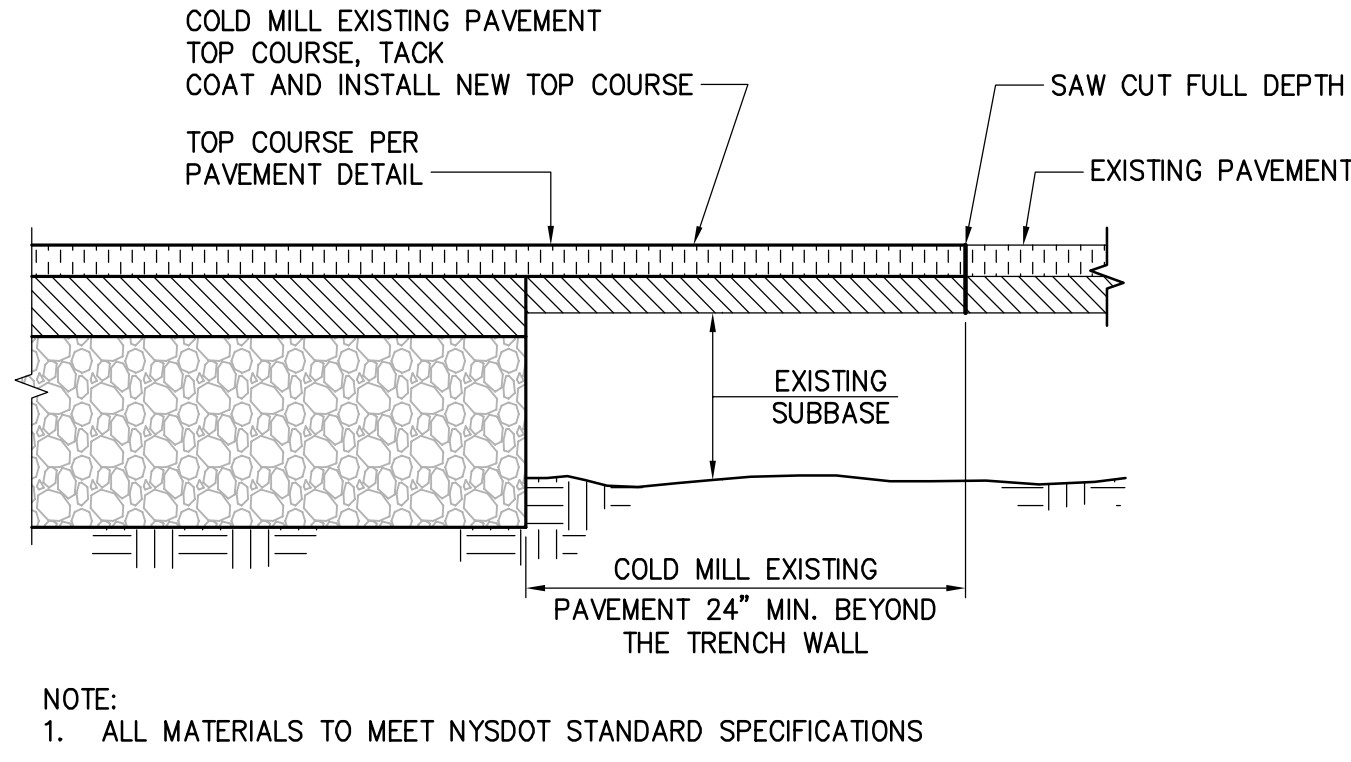




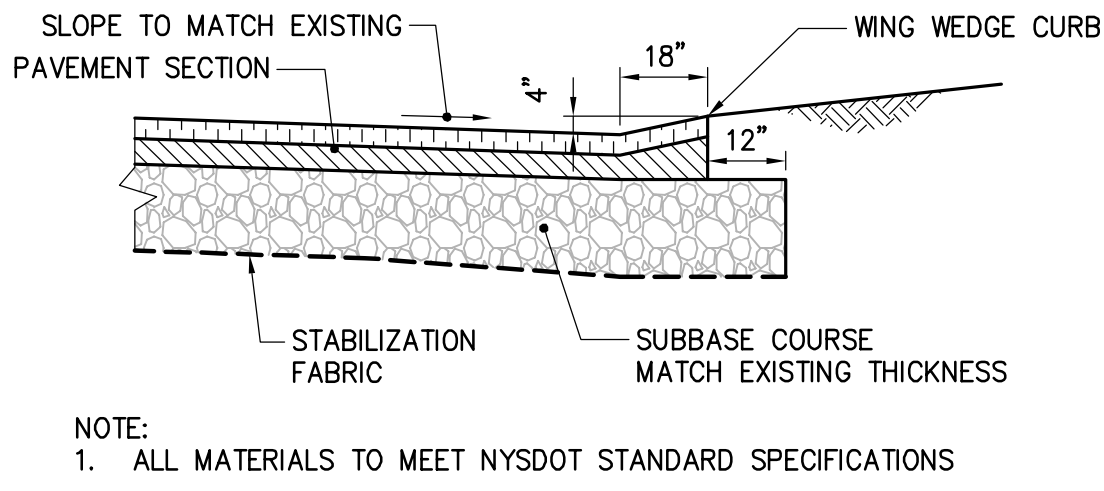
1 ASPHALT CONCRETE PAVEMENT DETAIL (PRIVATE DRIVEWAY)
SCALE: N.T.S.



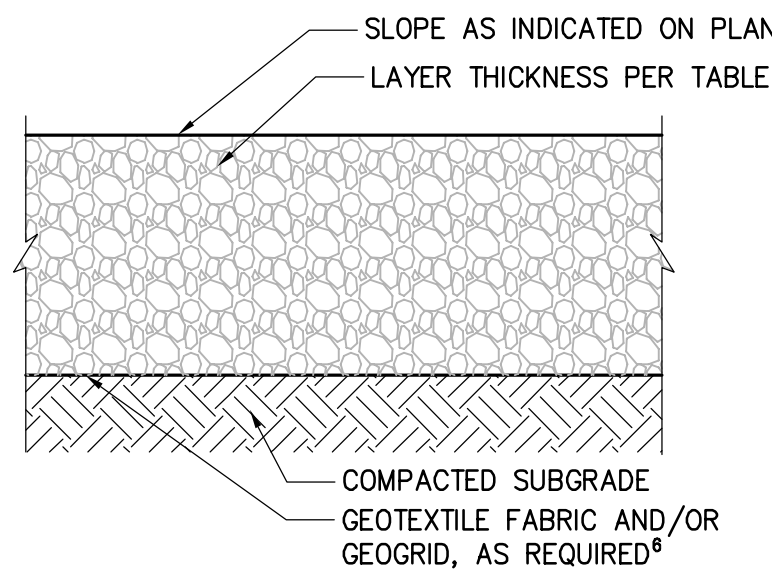
2 ASPHALT CONCRETE PAVEMENT (STATE/ROAD/COUNTY/TOWN)
SCALE: N.T.S.



3 PAVEMENT TRANSITION DETAIL
SCALE: N.T.S.



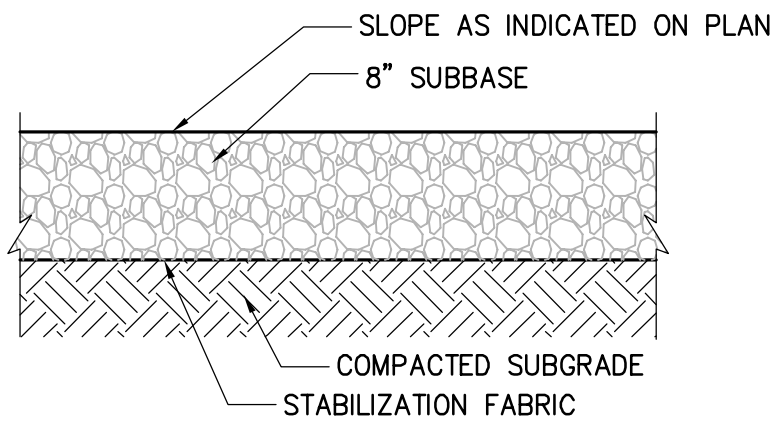
4 WING WEDGE CURB DETAIL
SCALE: N.T.S.



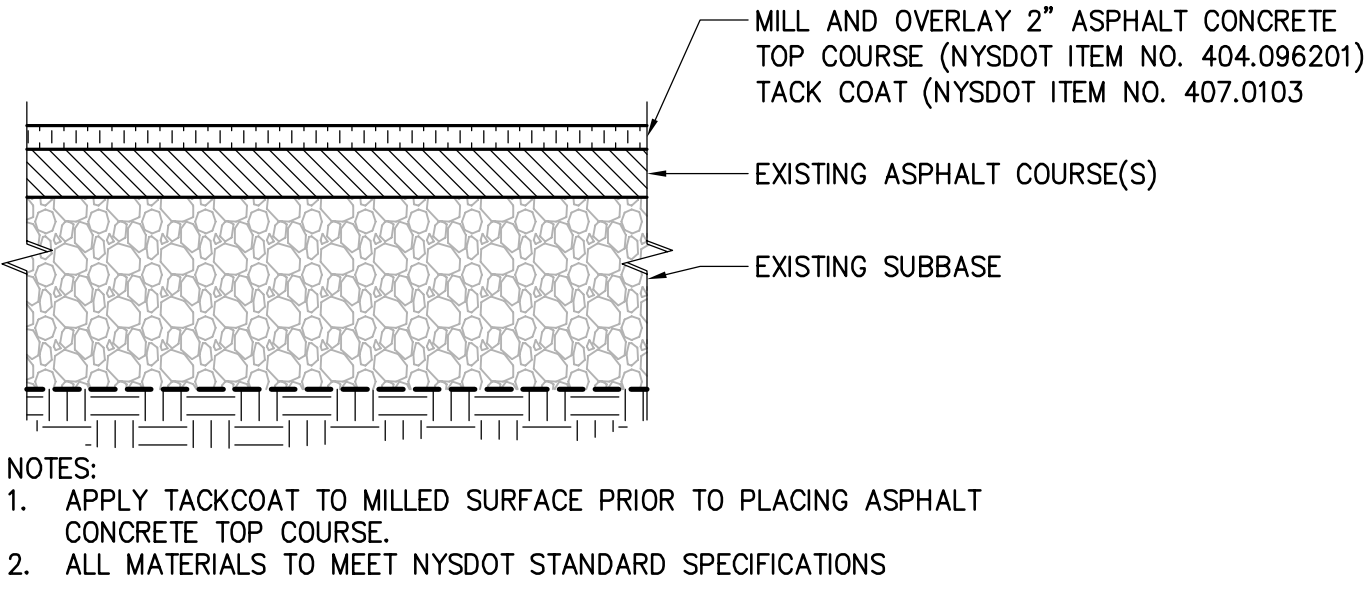
5 TEMPORARY ACCESS ROAD
SCALE: N.T.S.

TEMPORARY ACCESS ROAD SECTION ^{1,2,3,4}				
CBR ⁵	UNSTABILIZED	MIRAFI 180N GEOTEXTILE ⁶	TENSAR BX1200 GEOGRID ⁶	MIRAFI RSI SERIES GEOTEXTILE ⁶
0.5	--	20 INCH RIP RAP ⁷ + 6 INCH AGGREGATE	--	20 INCH RIP RAP ⁷ + 4 INCH AGGREGATE (RS580I)
1.0	--	18 INCH AGGREGATE	12 INCH AGGREGATE	15 INCH AGGREGATE (RS280I)
1.5	--	12 INCH AGGREGATE	9 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
2.0	18 INCH AGGREGATE	11 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
3.0+	15 INCH AGGREGATE	8 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)

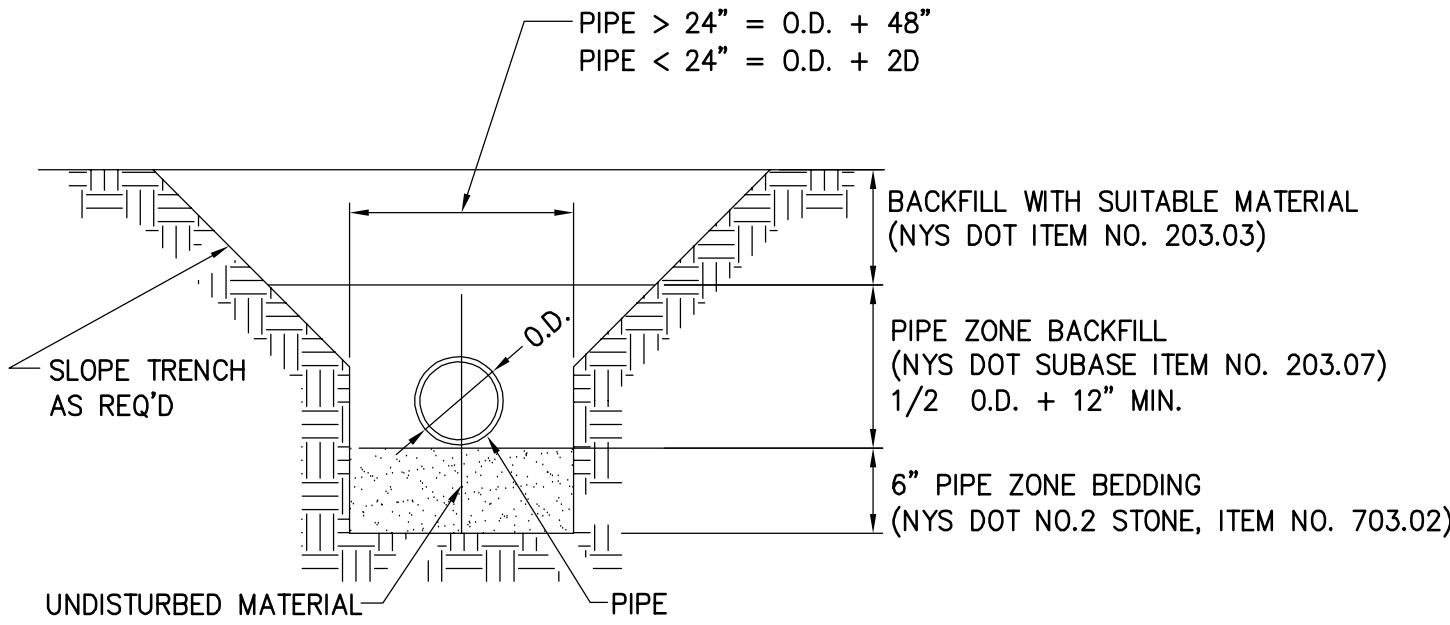
NOTES:
¹ TEMPORARY ACCESS ROAD SECTIONS PER KIEWIT ENGINEERING (NY) CORP.
² AGGREGATE SHALL BE NYSDOT TYPE 2 CRUSHED AGGREGATE OR APPROVED ALTERNATIVE.
³ DESIGN CONSIDERS 1,000 PASSES OF MAXIMUM 22-KIP AXLE LOAD AND A DESIGN RUT DEPTH OF 3 INCHES. ADDITIONAL AXLE PASSES, HEAVIER AXLE LOADS, AND DETERIORATED SUBGRADE CONDITIONS MAY REQUIRE THICKER AGGREGATE SECTIONS OR ADDITIONAL MAINTENANCE.
⁴ ALTERNATE TEMPORARY ACCESS ROAD DESIGNS MAY BE PROVIDED BY KIEWIT ENGINEERING, AS REQUIRED, BASED ON FIELD CONDITIONS AND TRAFFIC LOADING.
⁵ ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER OR ALTERNATIVE METHOD APPROVED BY GEOTECHNICAL ENGINEER OF RECORD (EOR). CBR OF IN-SITU SOIL MAY VARY SEASONALLY DUE TO FREEZE/THAW AND BASED ON MOISTURE CONDITIONS.
⁶ GEOGRID AND GEOTEXTILE
^A GEOGRID AND GEOTEXTILES SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS.
^B SPECIFIED GEOTEXTILE OR GEOGRID MAY BE REPLACED BY EQUIVALENT MATERIAL APPROVED BY EOR.
^C GEOTEXTILE IS REQUIRED IN REGULATED WETLANDS AND AGRICULTURAL LANDS.
^D GEOTEXTILE SEPARATOR FABRIC IS REQUIRED BENEATH GEOGRID ON COHESIVE SUBGRADE.
⁷ RIP RAP
^A RIP RAP SHALL BE NYSDOT LIGHT STONE FILL OR APPROVED ALTERNATIVE.
^B A LAYER OF #57 STONE IS RECOMMENDED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE GEOTEXTILE FABRIC WHERE RIP RAP IS USED.



6 GRAVEL PAVEMENT
SCALE: N.T.S.



7 MILL AND OVERLAY ASPHALT CONCRETE PAVEMENT DETAIL
SCALE: N.T.S.



8 CULVERT REPLACEMENT AND/OR TEMPORARY CULVERTS
SCALE: N.T.S.



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0	04/05/2023	FINAL EM&CP SUBMISSION	JJE	JPR

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENTS 4 & 5 (PACKAGE 3) - CP: FORT EDWARD TO MILTON
SURFACE RESTORATION DETAILS

DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR SCALE AS NOTED DATE 04/05/2023

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	068076
DRAWING NO.	C-631