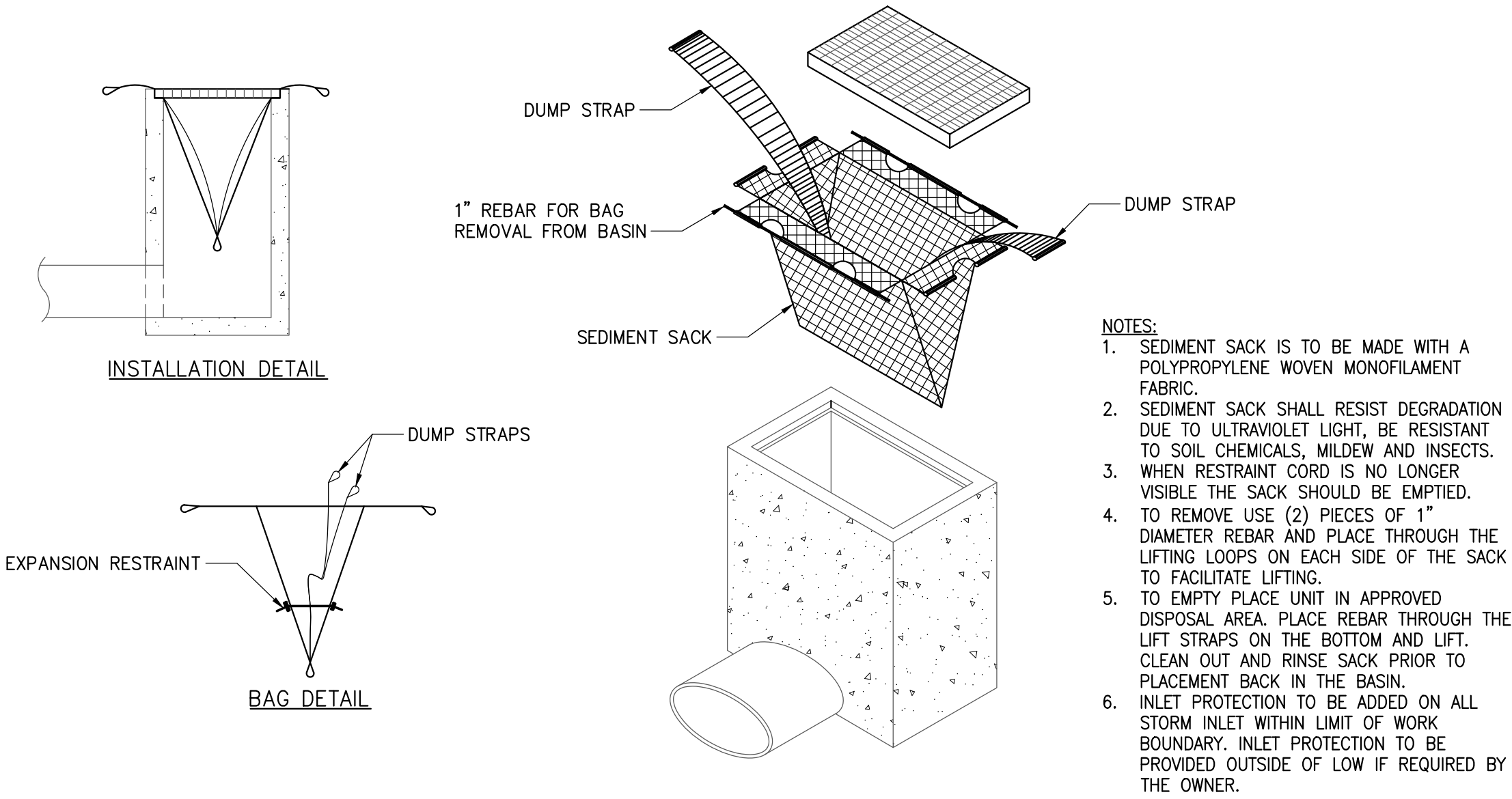






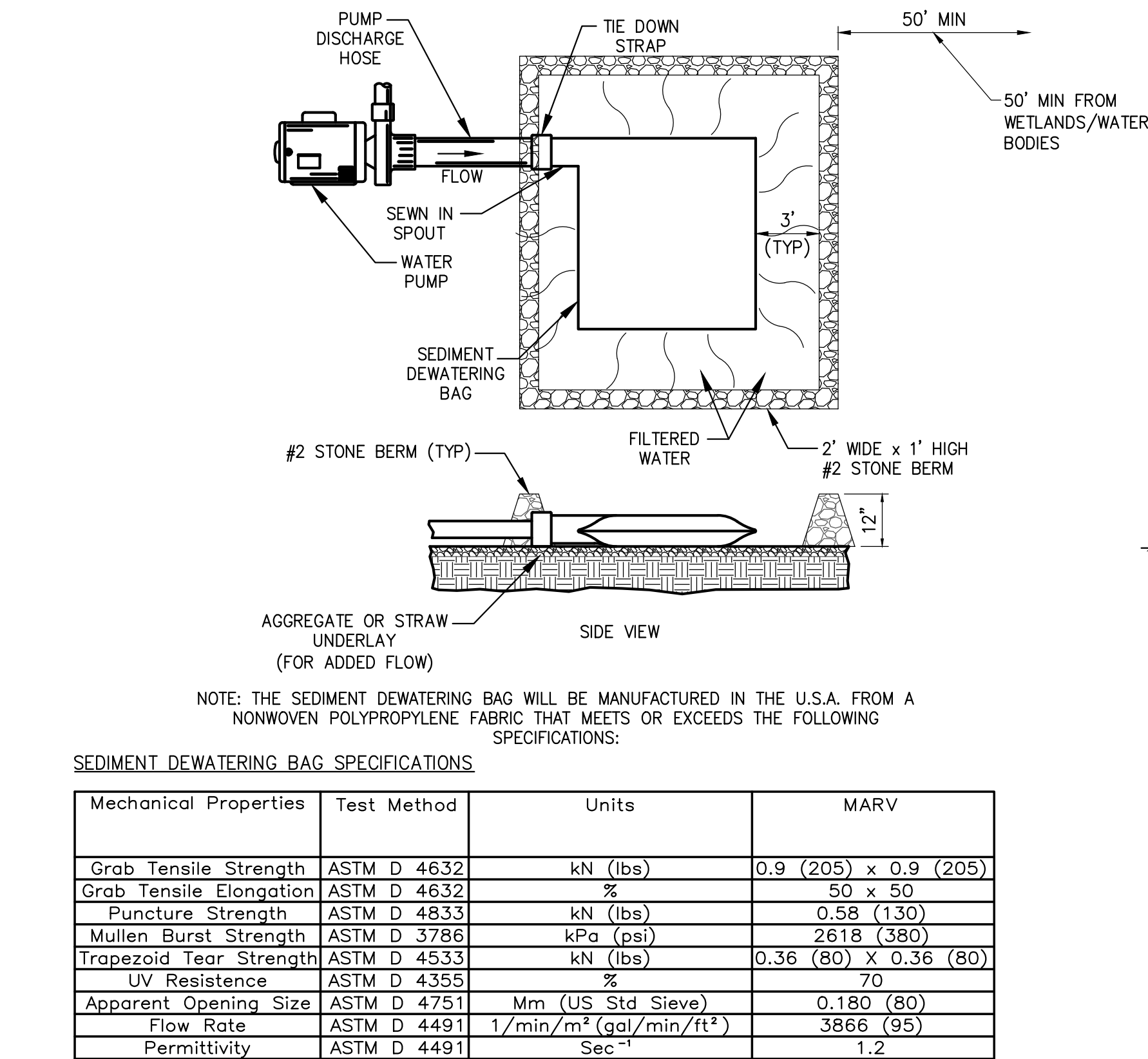
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1

INLET PROTECTION

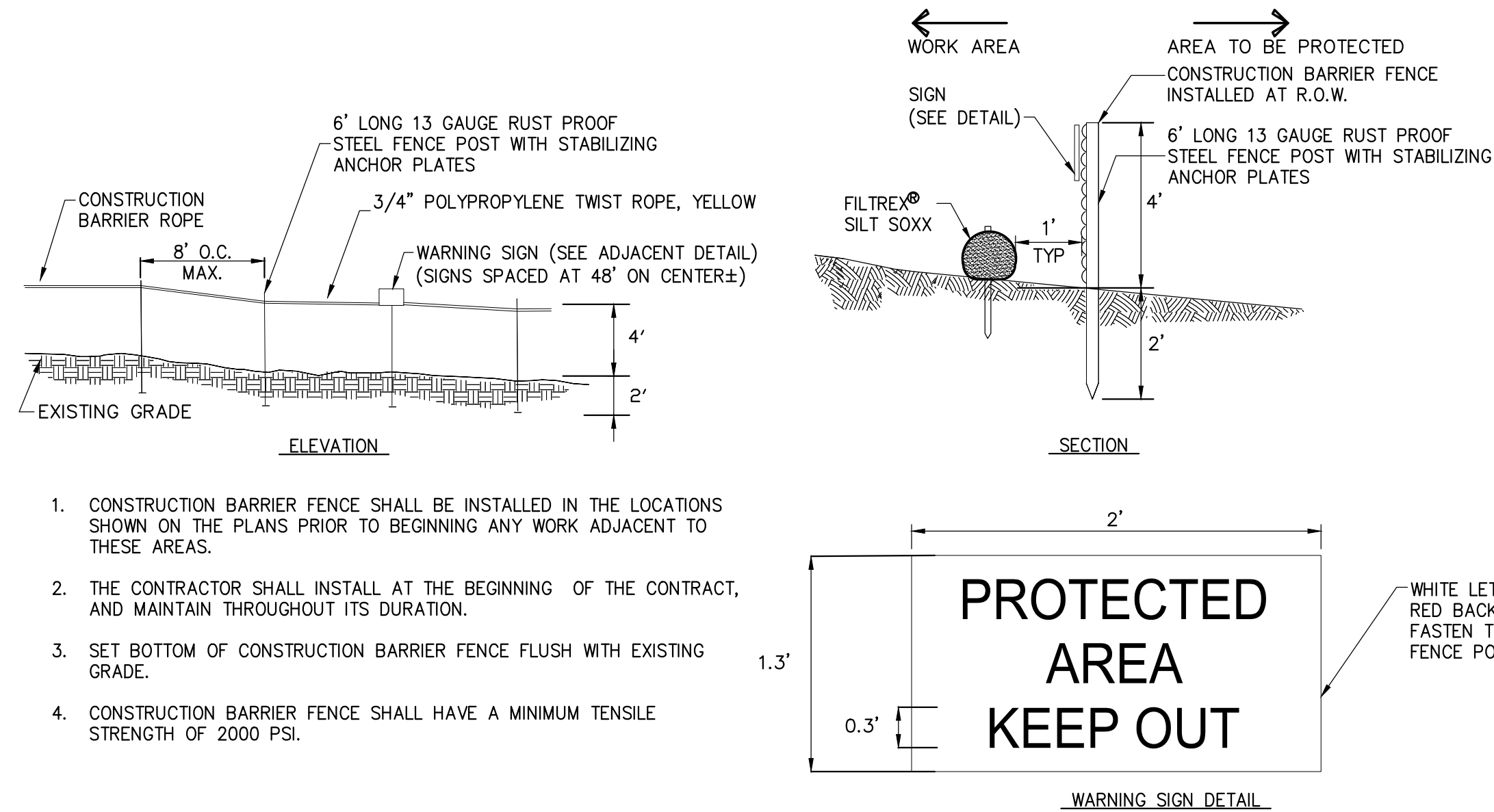
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4

SEDIMENT DEWATERING BAG

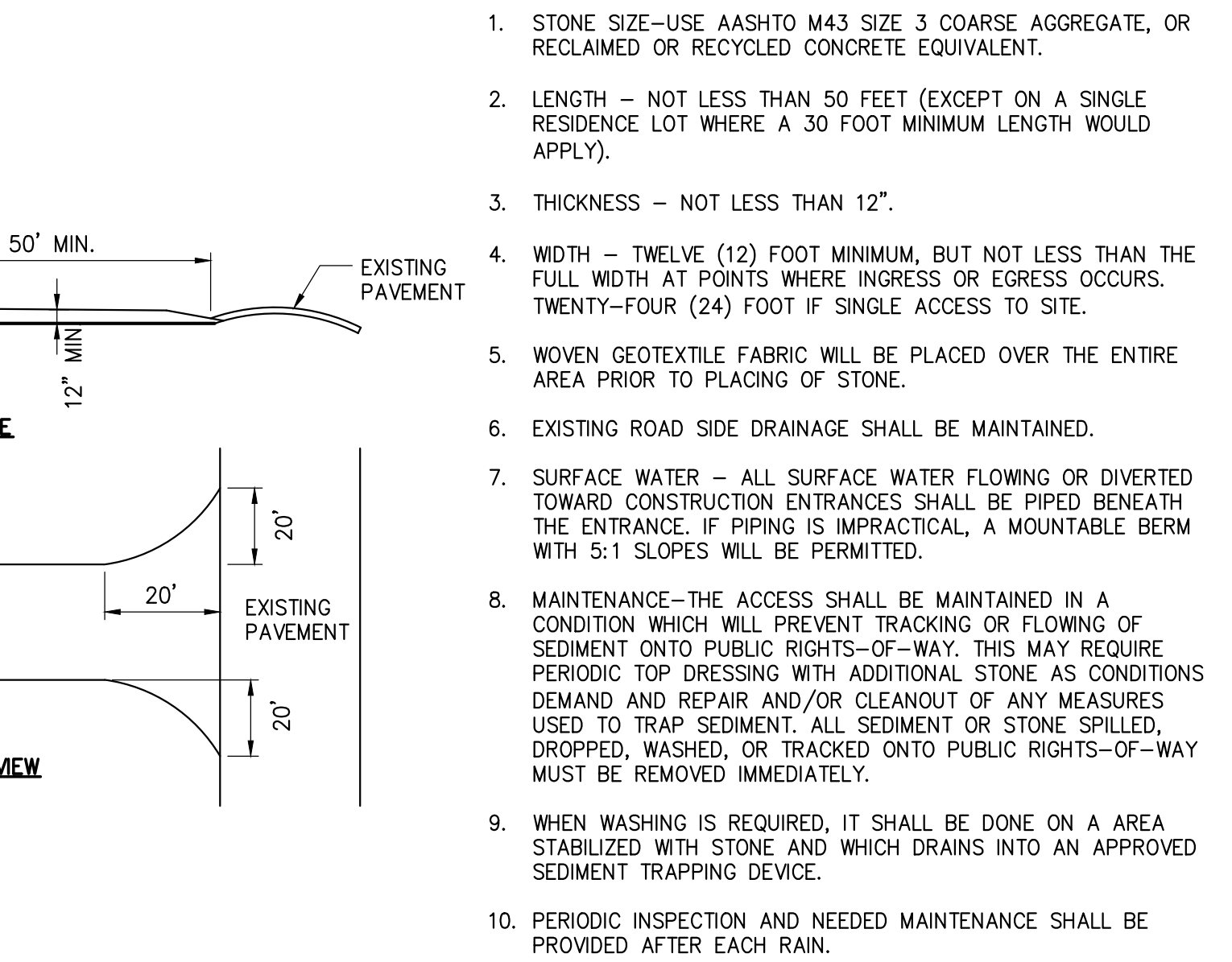
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2

WETLAND PROTECTION FENCE

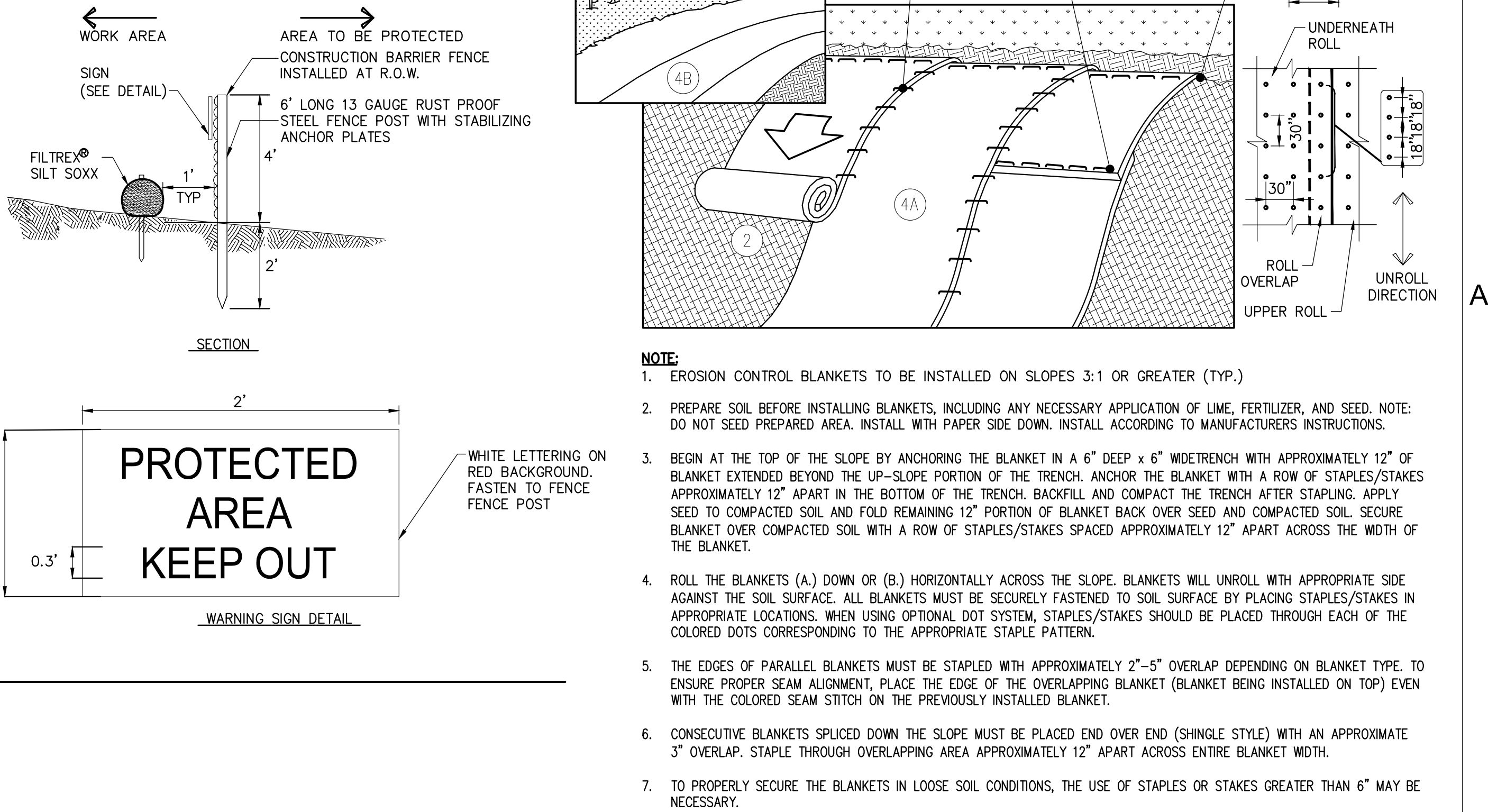
SCALE: N.T.S.



5

STABILIZED CONSTRUCTION ACCESS

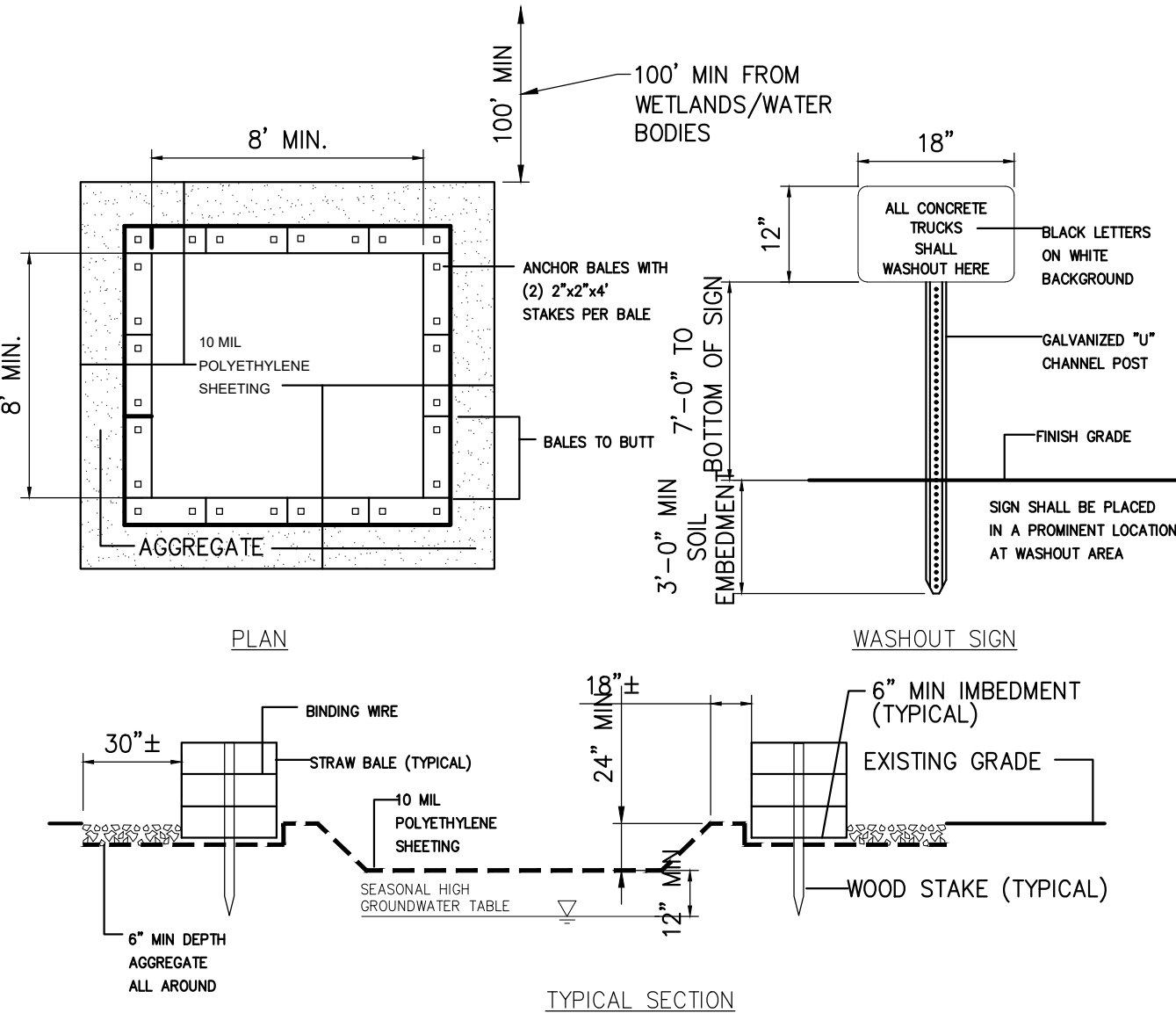
SCALE: N.T.S.



3

EROSION CONTROL BANK STABILIZATION DETAIL

SCALE: N.T.S.



6

CONCRETE WASHOUT AREA

SCALE: N.T.S.

- MAINTENANCE NOTES:**
1. ALL CONCRETE WASHOUT FACILITIES SHALL BE INSPECTED DAILY. DAMAGED OR LEAKING FACILITIES SHALL BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY. EXCESS RAINWATER THAT HAS ACCUMULATED OVER HARDENED CONCRETE SHALL BE PUMPED TO A STABILIZED AREA SUCH AS A GRASS FILTER STRIP.
  2. ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE CAPACITY OF THE STRUCTURE IS FILLED. ANY EXCESS WASH WATER SHALL BE PUMPED INTO A CONTAINMENT VESSEL AND PROPERLY DISPOSED OF OFF SITE.
  3. DISPOSAL OF THE HARDENED MATERIAL SHALL BE OFF-SITE IN A CONSTRUCTION/DEMOLITION LANDFILL.
  4. THE PLASTIC LINER SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.
  5. INSPECT THE PROJECT SITE FREQUENTLY TO ENSURE THAT NO CONCRETE DISCHARGES ARE TAKING PLACE IN NON-DESIGNATED AREAS.
  6. LOCATION(S) TO BE DETERMINED IN THE FIELD BY THE OWNER'S REPRESENTATIVE
  7. CONCRETE WASHOUTS SHALL NOT BE LOCATED WITHIN 200' OF ANY KNOWN WELL.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
F	04/14/2023	FINAL SUBMISSION	BL	BD
E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	BL	BD
D	11/16/2022	DRAFT FINAL SUBMISSION	BL	BD
C	05/20/2022	60% DESIGN SUBMISSION	BL	BD
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	BL	BD
A	02/18/2022	PRELIMINARY PROGRESS	BL	BD

**CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND**  
**EROSION AND SEDIMENT CONTROL DETAILS**

DRAWN BY:	BL	DESIGNED BY:	BL	APPROVED BY:	BD	SCALE	AS SHOWN	DATE	04/14/2023
						REV. NO.	F	SH.NO.	XX OF XXX

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	



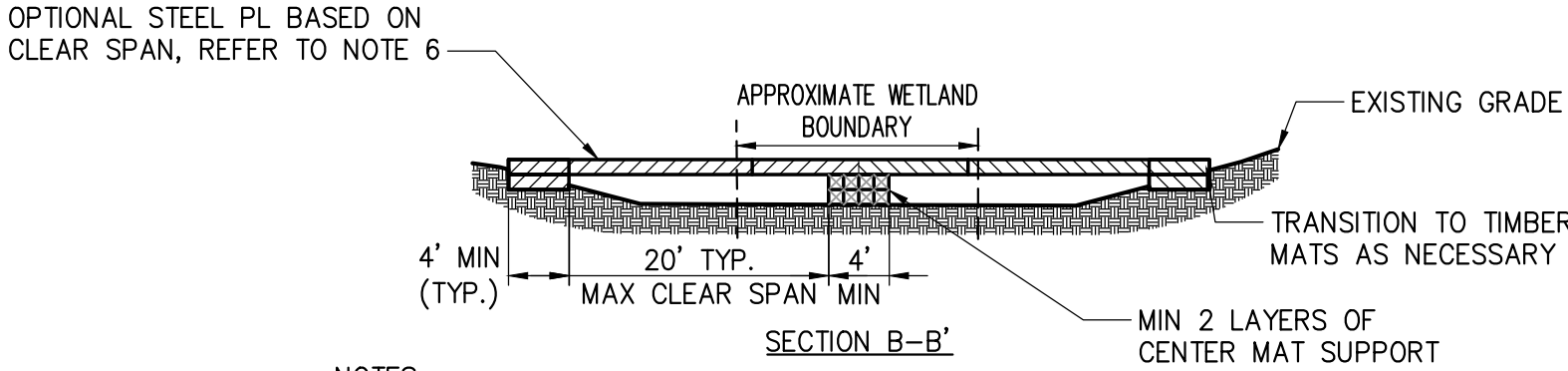
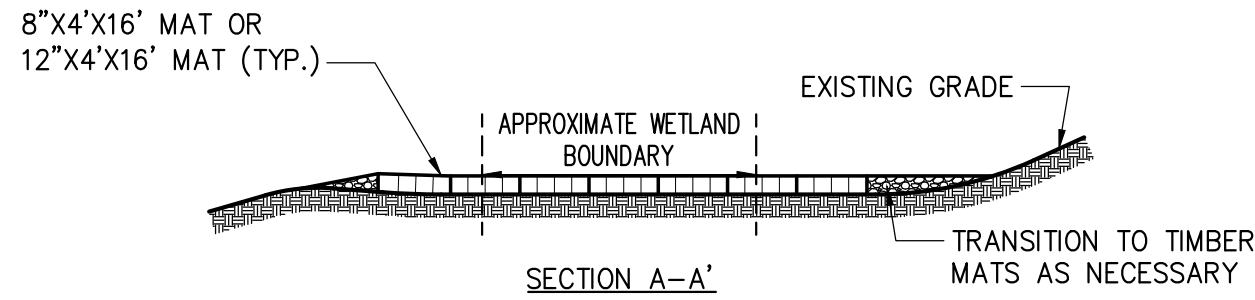
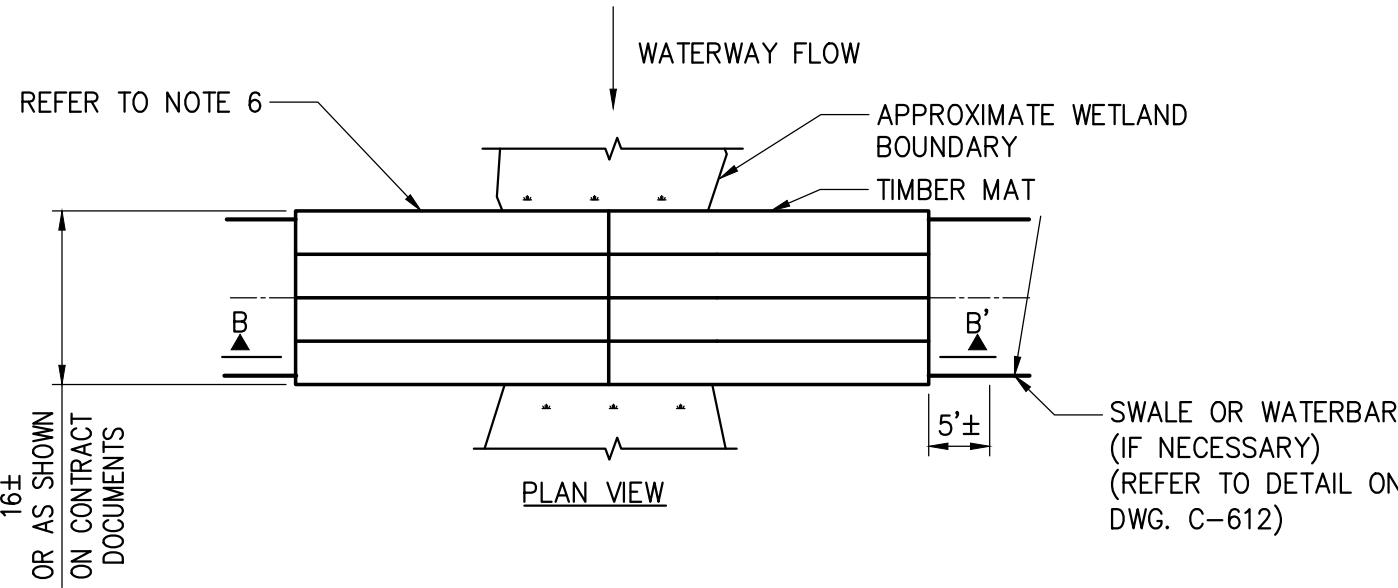
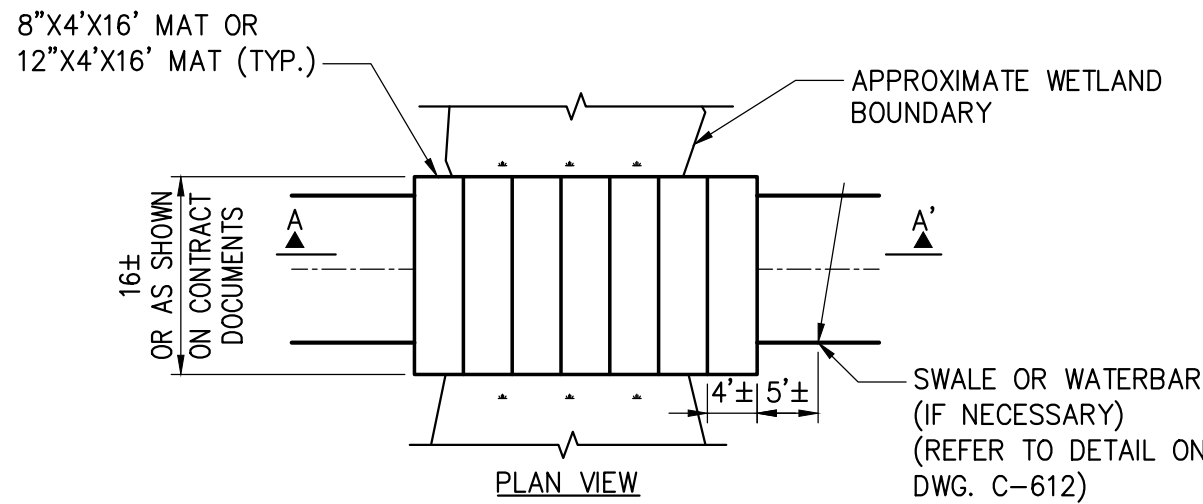
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1

2

3

4



NOTES:

1. TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO PREVENT RUTTING.
2. FOR CROSSINGS WITH LARGER SPANS THE CONTRACTOR SHALL CONSULT WITH THE TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER.
3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING.
4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND DURING RAIN EVENTS. SEDIMENT SHOULD BE REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION.
5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.
6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.
7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.
8. ALL EQUIPMENTS SHOULD MAINTAIN A MINIMUM OF 2 FT SETBACK FROM EDGE OF THE MATS WHILE CROSSING.
9. SINGLE OR MULTIPLE LAYERS OF MATS SHALL BE PLACED BASED ON EXISTING SOIL CONDITIONS.

NOTES:

1. IN-STREAM EXCAVATION SHOULD BE COMPLETED IN ACCORDANCE WITH "TEMPORARY ACCESS WATERWAY CROSSING" ON PAGE 2.32 OF THE 2016 NYSDEC STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (OR NEWEST VERSION) AND IN ACCORDANCE WITH SECTION 9.1 WATER BODIES IN THE PROJECT EM&CP.
2. THE CONSTRUCTION OF ANY CROSSING SHOULD NOT CAUSE A SIGNIFICANT WATER LEVEL DIFFERENCE BETWEEN THE UPSTREAM AND DOWNSTREAM WATER SURFACE ELEVATIONS. FISH SPAWNING OR MIGRATION DATES CAN VARY ACROSS NEW YORK, AND RESTRICTIONS IMPOSED BY THE NYSDEC MY VARY AND MUST BE VERIFIED. REFER TO CERTIFICATE OF CONDITIONS.
3. ALL FILL MATERIALS ASSOCIATED WITH THE ROADWAY APPROACH SHOULD BE LIMITED TO A MAXIMUM HEIGHT OF 2 FT ABOVE THE EXISTING FLOOD PLAIN ELEVATION.
4. A WATER DIVERTING STRUCTURE SUCH AS A SWALE OR WATER BAR SHOULD BE CONSTRUCTED (ACROSS THE ROADWAY ON BOTH ROADWAY APPROACHES) 50 FEET (MAXIMUM) ON EITHER SIDE OF THE WATERWAY CROSSING. THIS WILL PREVENT ROADWAY SURFACE RUNOFF FROM DIRECTLY ENTERING THE WATERWAY. THE 50 FEET MEASURED IS MEASURED FROM THE TOP OF THE WATERWAY BANK. IF THE ROADWAY APPROACH IS CONSTRUCTED WITH A REVERSE GRADE AWAY FROM THE WATERWAY, A SEPARATE DIVERTING STRUCTURE IS NOT REQUIRED.
5. ALL EQUIPMENTS SHOULD MAINTAIN A MINIMUM OF 2 FT SETBACK FROM EDGE OF THE MATS WHILE CROSSING.
6. CONTRACTOR SHALL CONSULT WITH TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER FOR APPROPRIATE MATTING SIZES AND LENGTHS AND REQUIRED SOIL BEARING PRESSURES.

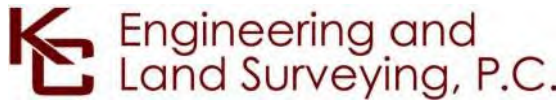
**OPTION "A"**  
NOT TO SCALE

**OPTION "B"**  
NOT TO SCALE

**1** **TIMBER MATTING (WETLAND CROSSING)**  
SCALE: N.T.S.

GENERAL NOTES:

1. TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.
2. CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER IMMEDIATELY.



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**ISSUED FOR PERMITTING**

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
F	04/14/2023	FINAL SUBMISSION	BL	BD
E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	BL	BD
D	11/16/2022	DRAFT FINAL SUBMISSION	BL	BD
C	05/20/2022	60% DESIGN SUBMISSION	BL	BD
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	BL	BD
A	02/18/2022	PRELIMINARY PROGRESS	BL	BD

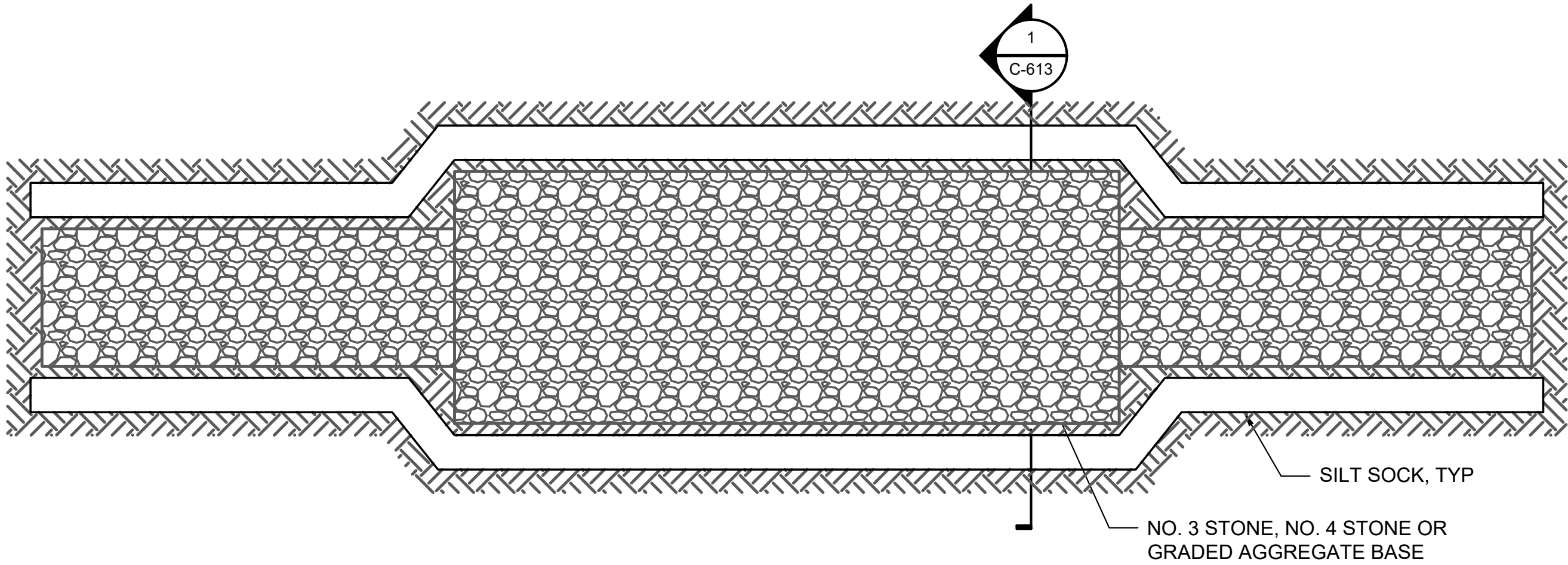
**CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WETLAND CROSSING DETAILS**

DRAWN BY:	BL	DESIGNED BY:	BL	APPROVED BY:	BD	SCALE	AS SHOWN	DATE	04/14/2023
						REV. NO.	F	SH.NO.	XX OF XXX

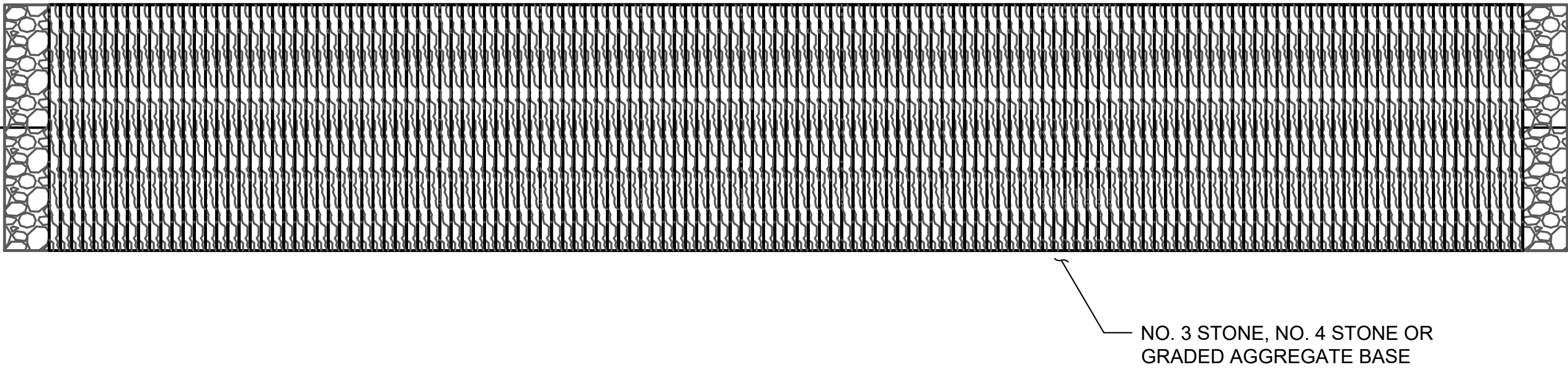
KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	<b>C-611</b>



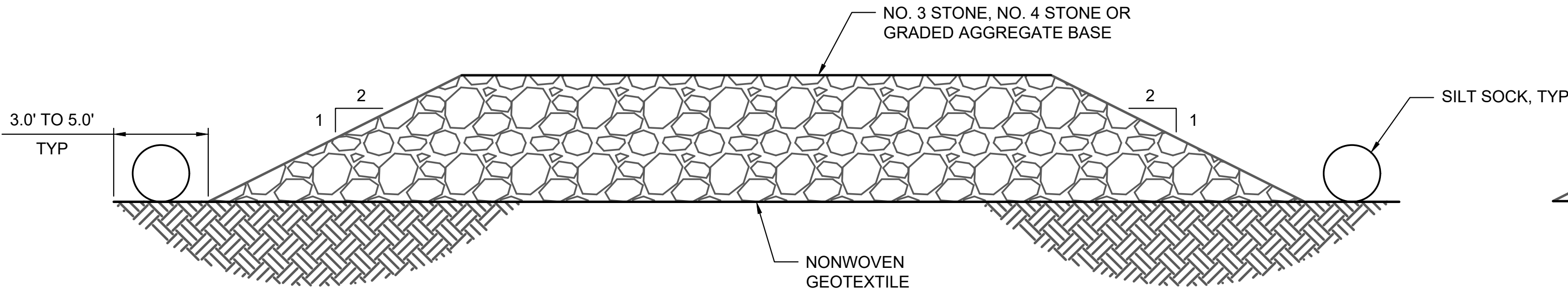
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**WETLAND WORKING SURFACE PLAN - OPTION A**  
NOT TO SCALE



**WETLAND WORKING SURFACE PLAN - OPTION B**  
NOT TO SCALE



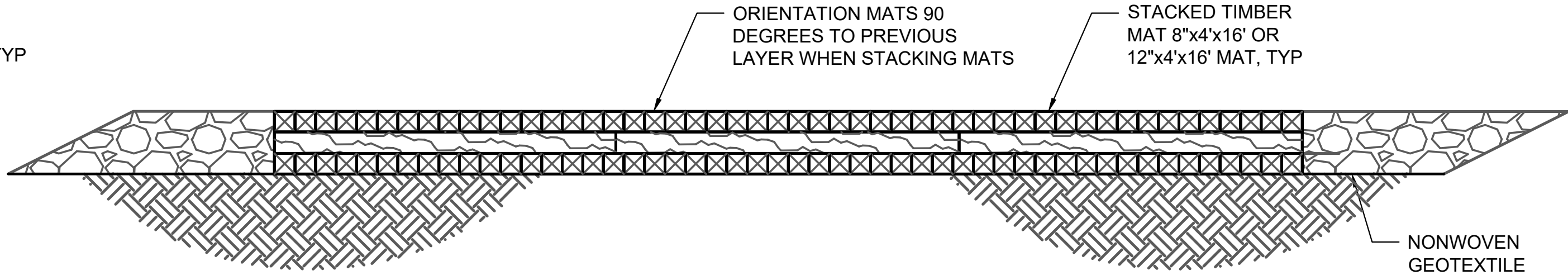
**SECTION 1**  
C-613 NOT TO SCALE

NOTES:

1. UNDERCUT AND REMOVE TOP SOIL PRIOR TO PLACING GEOTEXTILE FABRIC.
2. A LAYER OF CLEAN CRUSHED STONE SHALL BE LAID ON TOP OF THE GEOTEXTILE FABRIC.
3. GEOTEXTILE FABRIC SHALL EXTEND AT LEAST 3 FT TO 5 FT BEYOND THE EDGE OF STONE PLACEMENT TO MINIMIZE STONE ENTERING THE WETLAND AND FACILITATE REMOVAL OF THE ROAD.
4. SUITABLE CROSS DRAINING SHALL BE PROVIDED ACROSS THE ROAD FOR STREAM CHANNELS AND SURFACE FLOW.

GENERAL NOTES:

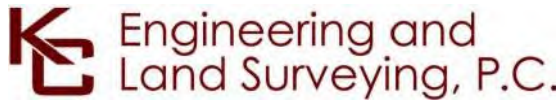
1. TIMBER:
  - A. TIMBER SHALL BE SELECT STRUCTURAL MIXED OAK WITH A MINIMUM BENDING STRESS OF 1250 PSI OR BETTER.
2. CONTRACTOR TO VERIFY ALL DIMENSIONS AND SITE CONDITIONS PRIOR TO COMMENCING WORK. ANY ERRORS, OMISSIONS, OR UNUSUAL CONDITIONS ARE TO BE REPORTED TO THE TEMPORARY STRUCTURES AND CONSTRUCTION DEVICES ENGINEER IMMEDIATELY.



**SECTION 2**  
C-613 NOT TO SCALE

NOTES:

1. TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO PREVENT RUTTING.
2. BASED ON ACTUAL SITE CONDITIONS, NUMBER OF TIMBER MAT LAYERS TO BE DETERMINED ON SITE.
3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING.
4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND DURING RAIN EVENTS. SEDIMENT SHOULD BE REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION.
5. PERIMETER EROSION AND SEDIMENT CONTROLS ARE REQUIRED TO BE INSTALLED PRIOR TO PLACING TIMBER MATTING.
6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.
7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.
8. GEOTEXTILE FABRIC TO BE INSTALLED UNDER MATTING. (TYP)



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ISSUED FOR PERMITTING

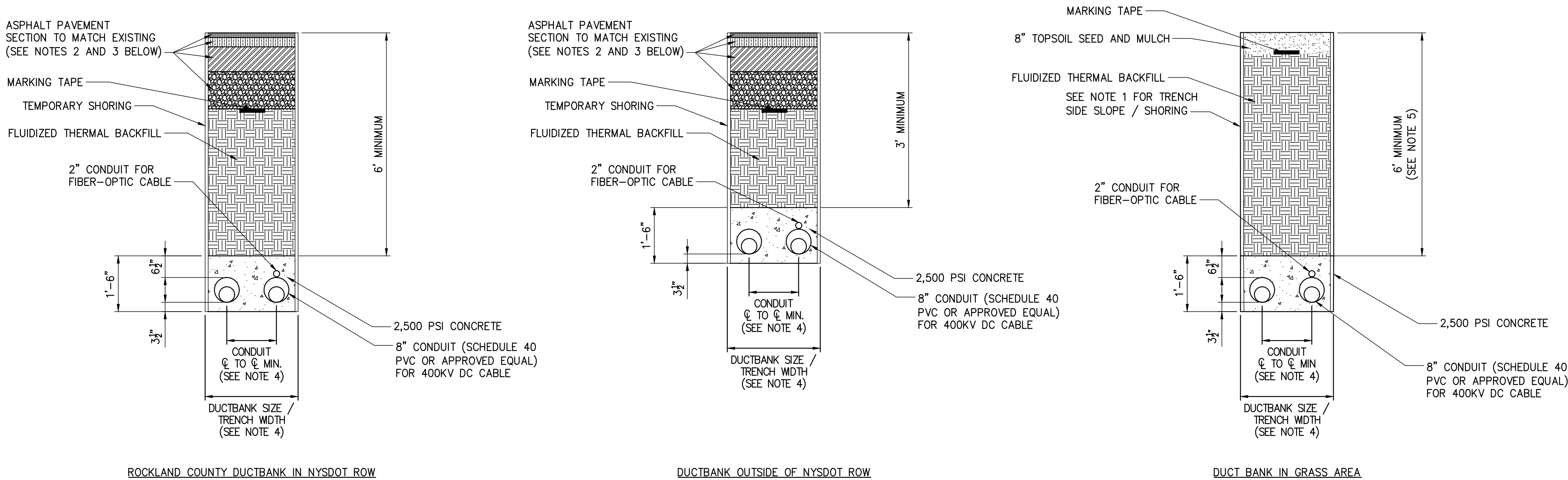
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E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	BL	BD
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C	05/20/2022	60% DESIGN SUBMISSION	BL	BD
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A	02/18/2022	PRELIMINARY PROGRESS	BL	BD

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WETLAND WORKING SURFACE

DRAWN BY: BL DESIGNED BY: BL APPROVED BY: BD  
SCALE AS SHOWN  
REV. NO. F

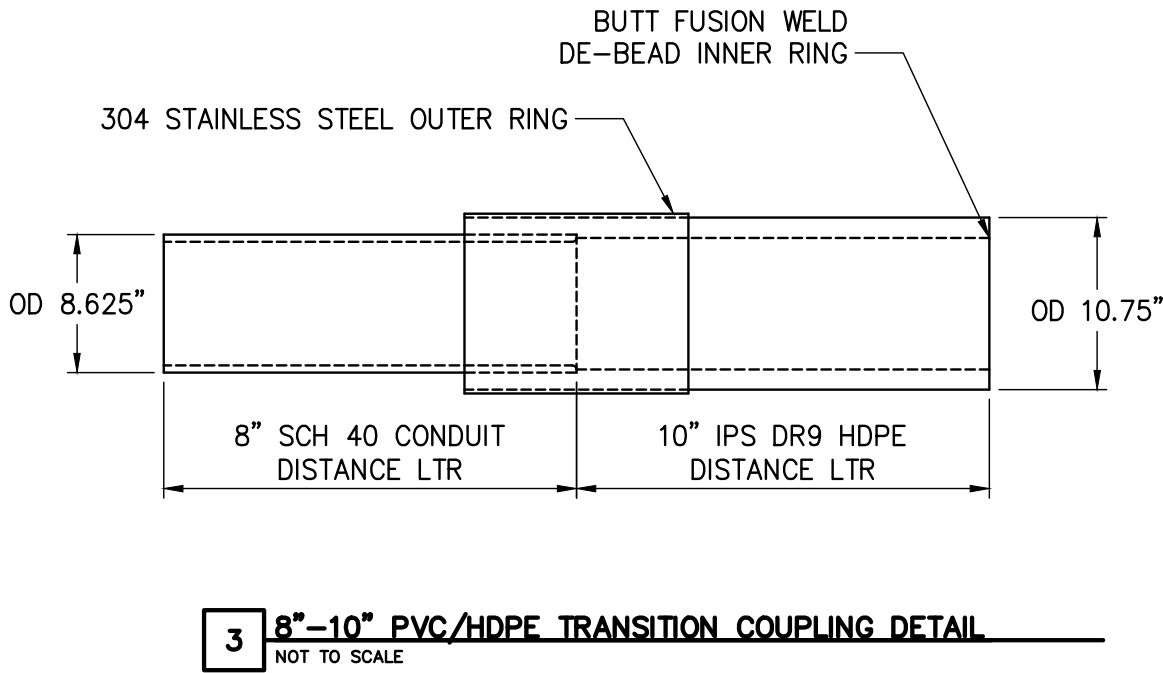
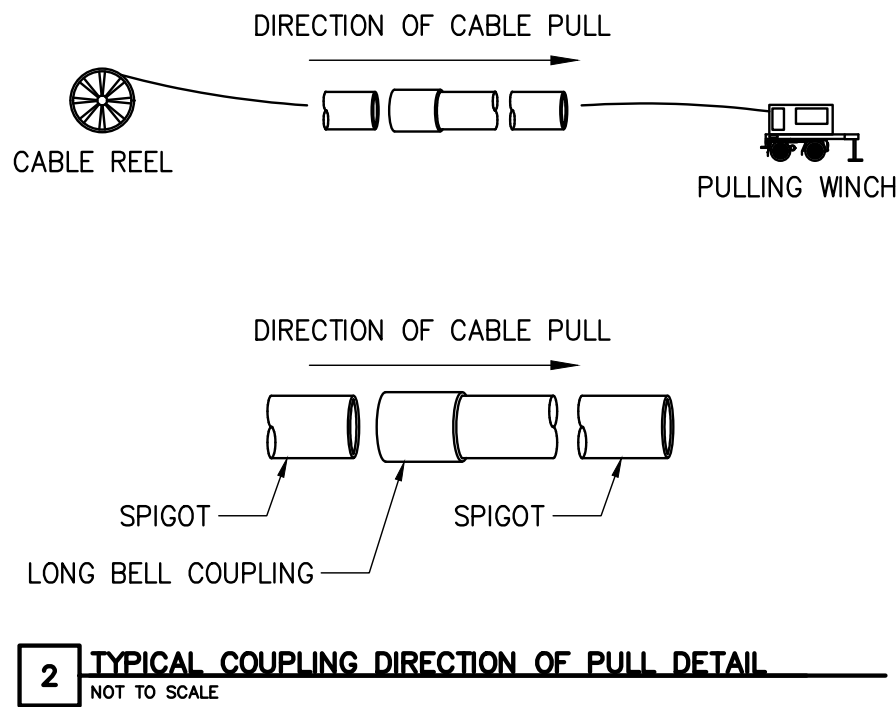
KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	C-613
DATE	04/14/2023
SH.NO.	XX OF XXX





- NOTES:
- SLOPING, BENCHING, OR SHORING SHALL BE IN ACCORDANCE WITH OSHA EXCAVATION STANDARDS, 29 CFR PART 1926, SUBPART P. AT LOCATIONS WHERE THE TRENCH IS NOT SHORED, SLOPING AND/OR BENCHING WILL DEPEND ON TYPE OF SOILS ENCOUNTERED ON SITE. SLOPE FROM EDGE OF ROADWAY TO BOTTOM OF EXCAVATIONS MAY BE FLATTER THAN 2:1 (H:V) FOR AASHTO HS-20 LOADING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EVALUATING SLOPE STABILITY BASED ON ACTUAL EQUIPMENT FOR SITE OPERATIONS AS DETERMINED BY A GEOTECHNICAL ENGINEER.
  - SEE DETAIL 1 ON DETAIL SHEET C-631 FOR PAVEMENT TRANSITION DETAIL.
  - SEE SHEET C-631 FOR SURFACE RESTORATION DETAILS. PARK ROAD RESTORATION IN STONY POINT SHALL HAVE 2" HMA SURFACE COURSE, 3" HMA BINDER BASE COURSE, AND 8" DENSE GRADE AGGREGATE SUB-BASE.
  - SEE PLAN AND PROFILE SHEETS FOR CONDUIT  $\phi$  TO  $\phi$  AND DUCTBANK SIZE TRENCH WIDTH (NOTE ABOVE PROFILE VIEW).
  - DEPTH OF COVER TO CONCRETE DUCTBANK SHALL BE 3' MIN OUTSIDE NYSDOT ROW.

1 TYPICAL TRENCHING DETAILS  
NOT TO SCALE



NOTE:  
THIS TRANSITION COUPLING COMES ASSEMBLED AS A UNIT. POLY-CAM, ISCO INDUSTRIES P/N: 737-1008PVC40PVI09 TRANSITION COUPLING SHALL ARRIVE FROM VENDOR WITH NO ROUGH EDGES OR PROTRUSIONS ON INTERIOR. INTEGRITY OF COUPLING TO BE FIELD-VERIFIED PRIOR TO INSTALLATION. IF UNSATISFACTORY, CONTRACTOR TO SHAPE OR SAND MINOR IRREGULARITIES PRIOR TO INSTALLATION.



TETRA TECH ENGINEERING AND SURVEYING P.C.  
(A NEW YORK PROFESSIONAL CORPORATION)

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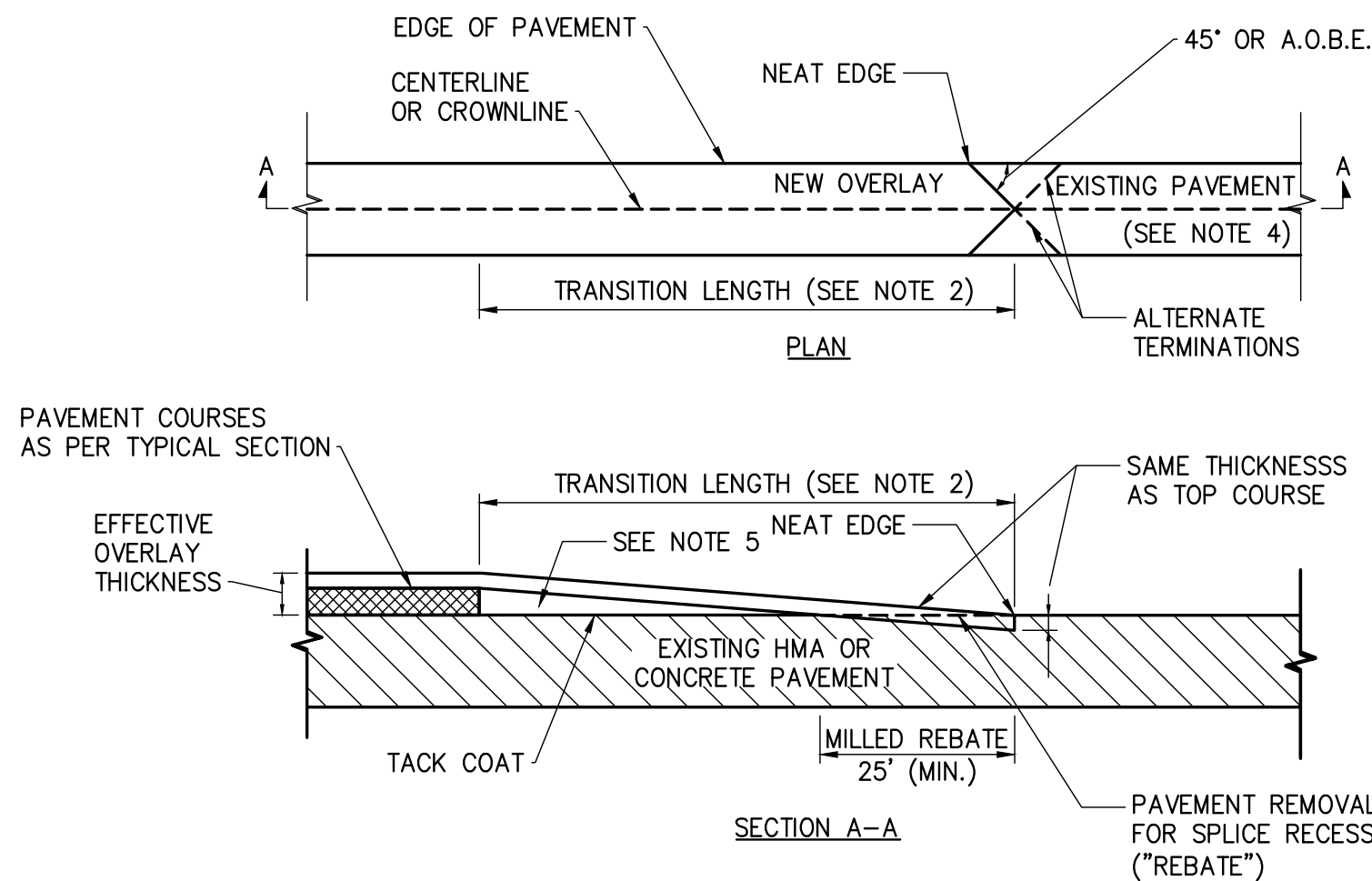
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E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	BV	TK
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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
TRENCH DETAILS

KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	C-621
DATE	04/14/2023
SH.NO.	OF --

DRAWN BY:	AR	DESIGNED BY:	BV	APPROVED BY:	TK	SCALE	AS SHOWN	DATE	04/14/2023
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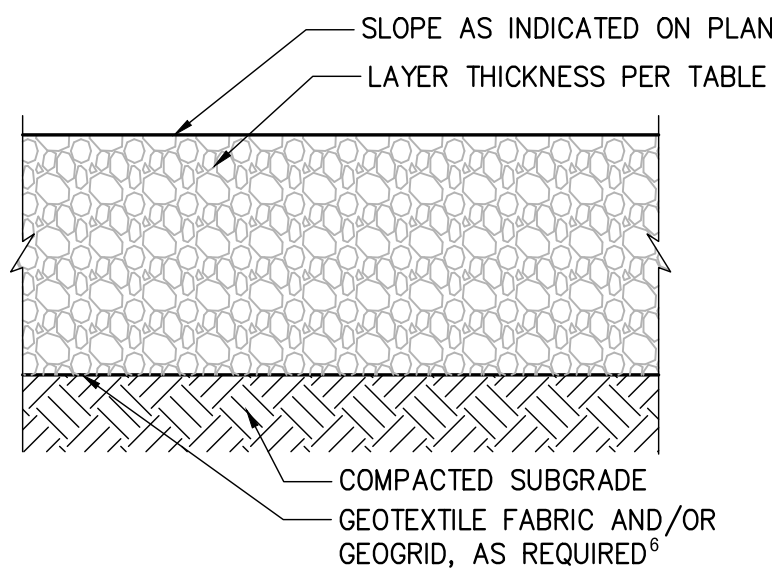




## NOTES:

- HOT MIX ASPHALT (HMA) OVERLAY SPLICES SHALL BE USED AT: ENDS OF HMA OVERLAY HIGHWAY SECTIONS, MAJOR INTERSECTIONS, AND OTHER LOCATIONS INDICATED IN THE PLANS.
  - THE TRANSITION LENGTH IN FEET SHALL NOT BE LESS THAN THE VALUE OBTAINED BY MULTIPLYING THE EFFECTIVE OVERLAY THICKNESS IN INCHES (DIFFERENCE BETWEEN THE EXISTING AND THE OVERLAID ELEVATIONS) BY THE K VALUE FROM THE TABLE FOR THE POSTED SPEED OF THE HIGHWAY. THE MINIMUM TRANSITION LENGTH IS 30'.
- EXAMPLE: IF THE POSTED SPEED IS 55 MPH.  
EFFECTIVE OVERLAY THICKNESS = 2"  
THEN THE MINIMUM TRANSITION LENGTH  
= 2" INCHES X 30 FT/IN = 60 FEET
- ALL SURFACES OF THE HMA OVERLAY SPLICE TRANSITION AREA SHALL BE CLEANED AND TACK-COATED PRIOR TO HMA PLACEMENT. THE COST OF MILLING REBATES, AND TACK COAT IN THE HMA OVERLAY SPLICE TRANSITION AREA SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS. COST OF LEAVING A NEAT EDGE SHALL BE INCLUDED IN THE MILLING ITEM.
  - SAW CUTS SHALL BE MADE SO THAT SURFACE RUNOFF IS DIRECTED TO THE EDGE OF PAVEMENT.
  - IN THE TRANSITION AREA, PAVEMENT COURSES OTHER THAN THE TOP COURSE SHALL BE FEATHERED OUT USING TOP COURSE OR OTHER APPROPRIATE MATERIAL. PAYMENT SHALL BE MADE UNDER THE APPROPRIATE ITEM.

		K VALUE TABLE FT/IN							
POSTED SPEED MPH	30	35	40	45	50	55	60	65	
K	16.67	19.17	21.67	24.17	27.50	30.00	32.50	35.00	



TEMPORARY ACCESS ROAD SECTION <sup>1,2,3,4</sup>				
CBR <sup>5</sup>	UNSTABILIZED	MIRAFI 180N GEOTEXTILE <sup>6</sup>	TENSAR BX1200 GEOGRID <sup>6</sup>	MIRAFI RSI SERIES GEOTEXTILE <sup>6</sup>
0.5	--	20 INCH RIP RAP <sup>7</sup> + 6 INCH AGGREGATE	--	20 INCH RIP RAP <sup>7</sup> + 4 INCH AGGREGATE (RS580I)
1.0	--	18 INCH AGGREGATE	12 INCH AGGREGATE	15 INCH AGGREGATE (RS280I)
1.5	--	12 INCH AGGREGATE	9 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
2.0	18 INCH AGGREGATE	11 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)
3.0+	15 INCH AGGREGATE	8 INCH AGGREGATE	6 INCH AGGREGATE	9 INCH AGGREGATE (RS280I)

## NOTES:

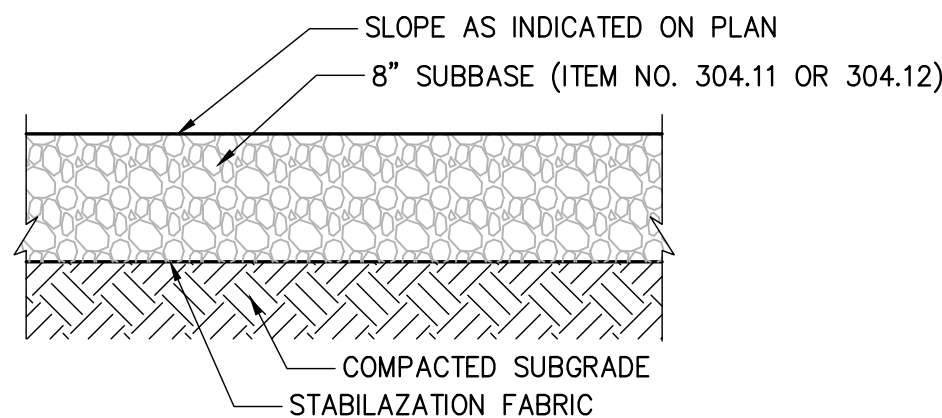
- TEMPORARY ACCESS ROAD SECTIONS PER KIEWIT ENGINEERING (NY) CORP.
- AGGREGATE SHALL BE NYSDOT TYPE 2 CRUSHED AGGREGATE OR APPROVED ALTERNATIVE.
- DESIGN CONSIDERS 1,000 PASSES OF MAXIMUM 22-KIP AXLE LOAD AND A DESIGN RUT DEPTH OF 3 INCHES. ADDITIONAL AXLE PASSES, HEAVIER AXLE LOADS, AND DETERIORATED SUBGRADE CONDITIONS MAY REQUIRE THICKER AGGREGATE SECTIONS OR ADDITIONAL MAINTENANCE.
- ALTERNATE TEMPORARY ACCESS ROAD DESIGNS MAY BE PROVIDED BY KIEWIT ENGINEERING, AS REQUIRED, BASED ON FIELD CONDITIONS AND TRAFFIC LOADING.
- ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER OR ALTERNATIVE METHOD APPROVED BY GEOTECHNICAL ENGINEER OF RECORD (EOR). CBR OF IN-SITU SOIL MAY VARY SEASONALLY DUE TO FREEZE/THAW AND BASED ON MOISTURE CONDITIONS.
- GEOGRID AND GEOTEXTILE
- GEOGRID AND GEOTEXTILES SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS.
- SPECIFIED GEOTEXTILE OR GEOGRID MAY BE REPLACED BY EQUIVALENT MATERIAL APPROVED BY EOR.
- GEOTEXTILE IS REQUIRED IN REGULATED WETLANDS AND AGRICULTURAL LANDS.
- GEOTEXTILE SEPARATOR FABRIC IS REQUIRED BENEATH GEOGRID ON COHESIVE SUBGRADE
- RIP RAP
- RIP RAP SHALL BE NYSDOT LIGHT STONE FILL OR APPROVED ALTERNATIVE.
- A LAYER OF #57 STONE IS RECOMMENDED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE GEOTEXTILE FABRIC WHERE RIP RAP IS USED.

## 1 HOT MIX ASPHALT OVERLAY SPLICE

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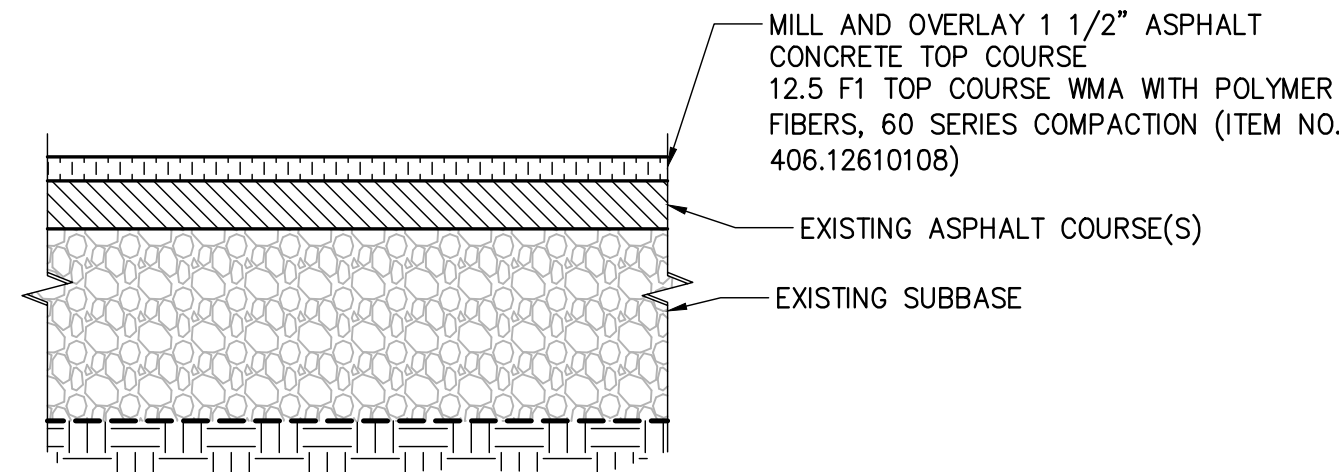
## 2 TEMPORARY ACCESS ROAD

SCALE: N.T.S.



## 3 GRAVEL PAVEMENT

SCALE: N.T.S.

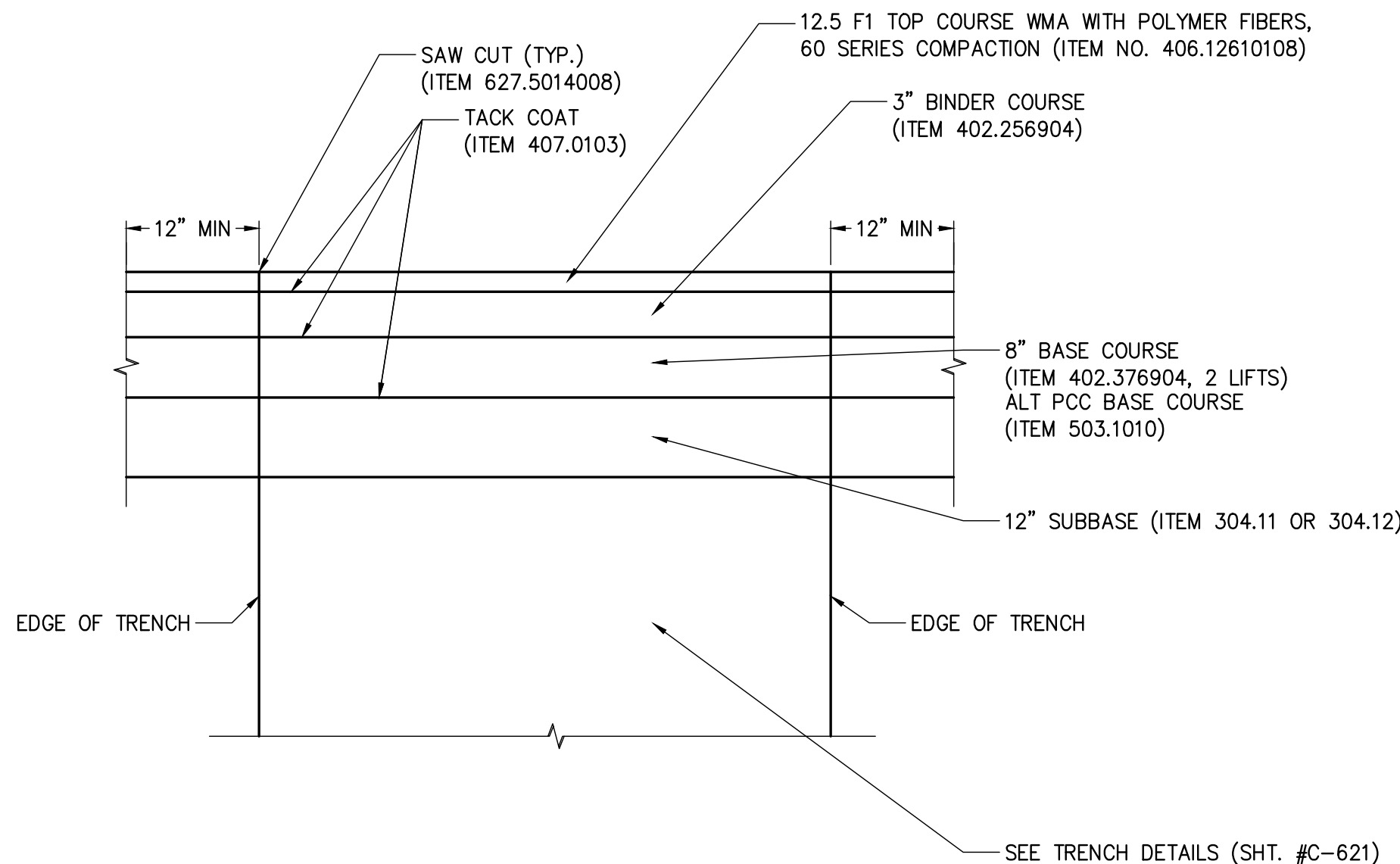


## NOTES:

- APPLY TACKCOAT TO MILLED SURFACE PRIOR TO PLACING ASPHALT CONCRETE TOP COURSE.
- ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

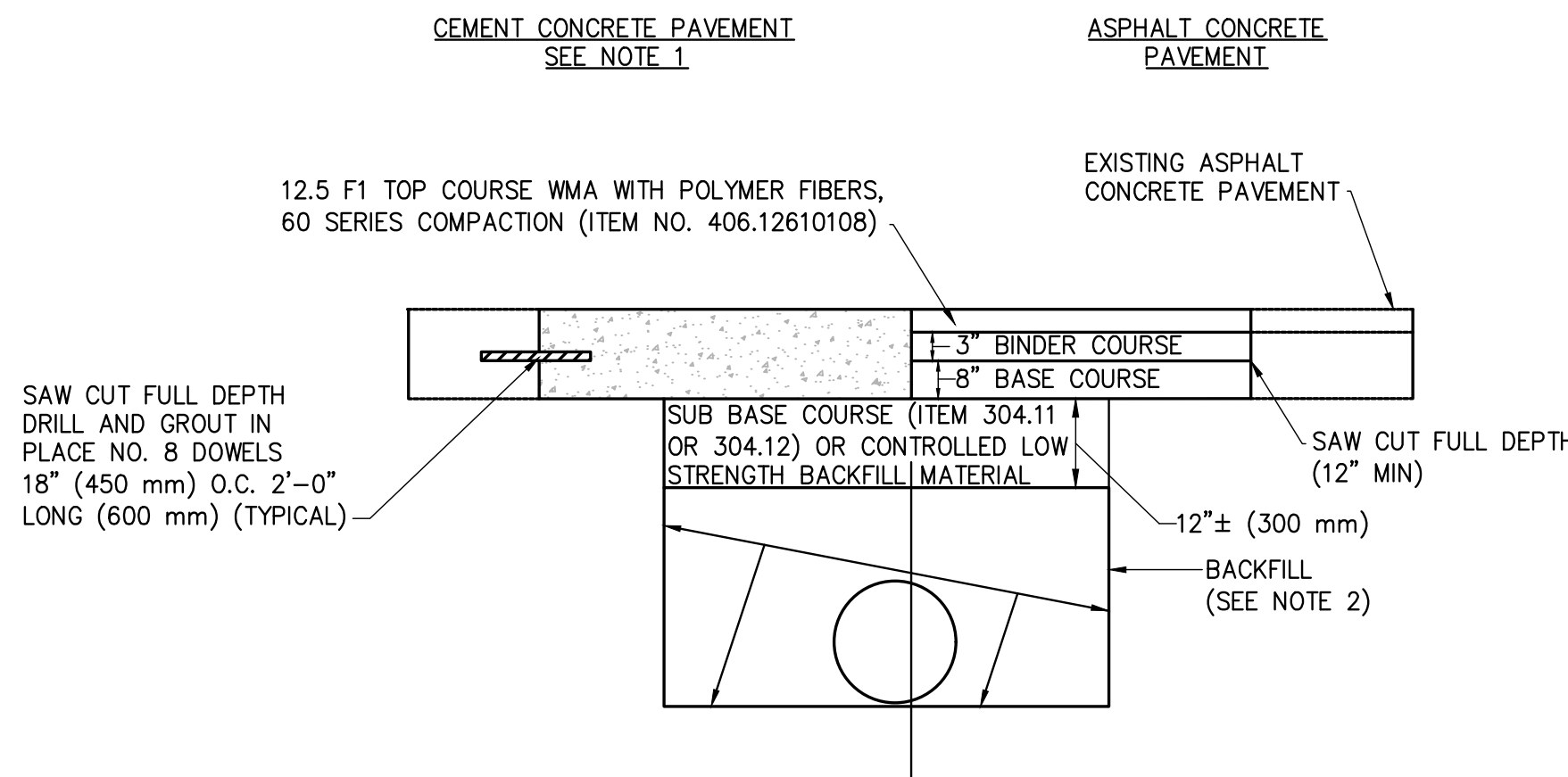
## 4 MILL AND OVERLAY ASPHALT CONCRETE PAVEMENT DETAIL

SCALE: N.T.S.



## 5 PAVEMENT RESTORATION DETAIL

SCALE: N.T.S.

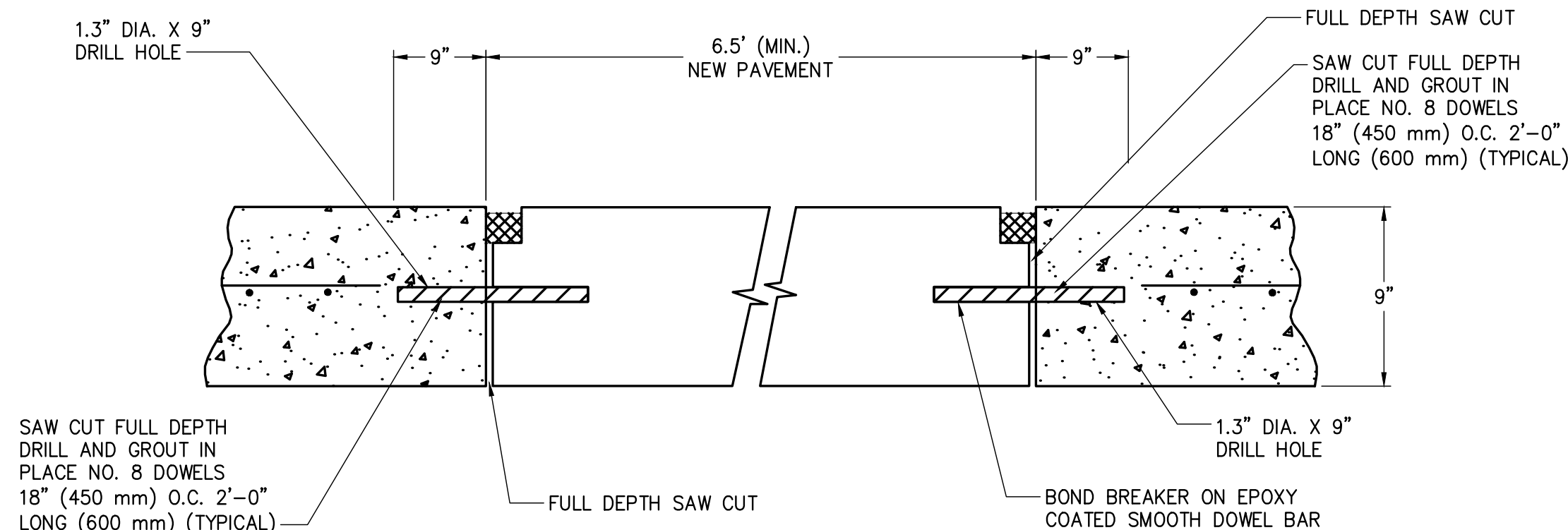


## 6 PAVEMENT REPLACEMENT DETAIL OF OPEN CUT CROSSING

SCALE: N.T.S.

## NOTES:

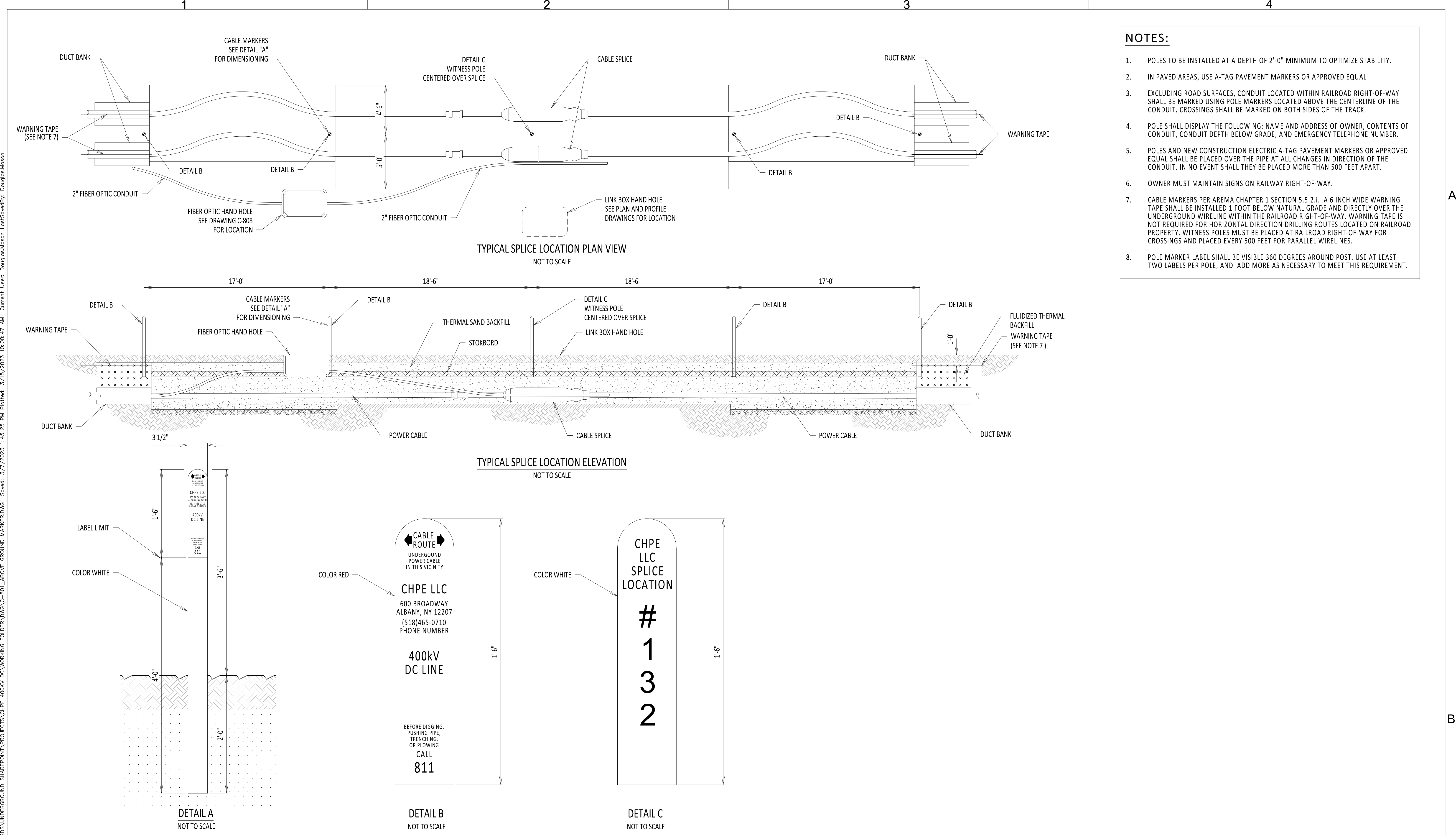
- WHEN CONCRETE IS USED, IT SHALL BE REINFORCED PORTLAND CEMENT CONCRETE WITH DOWELS INSTALLED AT THE REQUIRED SPACING. WHEN ASPHALT CONCRETE IS USED, IT SHALL CONSIST OF BASE, BINDER, AND TOP COURSES. THE TOTAL PAVEMENT THICKNESS SHALL MATCH EXISTING, UNLESS OTHERWISE REQUIRED BY REGION.
- WHEN "W" IS LESS THAN 18" (450 mm), CONTROLLED LOW STRENGTH BACKFILL MATERIAL SHALL BE USED. WHEN "W" IS 18" (450 mm) OR MORE, BACKFILL SHALL BE SELECT GRANULAR MATERIAL FROM THE BOTTOM OF THE TRENCH TO THE BOTTOM OF THE SUBBASE COURSE, PLACEMENT SHALL BE AS DESCRIBED IN SECTION 200, EARTHWORK OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION CURRENT SPECIFICATIONS AND AGENDA.



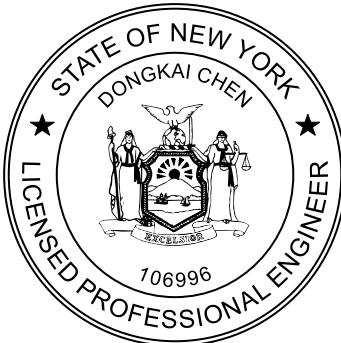
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E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	BV	TK
D	11/16/2022	DRAFT FINAL SUBMISSION	BV	TK
C	05/20/2022	60% DESIGN SUBMISSION	BV	TK
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	BV	TK
A	02/18/2022	PRELIMINARY PROGRESS	BV	TK



File: R:\INFO\IPD\USG STANDARDS\UNDERGROUND SHAREPOINT\PROJECTS\CHPE 400KV DC WORKING FOLDER\DWG\C-801\_ABOVE GROUND MARKER.DWG Saved: 3/7/2023 1:45:25 PM Plotted: 3/15/2023 10:00:47 AM Current User: Douglas Mason LetSaveBy: Douglas Mason



- NOTES:**
- POLES TO BE INSTALLED AT A DEPTH OF 2'-0" MINIMUM TO OPTIMIZE STABILITY.
  - IN PAVED AREAS, USE A-TAG PAVEMENT MARKERS OR APPROVED EQUAL
  - EXCLUDING ROAD SURFACES, CONDUIT LOCATED WITHIN RAILROAD RIGHT-OF-WAY SHALL BE MARKED USING POLE MARKERS LOCATED ABOVE THE CENTERLINE OF THE CONDUIT. CROSSINGS SHALL BE MARKED ON BOTH SIDES OF THE TRACK.
  - POLE SHALL DISPLAY THE FOLLOWING: NAME AND ADDRESS OF OWNER, CONTENTS OF CONDUIT, CONDUIT DEPTH BELOW GRADE, AND EMERGENCY TELEPHONE NUMBER.
  - POLES AND NEW CONSTRUCTION ELECTRIC A-TAG PAVEMENT MARKERS OR APPROVED EQUAL SHALL BE PLACED OVER THE PIPE AT ALL CHANGES IN DIRECTION OF THE CONDUIT. IN NO EVENT SHALL THEY BE PLACED MORE THAN 500 FEET APART.
  - OWNER MUST MAINTAIN SIGNS ON RAILWAY RIGHT-OF-WAY.
  - CABLE MARKERS PER AREMA CHAPTER 1 SECTION 5.5.2.i. A 6 INCH WIDE WARNING TAPE SHALL BE INSTALLED 1 FOOT BELOW NATURAL GRADE AND DIRECTLY OVER THE UNDERGROUND WIRELINE WITHIN THE RAILROAD RIGHT-OF-WAY. WARNING TAPE IS NOT REQUIRED FOR HORIZONTAL DIRECTION DRILLING ROUTES LOCATED ON RAILROAD PROPERTY. WITNESS POLES MUST BE PLACED AT RAILROAD RIGHT-OF-WAY FOR CROSSINGS AND PLACED EVERY 500 FEET FOR PARALLEL WIRELINES.
  - POLE MARKER LABEL SHALL BE VISIBLE 360 DEGREES AROUND POST. USE AT LEAST TWO LABELS PER POLE, AND ADD MORE AS NECESSARY TO MEET THIS REQUIREMENT.



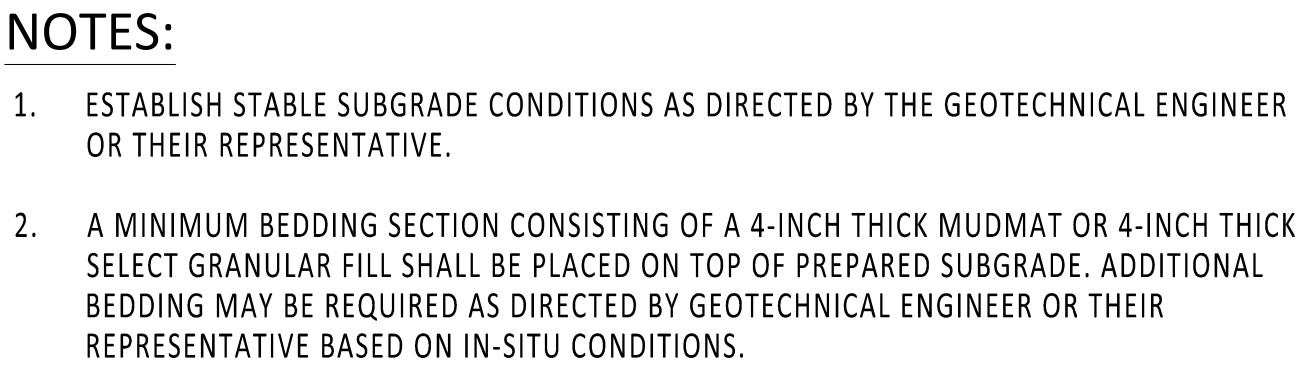
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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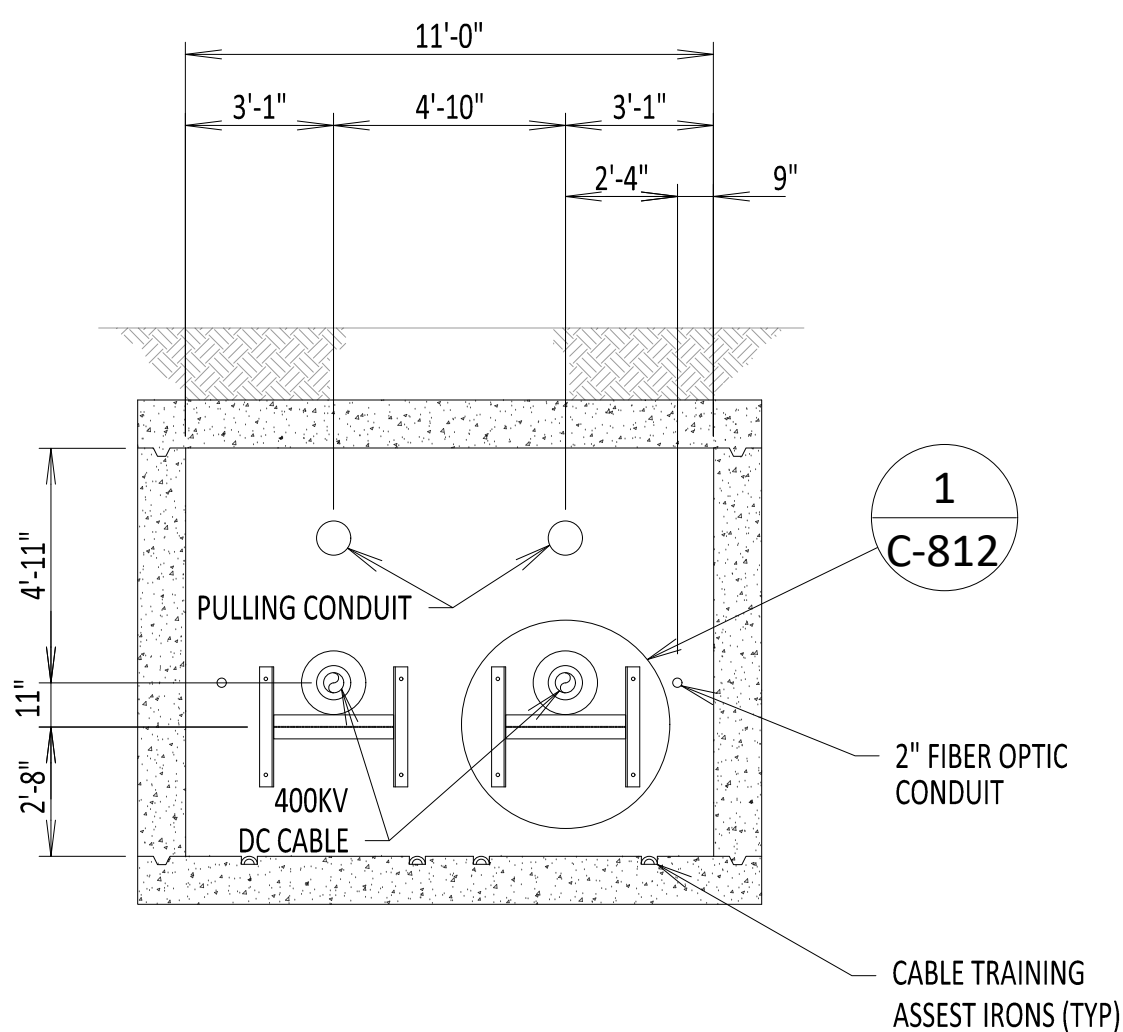
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS				KIEWIT PROJECT NO. 21162	
ABOVE GROUND MARKING DETAILS				CHA PROJECT NO. 066076	
				DRAWING NO. C-801	
				DATE 03/15/2023	
				SH.NO. OF	
DRAWN BY:	DLM	DESIGNED BY:	SD	APPROVED BY:	ASM
SCALE		REV.NO.			





REFERENCE DOCUMENTS		
LIST NO.	DOCUMENT NAME	DOCUMENT NO.
1	STRUCTURAL VAULT DRAWING	S-700
2	TYPICAL VAULT GROUNDING DETAILS	C-803
3	DUCT BANK CONNECTION DETAILS	C-812
4	FIBER OPTIC SPLICE DETAILS	C-855
5	FIBER OPTIC H-FRAME BRACKET DETAIL	C-856



SECTION A-A  
SCALE: 1/4" = 1'-0"

KIEWIT PROJECT NO.
21162
CHA PROJECT NO.
066076
DRAWING NO.

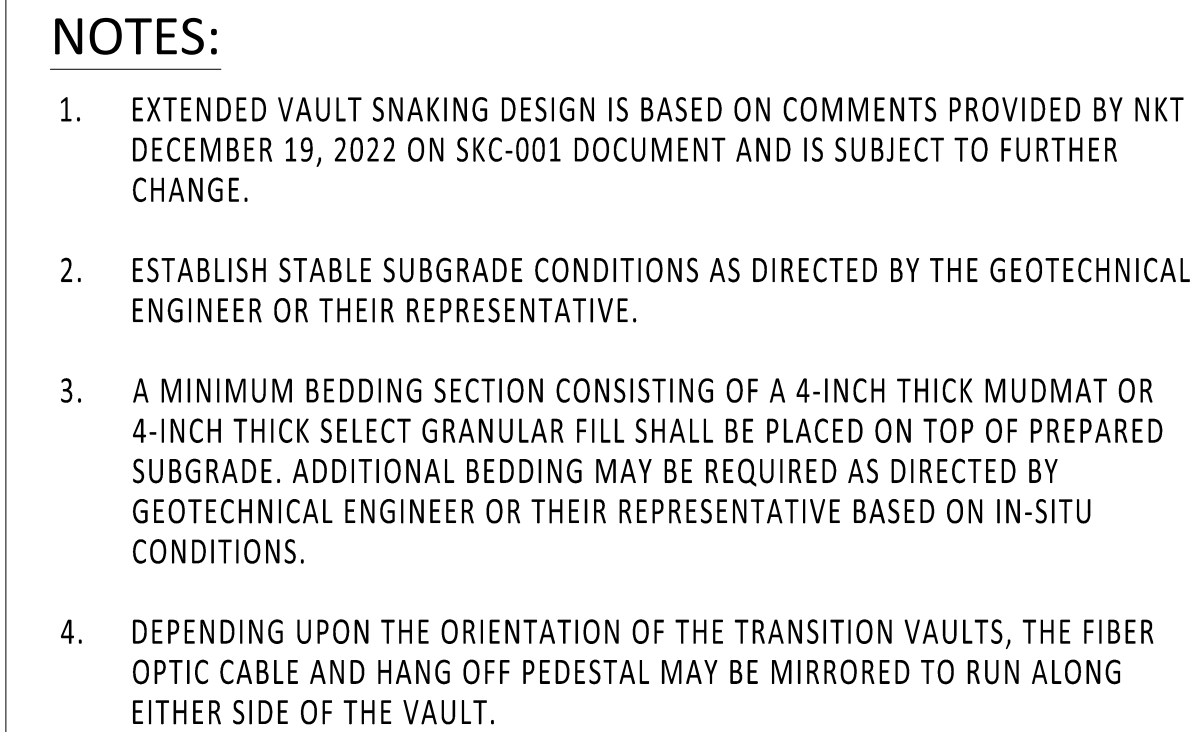
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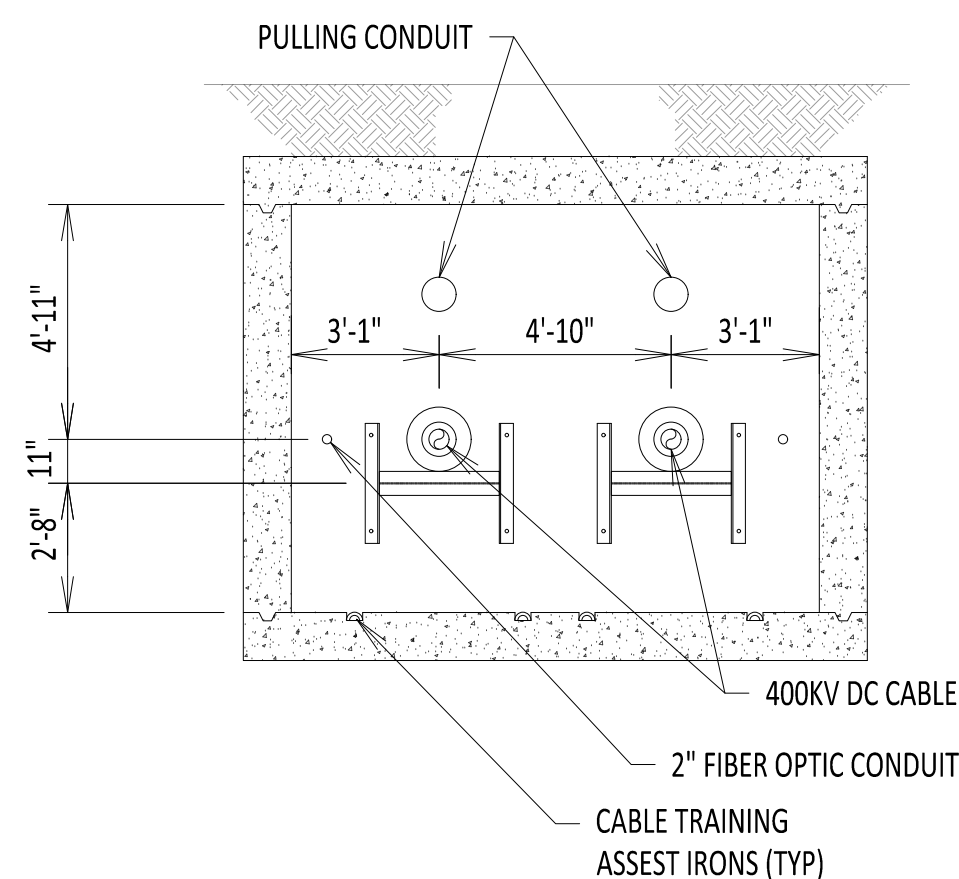








REFERENCE DOCUMENTS		
LIST NO.	DOCUMENT NAME	DOCUMENT NO.
1	TRANSITION VAULT PLAN AND ELEVATION	S-730
2	TRANSITION VAULT GROUNDING DETAILS	C-806
3	VAULT CONNECTION DETAILS	C-812
4	ENCLOSED VAULT WITH FIBER OPTICS	C-852
5	FIBER OPTIC SPLICE DIAGRAM	C-855
6	FIBER OPTIC H-FRAME BRACKET DETAIL	C-856



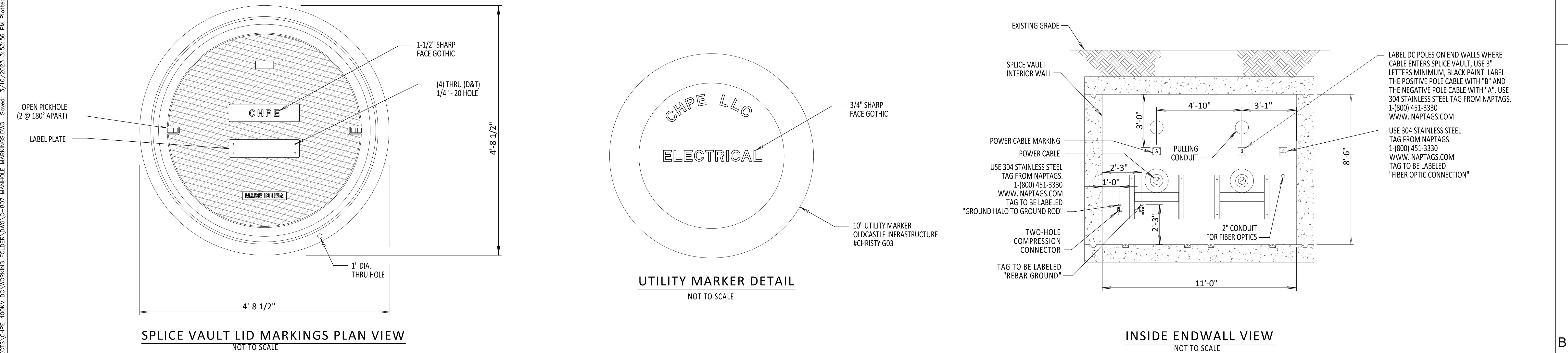
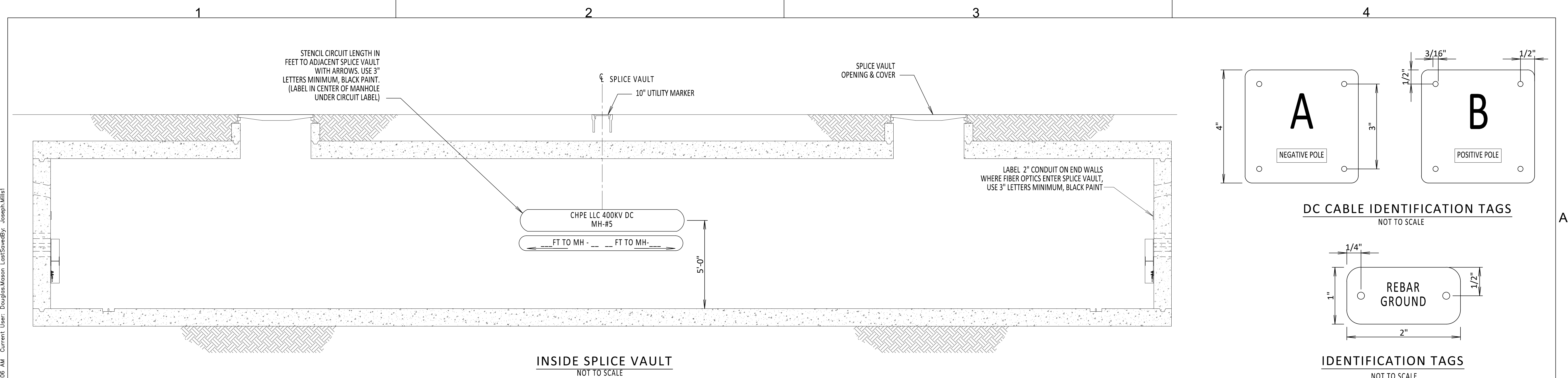
SECTION A-A  
SCALE: 1/4" = 1'-0"









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**CHPE**  
Champlain Hudson  
Power Express



**Kiewit**



STATE OF NEW YORK  
DONCKAI CHEN  
106996  
LICENSED PROFESSIONAL ENGINEER

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
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**CHAMPLAIN HUDSON POWER EXPRESS**

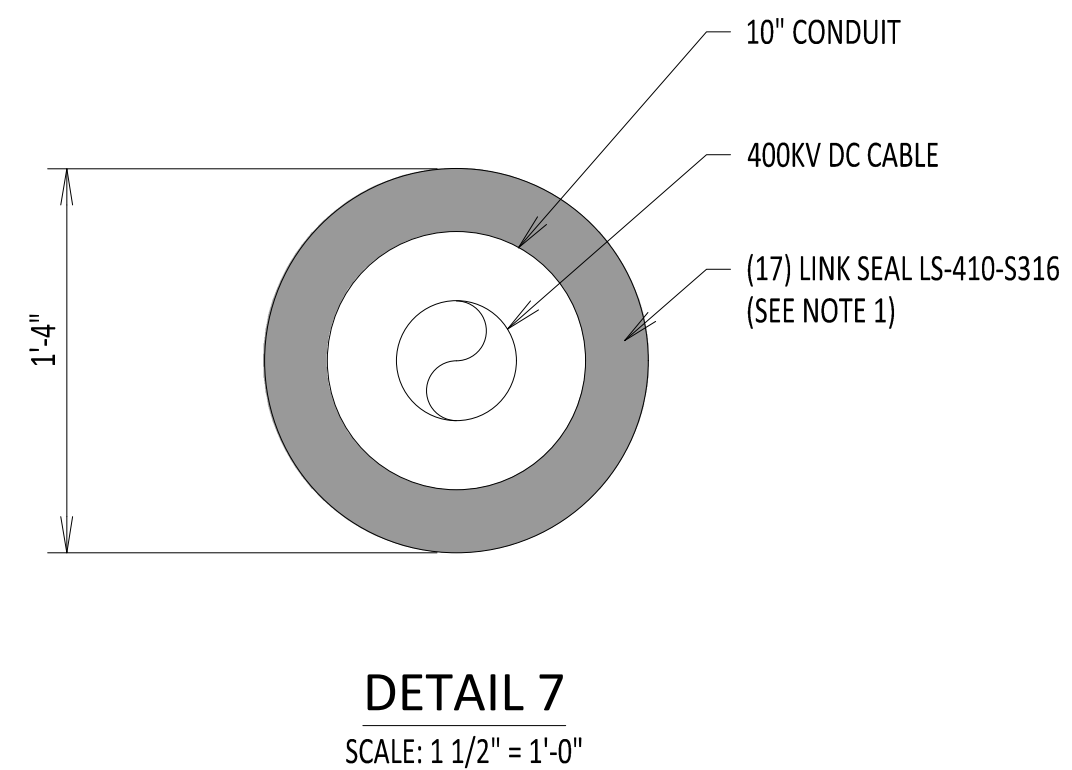
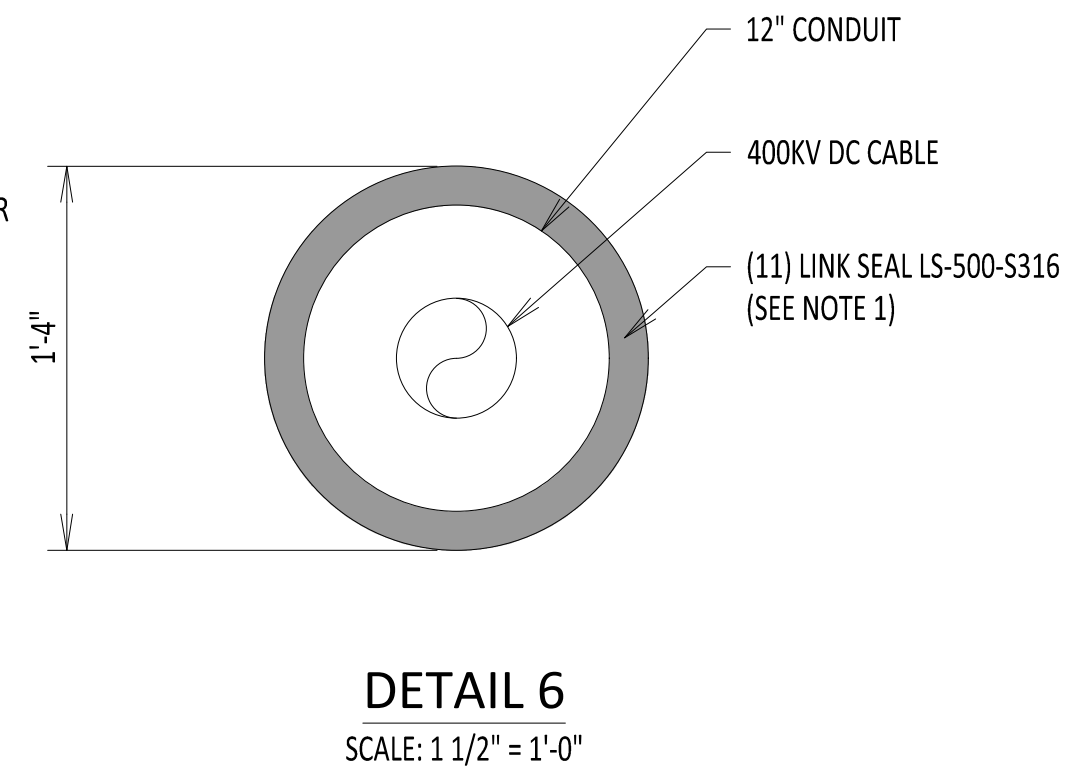
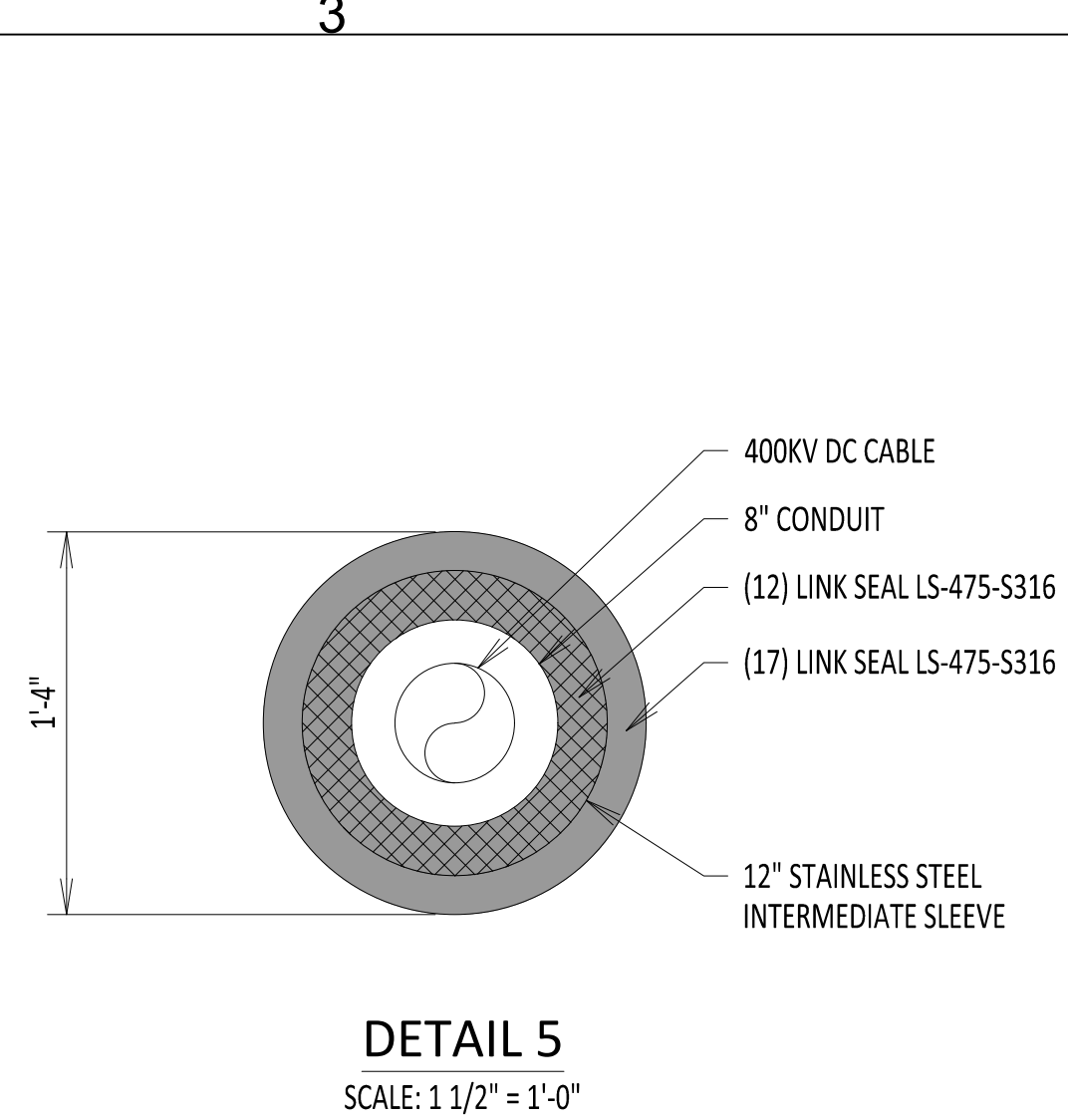
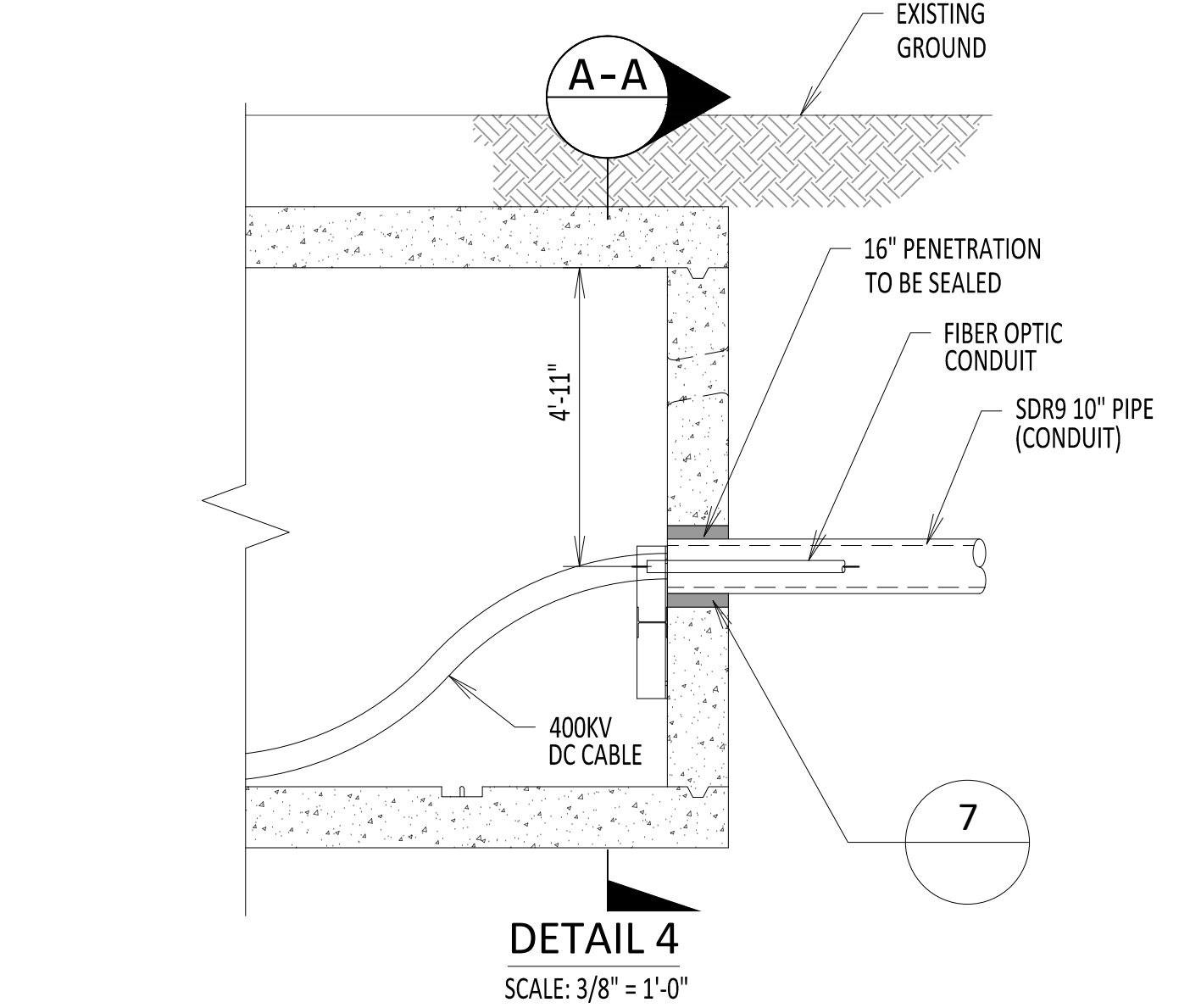
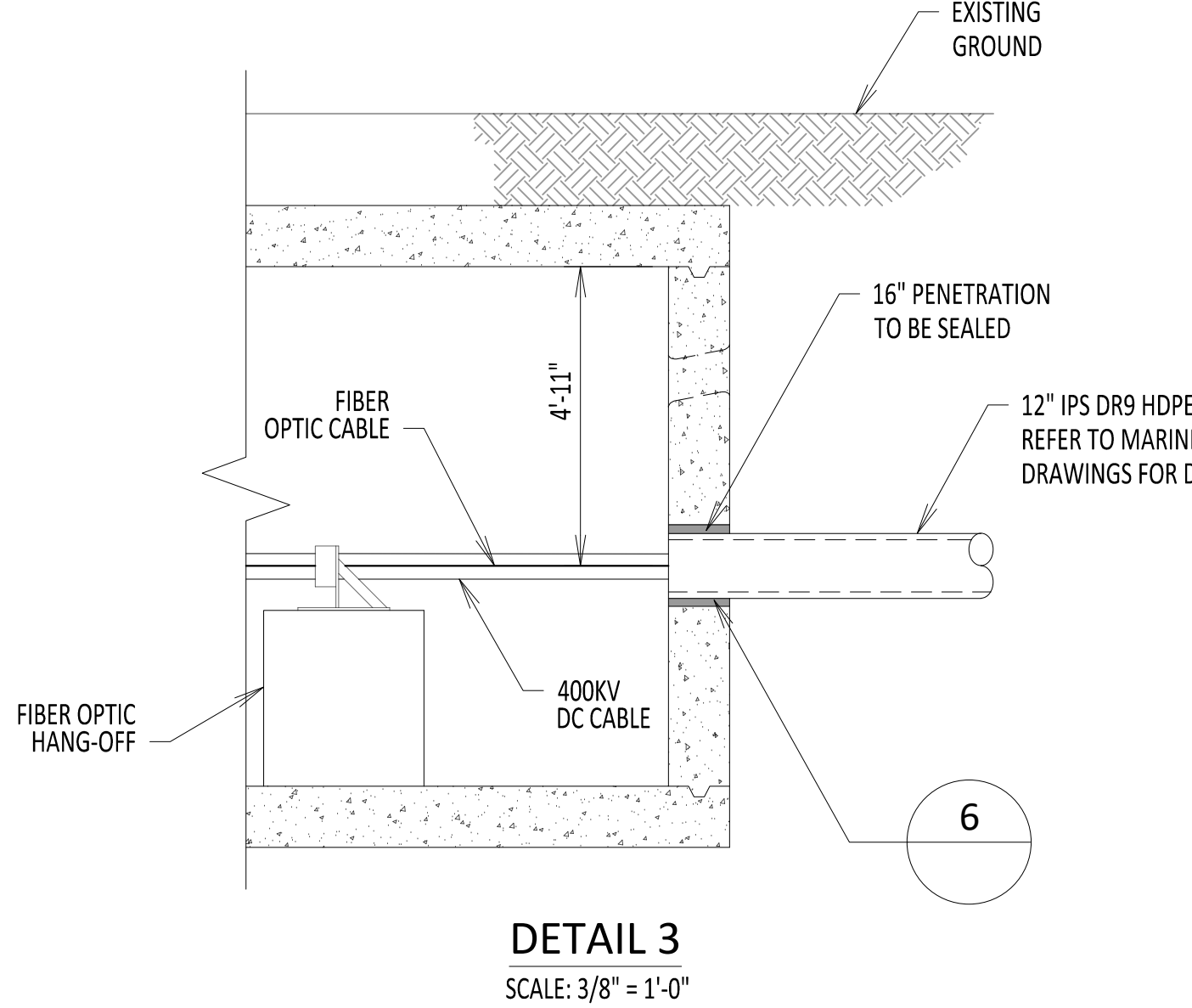
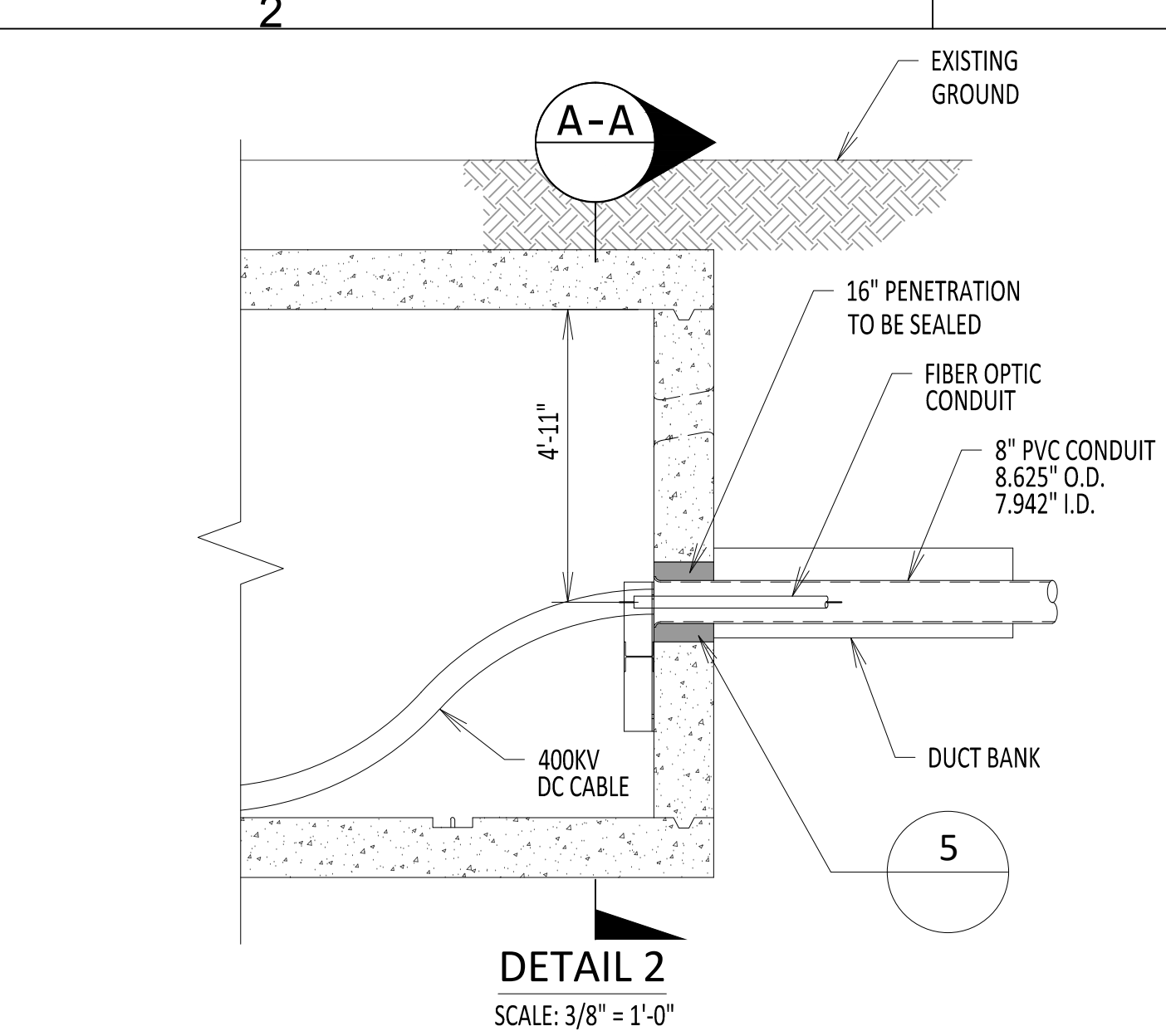
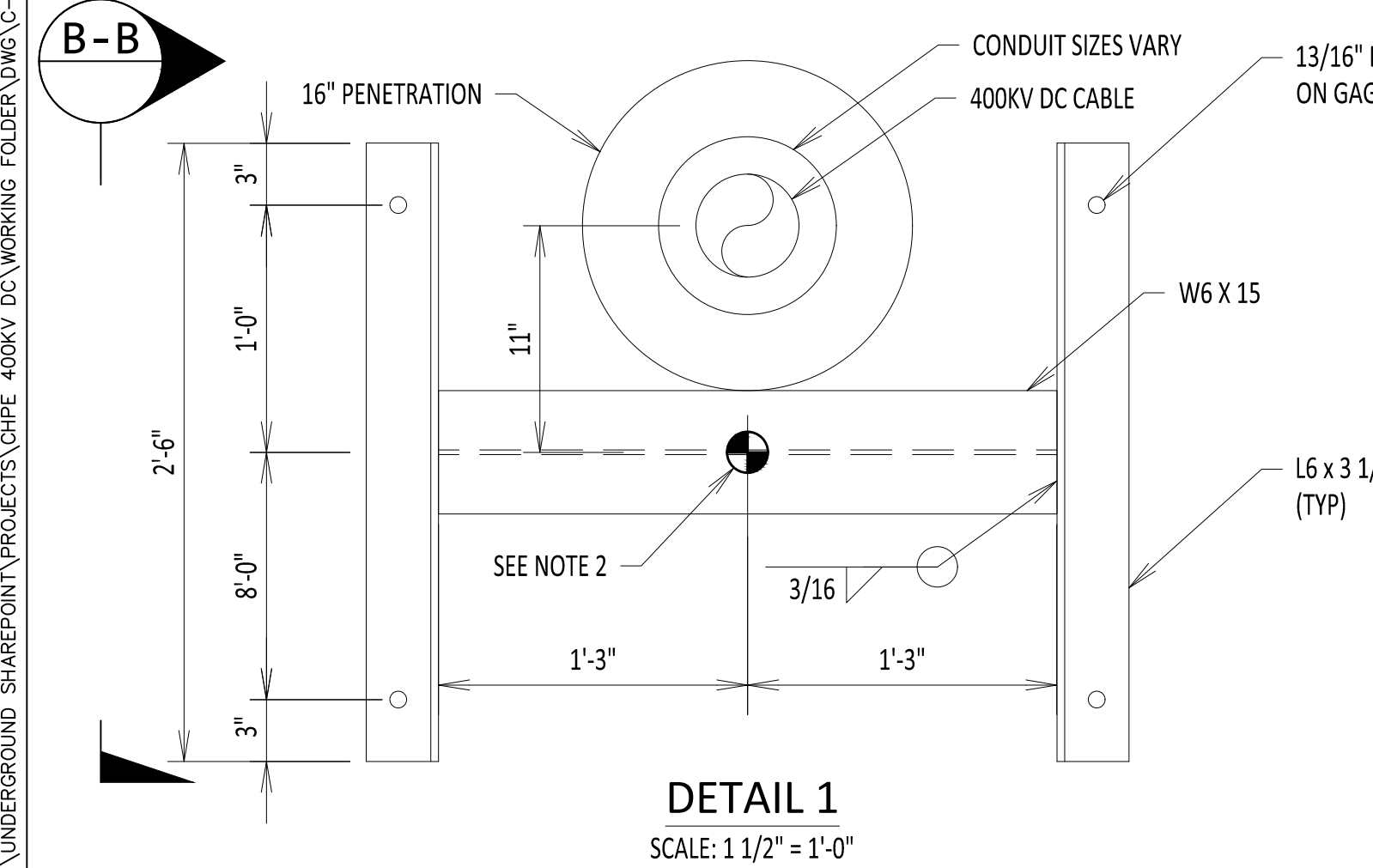
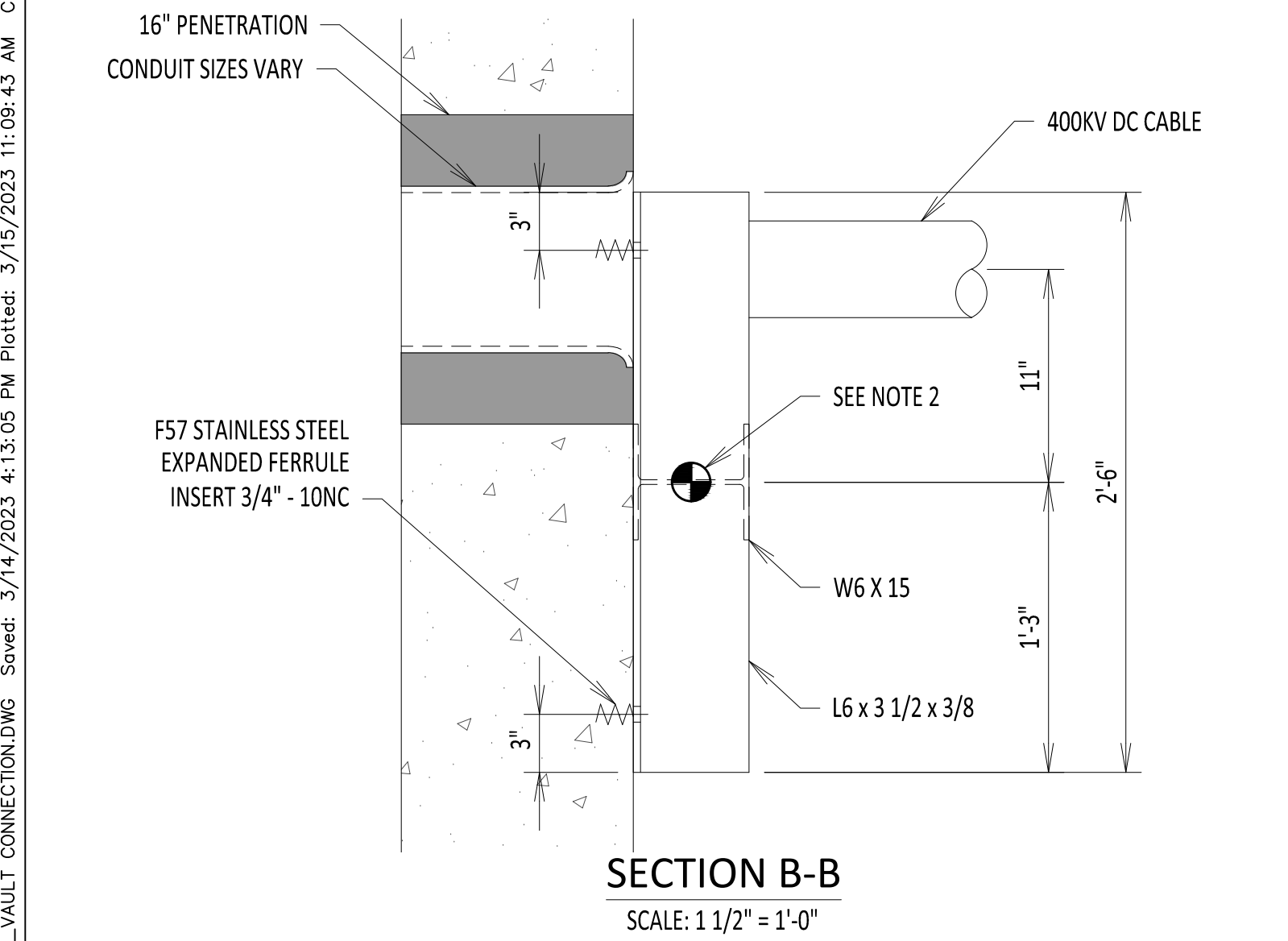
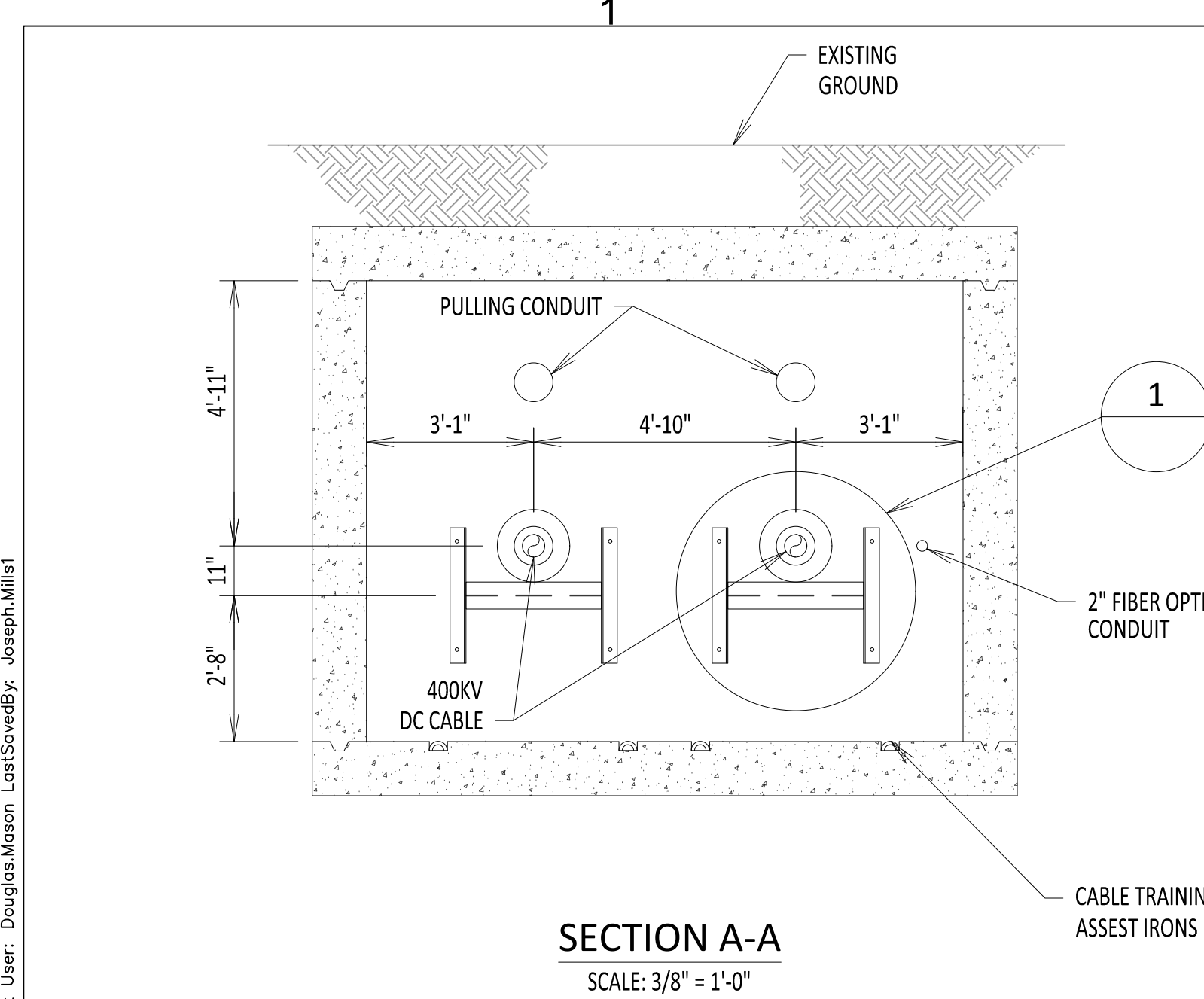
**SPLICE VAULT AND CABLE MARKING DETAILS**

DRAWN BY: DLM DESIGNED BY: SD APPROVED BY: ASM SCALE: 0

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CHA PROJECT NO. 066076  
DRAWING NO. **C-807**  
DATE 03/15/2023  
SH.NO. OF

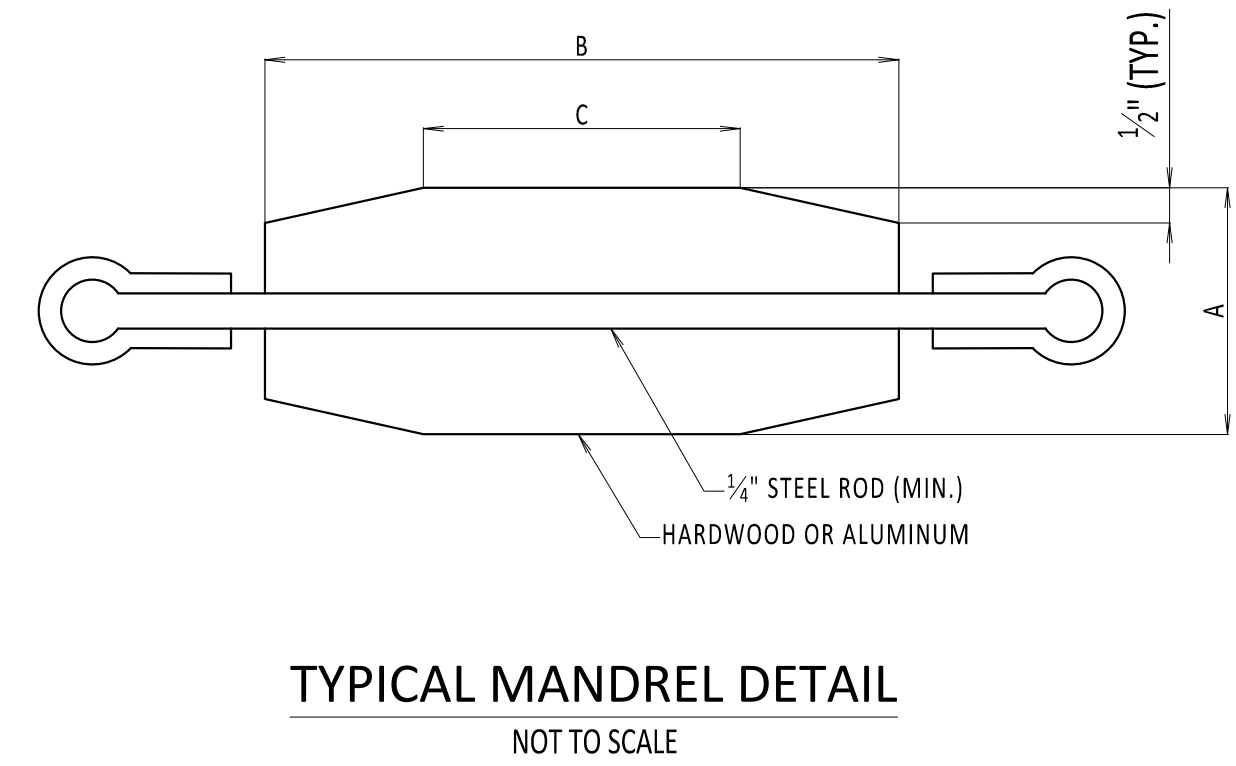


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- NOTE:
- NUMBER OF LINK SEAL WILL VARY WHEN APPLYING TO 10" CONDUIT AND 12" CONDUIT.
  - INDICATES LOCATION OF ATTACHMENT. END WALL CABLE CLAMP (BY OTHERS) TO BE ATTACHED CENTERED ON W6 WEB. CABLE AXIAL LOAD SHALL NOT EXCEED 9KIPS (ULTIMATE).

MANDREL SIZE DATA TABLE									
CONDUIT INFORMATION					MANDREL DIM.				
CONDUIT TYPE	SCH.	NOMINAL CONDUIT SIZE	MINIMUM CONDUIT RADIUS	CONDUIT O.D.	CONDUIT I.D.	A	B	C	
PVC	40	8"	8'-0"	8.625"	7.942"	7.481"	18.5"	10"	
PVC	40	8"	10'-0"	8.625"	7.942"	7.481"	20.5"	11"	
PVC	40	8"	12'-0"	8.625"	7.942"	7.481"	22.5"	12"	
HDPE	DR7	10"	8'-0"	10.75"	7.49"	7.481"	18.5"	10"	
HDPE	DR7	10"	10'-0"	10.75"	7.49"	7.481"	20.5"	11"	
HDPE	DR7	10"	12'-0"	10.75"	7.49"	7.481"	22.5"	12"	
HDPE	DR9	10"	8'-0"	10.75"	8.22"	7.481"	18.5"	10"	
HDPE	DR9	10"	10'-0"	10.75"	8.22"	7.481"	20.5"	11"	
HDPE	DR9	10"	12'-0"	10.75"	8.22"	7.481"	22.5"	12"	
FRE	-	8"	8'-0"	8.9"	8.4"	7.481"	18.5"	10"	
FRE	-	8"	10'-0"	8.9"	8.4"	7.481"	20.5"	11"	
FRE	-	8"	12'-0"	8.9"	8.4"	7.481"	22.5"	12"	



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS

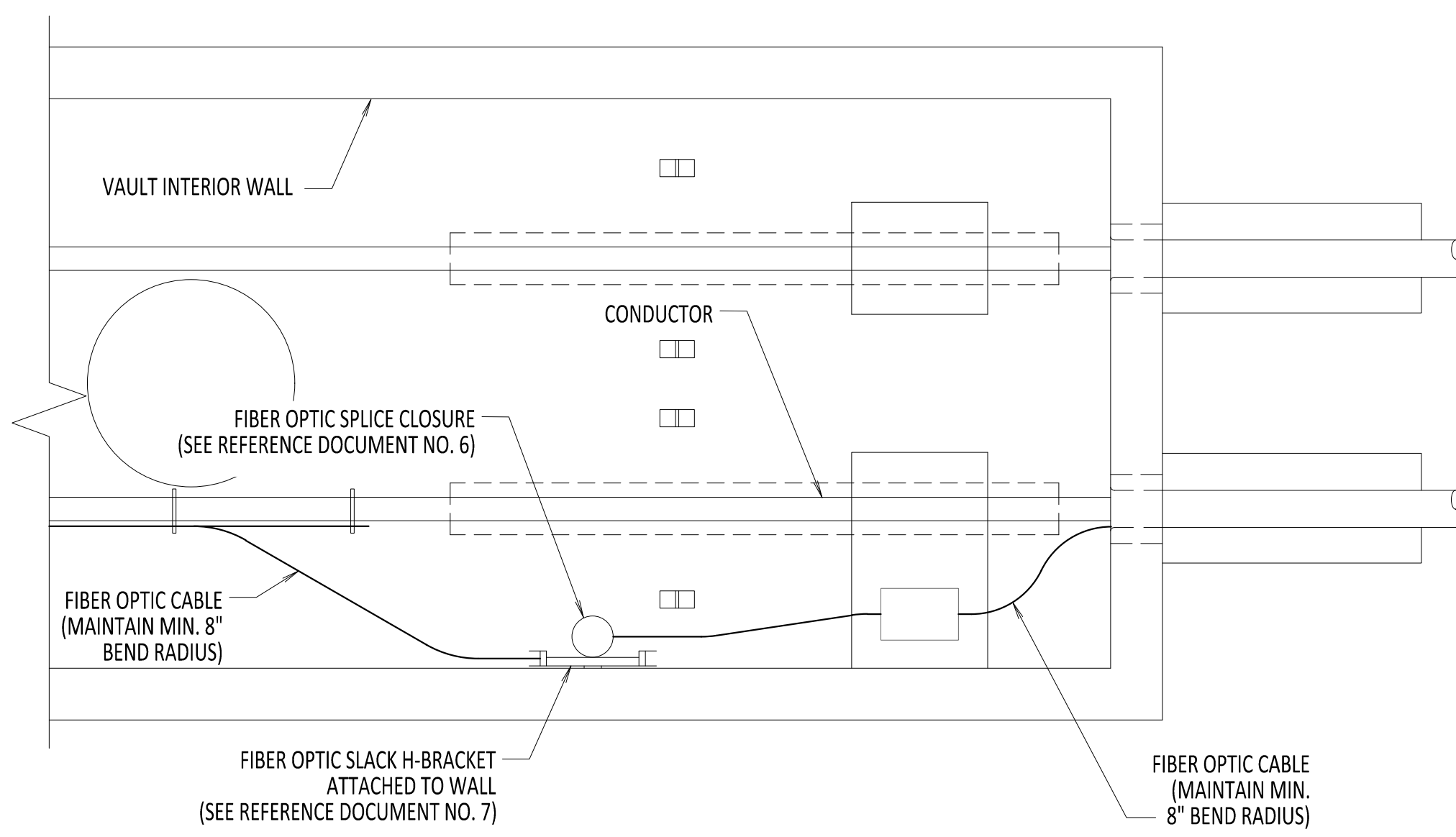
VAULT CONNECTION  
DETAILS

DRAWN BY: DLM DESIGNED BY: SS APPROVED BY: ASM SCALE: NOT TO SCALE REV. NO. 0

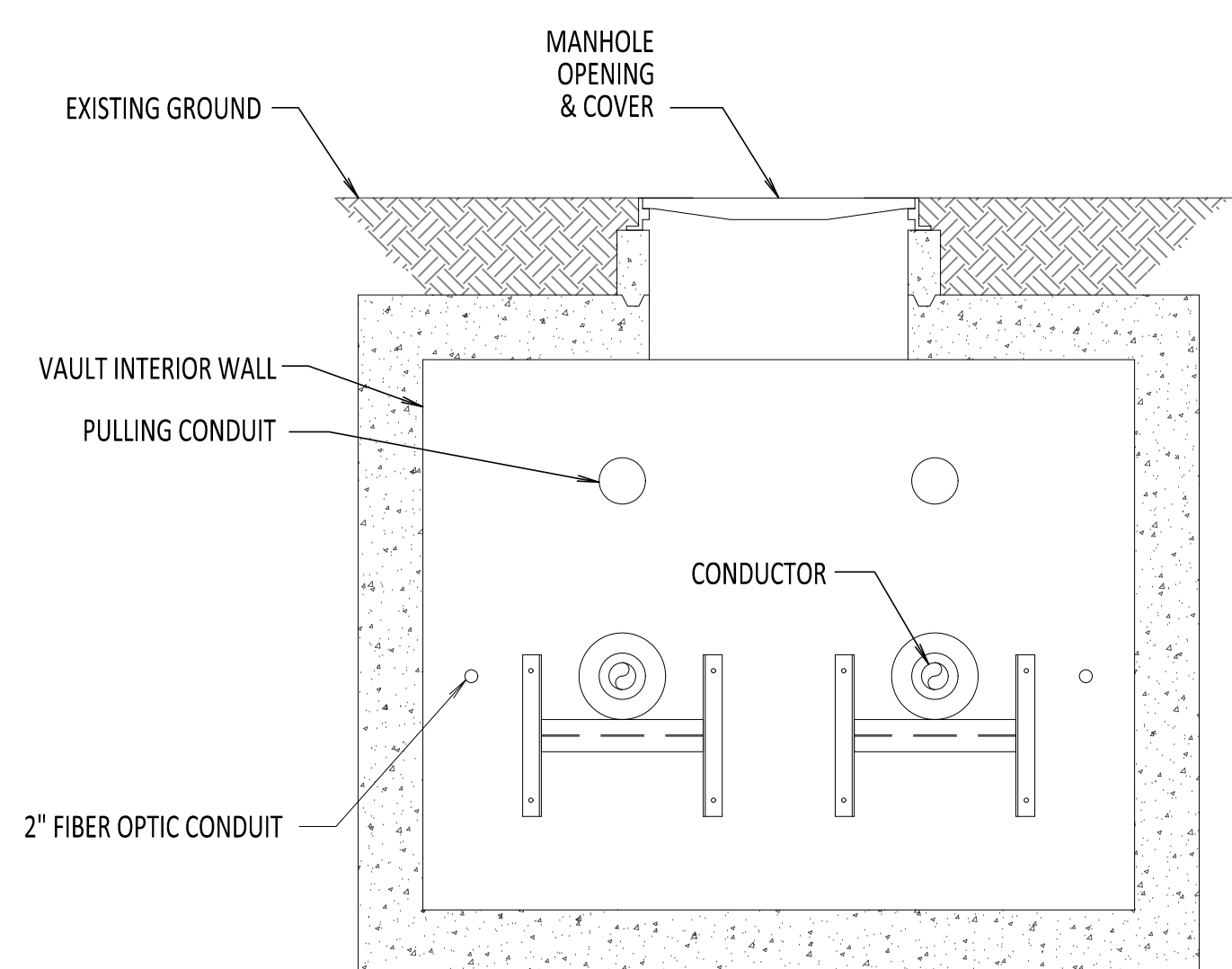
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CHA PROJECT NO.	066076
DRAWING NO.	C-812
DATE	03/15/2023
SH.NO.	OF



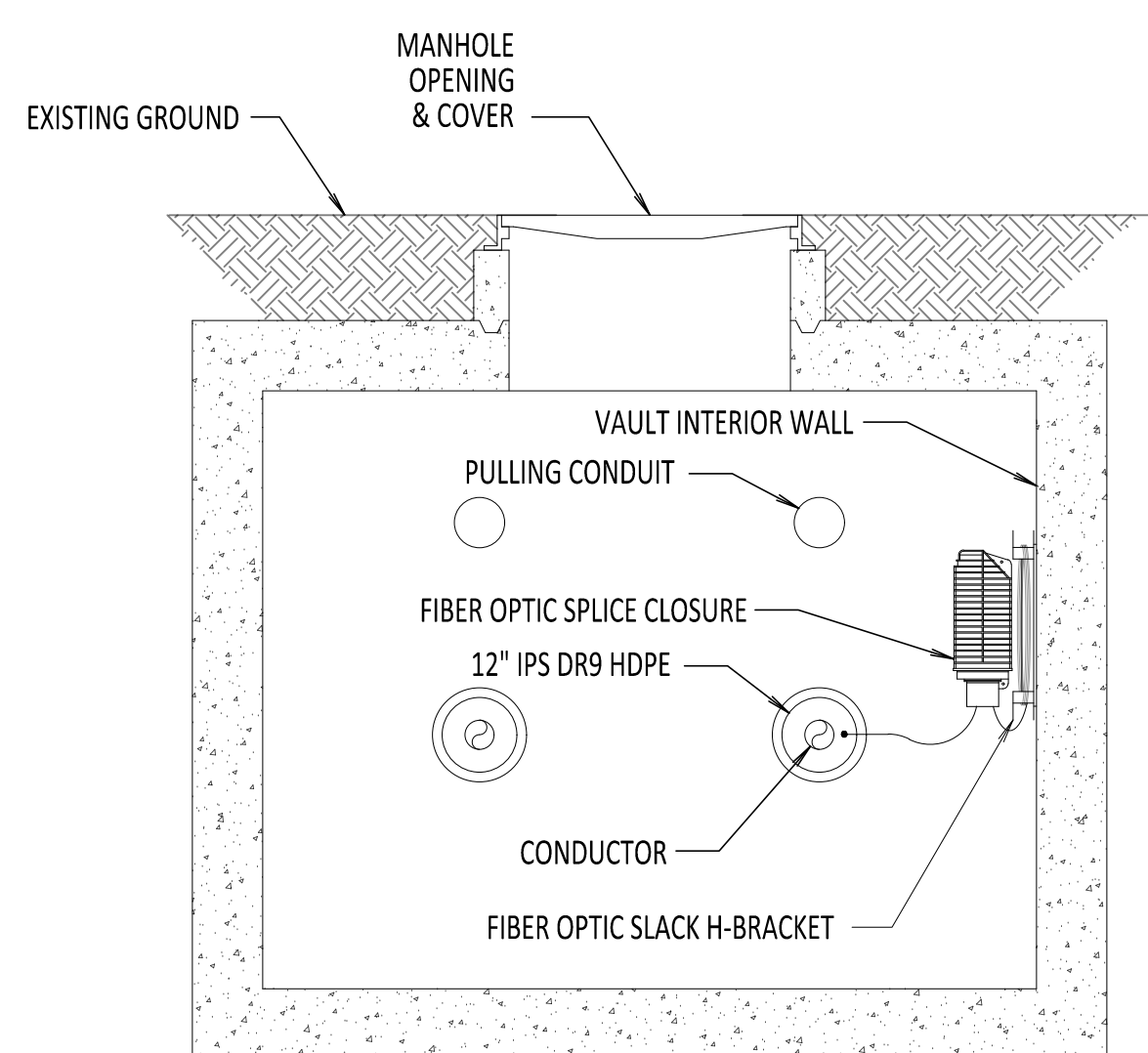
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**PARTIAL TRANSITION VAULT PLAN VIEW**  
SCALE: 3/8" = 1'-0"



**SECTION A-A**  
SCALE: 3/8" = 1'-0"



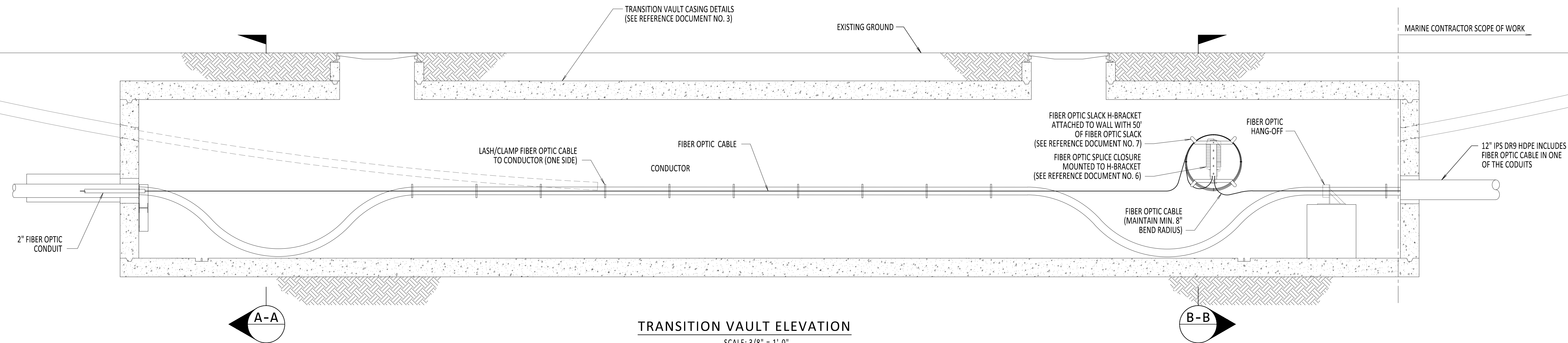
**SECTION B-B**  
SCALE: 3/8" = 1'-0"

**NOTES:**

- NKT TO LASH/CLAMP FIBER OPTIC CABLE AND ATTACH SPLICE CLOSURES.
- FIBER OPTIC CABLE SHALL FOLLOW THE NEGATIVE POLE HVDC CABLE NOTED ON THE PLAN AND PROFILE DRAWINGS, UNLESS OTHERWISE STATED. IN THE CASE THAT THE FIBER OPTIC CABLE ENTRY IS NOT ALIGNED WITH THE NEGATIVE POLE, THE FIBER OPTIC CONFIGURATION HEREIN SHALL BE MIRRORED ON THE OPPOSITE SIDE.
- SEE REFERENCE DOCUMENT 5 FOR DETAILS ON ROUTING FIBER TO DTS HUT.

**REFERENCE DOCUMENTS**

LIST NO.	DOCUMENT NAME	DOCUMENT NO.
1	TRANSITION VAULT PLAN AND ELEVATION	S-730
2	TRANSITION VAULT SECTION AND DETAILS	S-731
3	TRANSITION VAULT EXTENDED SNAKING DETAILS	C-805
4	TRANSITION VAULT GROUNDING DETAILS	C-806
5	DST HUT CONNECTION FIBER OPTIC SPLICING AND HANDHOLE	C-851
6	FIBER OPTIC SPLICING DETAILS	C-855
7	FIBER OPTIC H-FRAME BRACKET DETAILS	C-856



**TRANSITION VAULT ELEVATION**  
SCALE: 3/8" = 1'-0"



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION				DB	APP		

**CHAMPLAIN HUDSON POWER EXPRESS**

**TRANSITION VAULT  
FIBER OPTIC DETAIL**

DRAWN BY: DLM DESIGNED BY: NM APPROVED BY: MK SCALE: REV. NO. 0

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CHA PROJECT NO.	066076
DRAWING NO.	<b>C-850</b>
DATE	03/15/2023
SH.NO.	OF

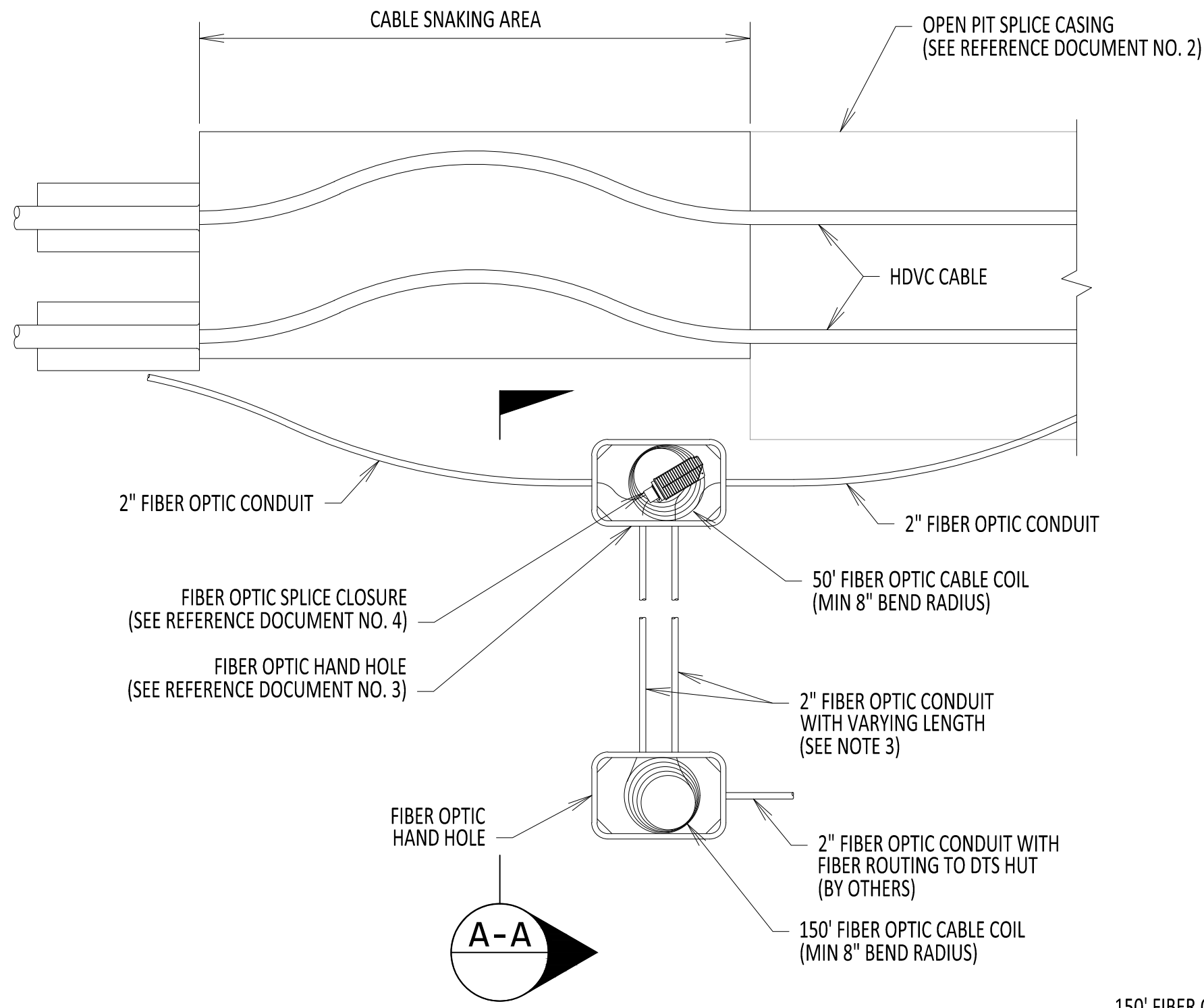


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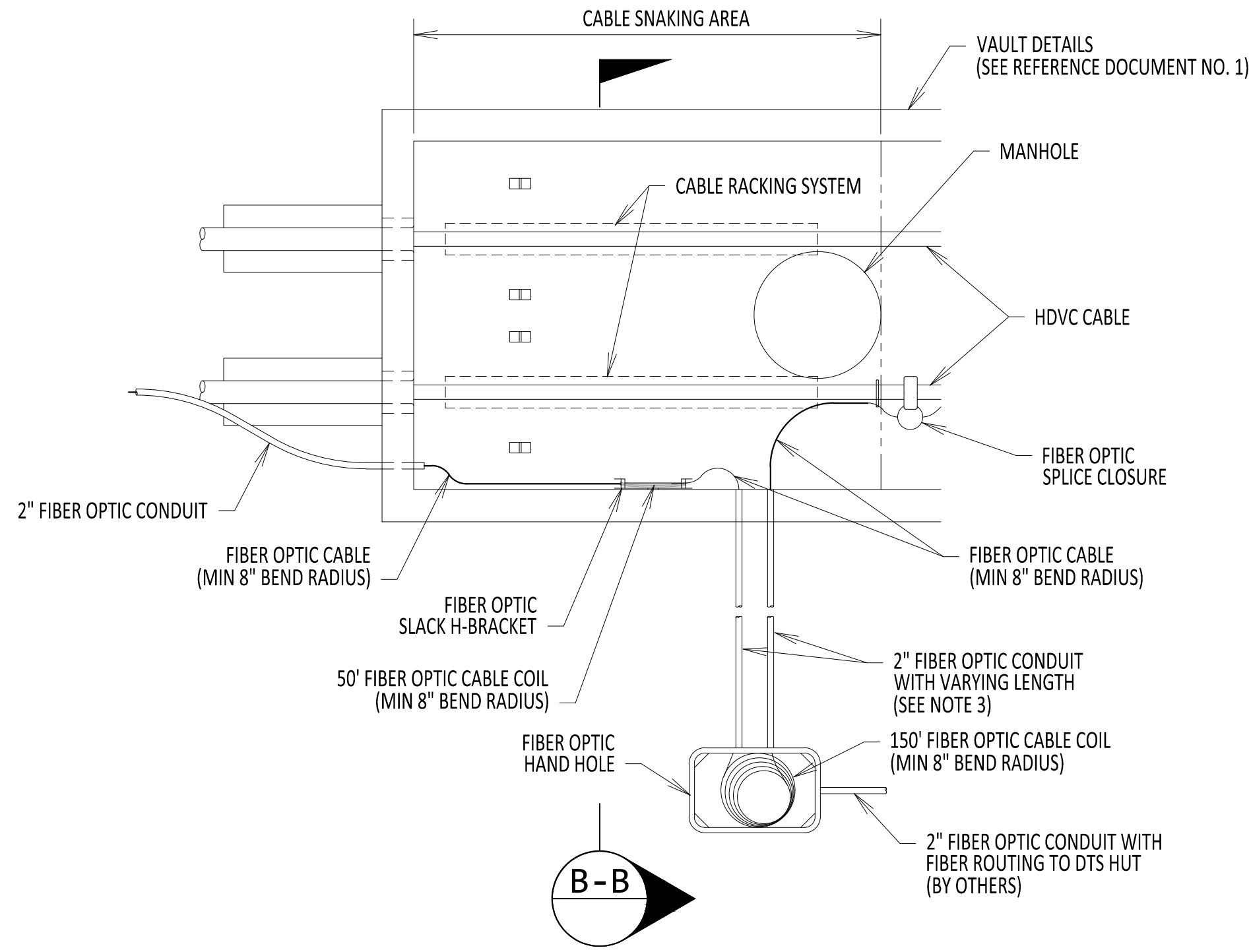
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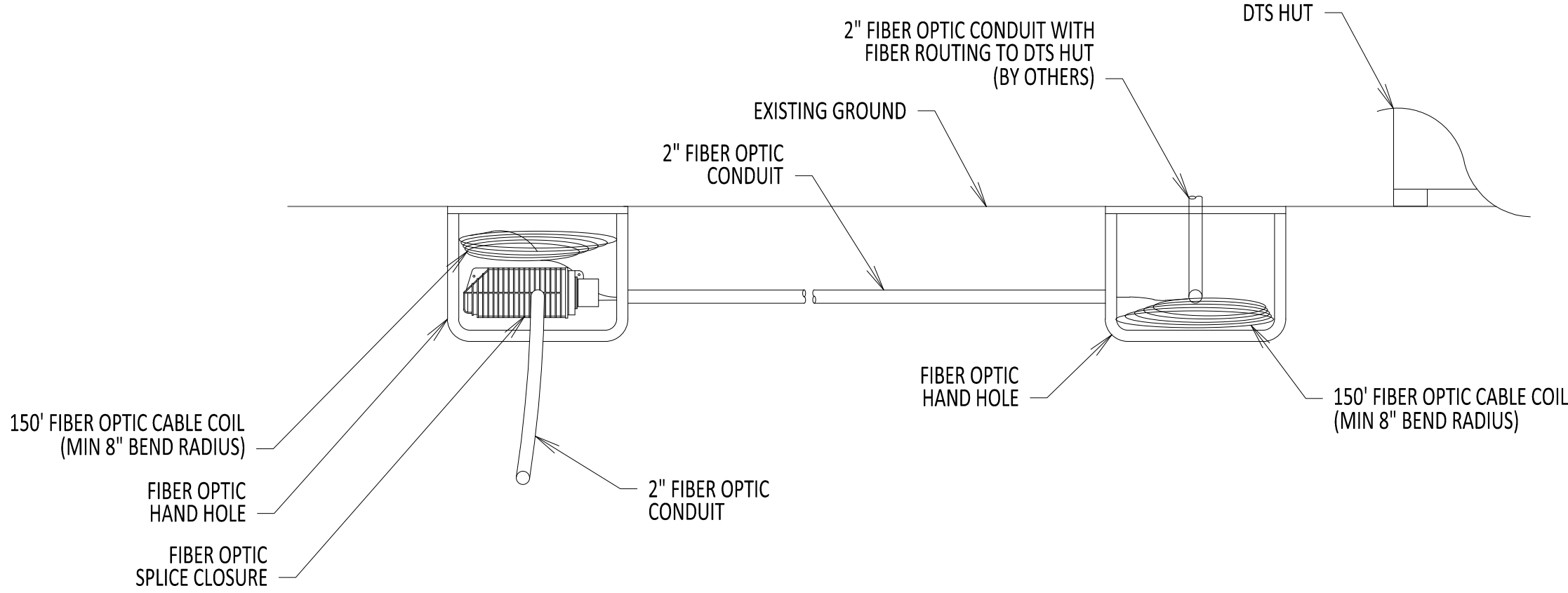
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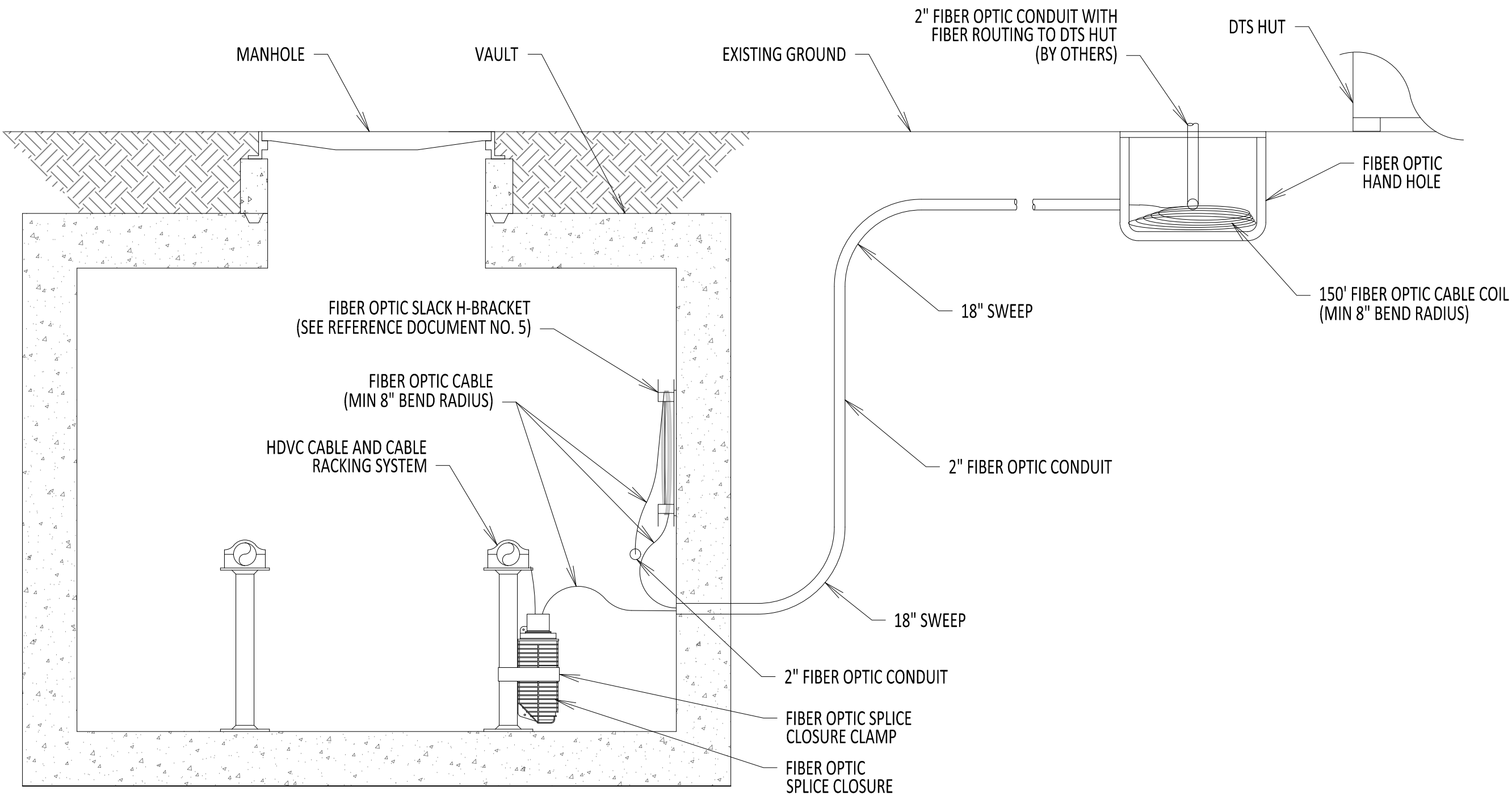
OPEN PIT DTS HUT CONNECTION  
PLAN VIEW  
SCALE: 1/4" = 1'-0"



ENCLOSED VAULT DTS HUT CONNECTION  
PLAN VIEW  
SCALE: 1/4" = 1'-0"



SECTION A-A  
SCALE: 1/2" = 1'-0"



SECTION B-B  
SCALE: 1/2" = 1'-0"

NOTES:

1. MAINTAIN A MINIMUM FIBER OPTIC CABLE BEND RADIUS OF 8" UNLESS OTHERWISE STATED (WHEN BENDING THE CABLE).
2. SPLICES WITH DTS HUTS ARE ONLY AT DESIGNATED VAULTS/SPLICE LOCATIONS. SEE PLAN AND PROFILES FOR DETAILS.
3. SEE PLAN AND PROFILES FOR LATEST DISTANCES TO DTS HUT.

REFERENCE DOCUMENTS

LIST NO.	DOCUMENT NAME	DOCUMENT NO.
1	TYPICAL VAULT DETAILS	C-802
2	TYPICAL OPEN PIT SPLICE DETAILS	C-808
3	FIBER OPTIC HAND HOLE DETAILS	C-854
4	FIBER OPTIC SPLICE DETAILS	C-855
5	FIBER OPTIC H-FRAME BRACKET DETAILS	C-856

A

B



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS

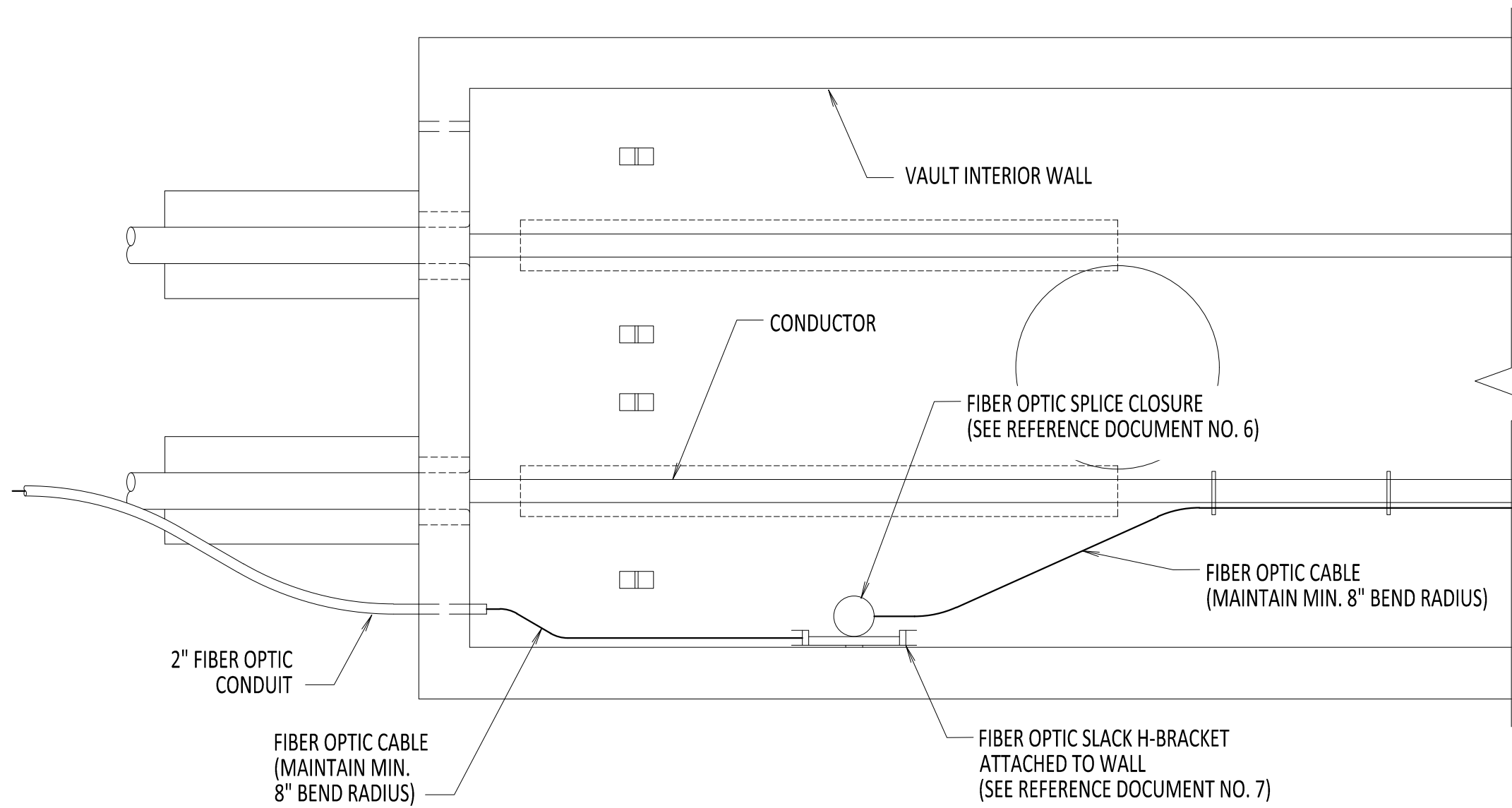
DTS HUT CONNECTION  
FIBER OPTIC SPLICE AND HAND HOLE

DRAWN BY: DLM DESIGNED BY: NM APPROVED BY: MK SCALE: 03/15/2023

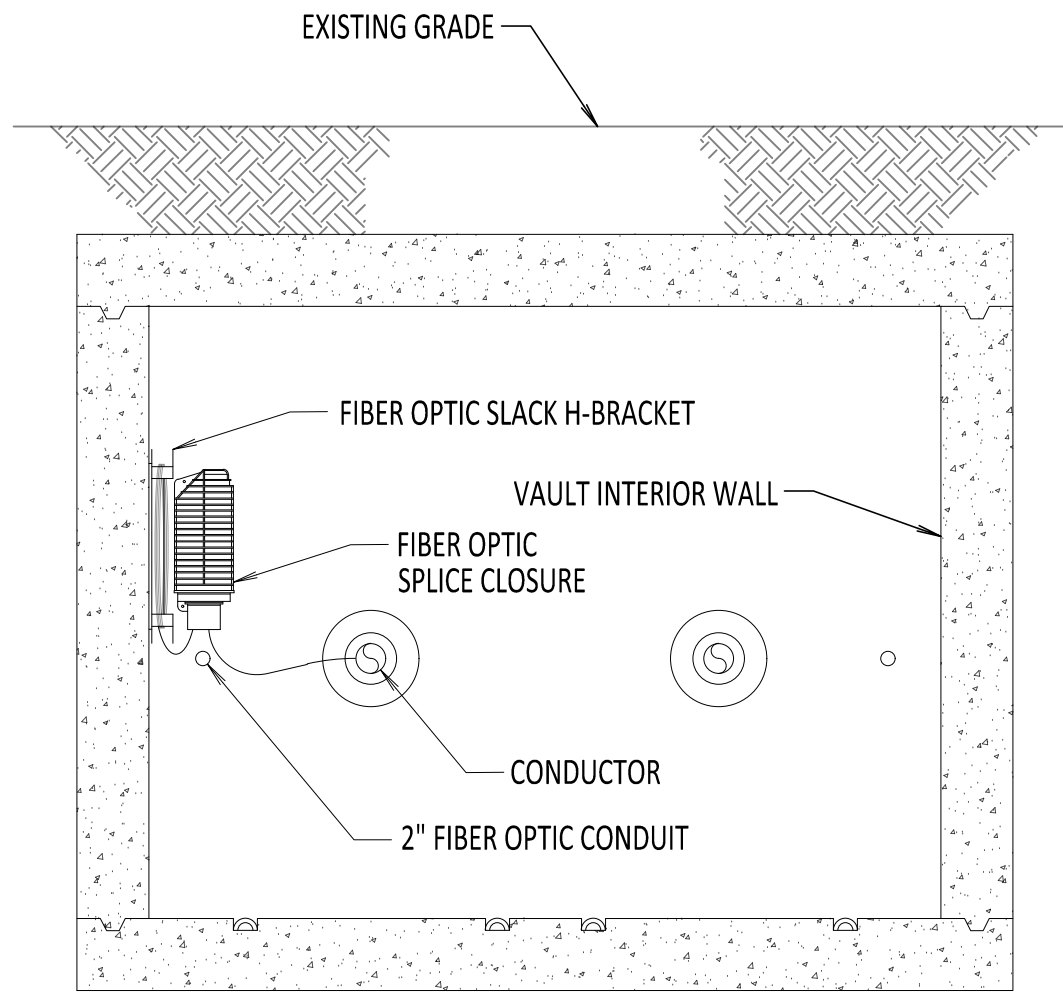
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SH.NO.	OF



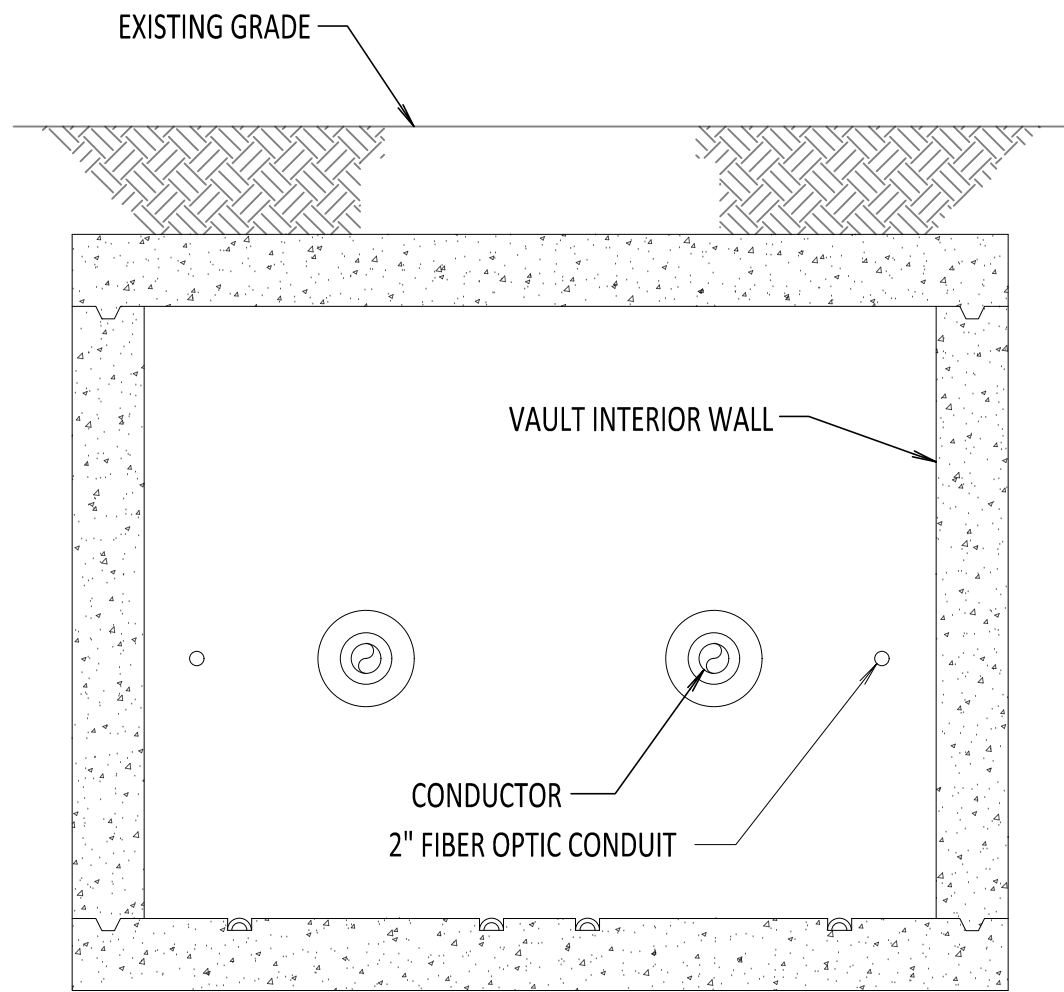
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ENCLOSED VAULT w/ FIBER OPTIC CABLE PLAN VIEW  
SCALE: 3/8" = 1'-0"



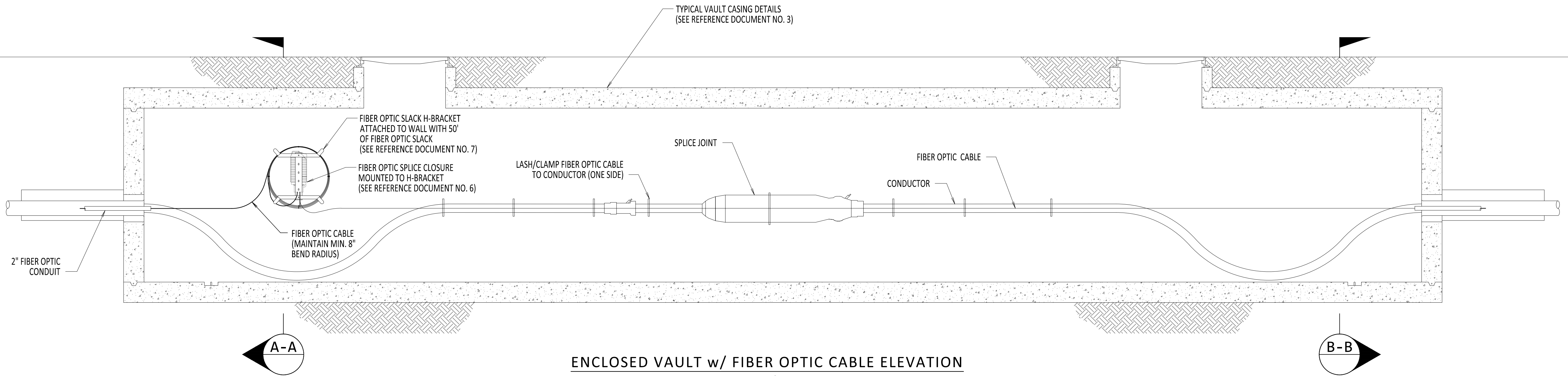
SECTION A-A  
SCALE: 3/8" = 1'-0"



SECTION B-B  
SCALE: 3/8" = 1'-0"

- NOTES:**
1. NKT TO LASH/CLAMP FIBER OPTIC CABLE AND ATTACH SPLICE CLOSURES AS NEEDED.
  2. FIBER OPTIC CABLE SHALL FOLLOW THE NEGATIVE POLE HVDC CABLE NOTED ON THE PLAN AND PROFILE DRAWINGS, UNLESS OTHERWISE STATED. IN THE CASE THAT THE FIBER OPTIC CABLE ENTRY IS NOT ALIGNED WITH THE NEGATIVE POLE, THE FIBER OPTIC CONFIGURATION HEREIN SHALL BE MIRRORED ON THE OPPOSITE SIDE.
  3. SPLICE CLOSURE ONLY AT SPECIFIED LOCATIONS. SEE PLAN AND PROFILE.
  4. SEE REFERENCE DOCUMENT 5 FOR DETAILS ON ROUTING FIBER TO DTS HUT.

REFERENCE DOCUMENTS		
LIST NO.	DOCUMENT NAME	DOCUMENT NO.
1	SPLICE VAULT PLAN AND ELEVATION	S-700
2	SPLICE VAULT SECTION AND DETAILS	S-701
3	TYPICAL VAULT DETAILS	C-802
4	TYPICAL VAULT GROUNDING DETAILS	C-803
5	DST HUT CONNECTION FIBER OPTIC SPLICE AND HANDHOLE	C-851
6	FIBER OPTIC SPLICE DETAILS	C-855
7	FIBER OPTIC H-FRAME BRACKET DETAILS	C-856



ENCLOSED VAULT w/ FIBER OPTIC CABLE ELEVATION  
SCALE: 3/8" = 1'-0"



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ISSUED FOR PERMITTING

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
0	03/15/2023	ISSUED FOR CONSTRUCTION	DLM	ASM

CHAMPLAIN HUDSON POWER EXPRESS

ENCLOSED VAULT  
w/ FIBER OPTICS

DRAWN BY: DLM DESIGNED BY: NM APPROVED BY: MK SCALE: REV. NO. 0

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-852
DATE	03/15/2023
SH.NO.	OF



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1

2

3

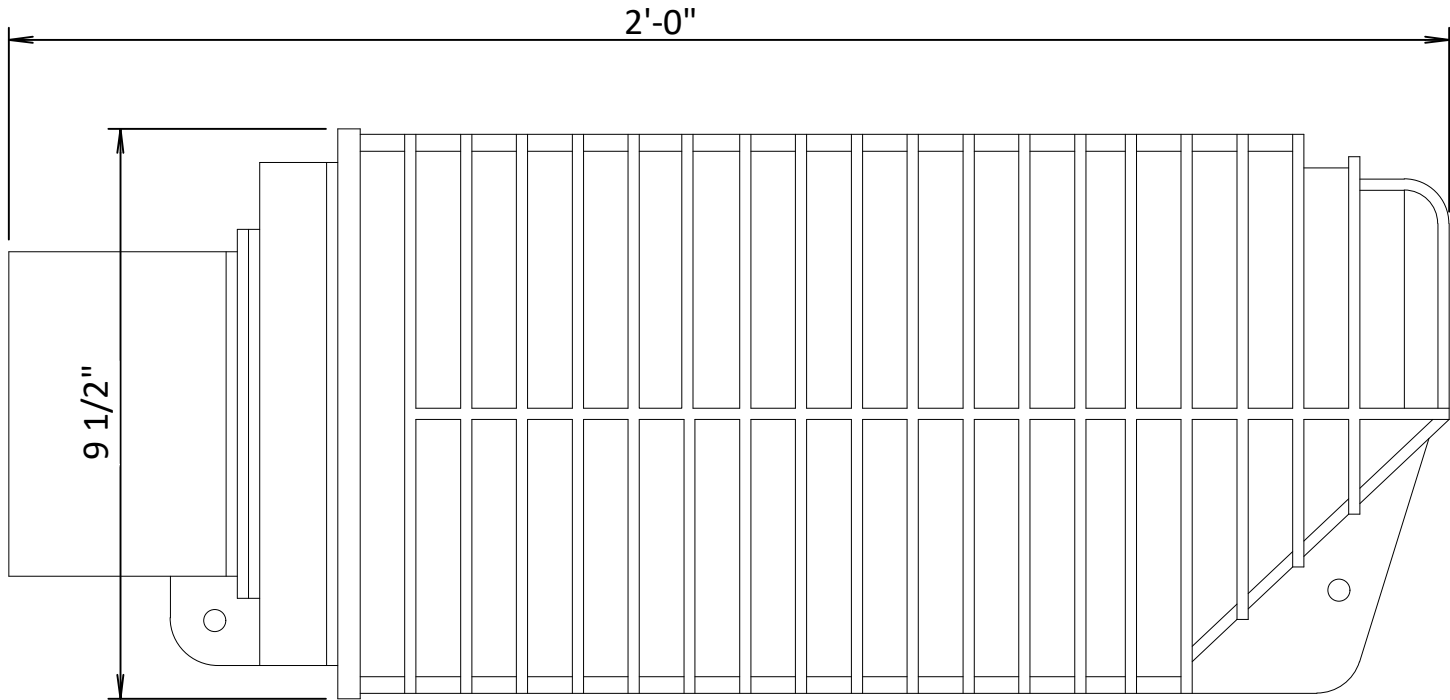
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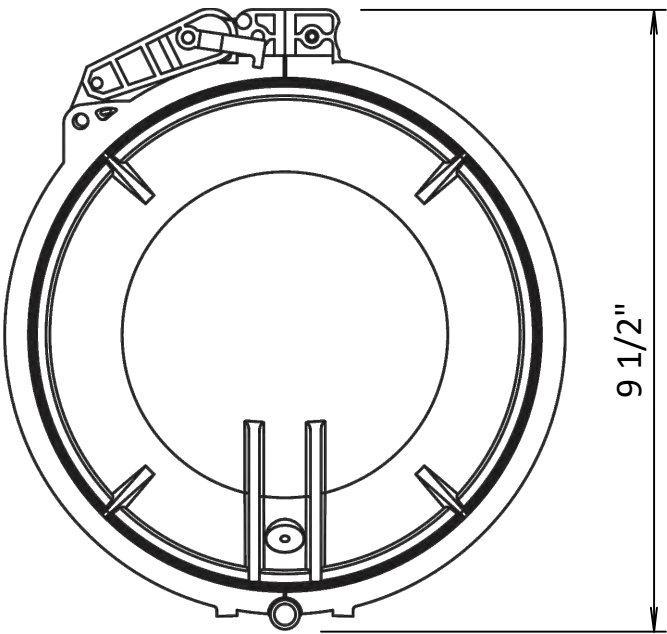
1. MAINTAIN 18" SWEEPS ON ALL CONDUITS.
2. DTS HUT FIBERS WILL SPLICE TO BLUE TUBE FIBER COLORS BLUE, ORANGE, GREEN AND BROWN.
3. SEE REFERENCE DOCUMENTS 2, 3, 4 AND 5 FOR PLACEMENT DETAILS.
4. FIBER OPTIC SPLICE CLOSURE ONLY IN SPECIFIC LOCATIONS. SEE PLAN AND PROFILES FOR DETAILS.

REFERENCE DOCUMENTS

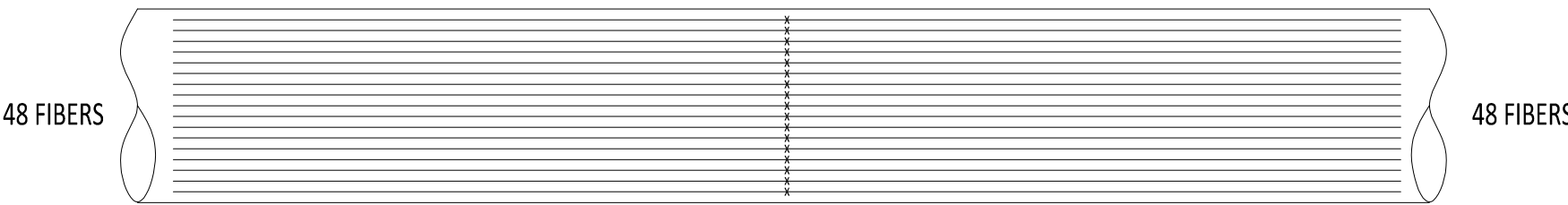
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1	FIBER OPTIC SPLICE CLOSURE	COMMSCOPE: MODEL NO. FOSC450-B6-6-24-1-83V
2	TRANSITION VAULT FIBER OPTIC DETAIL	C-850
3	DTS HUT CONNECTION FIBER OPTIC SPLICE AND HANDHOLE	C-851
4	ENCLOSED VAULT w/FIBER OPTICS	C-852
5	OPEN VAULT w/FIBER OPTICS HAND HOLE	C-853



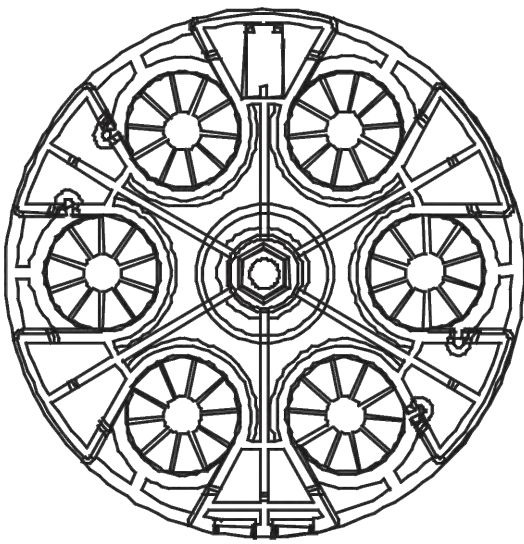
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NOT TO SCALE



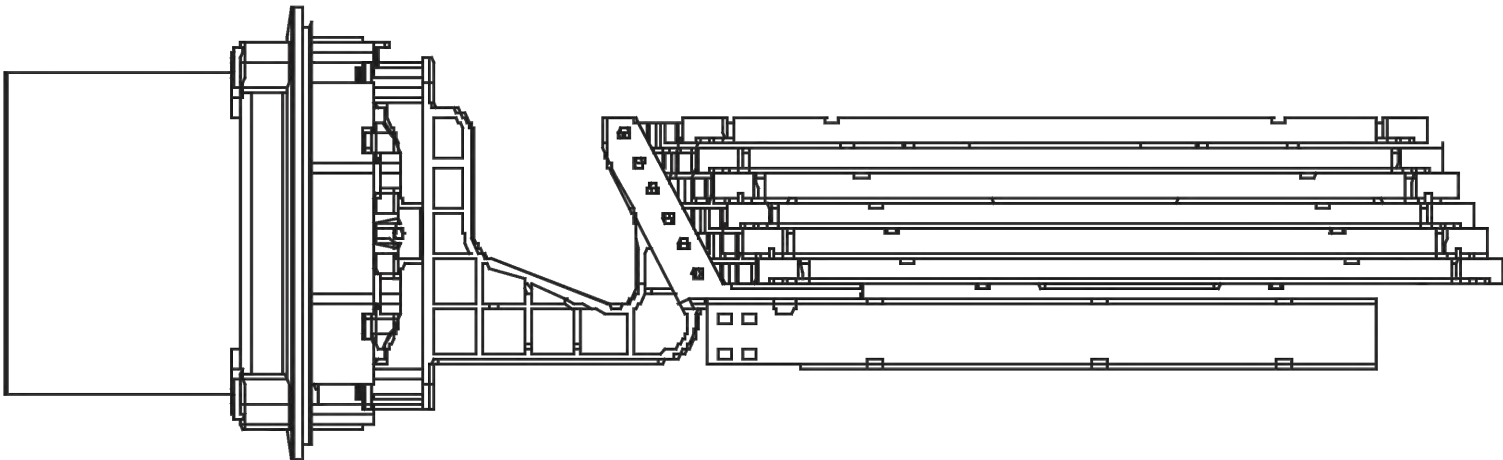
FIBER OPTIC SPLICE CLOSURE  
NOT TO SCALE



FIBER OPTIC CABLE SPLICE  
DIAGRAM TYPICAL  
NOT TO SCALE



FIBER OPTIC SPLICE CLOSURE  
GEL-BLOCK PROFILE  
NOT TO SCALE



FIBER OPTIC SPLICE CLOSURE  
INSIDE DIAGRAM  
NOT TO SCALE



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ISSUED FOR PERMITTING

0	03/15/2023	ISSUED FOR CONSTRUCTION	DLM	ASM	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS

FIBER OPTIC SPLICE DETAILS

DRAWN BY: DLMDESIGNED BY: NMAPPROVED BY: MKSCALE REV. NO. 0

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-855
DATE	03/15/2023
SH.NO.	OF



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CHPE

Champlain Hudson  
Power Express



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ISSUED FOR PERMITTING

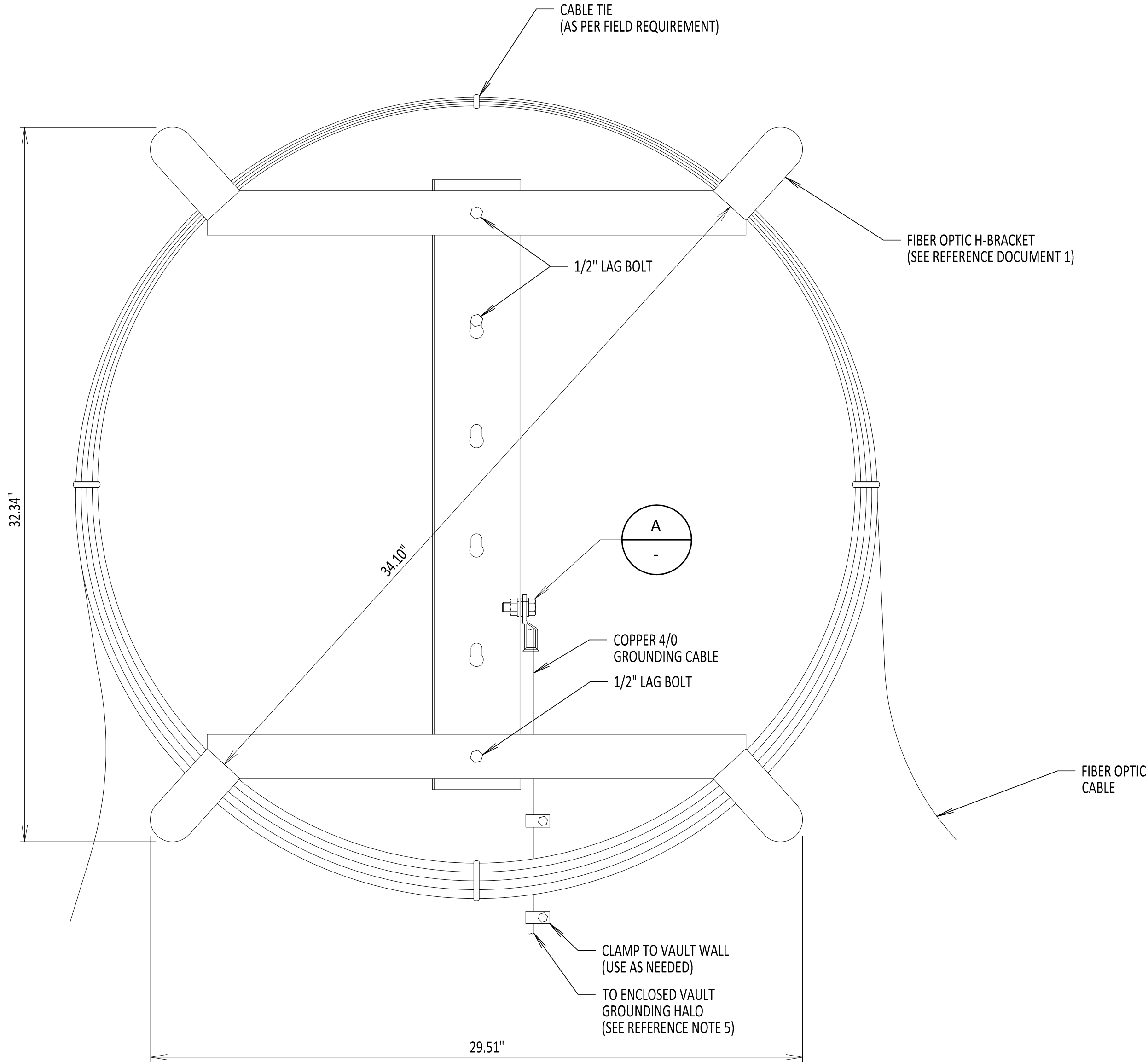
0	03/15/2023	ISSUED FOR CONSTRUCTION	DLM	ASM	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS

FIBER OPTIC H-FRAME BRACKET DETAIL

DRAWN BY:	DLM	DESIGNED BY:	NM	APPROVED BY:	MK	SCALE		DATE	03/15/2023
						REV. NO.	0	SH.NO.	OF

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-856
DATE	03/15/2023
SH.NO.	OF



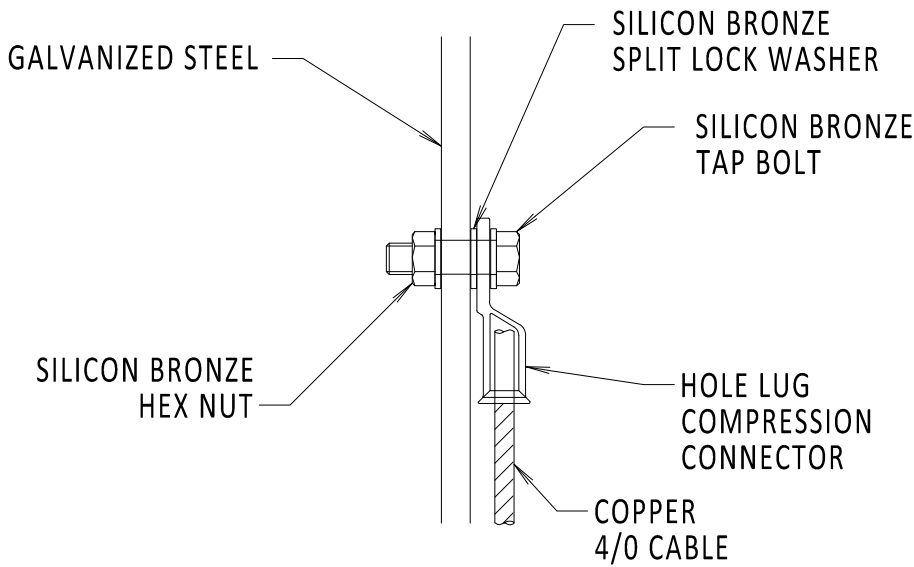
FIBER OPTIC H-FRAME BRACKET  
NOT TO SCALE

NOTES:

- DO NOT KINK OR TWIST FIBER OPTIC CABLE.
- IN VAULT PULL FROM DISTANT MANHOLE THEN COIL AND PLACE ON H-FRAME BRACKET.
- H-FRAME IS ONLY USED IN ENCLOSED VAULTS. SEE REFERENCE DOCUMENTS 2 AND 3 FOR PLACEMENT DETAILS.
- H-FRAME BRACKET DEPTH IS 4.50".
- H-FRAME BRACKET TO BE GROUNDED TO THE GROUNDING HALO WITHIN AN ENCLOSED VAULT. SEE DRAWING C-803 FOR ENCLOSED VAULT GROUNDING DETAILS.
- H-FRAME BRACKET IS MADE OF 12GA GALVANIZED STEEL.
- 1/2" LAG BOLT DOES NOT COME WITH H-BRACKET ASSEMBLY AND NEEDS TO BE PROCURED AS NEEDED. BOLT TO BE INSTALLED INTO VAULT WALL BY FIELD PERSONNEL.

REFERENCE DOCUMENTS

LIST NO.	DOCUMENT NAME	DOCUMENT NO.
1	AMERICAN PRODUCT	H-FRAME POLE BRACKET MODEL NO: AM-3432-FSF-G-B x2
2	TYPICAL VAULT GROUNDING DETAILS	C-803
3	TRANSITION VAULT FIBER OPTIC DETAILS	C-850
4	ENCLOSED VAULT w/FIBER OPTICS	C-852



DETAIL A  
NOT TO SCALE



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ISSUED FOR PERMITTING

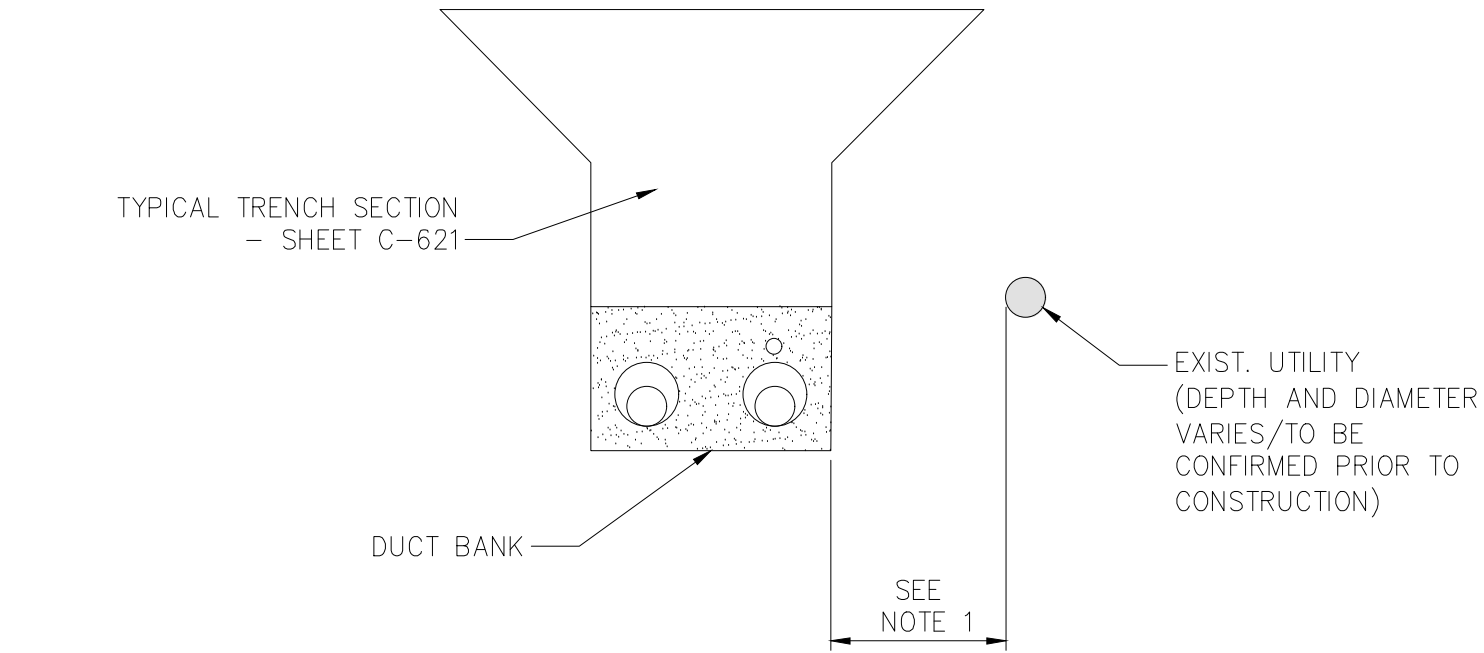
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D	11/16/2022	DRAFT FINAL SUBMISSION	BV	TK
C	05/20/2022	60% DESIGN SUBMISSION	BV	TK
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	BV	TK
A	02/18/2022	PRELIMINARY PROGRESS	BV	TK
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
UTILITY TYPICAL  
SEPERATION DETAILS

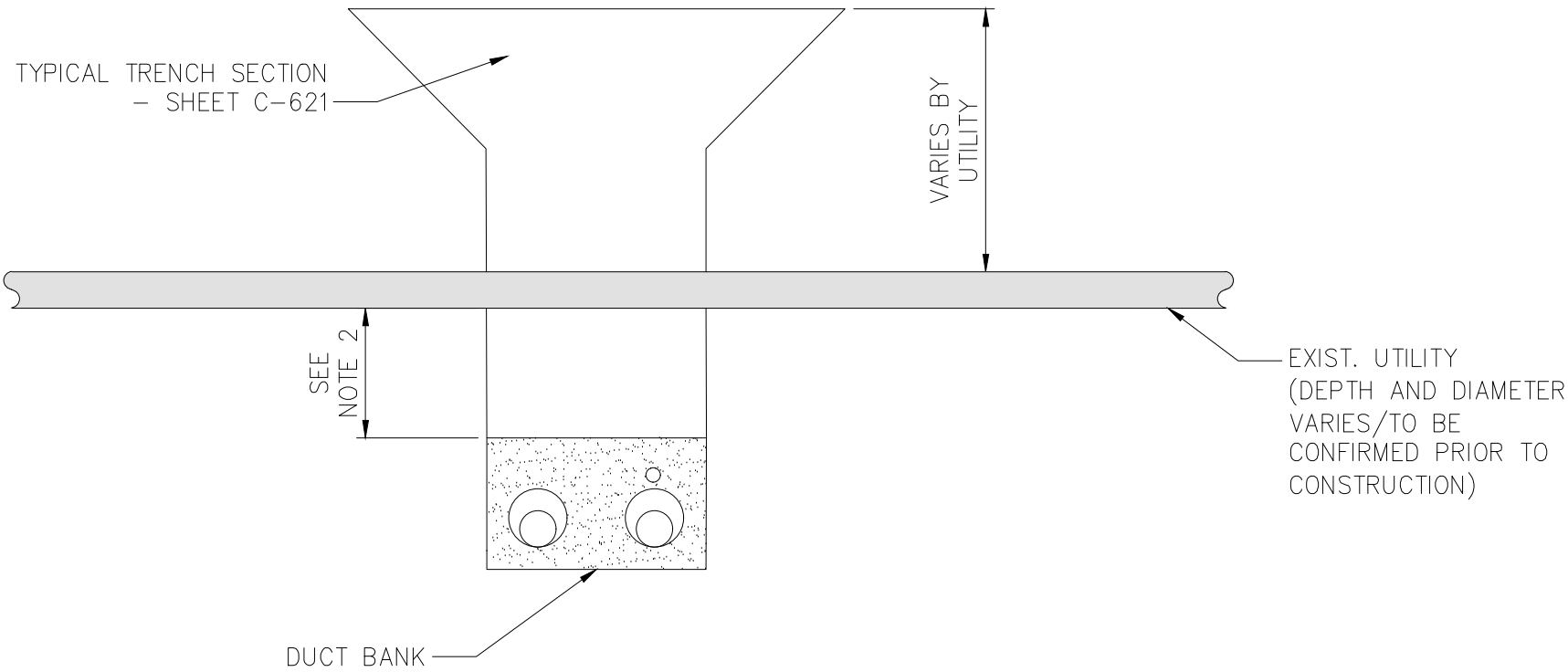
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DRAWING NO.	C-901
DATE	04/14/2023
SH.NO.	---- OF --

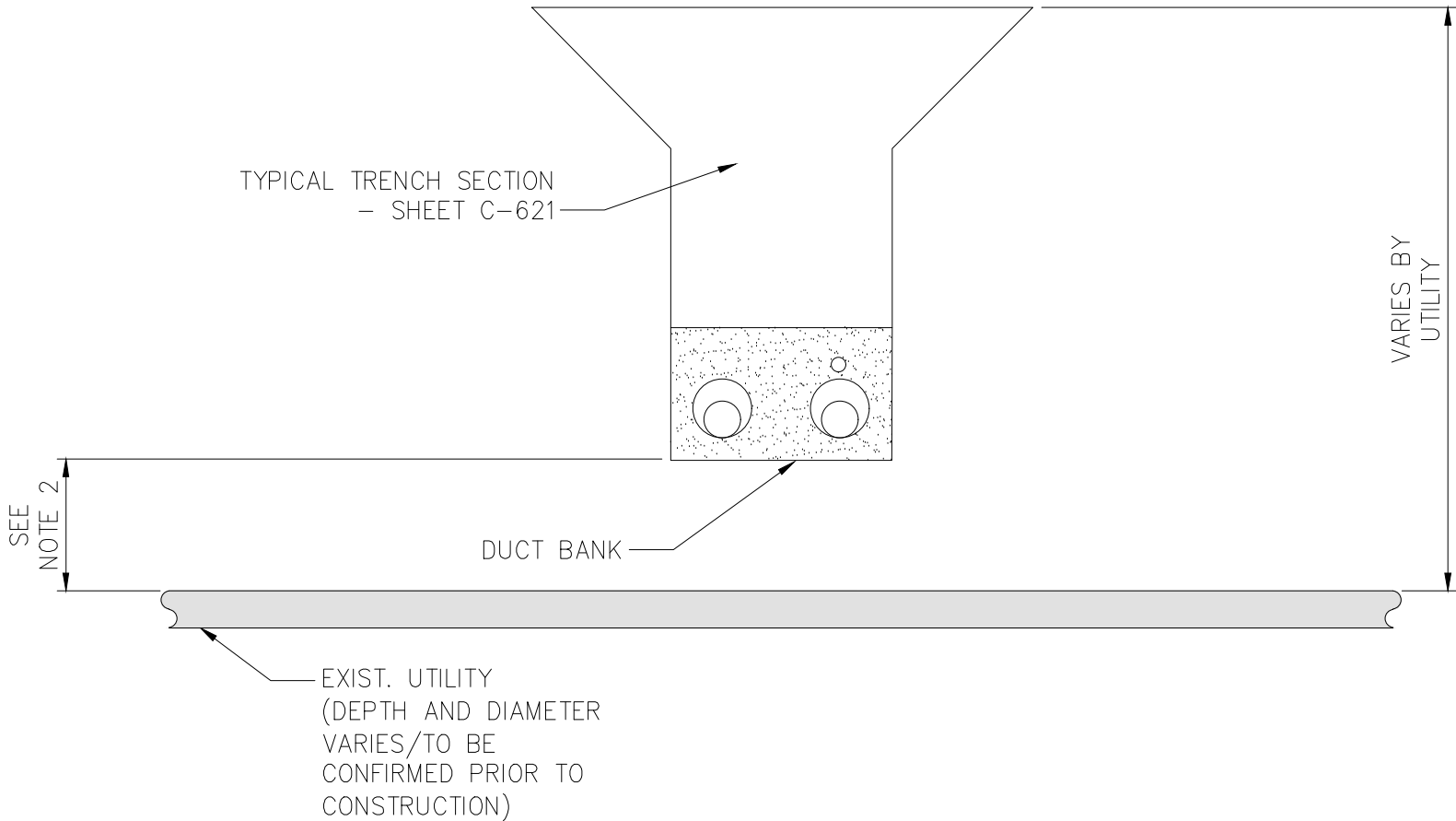
1 OPEN TRENCH – HORIZONTAL SEPARATION  
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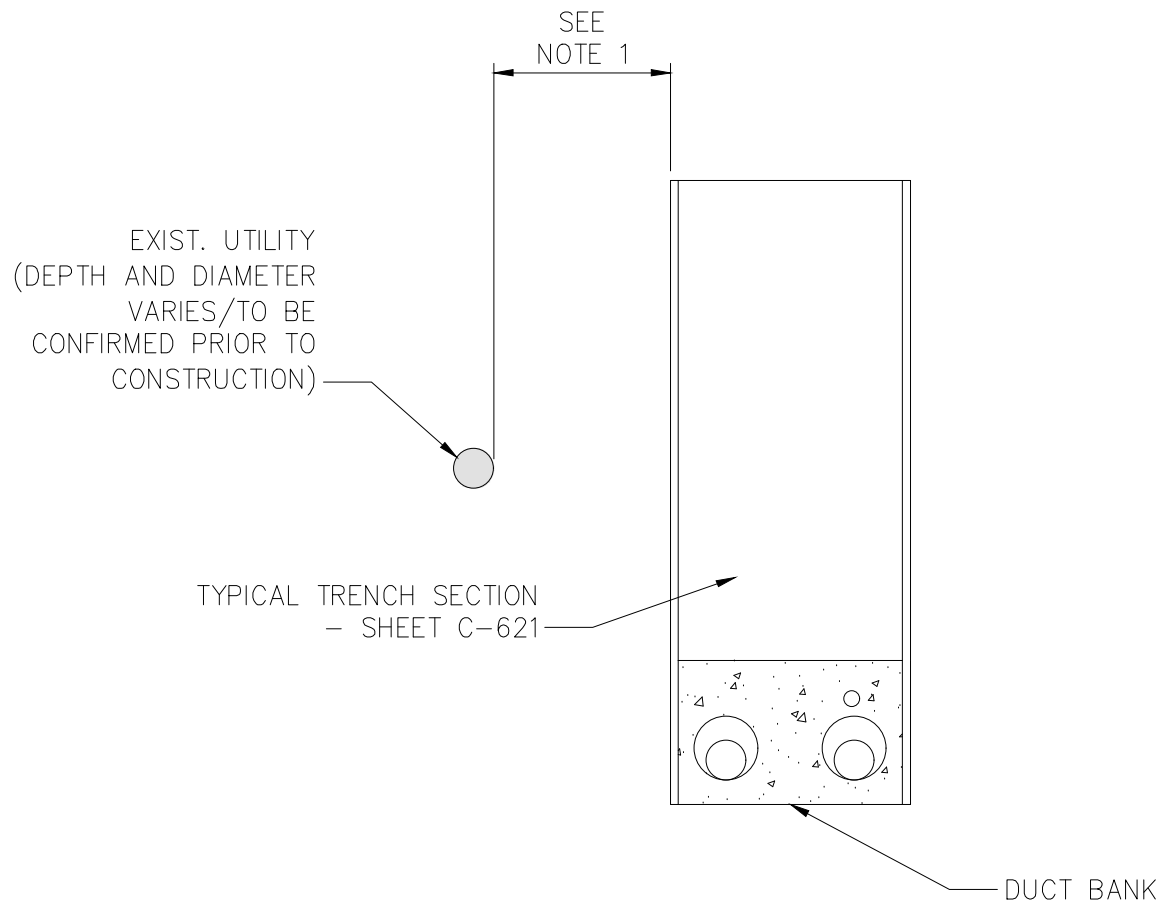
3 OPEN TRENCH – VERTICAL SEPARATION (UNDER)  
NOT TO SCALE



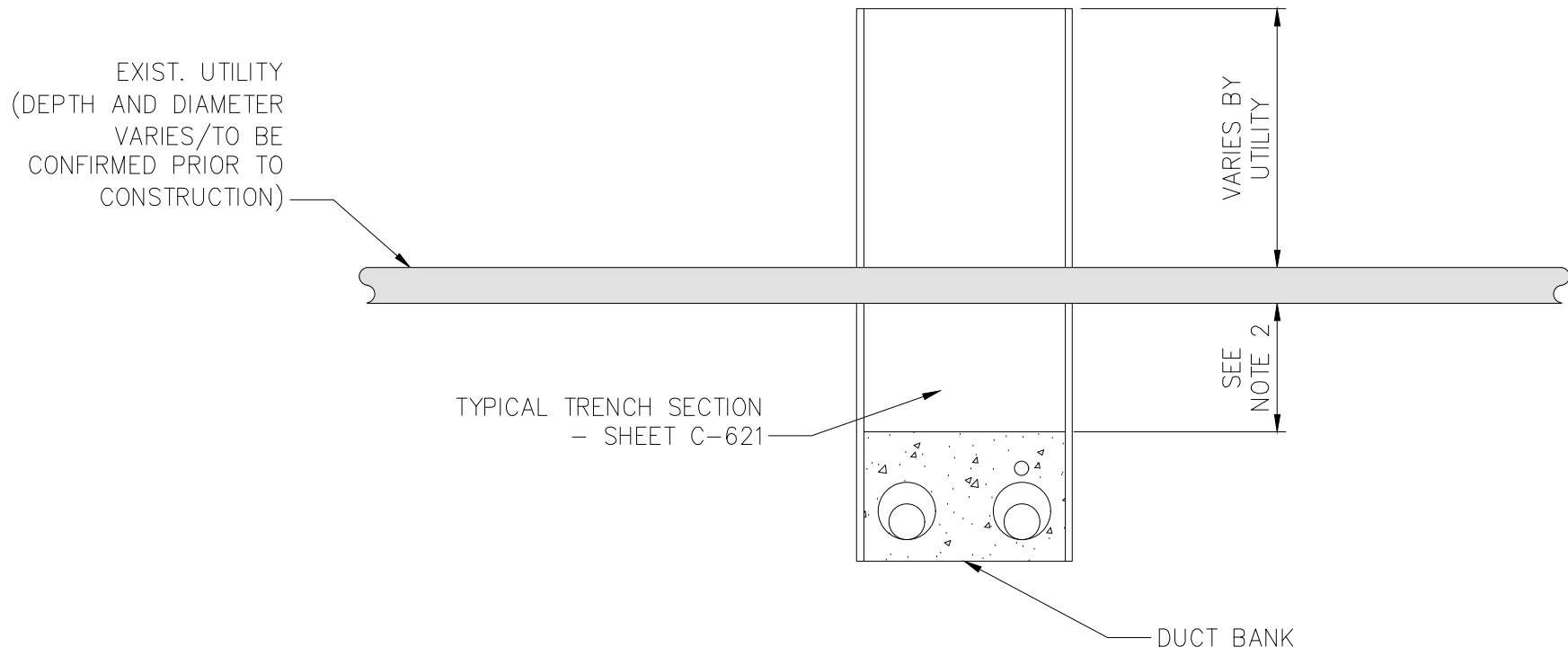
5 OPEN TRENCH – VERTICAL SEPARATION (OVER)  
NOT TO SCALE



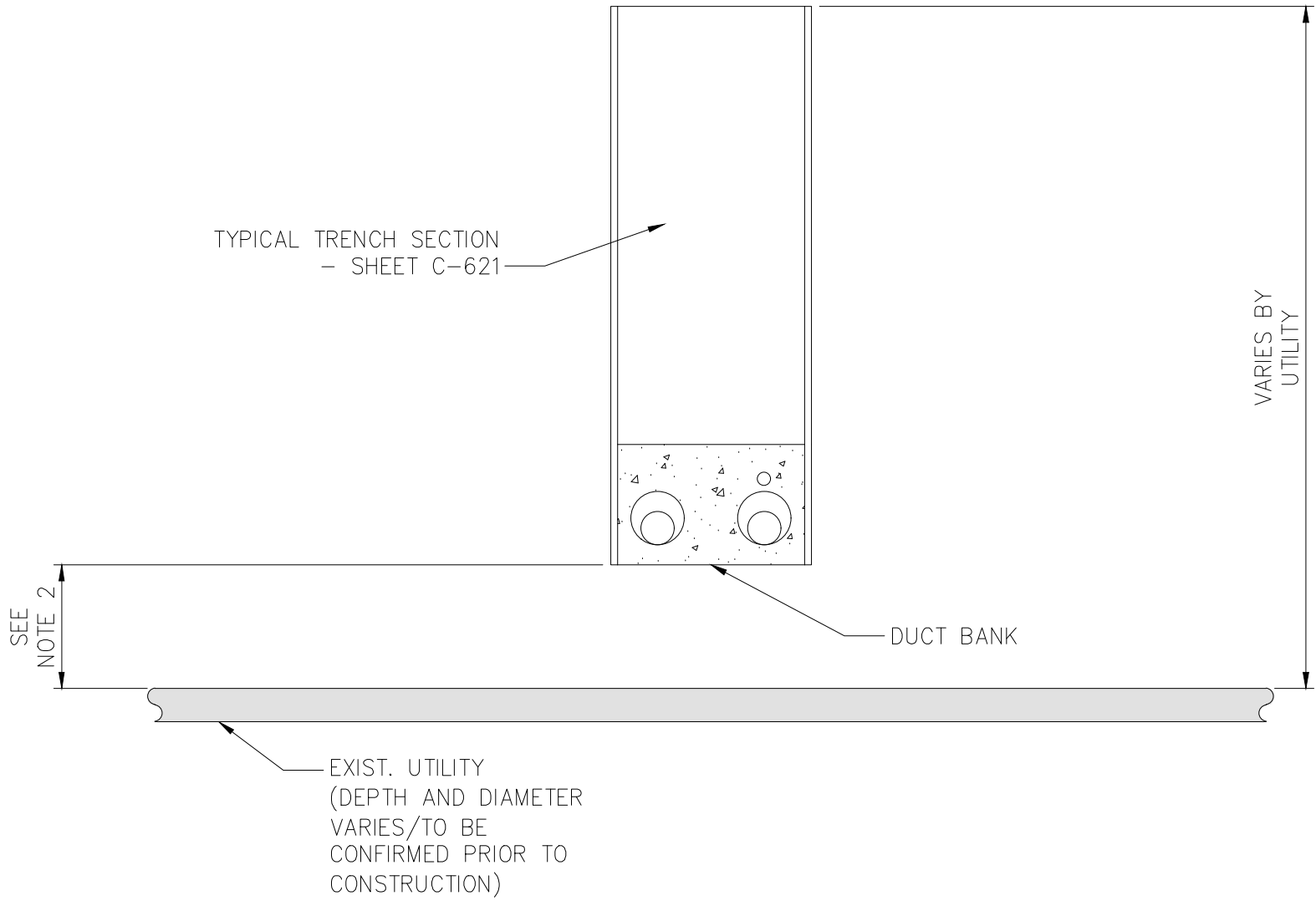
2 TYPICAL DUCTBANK IN ROADWAY – HORIZONTAL SEPARATION  
NOT TO SCALE



4 TYPICAL DUCTBANK IN ROADWAY – VERTICAL SEPARATION (UNDER)  
NOT TO SCALE



6 TYPICAL DUCTBANK IN ROADWAY – VERTICAL SEPARATION (OVER)  
NOT TO SCALE

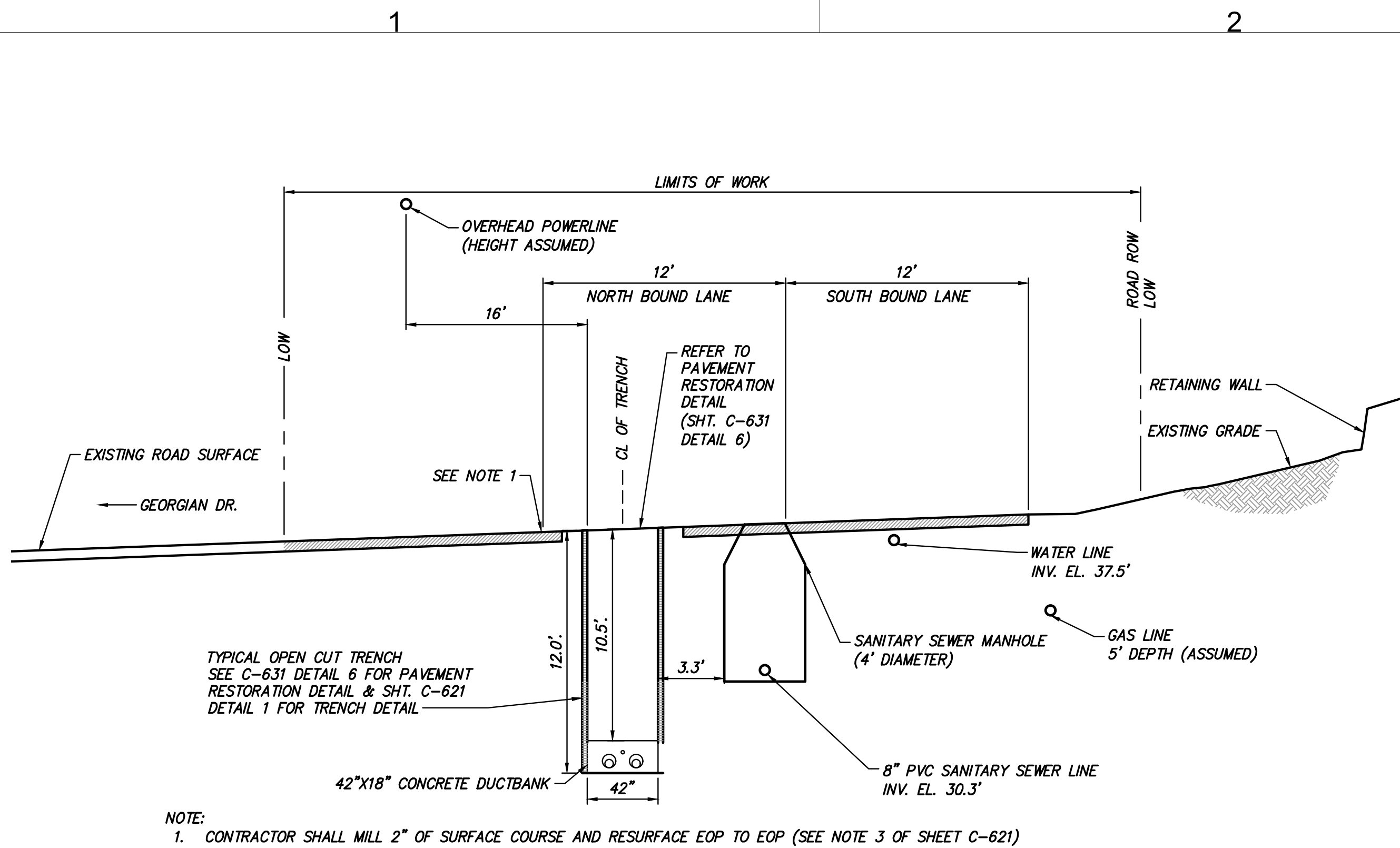


- NOTES:
- 2.0' MIN HORIZONTAL SEPARATION FOR ALL EXISTING UTILITIES, EXCEPT 5.0' MIN FOR GAS DISTRIBUTION LINES
  - 1.5' MIN VERTICAL SEPARATION FOR ALL EXISTING UTILITIES, EXCEPT 2.0' MIN FOR GAS DISTRIBUTION LINES

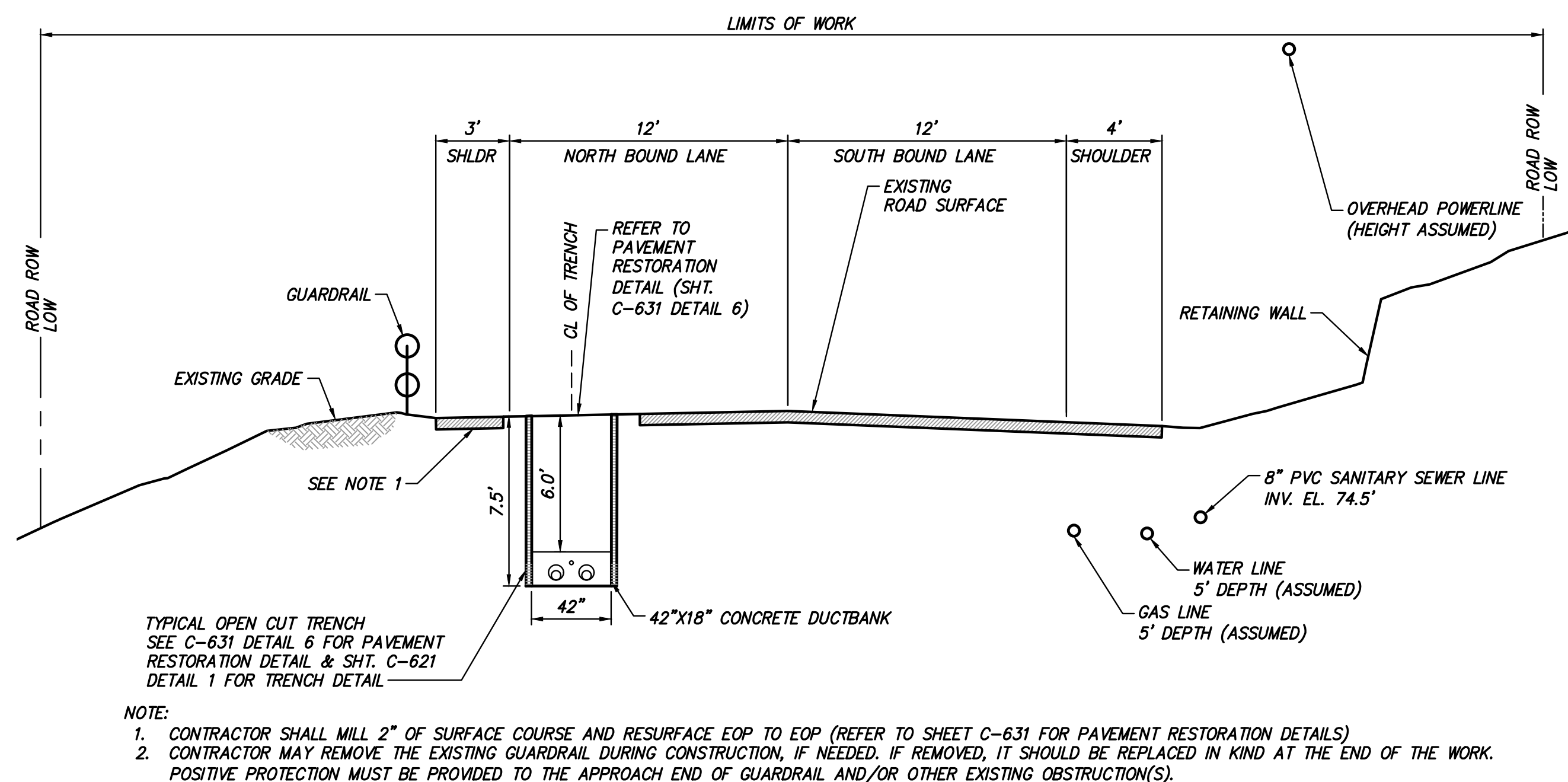




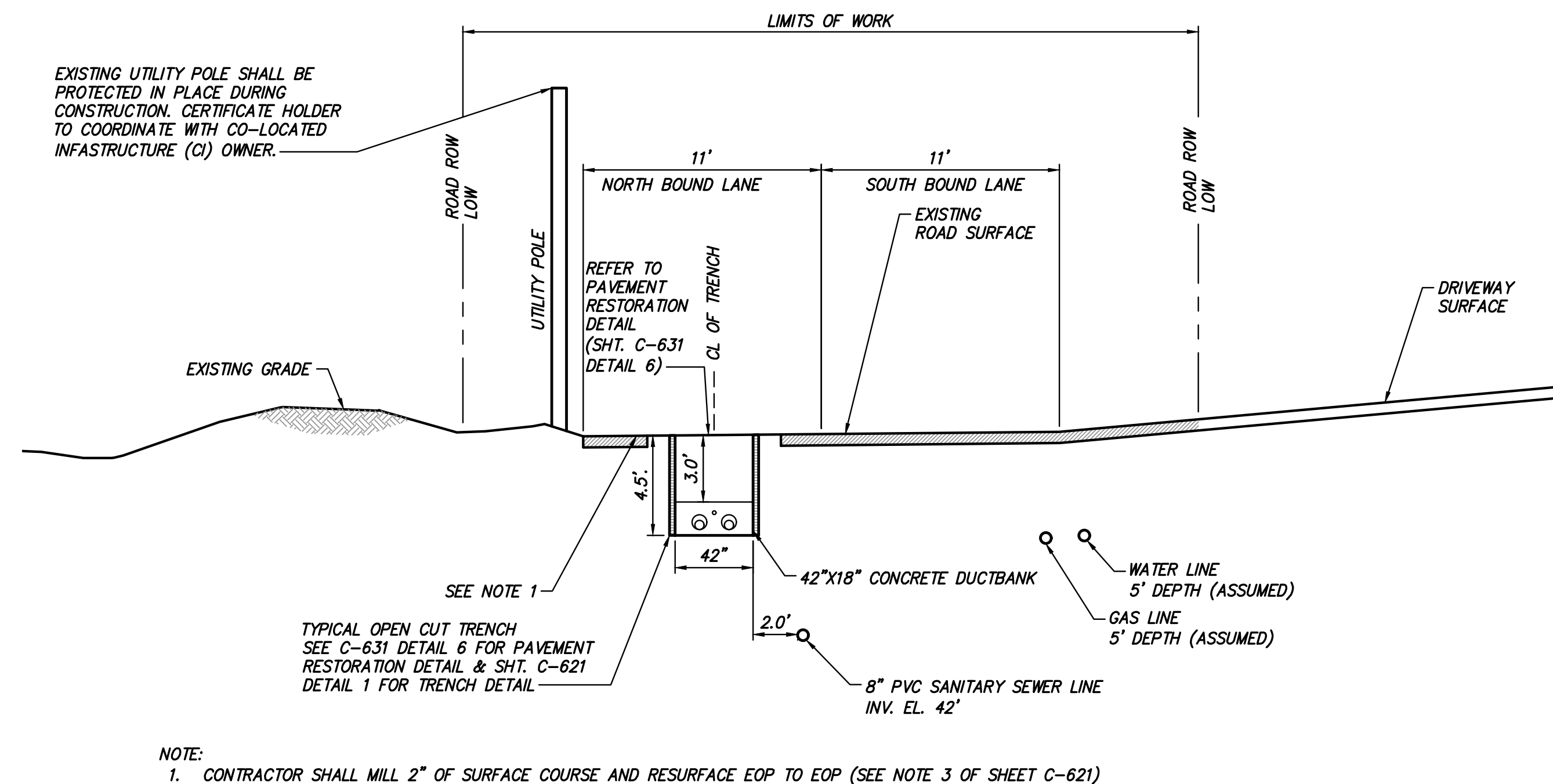




③ TRENCH AT STA. 72506+50  
TRENCH FROM EXIT PIT OF HDD 124 TO INTERSECTION OF PARK AVE AND BATTLEFIELD ROAD  
 SCALE: 1" = 5'



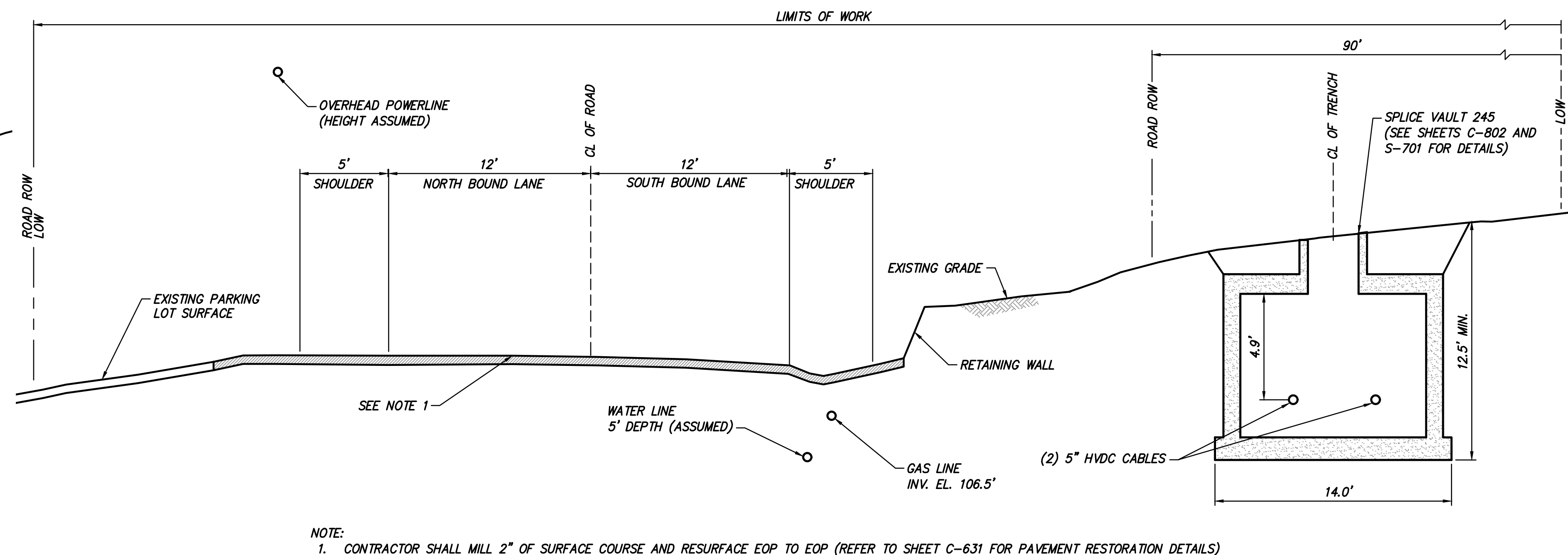
TRENCH AT STA. 72518+00  
④ TRENCH FROM INTERSECTION OF PARK AVE & RT 9W TO NORTH OF JAMES FARLEY BRIDGE  
SCALE: 1" = 5'



TRENCH AT STA. 72512+70

③ TRENCH FROM EXIT PIT OF HDD 124 TO INTERSECTION OF PARK AVE AND BATTLEFIELD ROAD

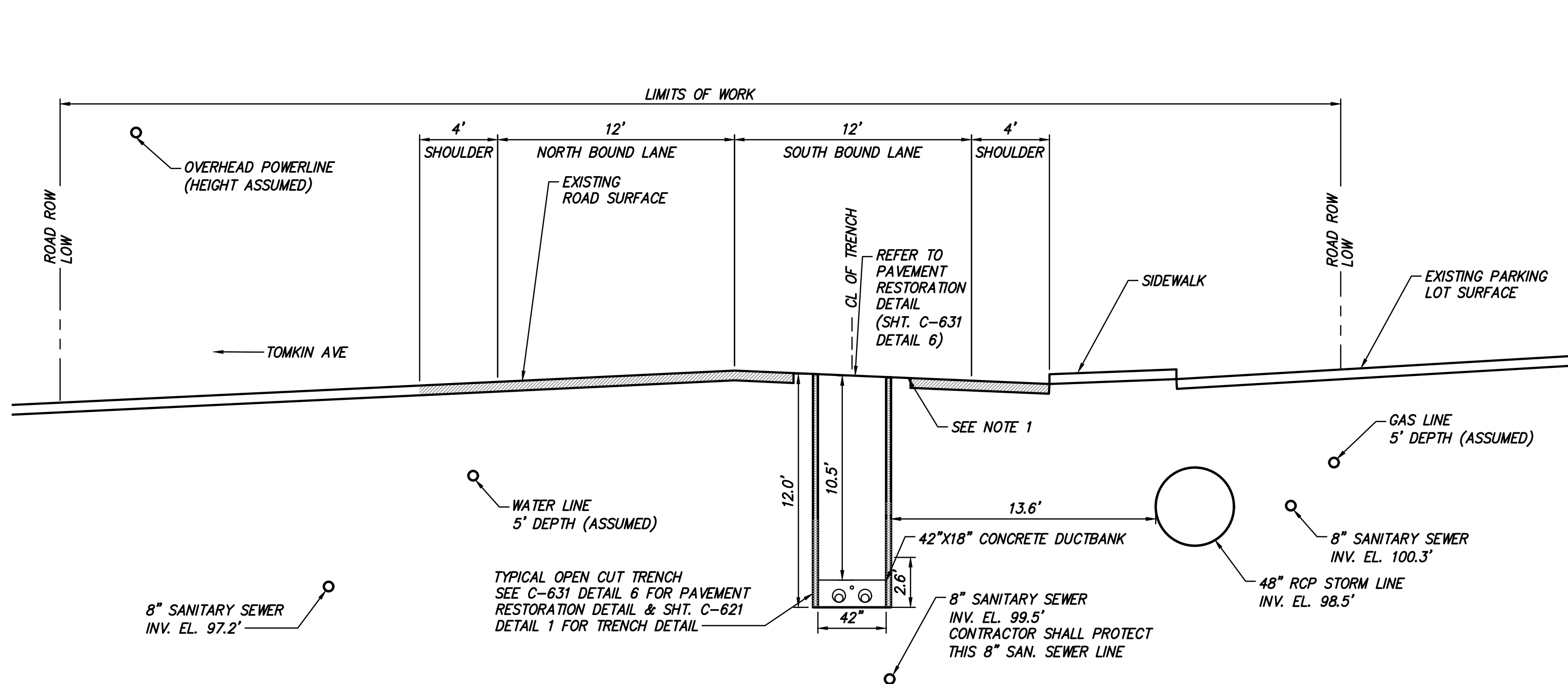
SCALE: 1" = 5'



④ SPLICE VAULT 245 AT STA. 72527+50  
SCALE: 1" = 5'



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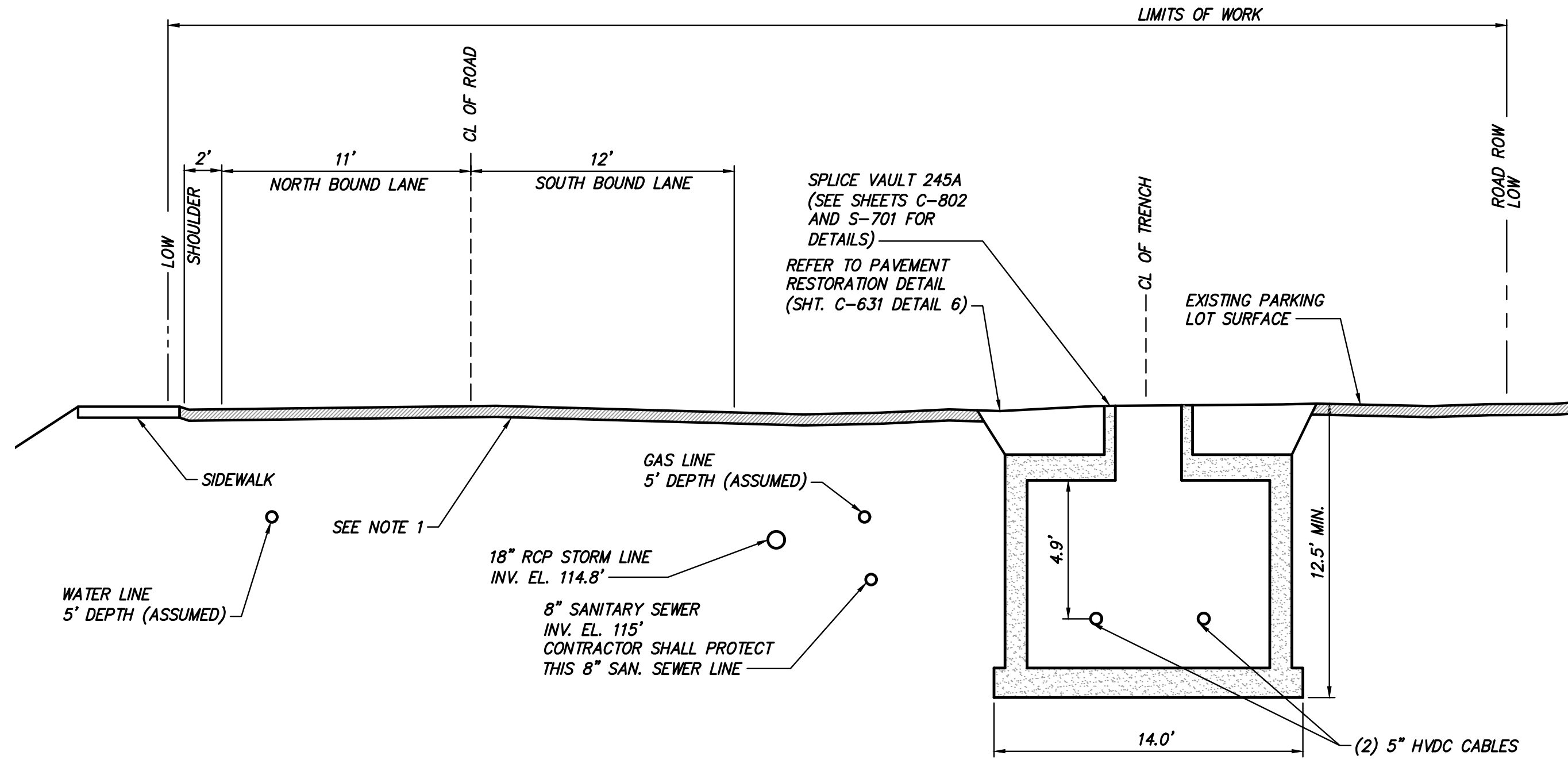


NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

#### TRENCH AT STA. 72531+75

④ TRENCH FROM INTERSECTION OF PARK AVE & RT 9W TO NORTH OF JAMES FARLEY BRIDGE

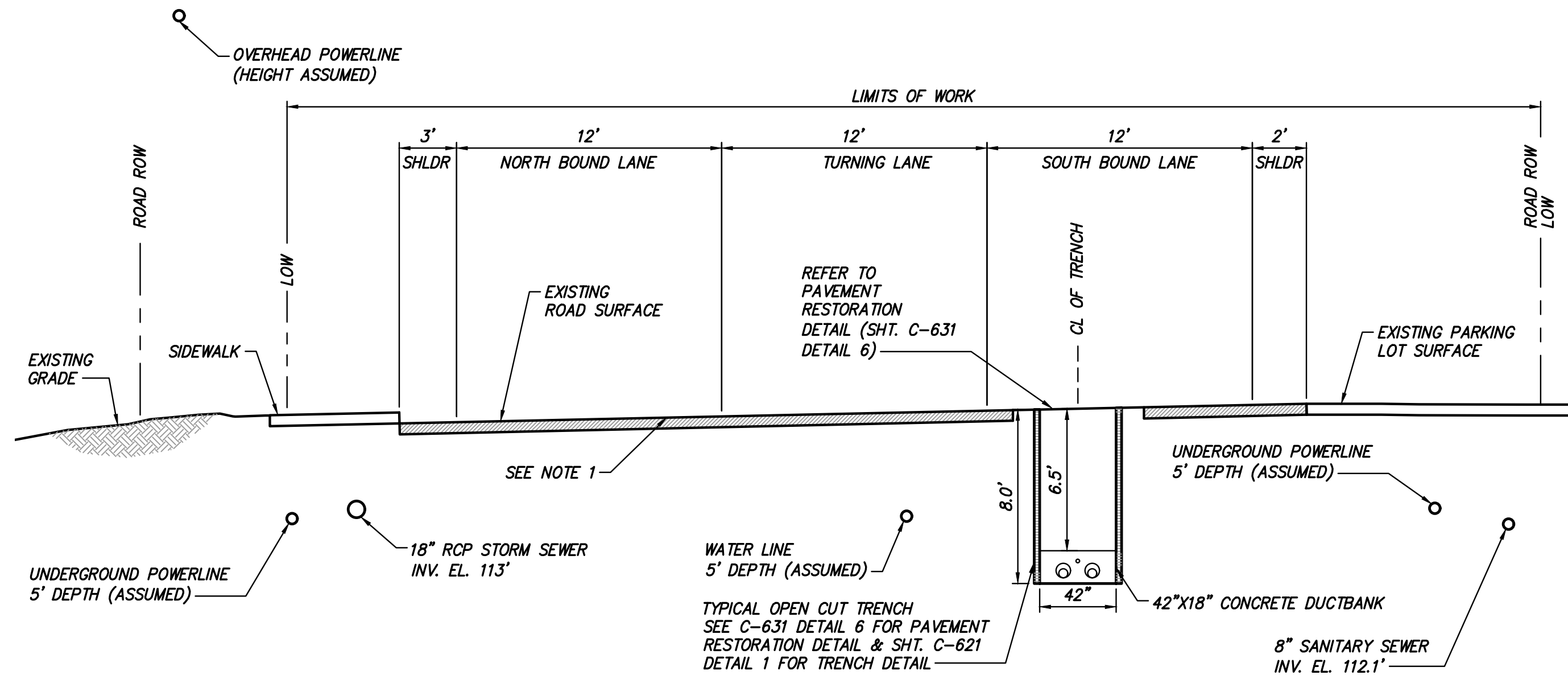
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

④ SPLICE VAULT 245A AT STA. 72538+40

SCALE: 1" = 5'

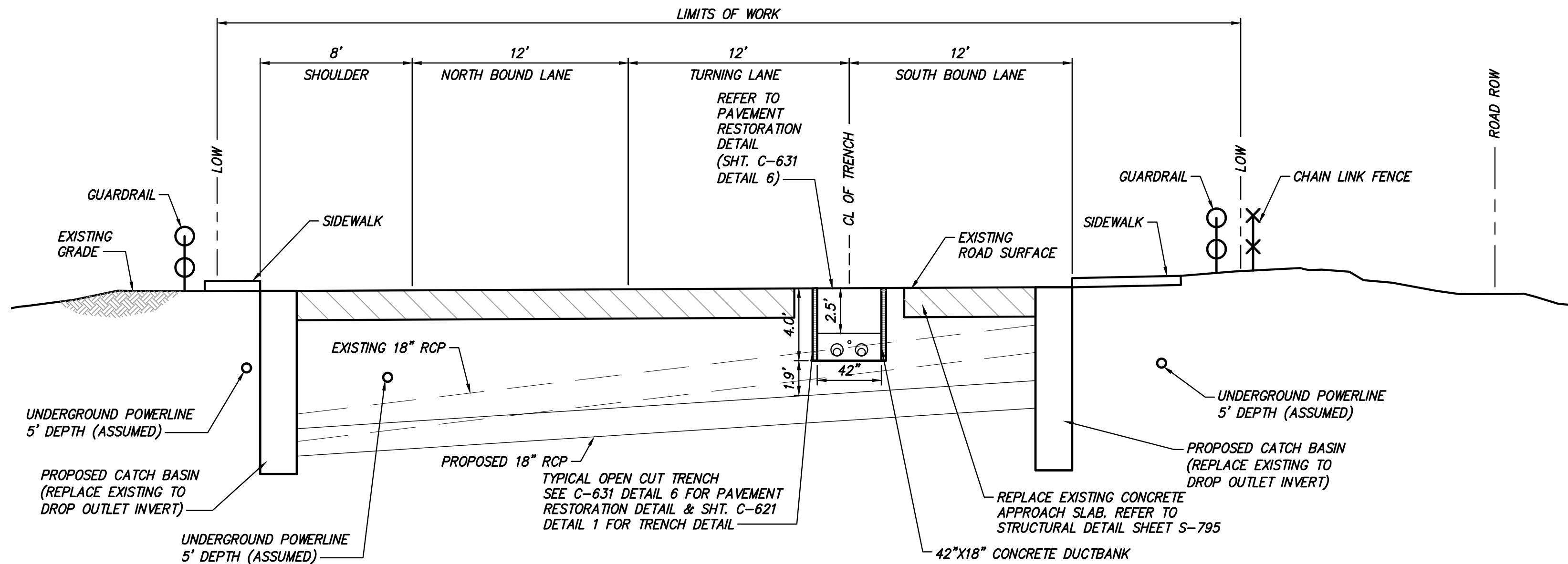


NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

#### TRENCH AT STA. 72546+00

④ TRENCH FROM INTERSECTION OF PARK AVE & RT 9W TO NORTH OF JAMES FARLEY BRIDGE

SCALE: 1" = 5'



#### TRENCH AT STA. 72548+90

⑤ NORTH AND SOUTH APPROACHES OF JAMES FARLEY BRIDGE

SCALE: 1" = 5'



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ISSUED FOR PERMITTING

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72531+75 TO STA. 72548+90

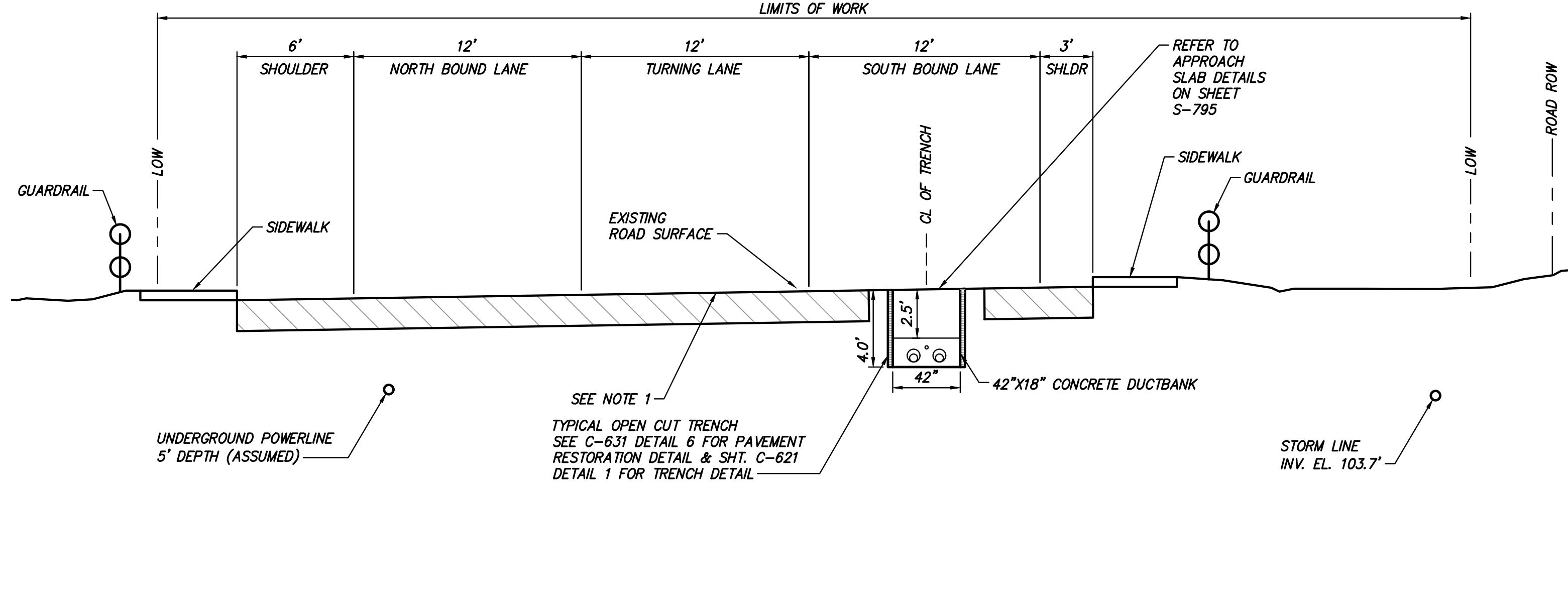
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REV. NO. C SH. NO. OF

KIEWIT PROJECT NO.  
21162  
TT PROJECT NO.  
204-3701  
DRAWING NO.

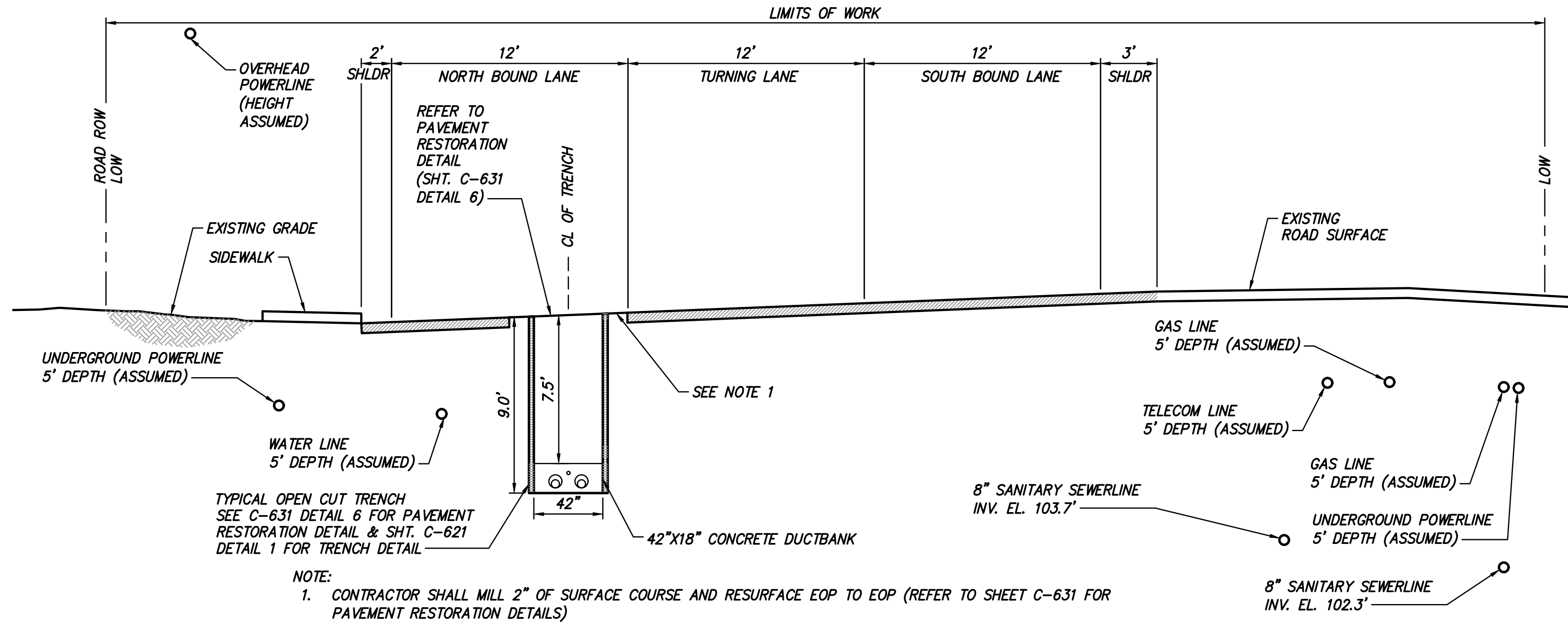
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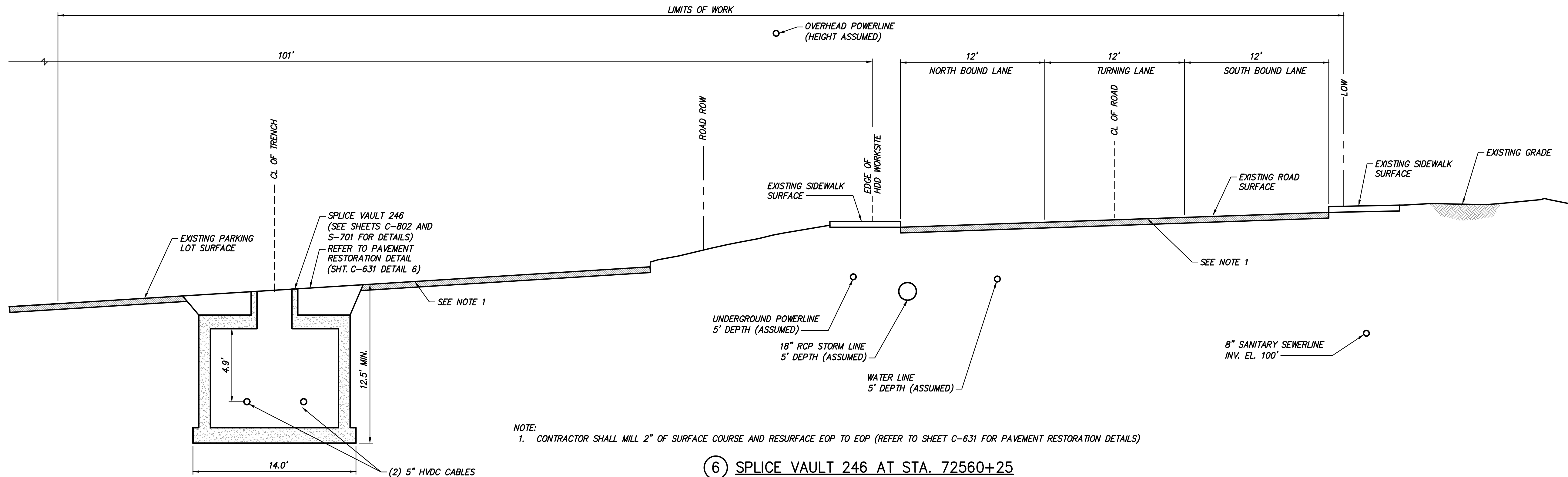
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⑤ TRENCH AT STA. 72553+15  
NORTH AND SOUTH APPROACHES OF JAMES FARLEY BRIDGE  
SCALE: 1" = 5'



⑥ TRENCH AT STA. 72557+00  
TRENCH FROM SOUTH OF JAMES FARLEY BRIDGE TO ENTRY PIT OF HDD-126  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

⑥ SPLICE VAULT 246 AT STA. 72560+25  
SCALE: 1" = 5'



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C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72553+15 TO STA. 72560+25

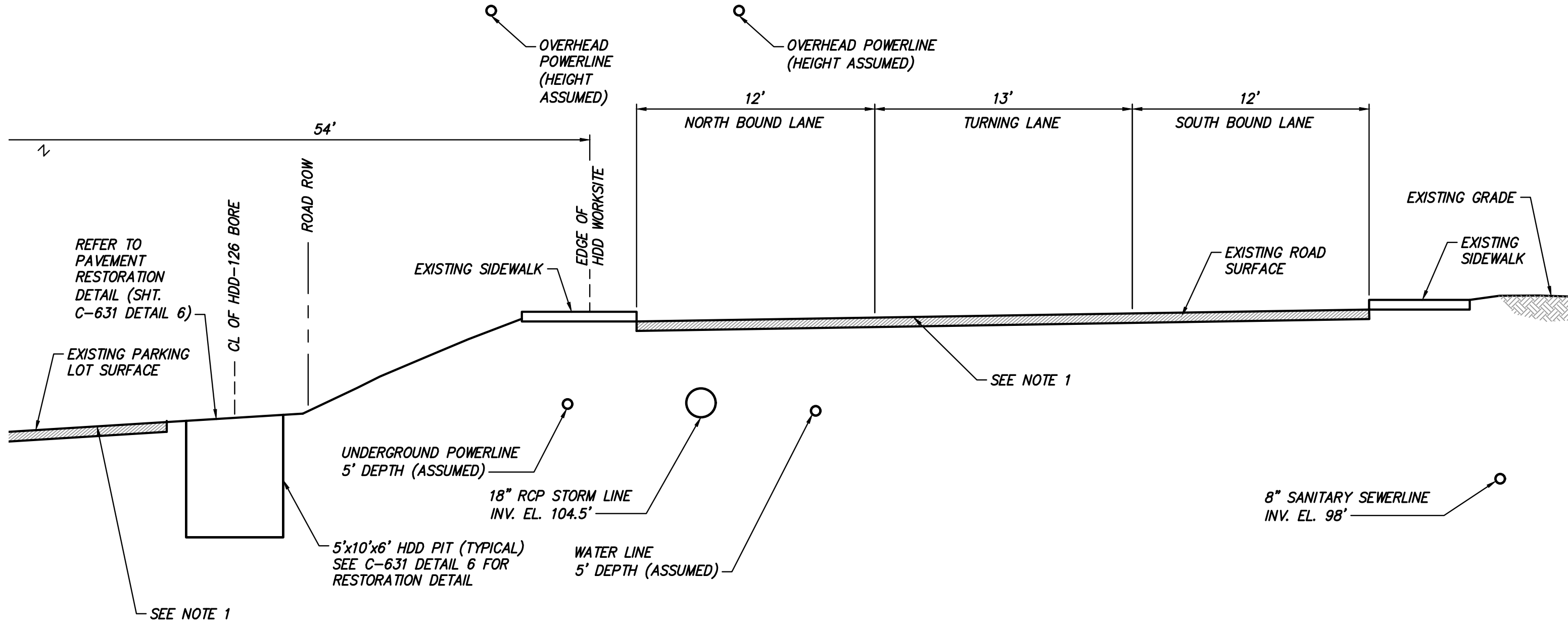
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21162  
TT PROJECT NO.  
204-3701  
DRAWING NO.

C-1004

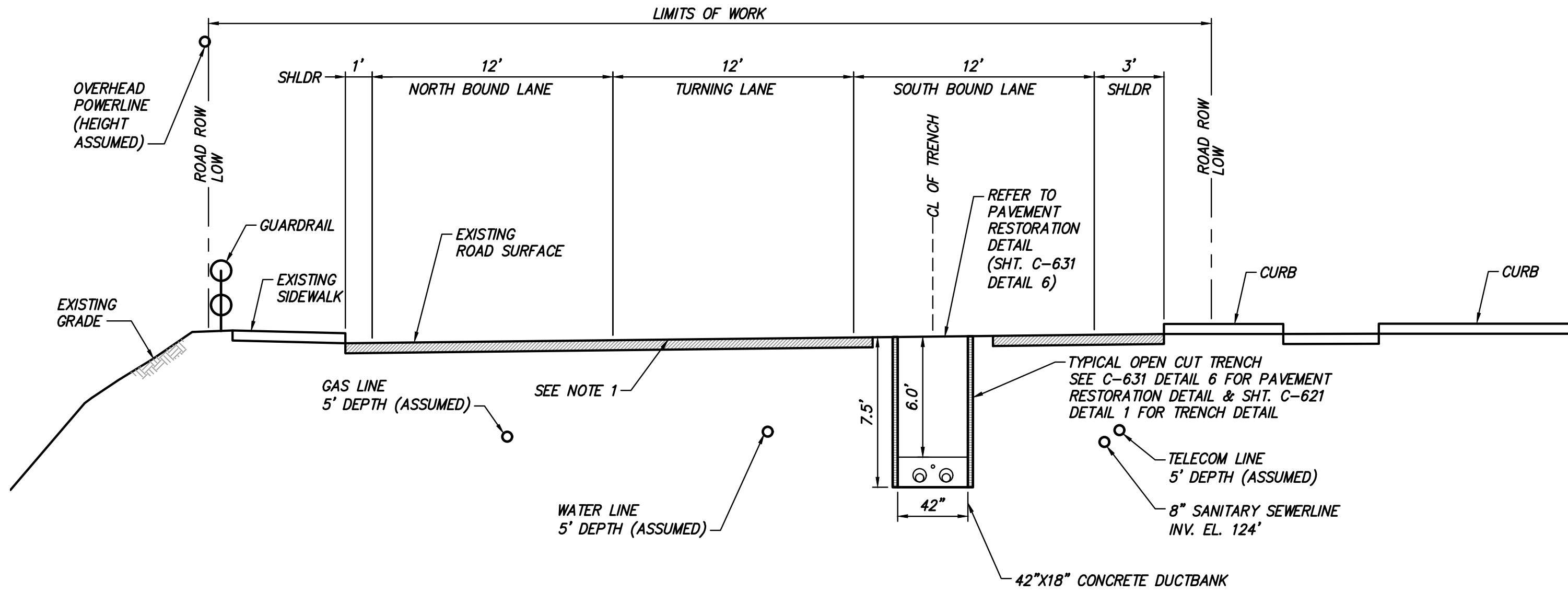


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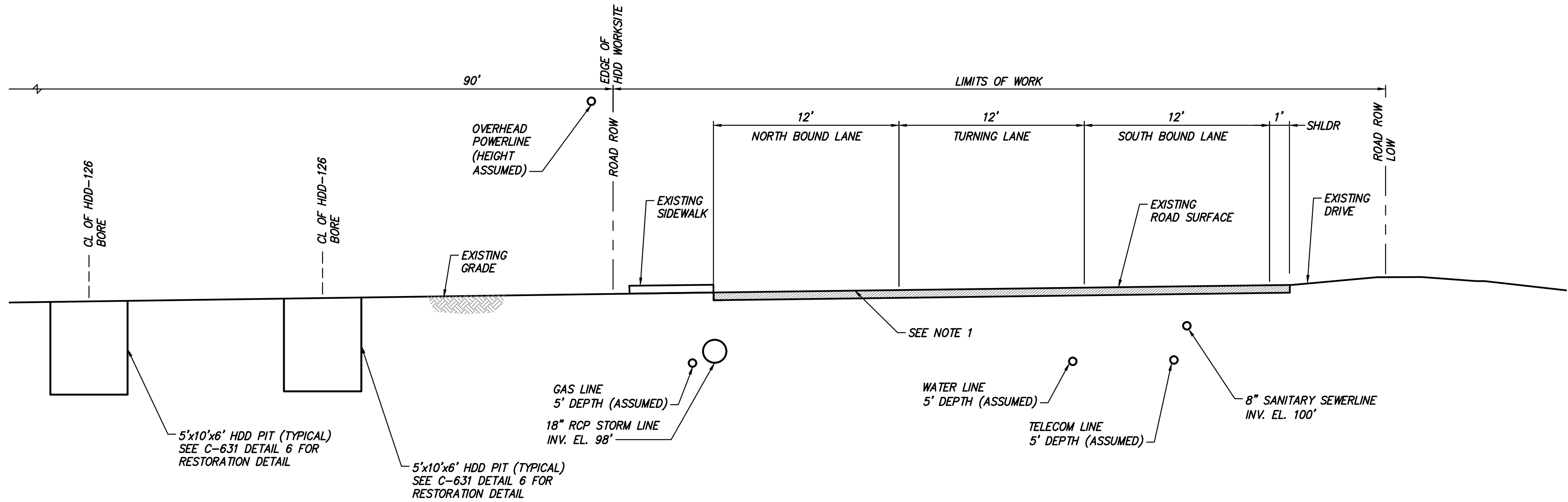
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**⑥ HDD 126 ENTRY PIT AT STA. 72561+20**  
**TRENCH FROM SOUTH OF JAMES FARLEY BRIDGE TO ENTRY PIT OF HDD-126**  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**⑦ TRENCH AT STA. 72588+50**  
**TRENCH FROM EXIT PIT OF HDD-126 TO ENTRY PIT OF HDD-127**  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**⑦ HDD 126 EXIT PIT AT STA. 72582+80**  
**TRENCH FROM EXIT PIT OF HDD-126 TO ENTRY PIT OF HDD-127**  
SCALE: 1" = 5'



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C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

**CHAMPLAIN HUDSON POWER EXPRESS**  
**SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:**  
**ROCKLAND**  
**ROAD SECTION EXHIBIT**  
**STA. 72561+20 TO STA. 72588+50**

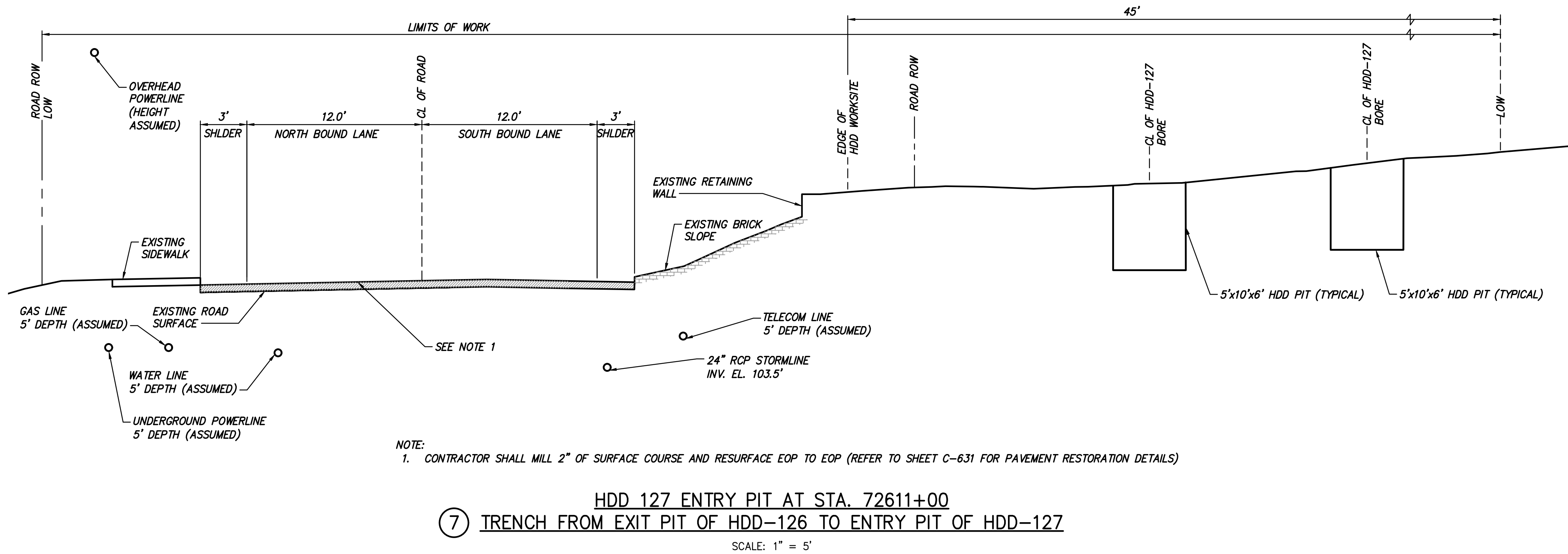
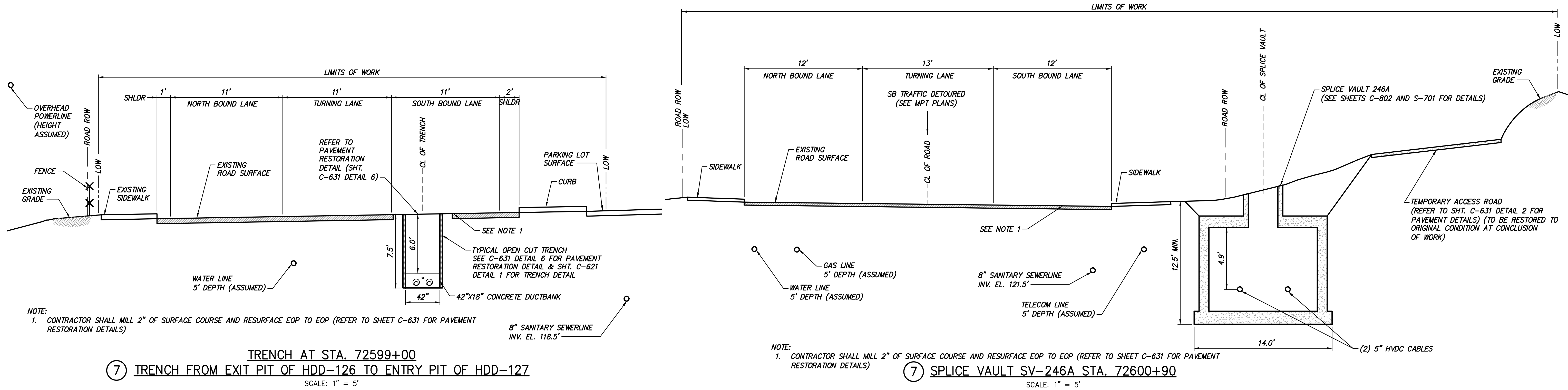
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KIEWIT PROJECT NO.  
21162  
TT PROJECT NO.  
204-3701  
DRAWING NO.

**C-1005**

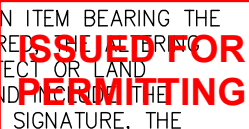


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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

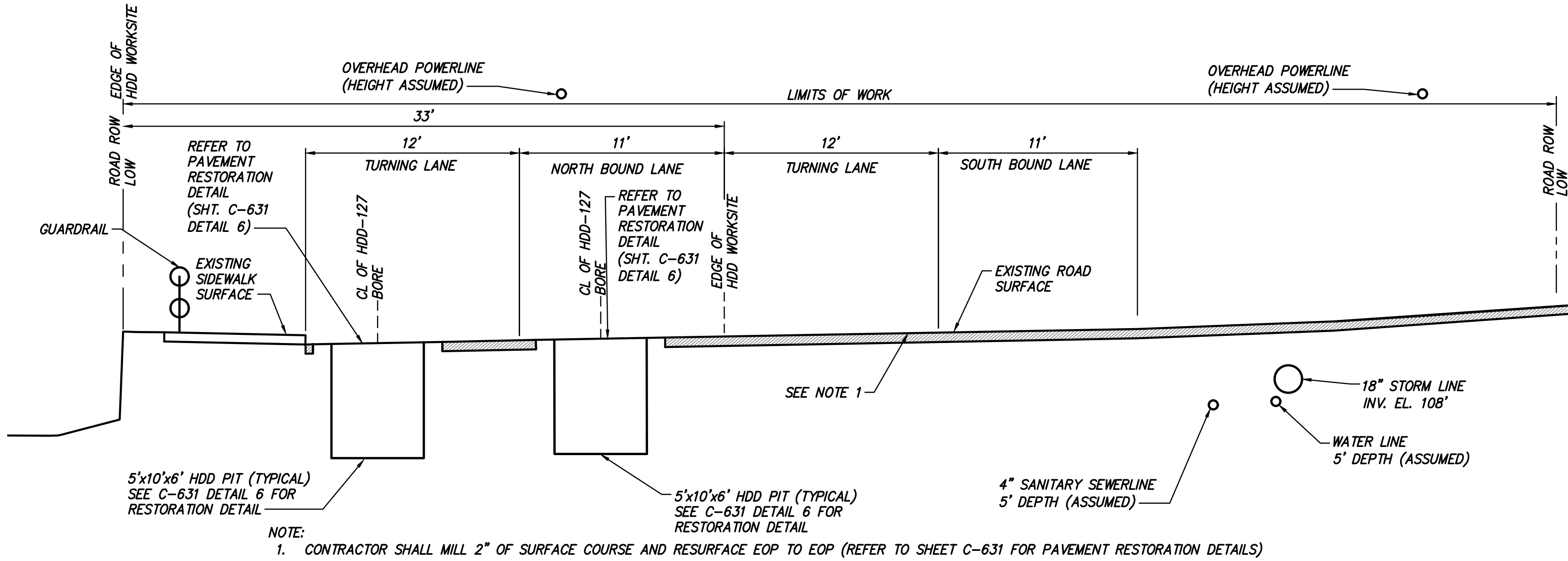
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72599+00 TO STA.72611+00

DRAWN BY:	WJ	DESIGNED BY:	AC	APPROVED BY:	JL	SCALE	AS SHOWN	DATE	04/14/2023
						REV. NO.	C	SH. NO.	OF

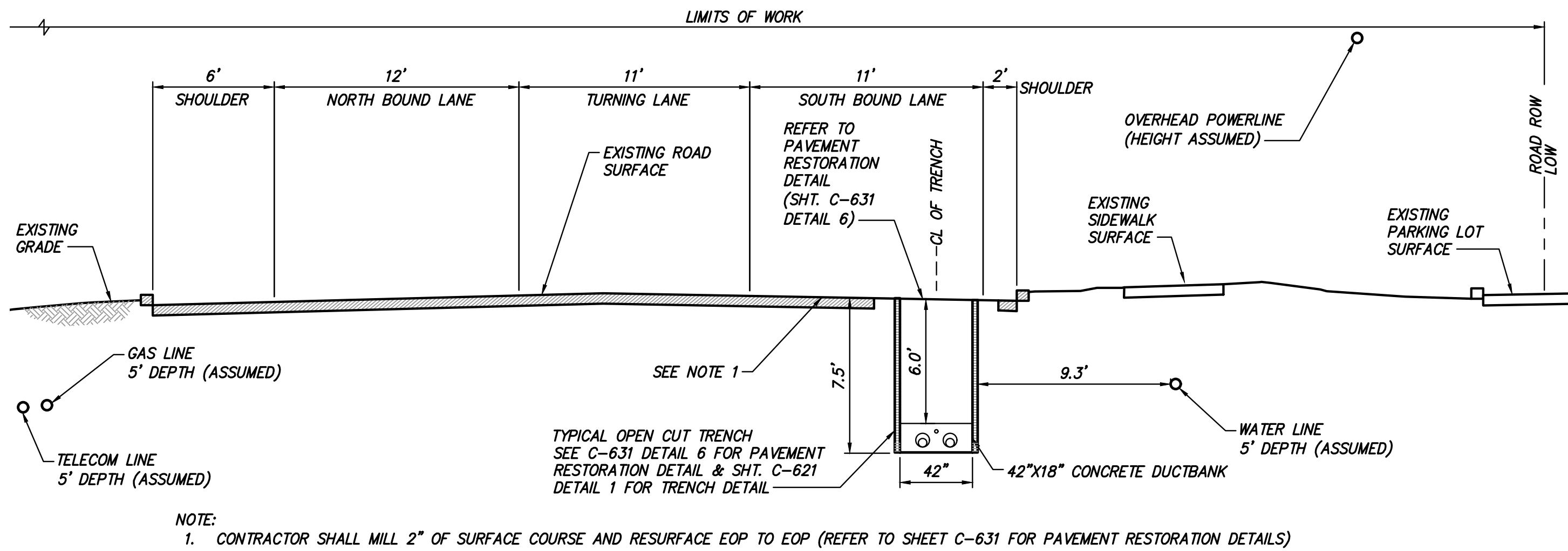
KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	C-1006



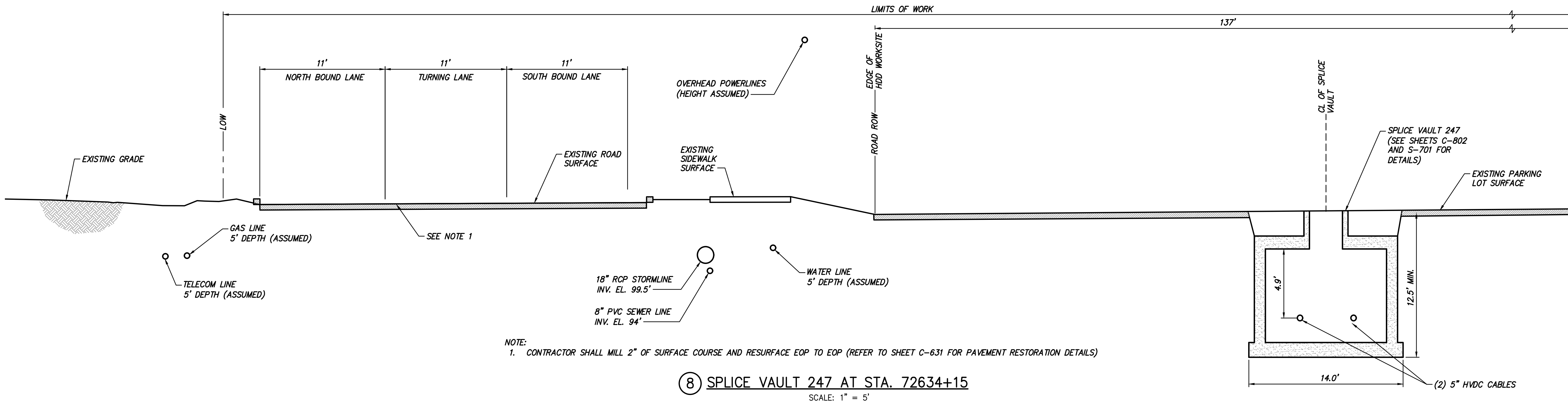
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HDD 127 EXIT PIT AT STA. 72625+45  
⑧ TRENCH FROM EXIT PIT OF HDD-127 TO ENTRY PIT OF HDD-129  
SCALE: 1" = 5'



TRENCH AT STA. 72628+50  
⑧ TRENCH FROM EXIT PIT OF HDD-127 TO ENTRY PIT OF HDD-129  
SCALE: 1" = 5'



⑧ SPLICE VAULT 247 AT STA. 72634+15  
SCALE: 1" = 5'



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ISSUED FOR PERMITTING

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72625+45 TO STA. 72634+15

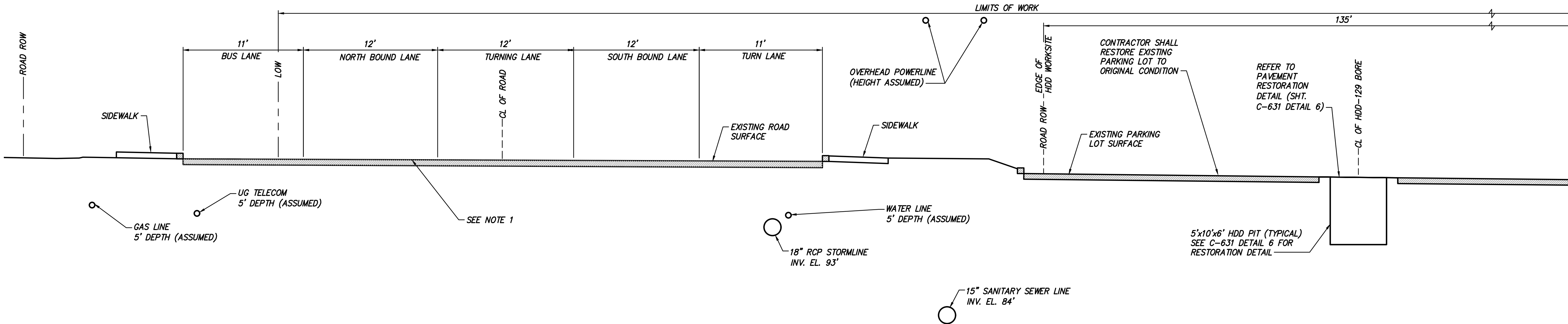
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KIEWIT PROJECT NO. 21162  
TT PROJECT NO. 204-3701  
DRAWING NO.

C-1007

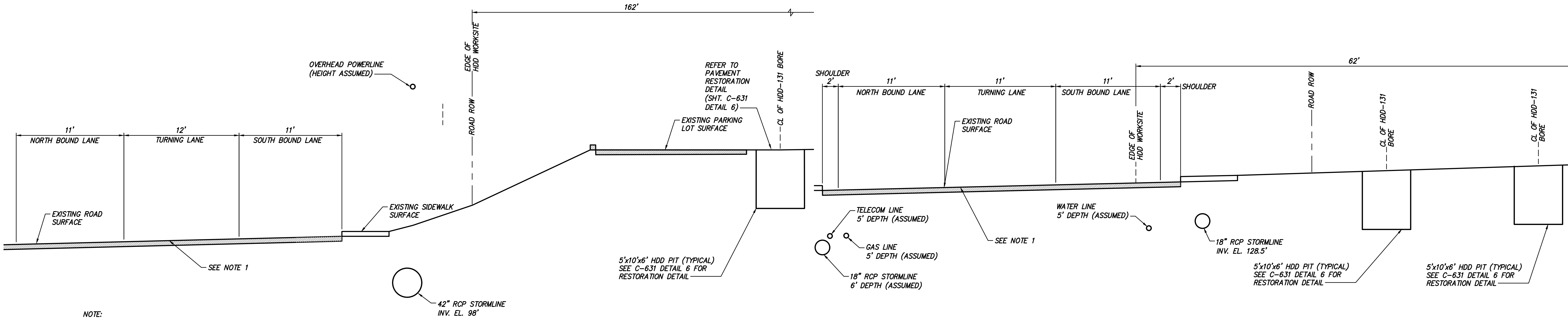


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NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**⑧ HDD 129 ENTRY PIT AT STA. 72635+50  
TRENCH FROM EXIT PIT OF HDD-127 TO ENTRY PIT OF HDD-129**  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**⑨ HDD 131 ENTRY PIT AT STA. 72655+20  
TRENCH AT EXIT PIT OF HDD-129 AND ENTRY PIT OF HDD-131**  
SCALE: 1" = 5'

NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**⑩ HDD 131 EXIT PIT AT STA. 72675+50  
TRENCH AT EXIT PIT OF HDD-131 TO ENTRY PIT OF HDD-132**  
SCALE: 1" = 5'



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**ISSUED FOR PERMITTING**

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

**CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROAD SECTION EXHIBIT  
STA. 72635+50 TO STA. 72675+70**

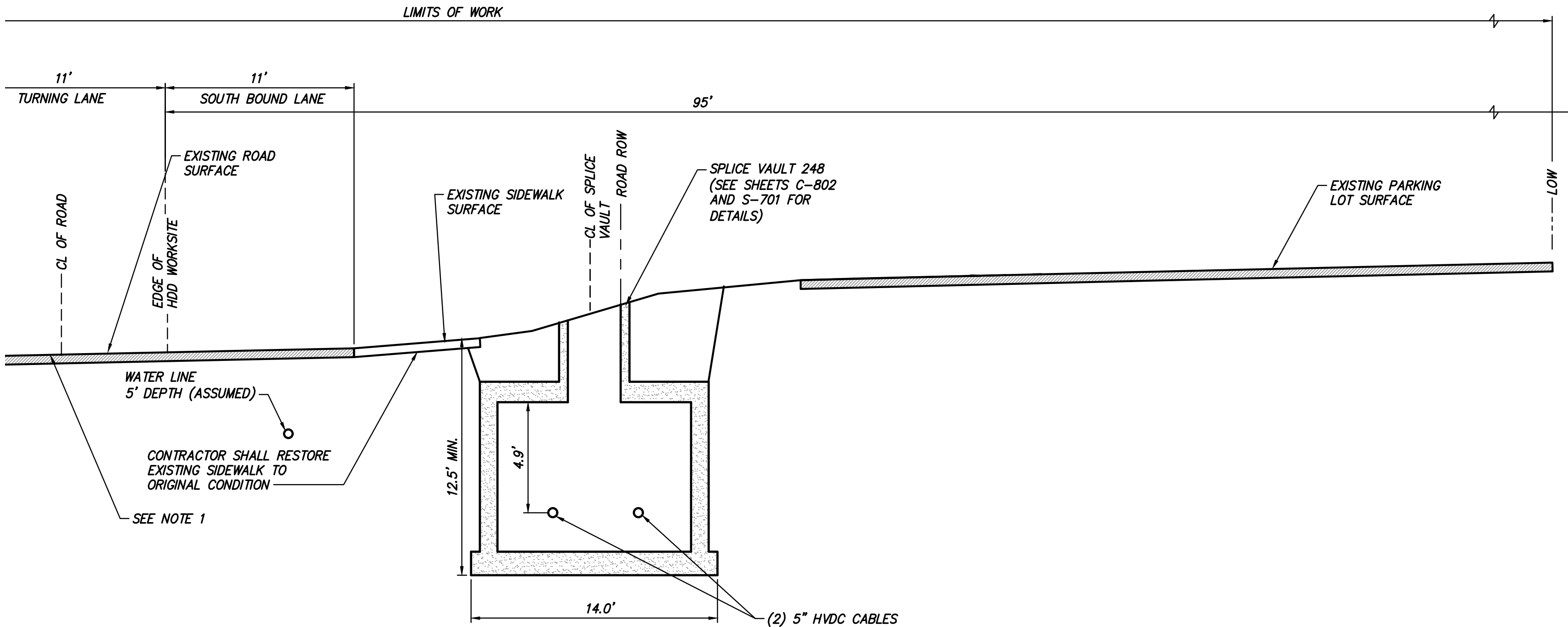
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REV. NO.	C						SH. NO.	OF

KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	

**C-1008**

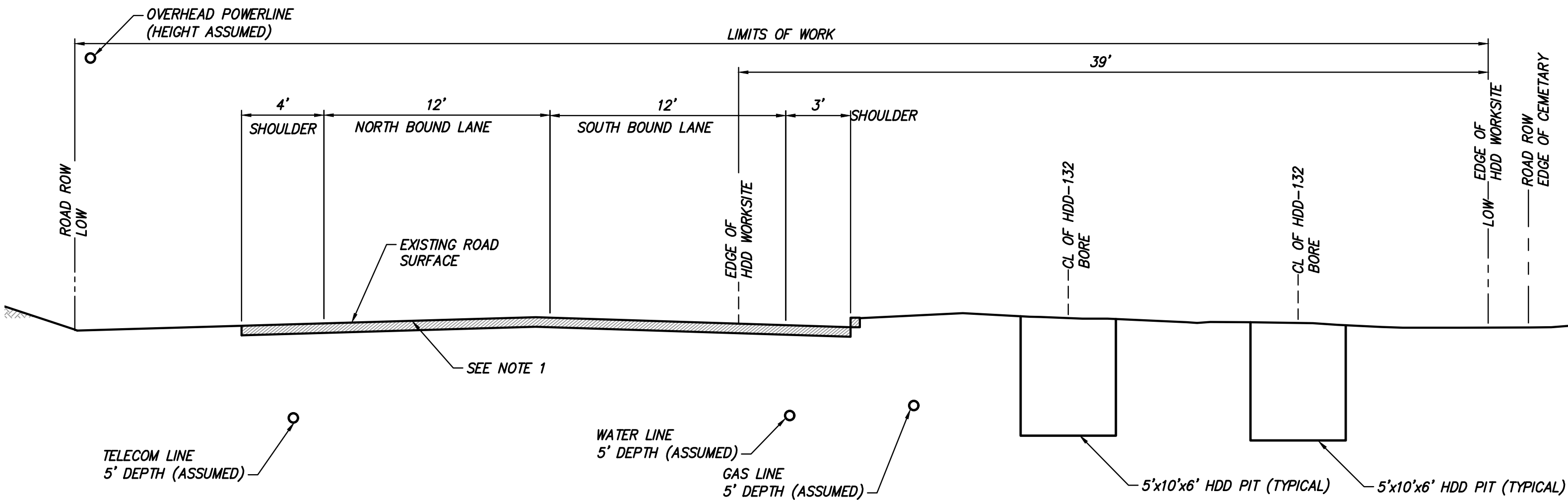


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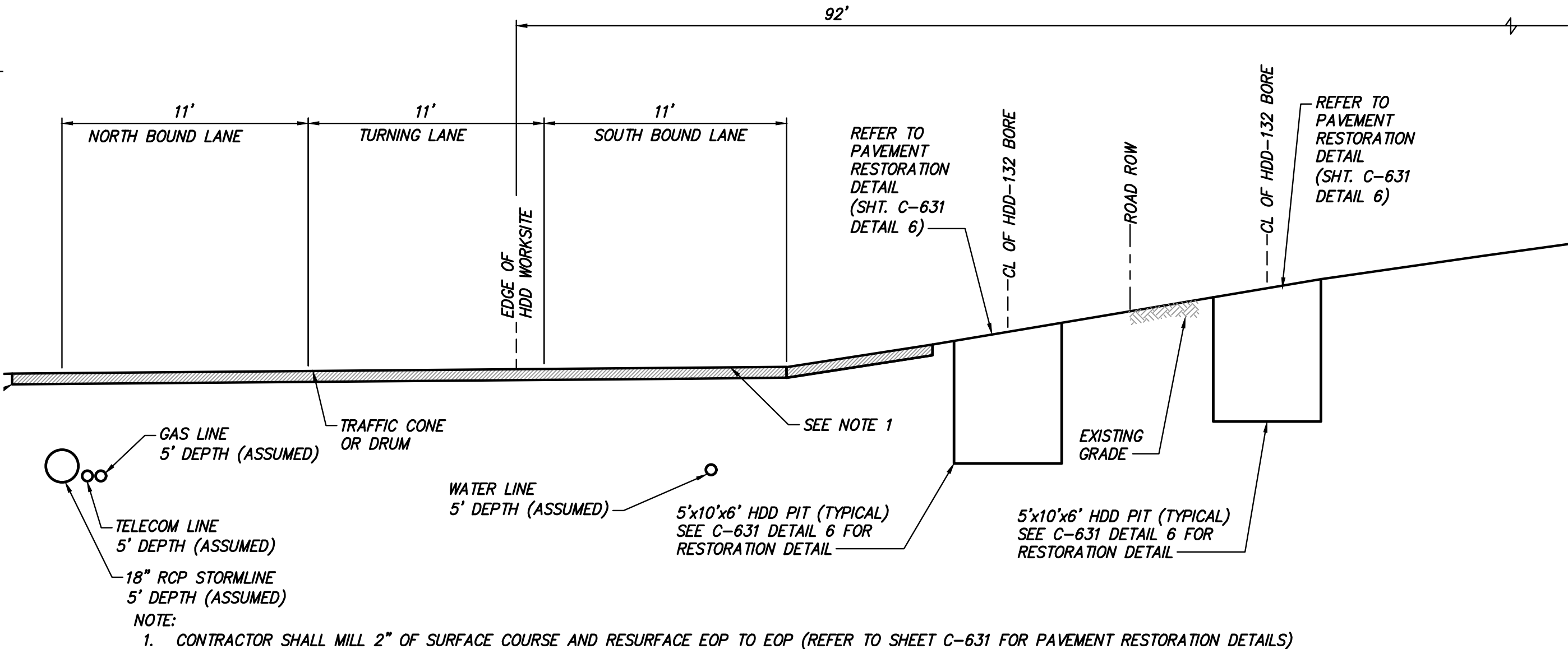
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

⑩ SPLICE VAULT 248 AT STA. 72676+75  
SCALE: 1" = 5'

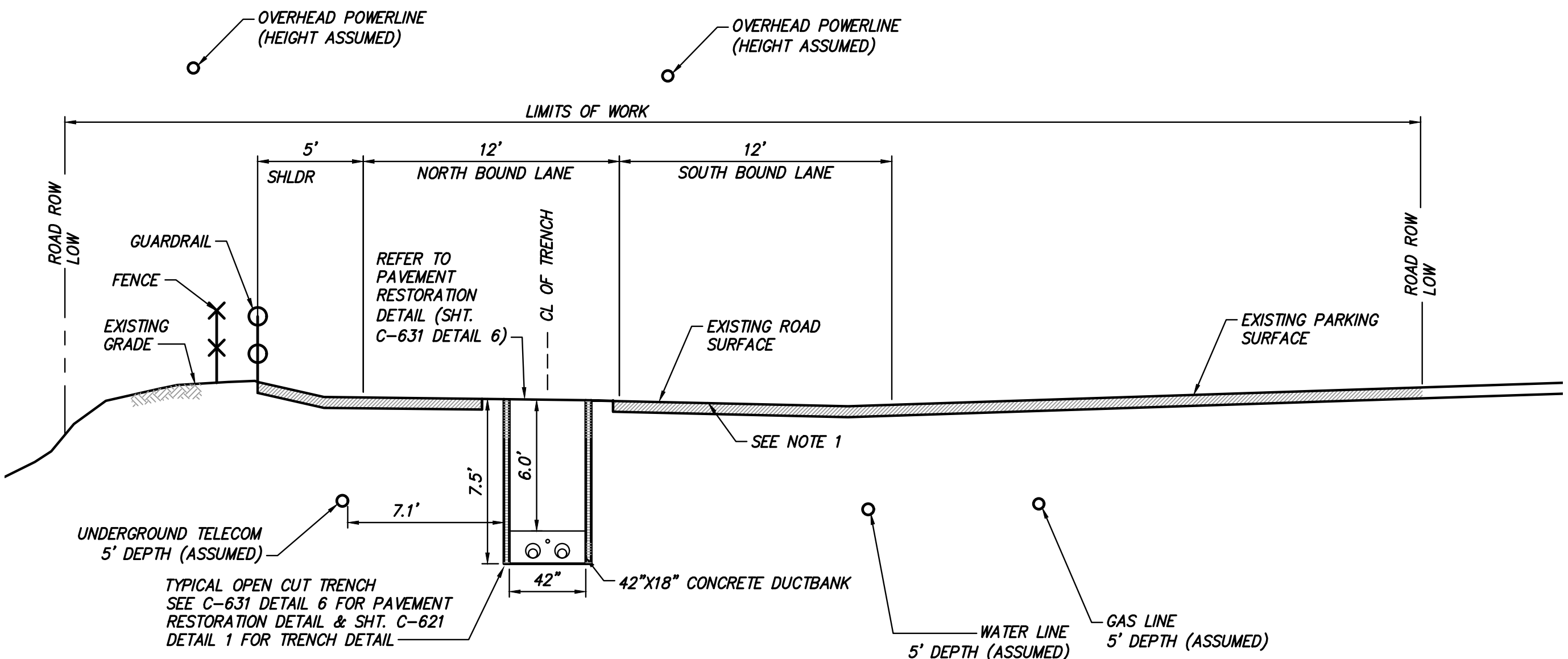


NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

⑪ TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD  
SCALE: 1" = 5'



HDD 132 ENTRY PIT AT STA. 72679+10  
⑩ TRENCH AT EXIT PIT OF HDD-131 TO ENTRY PIT OF HDD-132  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

TRENCH AT STA. 72698+95  
⑪ TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD  
SCALE: 1" = 5'



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ISSUED FOR PERMITTING

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C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

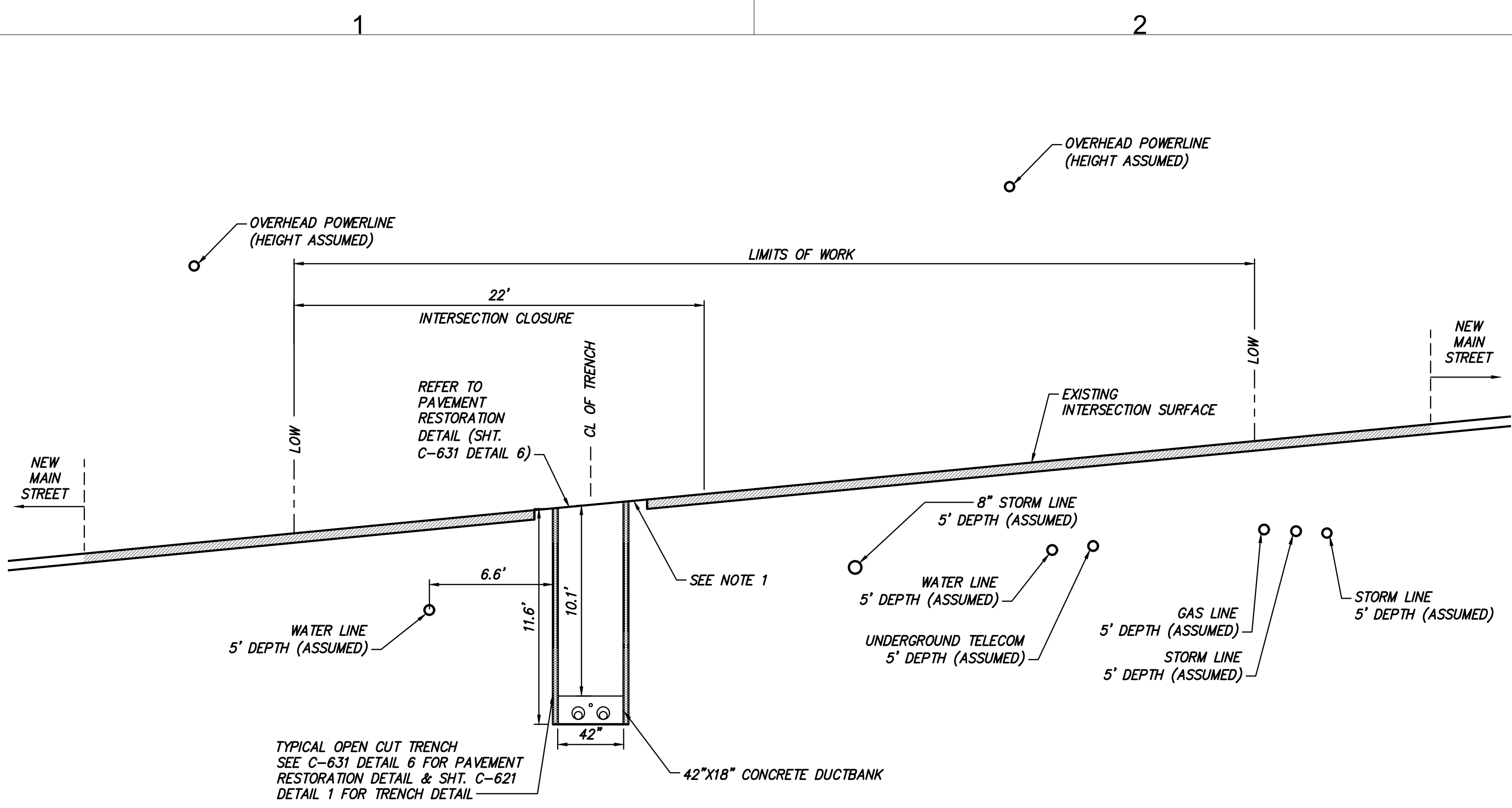
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72676+75 TO STA. 72698+95

DRAWN BY: WJ DESIGNED BY: AC APPROVED BY: JL AS SHOWN REV. NO. C DATE 04/14/2023 SH. NO. OF

KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	C-1009

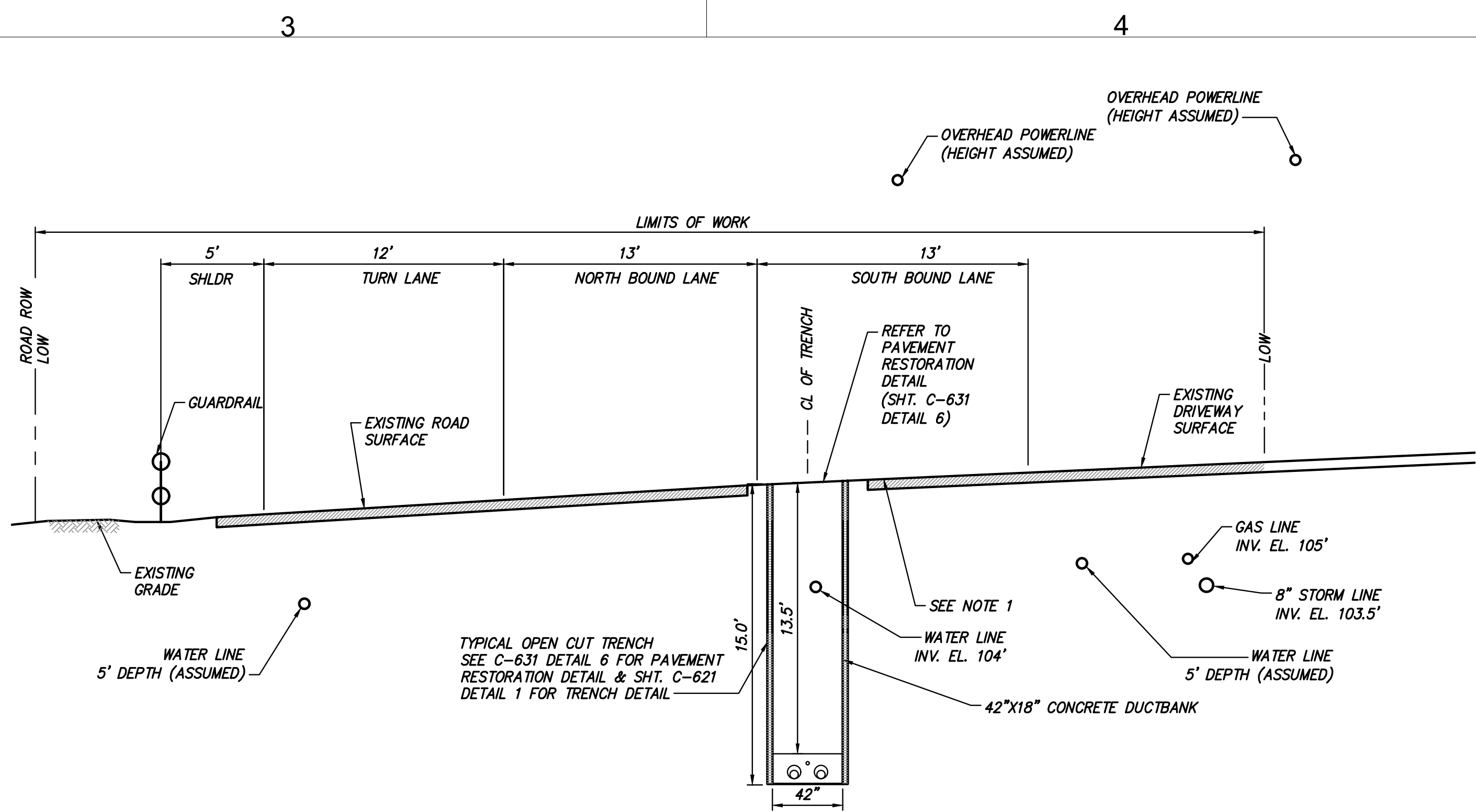


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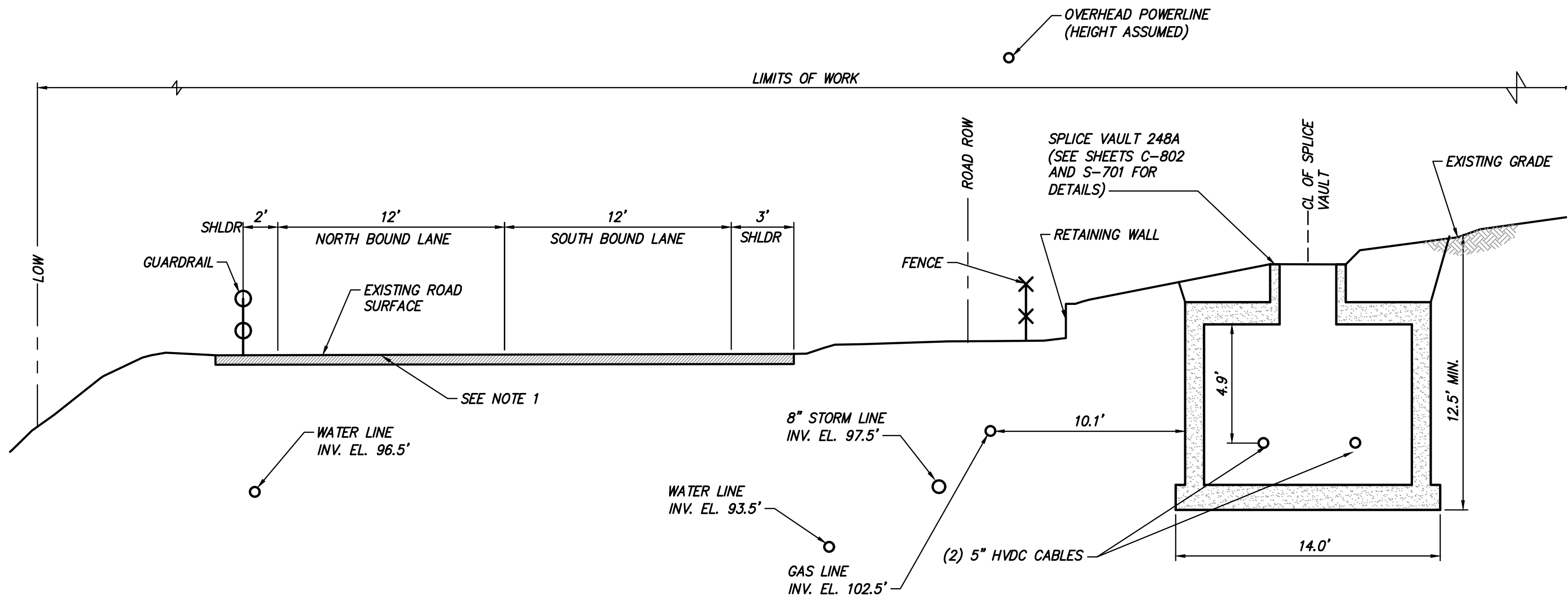
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**11 TRENCH AT STA. 72704+45**  
**TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD**  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**11 TRENCH AT STA. 72710+75**  
**TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD**  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

**11 SPLICE VAULT 248A AT STA. 72712+55**  
SCALE: 1" = 5'



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B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72704+45 TO STA. 72712+55

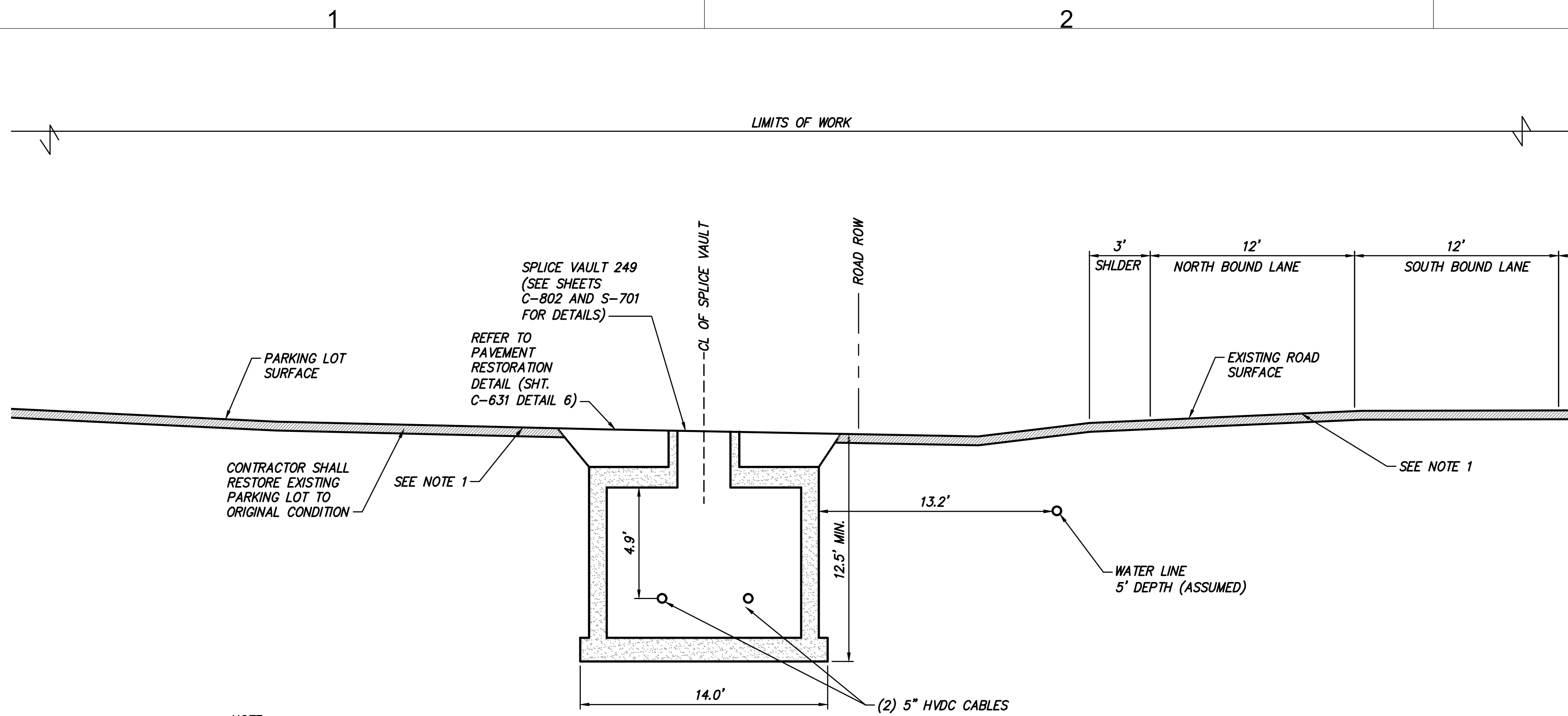
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KIEWIT PROJECT NO.  
21162  
TT PROJECT NO.  
204-3701  
DRAWING NO.

**C-1010**

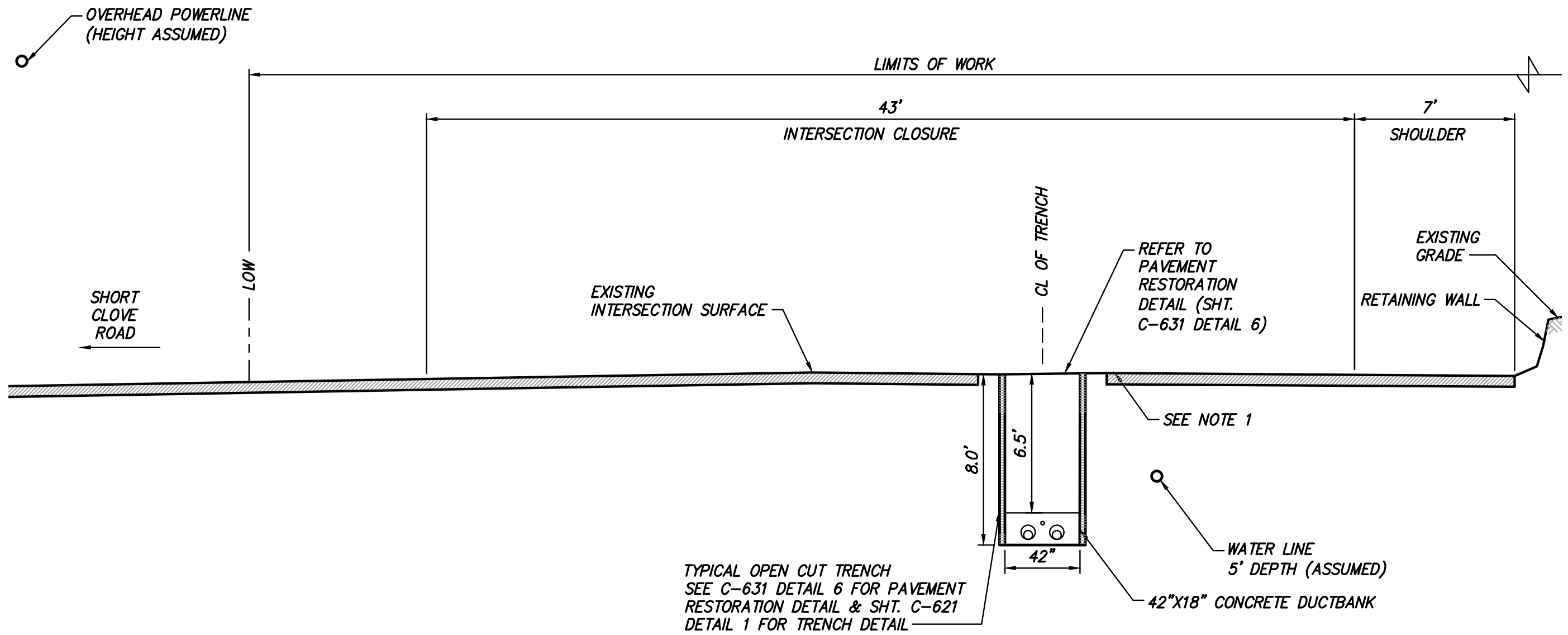


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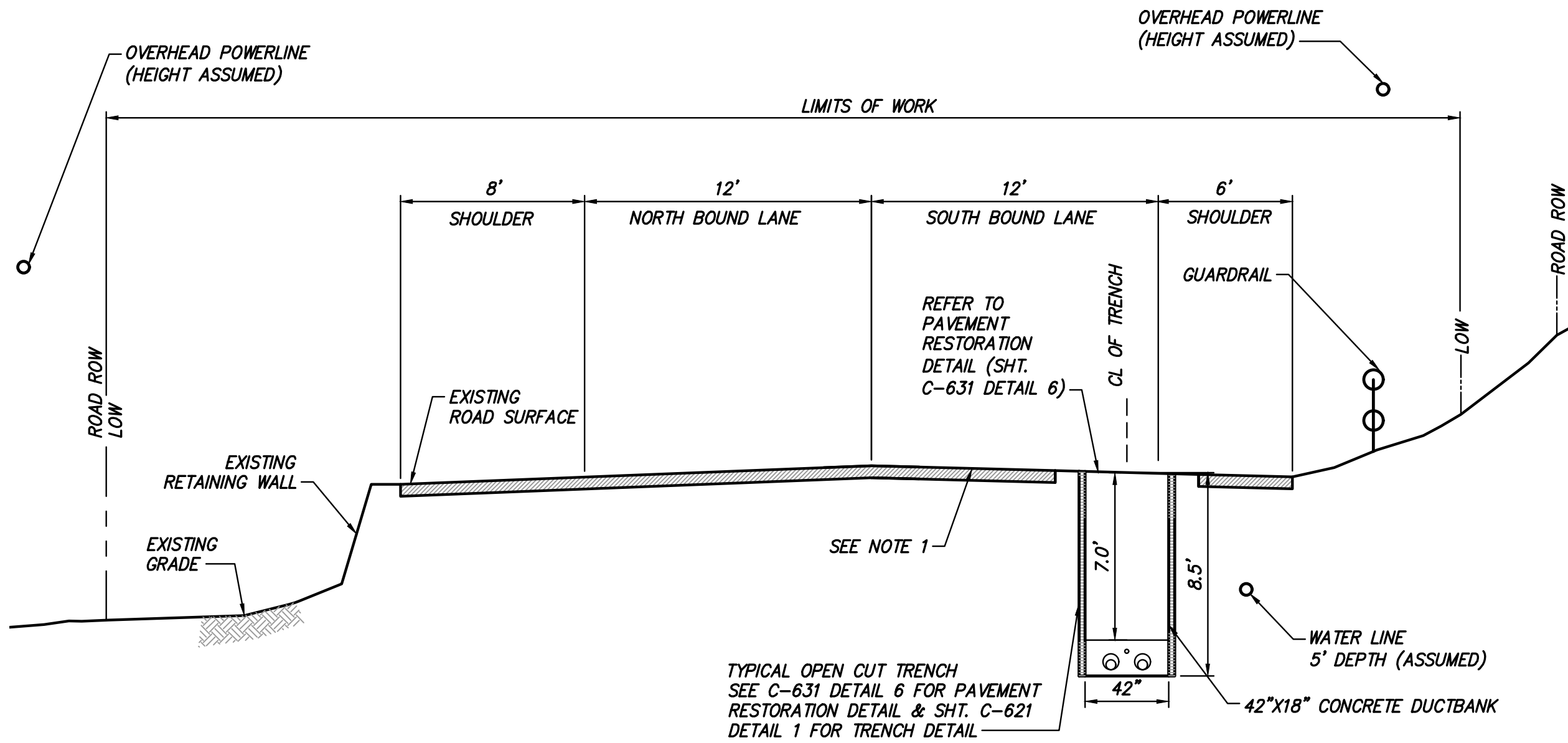
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

11 SPLICE VAULT 249 AT STA. 72730+40  
SCALE: 1" = 5'



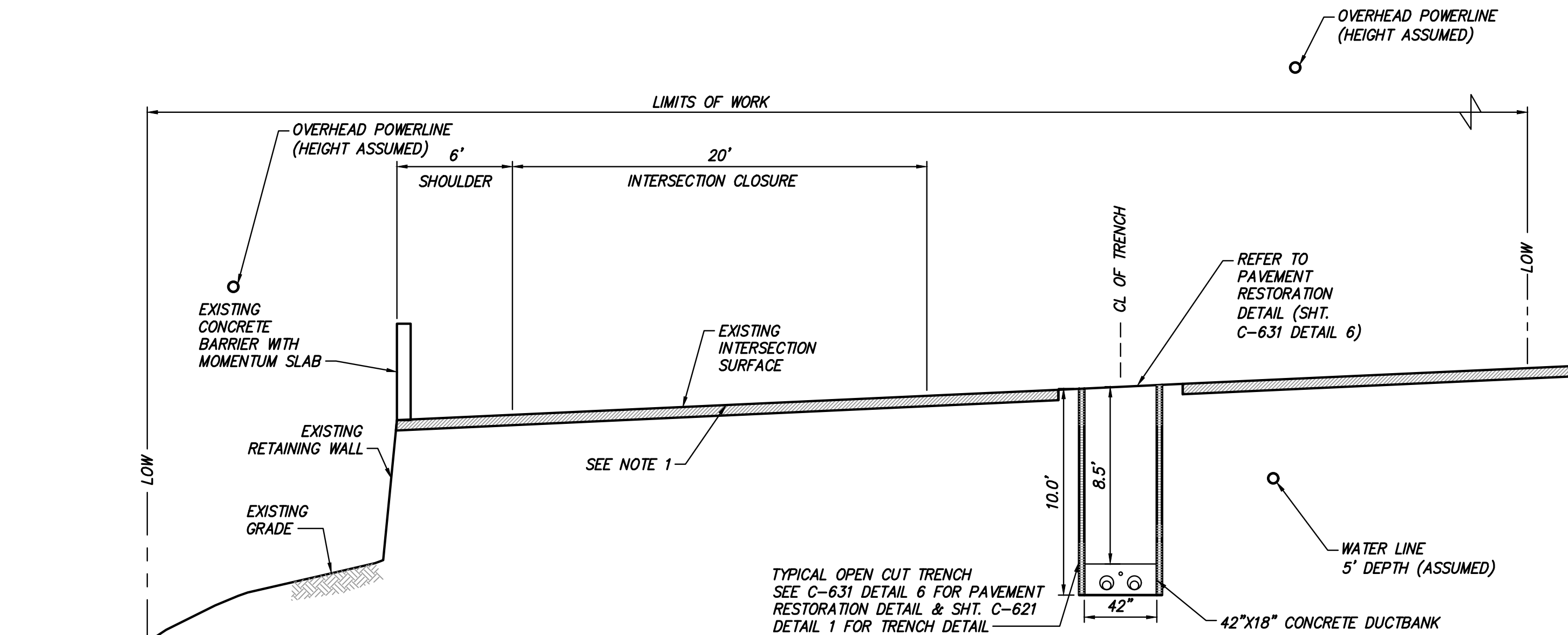
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

TRENCH AT STA. 72741+35  
11 TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

TRENCH AT STA. 72758+20  
11 TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

TRENCH AT STA. 72764+70  
11 TRENCH FROM EXIT PIT OF HDD-132 TO INTERSECTION OF RT 9W & SHORT CLOVE ROAD  
SCALE: 1" = 5'



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A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

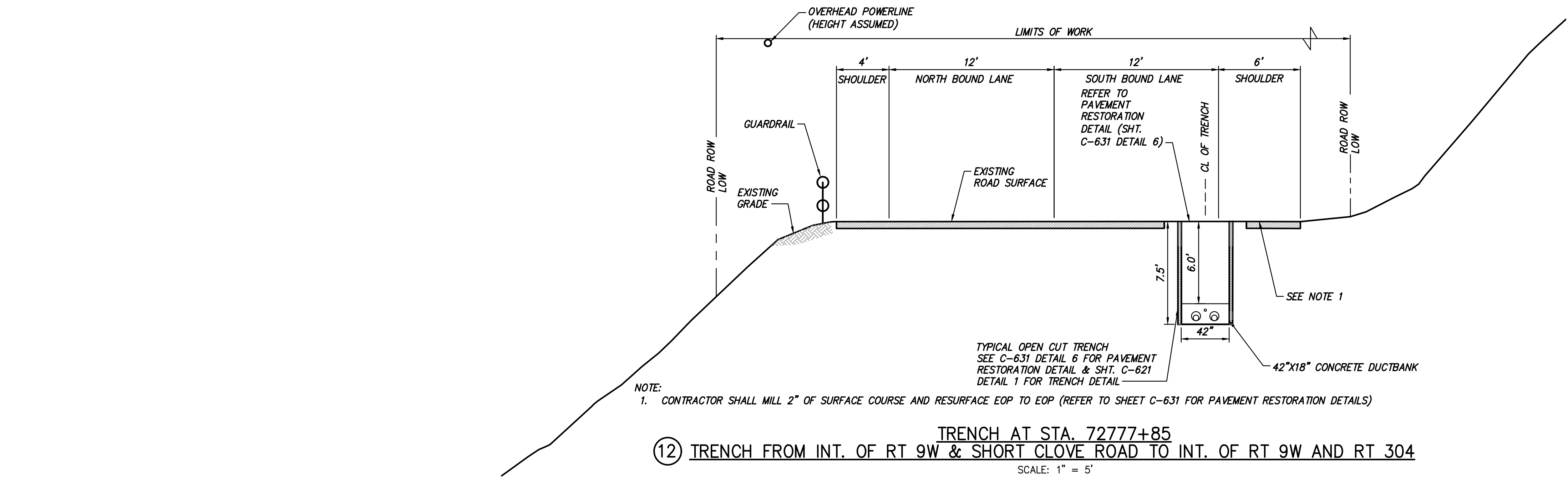
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROAD SECTION EXHIBIT  
STA. 72730+40 TO STA. 72764+70

DRAWN BY: WJ DESIGNED BY: AC APPROVED BY: JL SCALE: AS SHOWN REV. NO. C DATE: 04/14/2023 SH. NO. OF

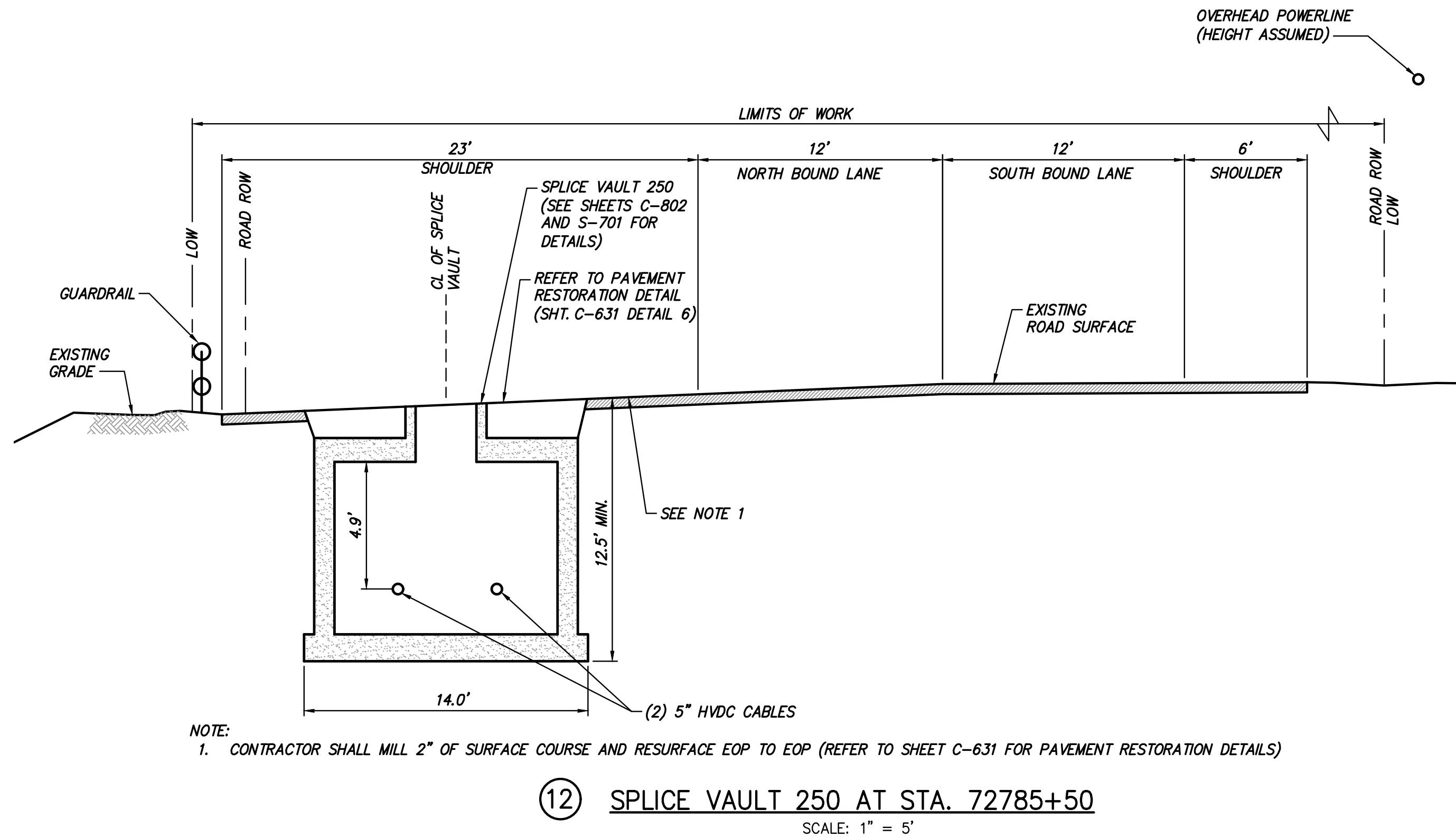
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TT PROJECT NO. 204-3701  
DRAWING NO. C-1011



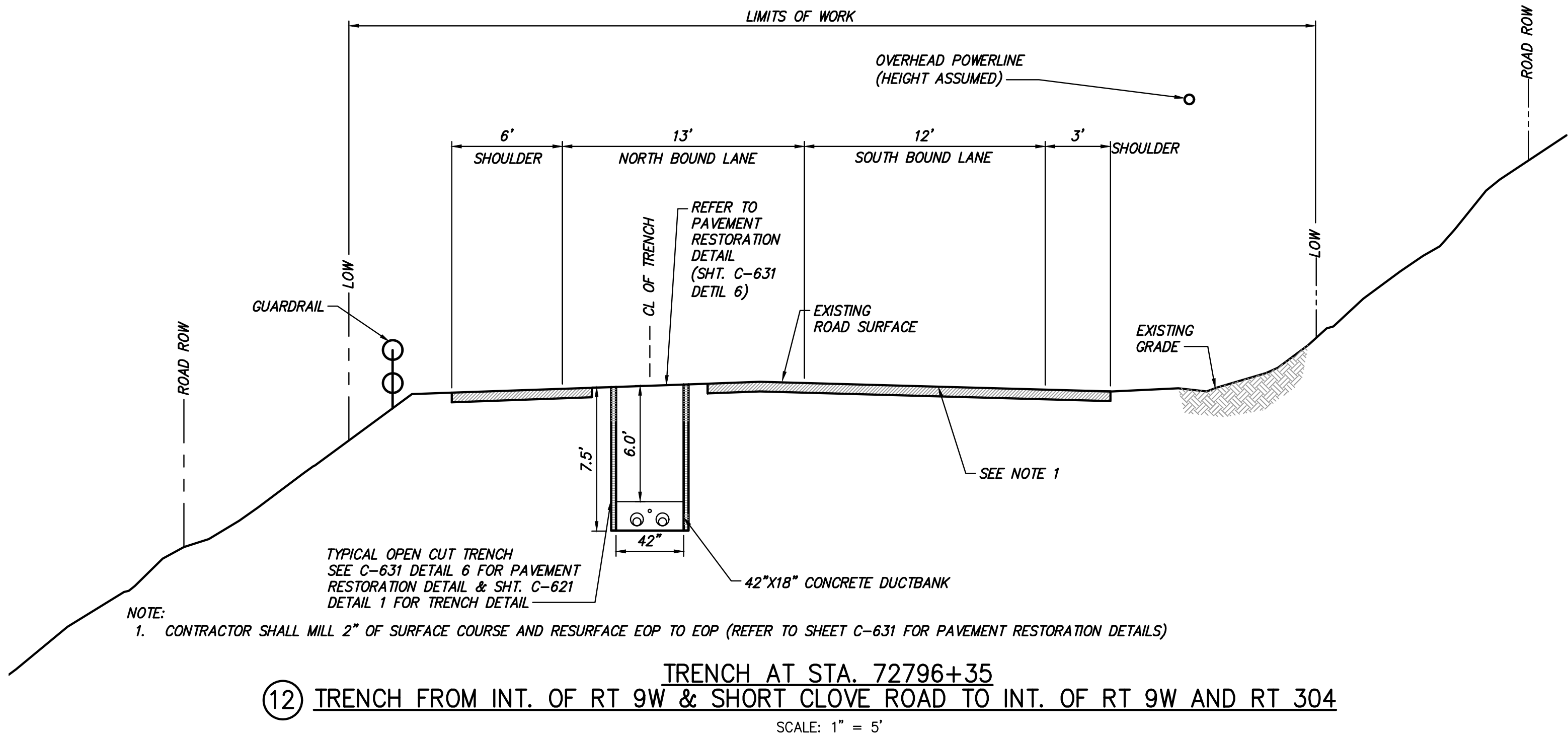
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12 TRENCH AT STA. 72777+85  
TRENCH FROM INT. OF RT 9W & SHORT CLOVE ROAD TO INT. OF RT 9W AND RT 304  
SCALE: 1" = 5'



12 SPLICE VAULT 250 AT STA. 72785+50  
SCALE: 1" = 5'



12 TRENCH AT STA. 72796+35  
TRENCH FROM INT. OF RT 9W & SHORT CLOVE ROAD TO INT. OF RT 9W AND RT 304  
SCALE: 1" = 5'



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B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

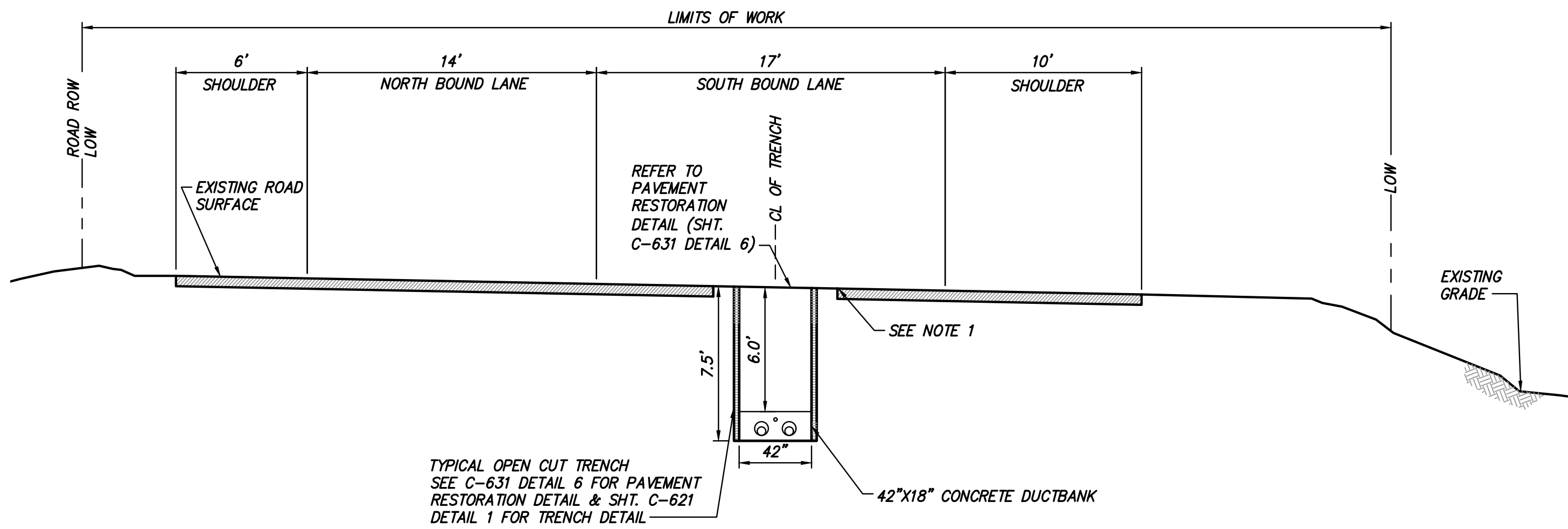
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72766+60 TO STA. 72796+35

DRAWN BY:	WJ	DESIGNED BY:	AC	APPROVED BY:	JL	SCALE:	AS SHOWN	DATE:	04/14/2023
						REV. NO.	C	SH. NO.	OF

KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	C-1012

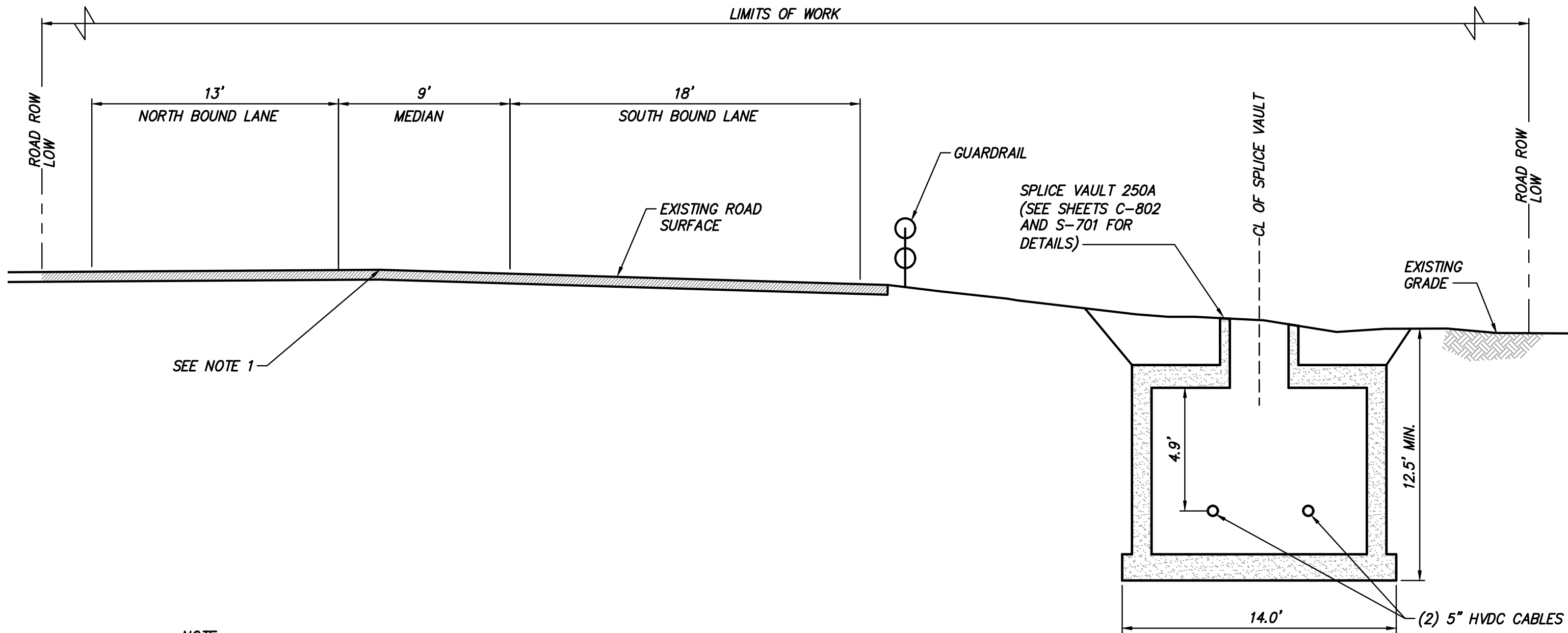


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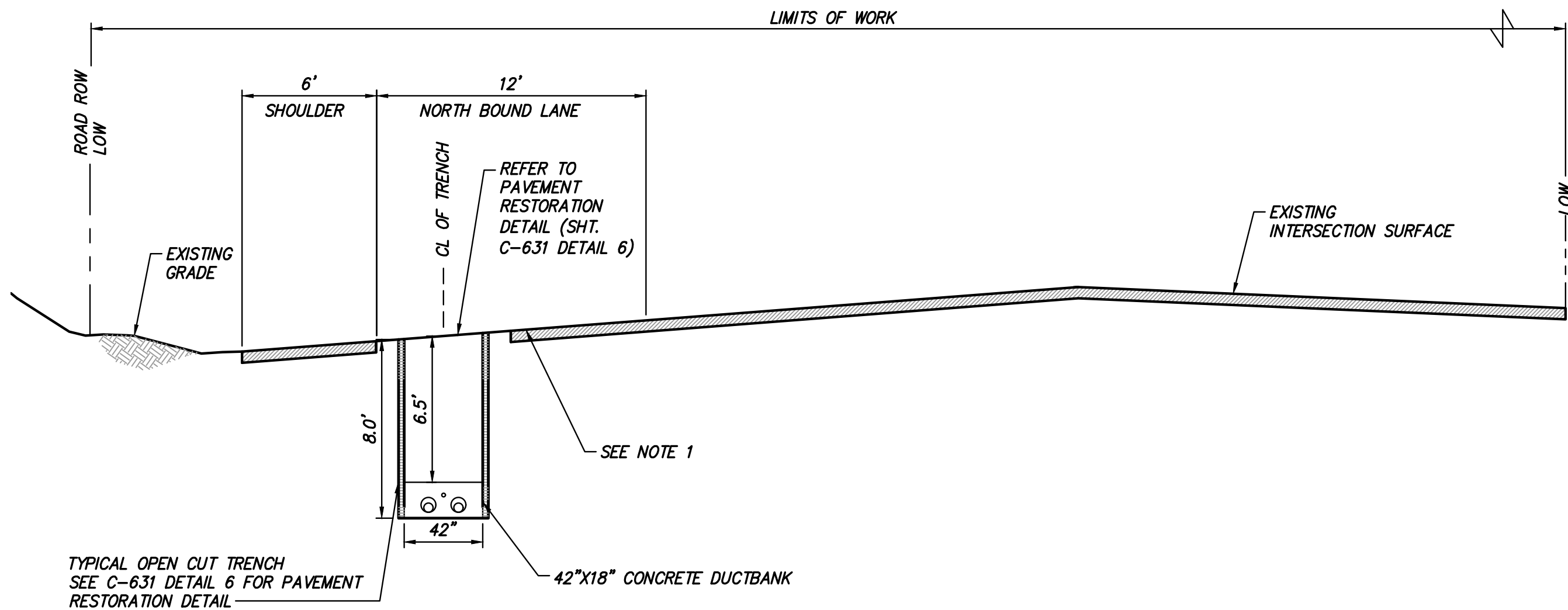
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

12 TRENCH FROM INT. OF RT 9W & SHORT CLOVE ROAD TO INT. OF RT 9W AND RT 304  
SCALE: 1" = 5'



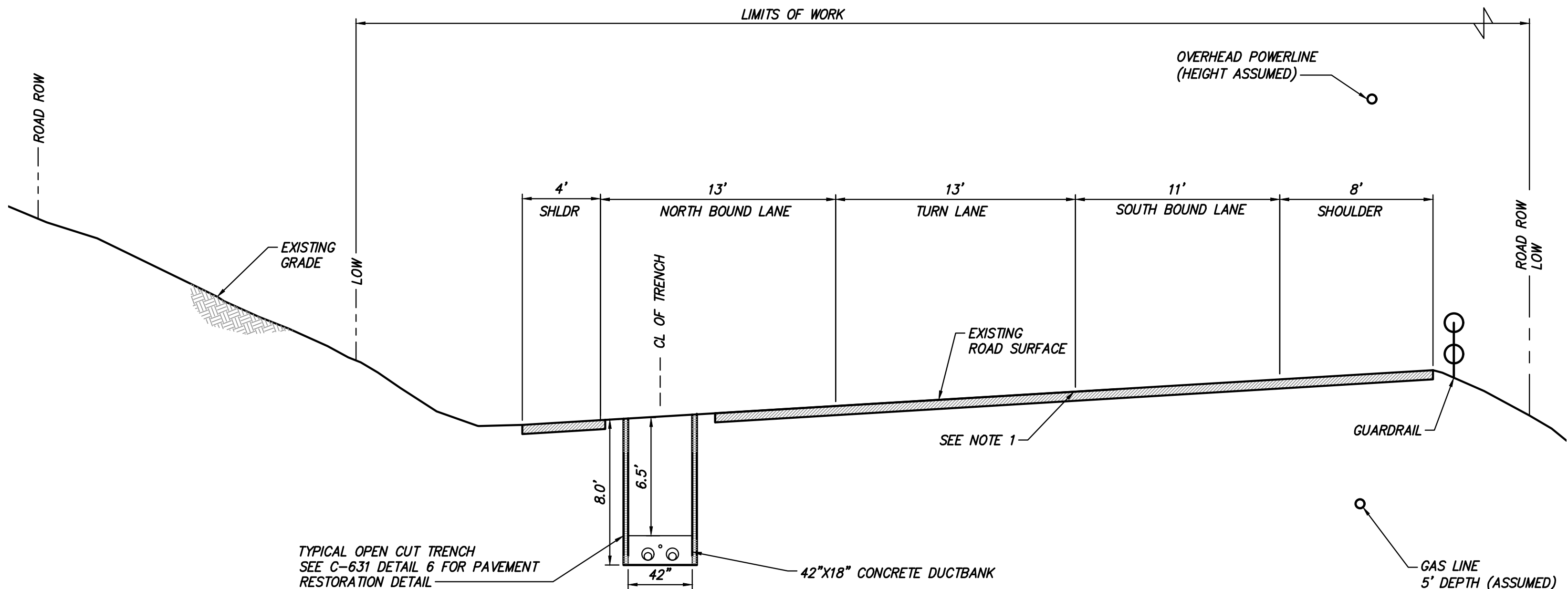
NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

12 SPLICE VAULT 250A AT STA. 72811+10  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

12 TRENCH FROM INT. OF RT 9W & SHORT CLOVE ROAD TO INT. OF RT 9W AND RT 304  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

13 TRENCH FROM INTERSECTION OF RT 9W AND RT 304 TO INTERSECTION WITH RT 303  
SCALE: 1" = 5'



TETRA TECH ENGINEERING AND SURVEYING P.C.  
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ISSUED FOR PERMITTING

C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72809+25 TO STA. 72820+75

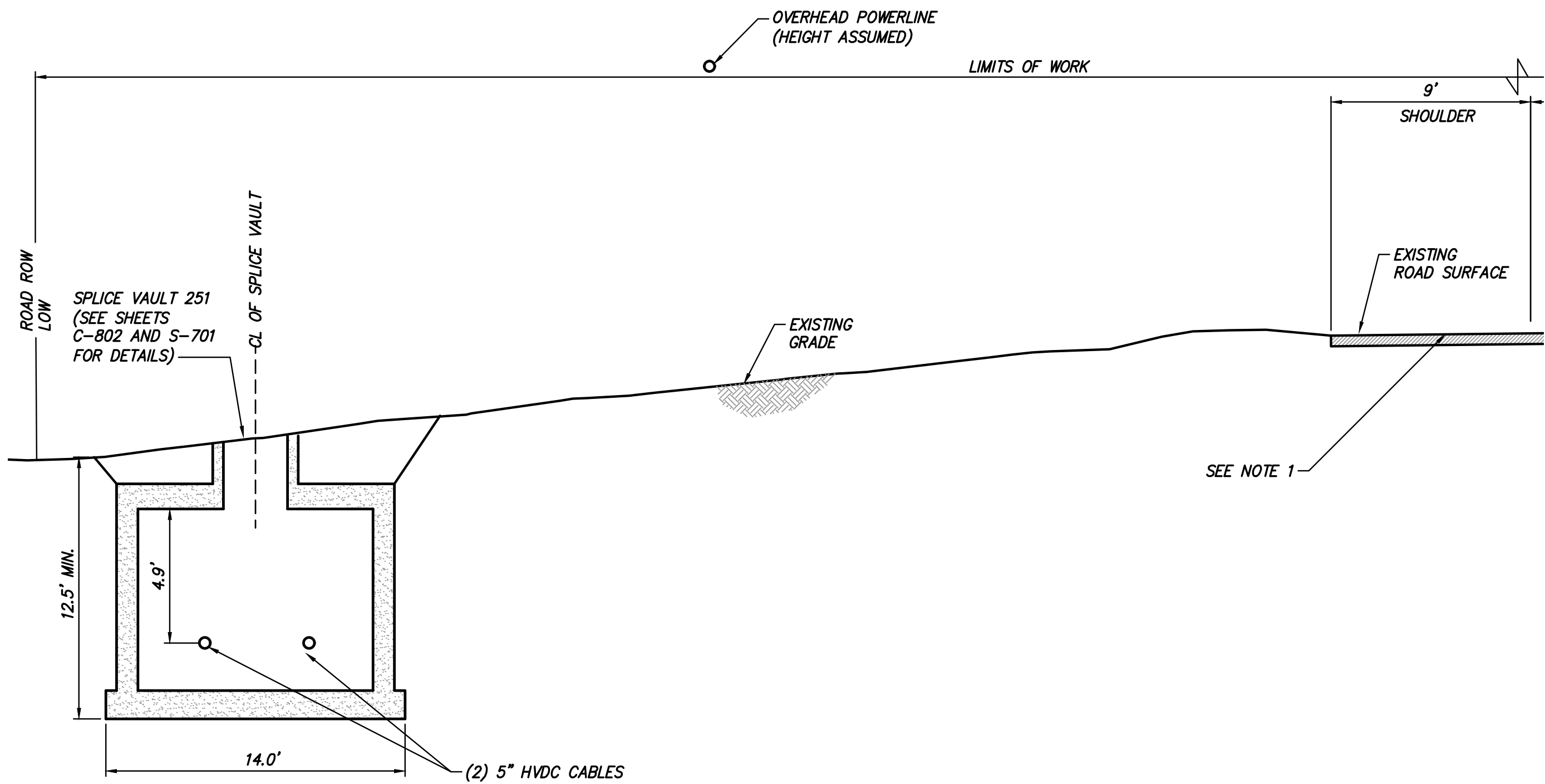
DRAWN BY: WJ DESIGNED BY: AC APPROVED BY: JL AS SHOWN REV. NO. C DATE 04/14/2023 SH. NO. OF

KIEWIT PROJECT NO.  
21162  
TT PROJECT NO.  
204-3701  
DRAWING NO.

C-1013

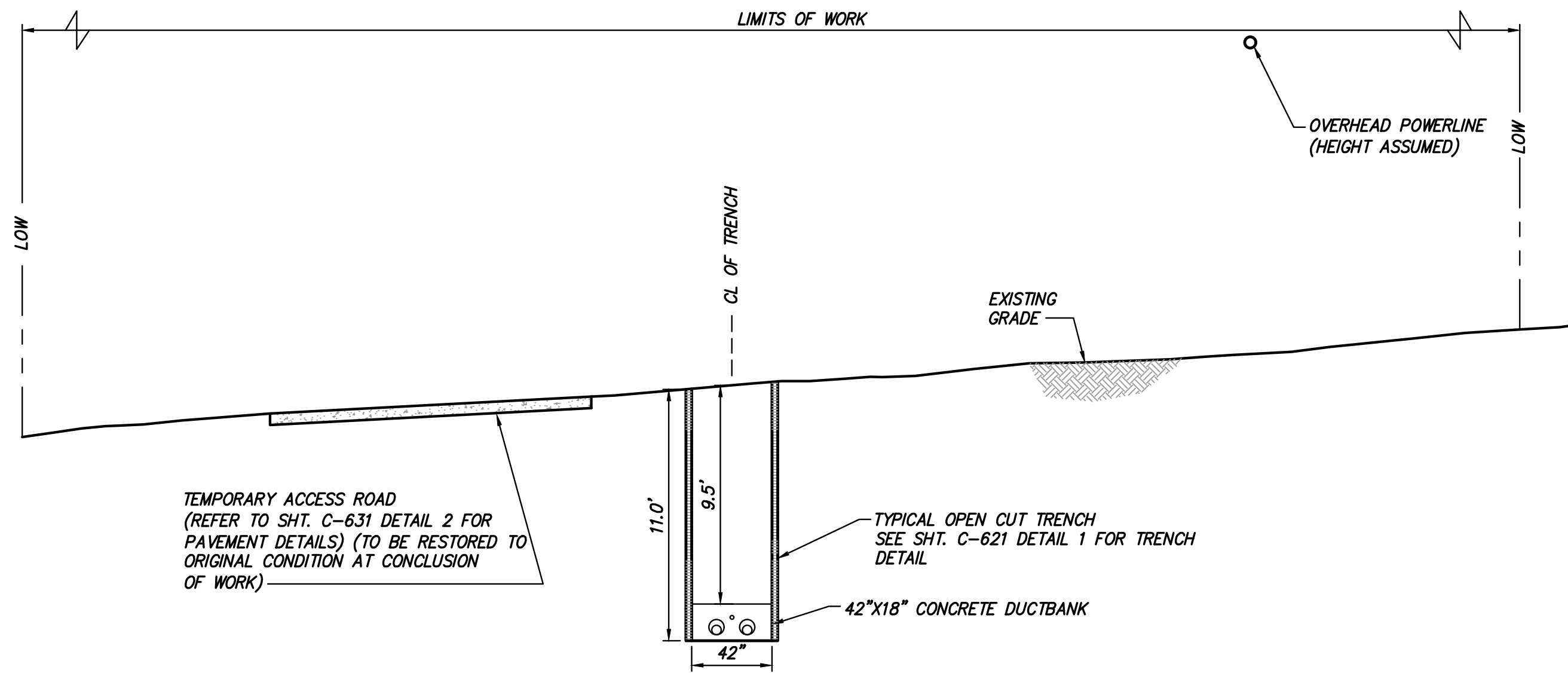


File: C:\PWORKING\REV\DWG\514\21162\_7B\_C\_RD\_SECTIONS C-1010 - C-1015.DWG Saved: 4/11/2023 9:32:30 AM Plotted: 4/11/2023 9:46:57 AM Current User: Jordan, Connor LastSavedBy: connor.jordan

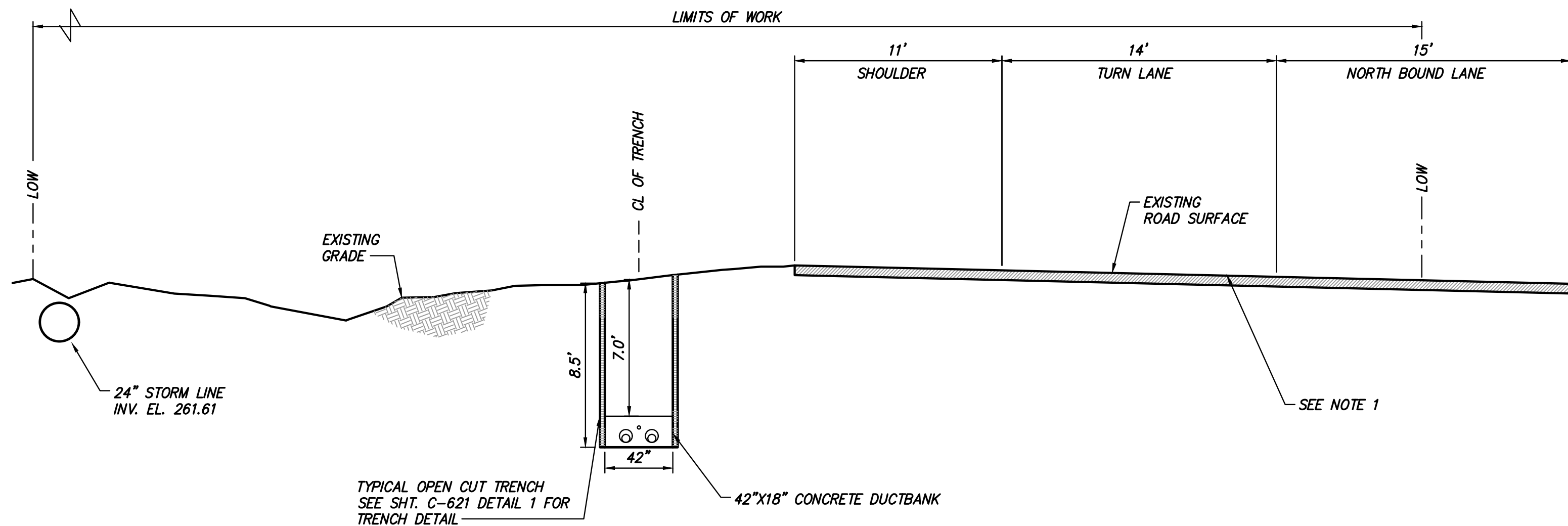


NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

13 SPLICE VAULT 251 AT STA. 72843+65  
SCALE: 1" = 5'

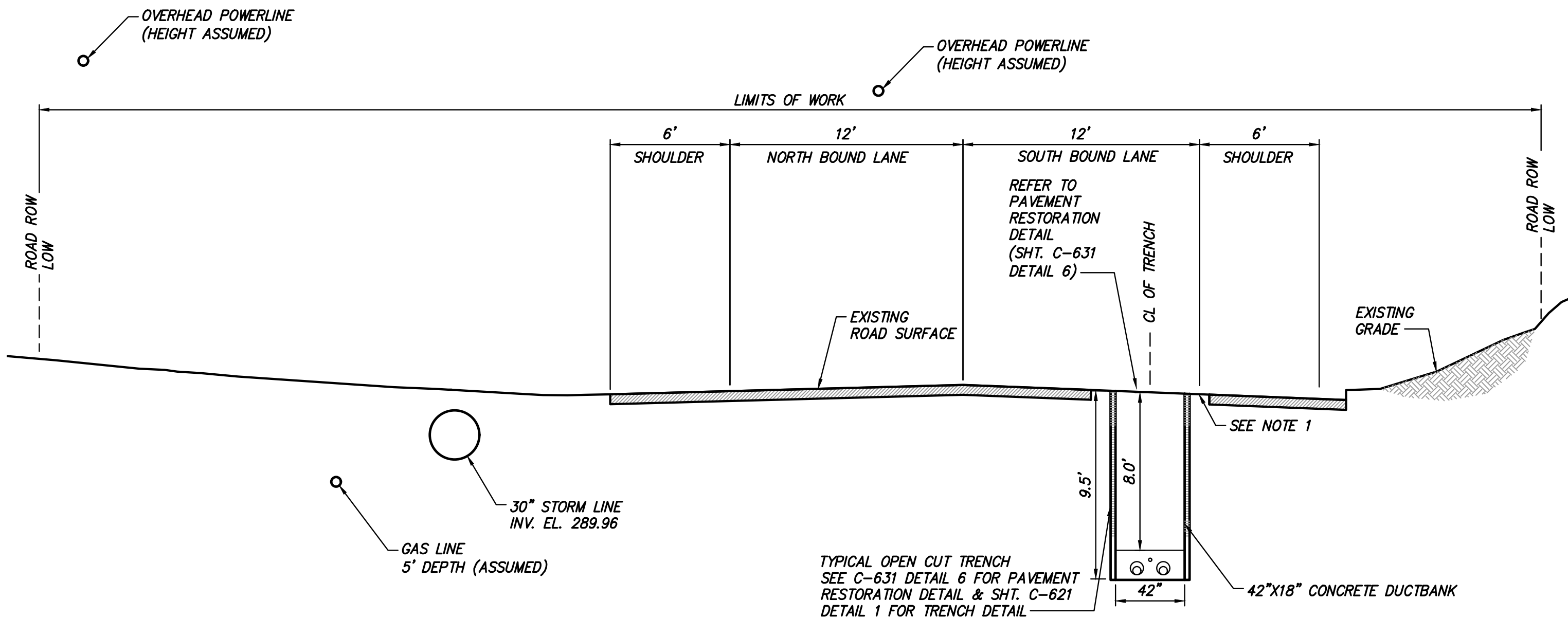


13 TRENCH FROM INTERSECTION OF RT 9W AND RT 304 TO INTERSECTION WITH RT 303  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT DETAILS)

14 TRENCH FROM INTERSECTION OF RT 9W & RT 303 TO TRANSITION VAULT #4  
SCALE: 1" = 5'



NOTE:  
1. CONTRACTOR SHALL MILL 2" OF SURFACE COURSE AND RESURFACE EOP TO EOP (REFER TO SHEET C-631 FOR PAVEMENT RESTORATION DETAILS)

14 TRENCH FROM INTERSECTION OF RT 9W & RT 303 TO TRANSITION VAULT #4  
SCALE: 1" = 5'



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
C	04/14/2023	FINAL SUBMISSION	WJ	JL
B	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MP	JL
A	10/21/2022	DOT PROGRESS SUBMISSION	MP	AR

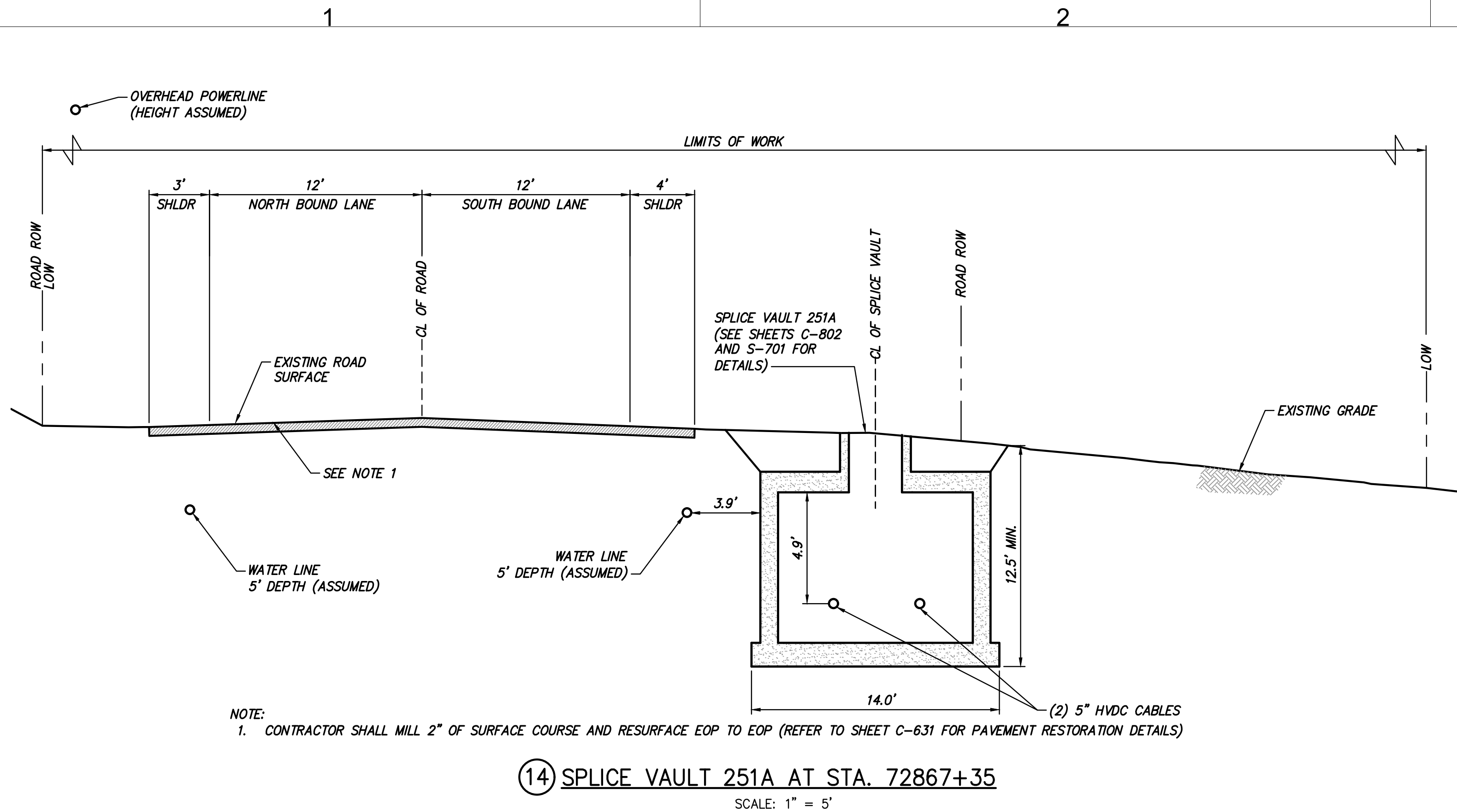
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72843+65 TO STA. 72853+25

DRAWN BY: WJ DESIGNED BY: AC APPROVED BY: JL  
SCALE AS SHOWN REV. NO. C

KIEWIT PROJECT NO.	21162
TT PROJECT NO.	204-3701
DRAWING NO.	C-1014
DATE	04/14/2023
SH.NO.	OF

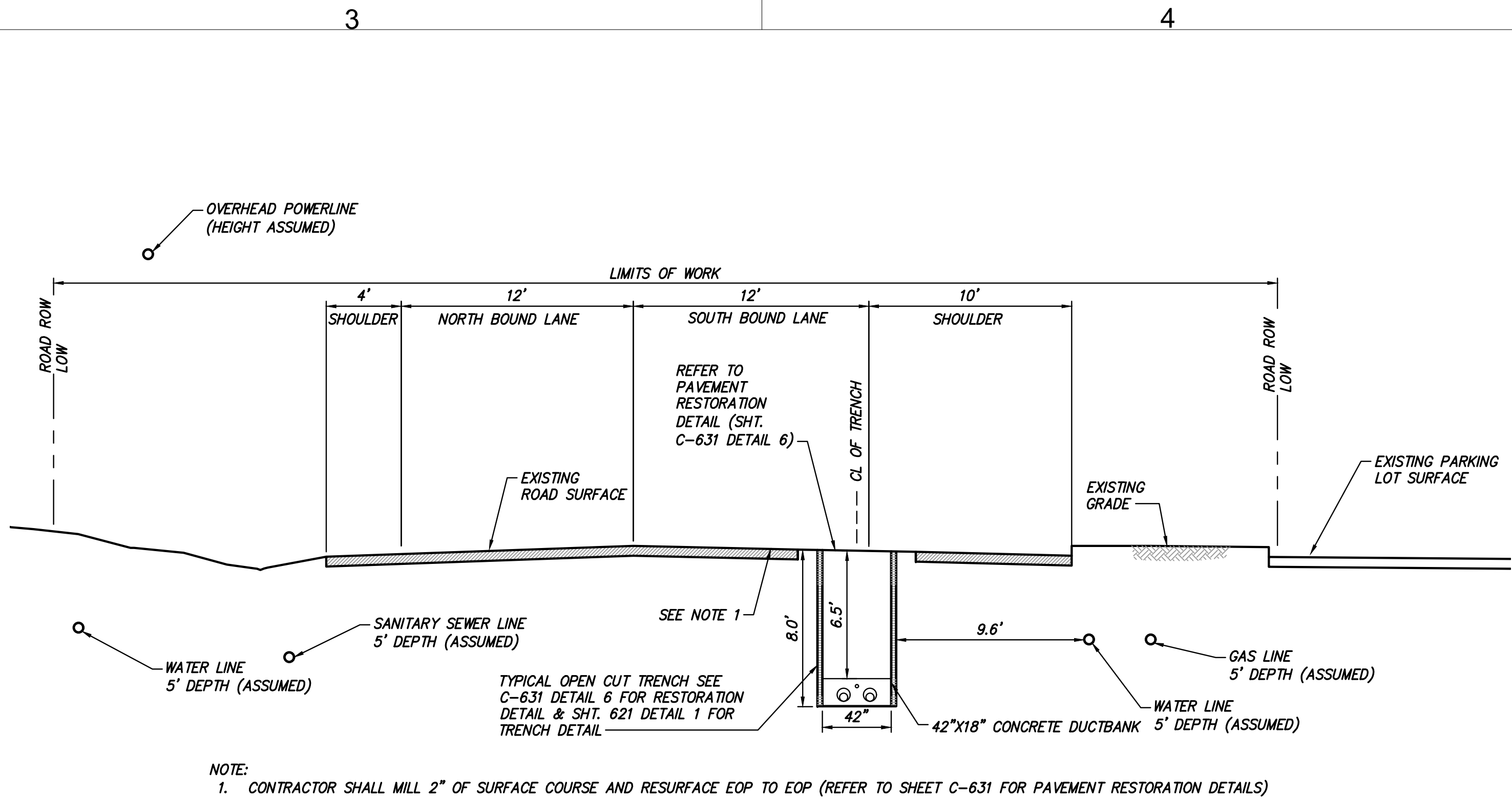


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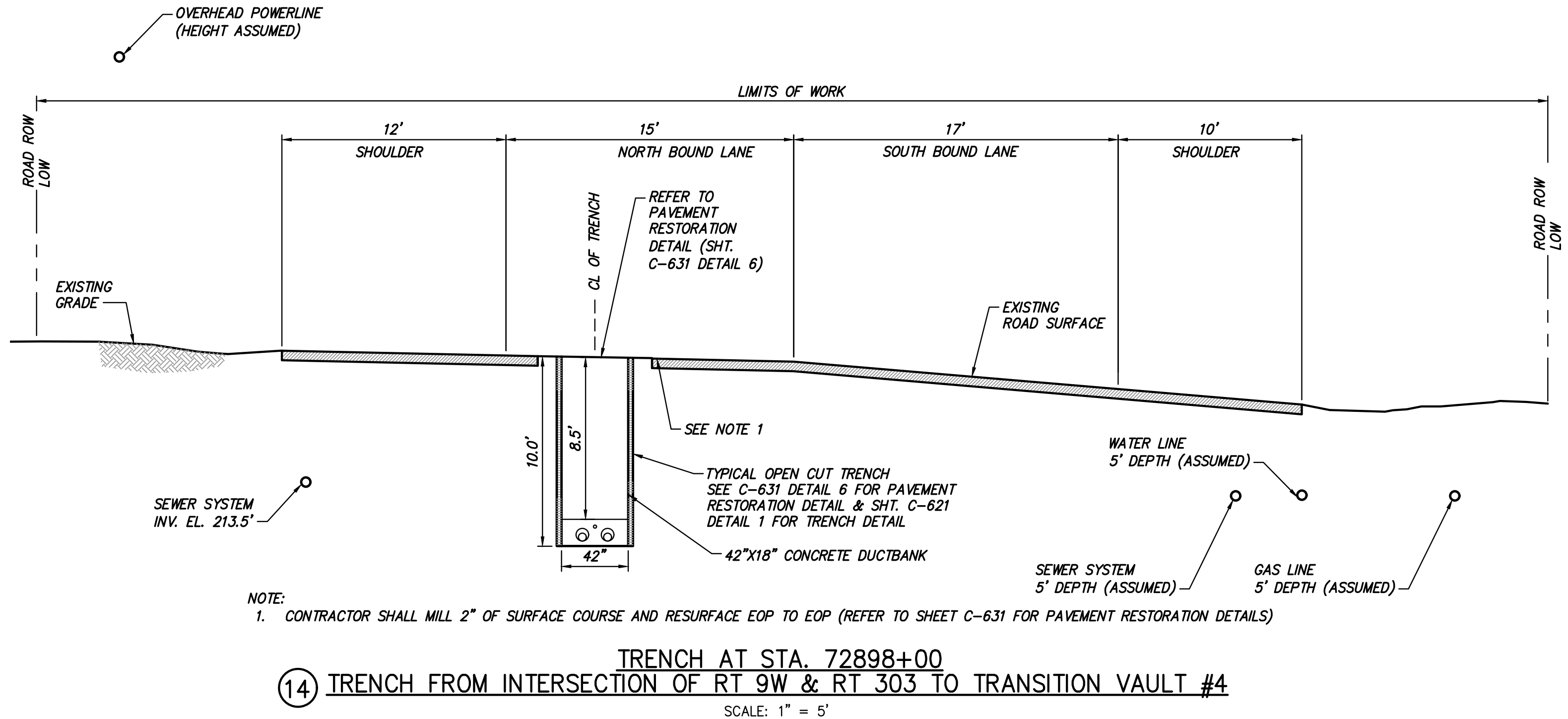
⑭ SPLICE VAULT 251A AT STA. 72867+35

SCALE: 1" = 5'



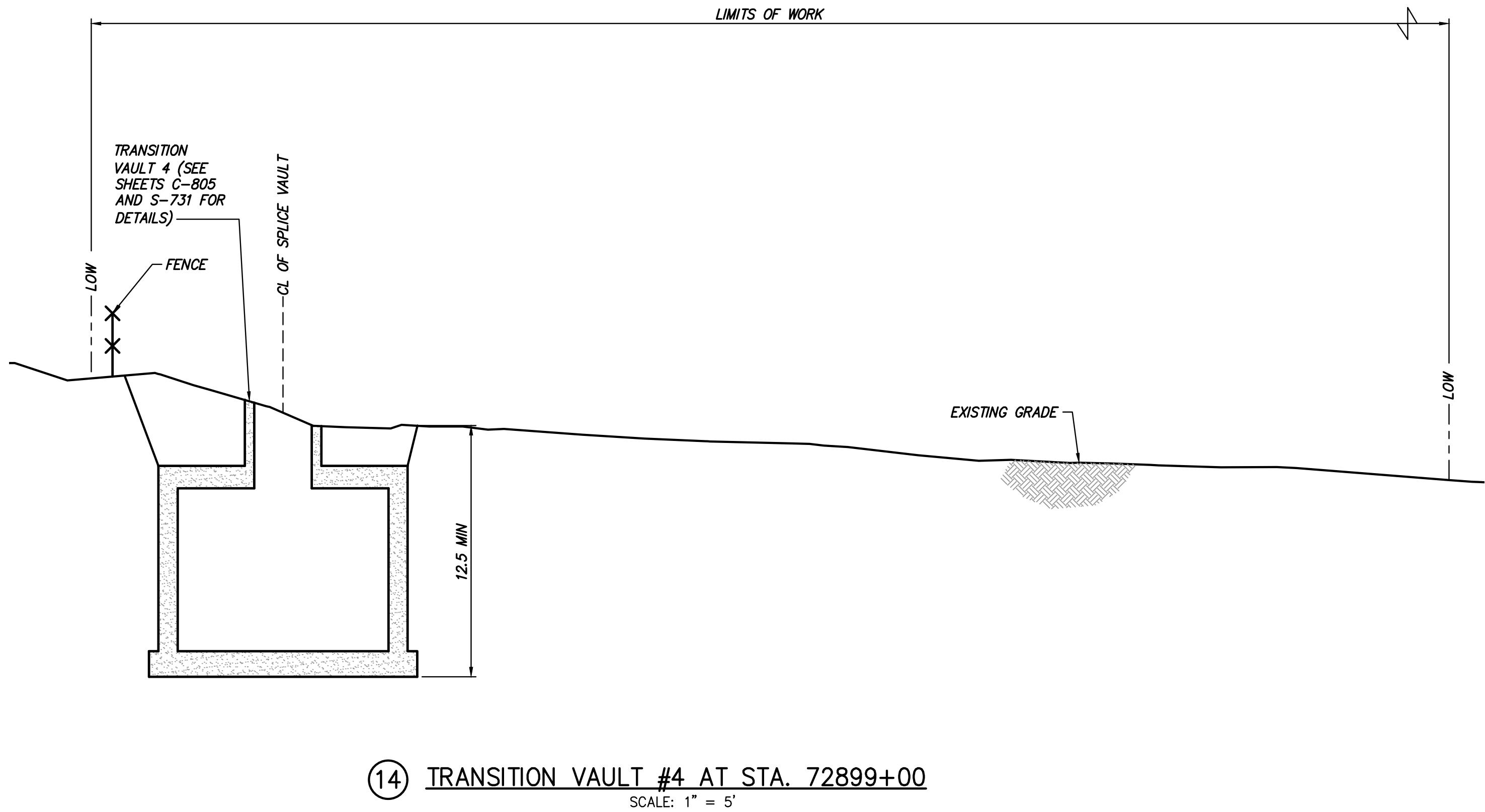
⑭ TRENCH FROM INTERSECTION OF RT 9W & RT 303 TO TRANSITION VAULT #4

SCALE: 1" = 5'



⑭ TRENCH FROM INTERSECTION OF RT 9W & RT 303 TO TRANSITION VAULT #4

SCALE: 1" = 5'



⑭ TRANSITION VAULT #4 AT STA. 72899+00

SCALE: 1" = 5'



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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W:  
ROCKLAND  
ROAD SECTION EXHIBIT  
STA. 72867+35 TO STA. 72899+00

DRAWN BY: WJ DESIGNED BY: AC APPROVED BY: JL  
SCALE: AS SHOWN REV. NO. C SH. NO. OF

KIEWIT PROJECT NO.  
21162  
TT PROJECT NO.  
204-3701  
DRAWING NO.

C-1015

DATE: 04/14/2023  
SH. NO. OF



1. WORK ZONE TRAFFIC CONTROL GENERAL NOTES:

- A. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE CURRENT VERSION OF SECTION 619 OF THE STANDARD SPECIFICATIONS, THE CURRENT WORK ZONE TRAFFIC CONTROL (619 SERIES) STANDARD SHEETS, ANY PROVISIONS CONTAINED IN THE PLANS AND/OR PROPOSAL OF THIS CONTRACT, AND AS DIRECTED BY THE ENGINEER.
- B. PAYMENT FOR WORK ZONE TRAFFIC CONTROL WILL BE MADE IN ACCORDANCE WITH SECTION 109-05B AND 109-05C OF THE STANDARD SPECIFICATIONS. SECTIONS 619-4 AND 619-5 OF THE STANDARD SPECIFICATIONS DO NOT APPLY.
- C. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE WORK ZONE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE A MINIMUM OF SEVEN (7) CALENDAR DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE BASIC CONCEPT OR SCOPE OF THE WORK ZONE TRAFFIC CONTROL PLAN. SUCH CHANGES TO THE BASIC CONCEPT OR SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE A MINIMUM OF FORTY-FIVE (45) CALENDAR DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
- D. ALL MAINTENANCE AND PROTECTION OF TRAFFIC WORK SHALL CONFORM TO THE CONTRACT DRAWINGS. MAINTENANCE AND PROTECTION OF TRAFFIC SCHEMES SHOWN ON THE CONTRACT DRAWINGS SHALL NOT BE CHANGED BY THE CONTRACTOR WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.
- E. THE TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION DEVICE LOCATIONS SHOWN ON THE CONTRACT DRAWINGS ARE SCHEMATIC EXCEPT AS NOTED. LOCATION OF TRAFFIC CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- F. ALL CLOSURES SHALL BE COORDINATED WITH THE ENGINEER. A SCHEDULE OF WORK SHALL BE SUBMITTED AT LEAST TWO WEEKS IN ADVANCE FOR APPROVAL BY THE ENGINEER.
- G. THERE MAY BE ONGOING CONSTRUCTION CONTRACTS WITHIN THE VICINITY OF THE WORK AREA. DO NOT MOVE, MODIFY, OR RELOCATE ANY ITEM ASSOCIATED WITH THESE CONTRACTS WITHOUT PROPER APPROVAL OF AND COORDINATION WITH THE ENGINEER.
- H. PERFORM WORK IN SUCH A MANNER AND SEQUENCE AS TO INTERFERE AS LITTLE AS POSSIBLE WITH THE PASSAGE OF VEHICLES, PEDESTRIANS, AND OTHER KINDS OF PUBLIC TRAFFIC.
- I. CONTRACTOR SHALL TAKE CARE TO PREVENT DAMAGE TO EXISTING UTILITIES WITHIN THE LIMIT OF WORK. DAMAGED UTILITIES SHALL BE IMMEDIATELY REPAIRED BY CONTRACTOR AT THE CONTRACTOR'S EXPENSE. EXISTING AND PROPOSED UTILITIES ARE SHOWN FOR INFORMATION ONLY. SEE PNP SHEETS FOR ACCURATE INFORMATION.
- J. THE COST AND RESPONSIBILITY TO PROVIDE AND MAINTAIN SAFE AND ADEQUATE ACCESS FOR INTERSECTING ROADWAYS, HOMES, AND BUSINESSES, AT ALL TIMES, TO THE SATISFACTION OF THE ENGINEER, SHALL BE INCLUDED AS PER NYSDOT STANDARD PAV ITEM 619.01.
- K. ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES. UNLESS OTHER AGREEMENTS SUITABLE TO THE PROPERTY OWNERS CAN BE MADE, PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ANY ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE, OR AS DIRECTED BY THE ENGINEER. CONTRACTOR SHALL PLATE OVER OPEN TRENCHES AT THE END OF EACH SHIFT FOR THE ROADWAY TO BE USED FOR DAYTIME OPERATIONS.
- L. ACCESS FOR EMERGENCY VEHICLES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL NOTIFY ALL AFFECTED EMERGENCY RESPONDERS AND TOWN/VILLAGE/CITY OFFICIAL AT LEAST FOURTEEN (14) DAYS IN ADVANCE OF ANY LANE/ROAD CLOSING.
- M. CONTRACTOR SHALL COORDINATE WITH LOCAL SCHOOLS AND FIRE DEPARTMENTS TO MAKE THEM AWARE OF ANY UPCOMING WORK, CLOSURES, AND DETOUR ROUTES.
- N. ALL TRAFFIC CONTROL AND WORK AREA PROTECTIONS DEVICES WITH NYSDOT STANDARDS SHALL BE FABRICATED AND INSTALLED IN ACCORDANCE WITH NYSDOT STANDARDS.
- O. NYSDOT STANDARD SHEET REFERENCES  
619-001, 619-002, 619-004, 619-010, 619-011, 619-012, 619-080, 619-090, 619-091, 619-201, 619-307, 619-308, 619-310, 619-323, 619-324, 619-407, 619-410, 619-414, 619-421, 619-422, 619-501, 619-502, 619-503, 619-504, 619-519, 619-520, 619-524
- P. CONTRACTOR SHALL SAFELY MANEUVER CONSTRUCTION EQUIPMENTS AND VEHICLES IN AND AROUND THE WORK AREA AS APPLICABLE PER NYSDOT STANDARD SHEET 619-10.
- Q. ALL TEMPORARY TRAFFIC CONTROL AND WORK AREA PROTECTION DEVICES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED ON THE CONTRACT DRAWINGS AND APPROVED BY THE ENGINEER PRIOR TO COMMENCING WORK
- R. CONSTRUCTION SPEED LIMIT SHALL BE DETERMINED USING EXHIBIT 16-9 IN CHAPTER 16 OF THE NYSDOT HIGHWAY DESIGN MANUAL.
- S. CONTRACTOR MAY PERFORM OPEN CUT TRENCHING DURING LANE CLOSURES OF HDD WORK. CONTRACTOR SHALL LIMIT WORK TO NIGHT TIME OPERATIONS WHEN IN THE VICINITY OF BUSINESS AREAS, AND DAY TIME WORK WHEN IN THE VICINITY OF RESIDENTIAL AREAS.
- T. CONTRACTOR SHALL COORDINATE WITH BUS COMPANIES TO MAINTAIN EXISTING BUS STOPS.
- U. ON DETOUR ROUTES CONTRACTOR SHALL TAKE PHOTOGRAPHS TO DOCUMENT THE EXISTING CONDITION OF THE ROADS. IF AFTER COMPLETION OF THE PROJECT, THE DETOUR ROADS ARE DEEMED TO HAVE SIGNIFICANT DETERIORATION, BY THE ENGINEER, CONTRACTOR SHALL MILL AND OVERLAY THE AFFECTED ROADWAYS.

2. WORK ZONE REQUIREMENTS

- A. TRAFFIC LANES AND OTHER AREAS CLOSED BY THE CONTRACTOR DURING PERMITTED WORK HOURS SHALL BE CLEARED OF ALL MATERIAL, EQUIPMENT, AND DEBRIS, AND SAFELY REOPENED TO TRAFFIC BY THE END OF THE WORK PERIOD UNLESS OTHERWISE INDICATED ON THE CONTRACT DRAWINGS.
- B. CONTRACTOR SHALL PLATE OVER OPEN TRENCHES AT THE END OF EACH SHIFT FOR THE ROADWAY TO BE USED DURING NON-CONSTRUCTION HOURS.
- C. IF THE CONTRACTOR REQUIRES A LANE CLOSURE TO PERFORM OPERATIONS, THEY MAY DO SO WITH THE APPROVAL OF THE ENGINEER. ANY LANE CLOSURES WILL BE IN ACCORDANCE WITH NYS STANDARD SHEETS UNLESS OTHERWISE SHOWN IN CONTRACT PLANS.
- D. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.
- E. EXISTING STRIPING THAT CONFLICTS WITH PROPOSED WORKZONE TRAFFIC FLOW SHALL BE REMOVED AND TEMPORARY STRIPING SHALL BE PUT IN PLACE IN ORDER TO FACILITATE WORK ZONE TRAFFIC FLOW.

3. TIME / DATE RESTRICTIONS

- A. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING HOLIDAY WORK RESTRICTIONS ARE APPLICABLE TO THIS PROJECT. DURING THESE HOLIDAY PERIODS, THE CONTRACTOR WILL NOT BE ALLOWED TO PERFORM ANY WORK THAT WILL BE DISRUPTIVE TO TRAFFIC, INCLUDING BUT NOT LIMITED TO LANE CLOSURE AND DETOURS. LANE CLOSURES AND DETOURS WILL NOT BE PERMITTED DURING THE FOLLOWING STATE RECOGNIZED HOLIDAYS:
- 1) NEW YEAR'S DAY

2) MEMORIAL DAY

3) INDEPENDENCE DAY

4) LABOR DAY

5) THANKSGIVING DAY

6) CHRISTMAS DAY

– SUNDAY, JANUARY 1, 2023

– MONDAY, MAY 29, 2023

– TUESDAY, JULY 4, 2023

– MONDAY, SEPTEMBER 4, 2023

– THURSDAY, NOVEMBER 23, 2023

– MONDAY, DECEMBER 25, 2023
- 7) NEW YEAR'S DAY

8) MEMORIAL DAY

9) INDEPENDENCE DAY

10) LABOR DAY

11) THANKSGIVING DAY

12) CHRISTMAS DAY

– MONDAY, JANUARY 1, 2024

– MONDAY, MAY 27, 2024

– THURSDAY, JULY 4, 2024

– MONDAY, SEPTEMBER 2, 2024

– THURSDAY, NOVEMBER 28, 2024

– WEDNESDAY, DECEMBER 25, 2024
- 13) NEW YEAR'S DAY

14) MEMORIAL DAY

15) INDEPENDENCE DAY

16) LABOR DAY

17) THANKSGIVING DAY

18) CHRISTMAS DAY

– WEDNESDAY, JANUARY 1, 2025

– MONDAY, MAY 26, 2025

– FRIDAY, JULY 4, 2025

– MONDAY, SEPTEMBER 1, 2025

– THURSDAY, NOVEMBER 27, 2025

– THURSDAY, DECEMBER 25, 2025

– BEGINNING 6AM, FRIDAY DECEMBER 30, 2022 AND ENDING 10AM, TUESDAY JANUARY 3, 2023

– BEGINNING 6AM, FRIDAY MAY 26, 2023 AND ENDING 10AM, TUESDAY MAY 30, 2023

– BEGINNING 6AM, FRIDAY JUNE 10, 2023 AND ENDING 10AM, WEDNESDAY JULY 5, 2023

– BEGINNING 6AM, FRIDAY SEPTEMBER 1, 2023 AND ENDING 10AM, TUESDAY SEPTEMBER 5, 2023

– BEGINNING 6AM, WEDNESDAY NOVEMBER 22, 2023 AND ENDING 10AM, MONDAY NOVEMBER 27, 2023

– BEGINNING 6AM, FRIDAY DECEMBER 22, 2023 AND ENDING 10AM, TUESDAY DECEMBER 26, 2023

– BEGINNING 6AM, FRIDAY DECEMBER 29, 2023 AND ENDING 10AM, TUESDAY JANUARY 2, 2024

– BEGINNING 6AM, FRIDAY MAY 24, 2024 AND ENDING 10AM, TUESDAY MAY 28, 2024

– BEGINNING 6AM, THURSDAY JULY 4, 2024 AND ENDING 10AM, MONDAY JULY 8, 2024

– BEGINNING 6AM, FRIDAY AUGUST 30, 2024 AND ENDING 10AM, TUESDAY SEPTEMBER 3, 2024

– BEGINNING 6AM, WEDNESDAY NOVEMBER 27, 2024 AND ENDING 10AM, MONDAY DECEMBER 2, 2024

– BEGINNING 6AM, SATURDAY DECEMBER 22, 2024 AND ENDING 10AM, THURSDAY DECEMBER 26, 2024

– BEGINNING 6AM, SUNDAY DECEMBER 29, 2024 AND ENDING 10AM, THURSDAY JANUARY 2, 2025

– BEGINNING 6AM, FRIDAY MAY 23, 2025 AND ENDING 10AM, TUESDAY MAY 27, 2025

– BEGINNING 6AM, FRIDAY JULY 4, 2025 AND ENDING 10AM, TUESDAY JULY 8, 2025

– BEGINNING 6AM, FRIDAY AUGUST 29, 2025 AND ENDING 10AM, TUESDAY SEPTEMBER 2, 2025

– BEGINNING 6AM, WEDNESDAY NOVEMBER 26, 2025 AND ENDING 10AM, MONDAY DECEMBER 1, 2025

– BEGINNING 6AM, MONDAY DECEMBER 22, 2025 AND ENDING 10AM, FRIDAY DECEMBER 26, 2025

4. CONSTRUCTION VEHICLES, EQUIPMENT AND MATERIALS

- A. IF CONDITION WARRANTS, PROTECTIVE VEHICLE WITH APPROPRIATE ROLL AHEAD DISTANCE MAY BE USED IN ADVANCE OF THE WORK AREA. TO USE PROTECTIVE VEHICLE, BUFFER SPACE SHALL BE PROVIDED ACCORDINGLY, AND THE WHEELS SHALL BE ALIGNED WITH THE LANE STRIPING.
- B. ALL VEHICLES AND EQUIPMENT WITHIN THE CONTRACT LIMITS AND ON TRAVEL LANES AND/OR SHOULDERS SHALL BE EQUIPPED WITH AND SHALL OPERATE A MINIMUM OF ONE AMBER ROTATING OR FLASHING LIGHT EMITTING DIODE (LED) BEACON VISIBLE FROM ALL DIRECTIONS FOR A MINIMUM OF 1000 FEET DURING DAYLIGHT. IF VISIBILITY OF A SINGLE BEACON IS BLOCKED BY A PORTION OF THE VEHICLE OR EQUIPMENT, ADDITIONAL BEACONS SHALL BE PROVIDED. BEACONS SHALL BE MOUNTED IN A MANNER WHICH DOES NOT CAUSE GLARE FOR DRIVERS USING THE ROADWAY OR THE OPERATOR OF THE VEHICLE OR EQUIPMENT.
- C. ALL BARRIER/SHADOW VEHICLES SHALL WEIGH A MINIMUM OF 24,000 LB AND SHALL BE EQUIPPED WITH TRUCK-MOUNTED IMPACT ATTENUATORS AND TRUCK-MOUNTED ARROW PANELS. IMPACT ATTENUATORS SHALL MEET THE REQUIREMENTS OF NCHRP 350 TEST LEVEL 3 AND SECTION 729-12 OF THE STANDARD SPECIFICATIONS. ARROW PANELS SHALL BE TRUCK-MOUNTED SERIES B OR C PANELS MEETING THE REQUIREMENTS OF SECTION 729-15 OF THE STANDARD SPECIFICATIONS AND SECTION 6F.61 OF THE MUTCD. THE PRICE OF TRUCK-MOUNTED IMPACT ATTENUATORS AND TRUCK-MOUNTED ARROW PANELS SHALL BE INCLUDED IN THE PRICE BID FOR THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.

5. CHANNELIZING DEVICES

- A. RECOMMENDED PRACTICES FOR CHANNELIZING DEVICES ARE PROVIDED IN THE MUTCD AND SECTION 619-3.02 J OF NYSDOT STANDARD SPECIFICATIONS.
- B. TALL CONES, MEETING THE REQUIREMENTS OF SECTION 729-02 OF THE STANDARD SPECIFICATIONS, ARE NORMALLY ADEQUATE FOR WORK ZONES SET UP AND REMOVED ON A DAILY BASIS DURING DAYLIGHT HOURS. DRUMS OR OVERSIZED VERTICAL PANELS ARE PREFERRED FOR ALL OTHER WORK ZONES AND AT ANY LOCATIONS WHERE THE RISK OF INTRUSION IS HIGH, AS DETERMINED BY THE ENGINEER.
- C. THE CONTRACTOR SHALL MAKE FREQUENT CHECKS COMMENSURATE WITH TRAFFIC CONDITIONS TO IDENTIFY AND RESET CHANNELIZING DEVICES DISLOOGED BY TRAFFIC.
- D. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE, UNLESS OTHERWISE NOTED, FOR INTERMEDIATE OR LONG TERM OPERATIONS. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK SPACE, AND 20' IN TAPER SPACE FOR SHORT TERM OR MOBILE OPERATIONS, UNLESS OTHERWISE NOTED.
- E. CHANNELIZING DEVICES SHALL BE PLACED TRANSVERSELY A MINIMUM OF EVERY 800' AS SHOWN WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 800'.
- F. DURING NON-WORK HOURS, LEAVE CHANNELIZING DEVICES AND TEMPORARY CONCRETE BARRIER (TCB) IN PLACE, OR AS DIRECTED AT THE ENGINEER.

6. FLAGGING AND TRAFFIC CONTROL

- A. TO INSURE A PROPER LEVEL OF TRAFFIC SAFETY, EACH FLAGGER SHALL BE EQUIPPED WITH A TWO-WAY RADIO DEVICE. THE COST SHALL BE INCLUDED IN THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.
- B. ALL FLAGGER STATIONS SHALL BE ENHANCED WITH ADDITIONAL CONES AND A FLAG TREE AS SHOWN ON THE WORK ZONE TRAFFIC CONTROL (619 SERIES) STANDARD SHEETS AND AS DIRECTED BY THE ENGINEER. THE FLAG TREE SHALL MEET THE REQUIREMENTS OF SECTION 6F.62 OF THE MUTCD EXCEPT THAT A MINIMUM OF THREE (3) FLAGS ARE REQUIRED. THIS SETUP SHALL BE USED FOR ALL FLAGGER STATIONS EXCEPT THOSE THAT ARE CONSTANTLY MOVING. ALL COSTS ASSOCIATED WITH THESE REQUIREMENTS SHALL BE INCLUDED UNDER THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.
- C. FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
- D. ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF. THE PADDLE IS THE PREFERRED DEVICE, BUT THE FLAG MAY BE USED AT INTERSECTIONS WHERE THE STOP/SLOW PADDLE WOULD OFFER CONTRADICTING INFORMATION TO DRIVERS TRAVELING IN OPPOSITE DIRECTIONS/LEGS OF THE INTERSECTION OR DURING INCIDENT MANAGEMENT SITUATIONS.
- E. CENTERLINE CONES MAY BE ADDED TO ENHANCE THE VISIBILITY OF THE FLAGGER STATION. IF CONES ARE USED, PLACE THEM 100 FT. (MINIMUM) FROM FLAGGER.
- F. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.

7. SIGNS AND DEVICES

- A. TRAFFIC CONTROL DEVICES SHALL NOT BE PLACED AT ANY LOCATIONS WHERE THEY MAY OBSCURE OR INTERFERE WITH THE MOTORIST VIEW OF APPROACHING, MERGING, OR INTERSECTING TRAFFIC; OBSTRUCT OTHER TEMPORARY OR PERMANENT TRAFFIC CONTROL DEVICES WHICH ARE STILL APPLICABLE TO ROADWAY CONDITIONS; MISLEAD OR MISDIRECT MOTORISTS, OR ARE BLOCKED BY OTHER TEMPORARY OR PERMANENT OBJECTS.
- B. ALL EXISTING ROADWAY ITEMS SUCH AS GUIDE RAILS, PAVEMENT MARKINGS, CURBS, SIGNALS AND SIGNS DAMAGED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.
- C. ALL TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION SIGN SUPPORTS AND MOUNTING SHALL BE IN CONFORMANCE WITH NYSDOT STANDARD SHEETS AND STANDARD SPECIFICATIONS.
- D. ALL CONSTRUCTION SIGNS SHALL BE COVERED WITH THICK PLASTIC WHEN THE WORK THEY ARE INTENDED FOR IS NOT IN PROGRESS.
- E. ALL TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF THE WORK TO WHICH THEY APPLY, AND SHALL BE MAINTAINED BY THE CONTRACTOR THEREAFTER. THE DEVICES SHALL REMAIN IN PLACE AS LONG AS THEY ARE APPLICABLE.
- F. WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK IS 2 MILES TO 5 MILES, A SUPPLEMENTAL DISTANCE PLAQUE (W7-36P) SHOULD BE USED WITH THE SHOULDER WORK SIGN (W21-5).
- G. THE ROAD WORK NEXT XX MILES SIGN (G20-1) MAY BE USED INSTEAD OF THE ROAD WORK AHEAD SIGN (W20-1) IF WORK LOCATIONS OCCUR OVER A DISTANCE OF MORE THAN 2 MILES.
- H. IF THE TRAFFIC IS EXPECTED TO QUEUE PAST THE W20-4 SIGN, A W3-4 SIGN SHOULD BE ADDED HALFWAY BETWEEN THE W20-4 AND W20-1 SIGNS.
- I. THE N19-11 SIGN IS RECOMMENDED. IT SHALL BE PLACED IN ADVANCE OF THE FIRST ADVANCE WARNING SIGN. THE PLACEMENT DISTANCE SHALL BE 1000' FOR POSTED SPEED LIMITS OF 45 MPH OR HIGHER, AND 500' – 500' FOR POSTED SPEED LIMITS OF LESS THAN 45 MPH.
- J. EXISTING TRAFFIC SIGNAL TIMING SHALL BE ADJUSTED FOR TEMPORARY CONDITIONS. ALL EXISTING VIDEO AND LOOP DETECTORS SHALL BE ADJUSTED TO REFLECT THE CHANGE IN SIGNAL TIMING. CONTRACTOR SHALL ALSO COVER ALL SIGNALS HEADS THAT ARE NOT IN USE.
- K. ALL ADDITIONAL TEMPORARY SIGNAGE, NOT COVERED IN FHWA MUTCD, SHALL COMPLY WITH NYS SUPPLEMENT TO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- L. THE CONTRACTOR SHALL INSTALL PWMS AT MAJOR INTERSECTIONS/ LOCATIONS IN ADVANCE OF ROAD CLOSURE, IN CONSULTATION WITH THE ENGINEER, LOCAL MUNICIPALITIES AND NYSDOT.
- M. THE CONTRACTOR SHALL COORDINATE THE PROPERTY OWNER AND OBTAIN THEIR WRITTEN CONSENSUS, BEFORE INSTALLING ANY PWMS ON THEIR PROPERTY.
- N. "END ROAD WORK" SIGN G20-2 SHALL BE LOCATED A MINIMUM DISTANCE OF 80 FT. AND MAXIMUM OF 400 FT. PAST THE END OF THE DOWNSSTREAM ON ALL WORKZONE SETUPS.

8. NIGHTTIME WORK GENERAL NOTES:

- A. WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.
- B. ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.
- C. ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMET AND NIGHTTIME APPAREL IN ACCORDANCE WITH 107-05A, HIGH VISIBILITY APPAREL AT ALL TIMES.
- D. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACON AT ALL TIMES.
- E. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE, TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.
- F. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.
- G. LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.
- H. ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.
- I. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL.
- J. REFER TO NYSDOT HIGHWAY DESIGN MANUAL (HDM) SECTION 16.5.7 (NIGHTTIME CONSTRUCTION), AND NYSDOT STANDARD SPECIFICATIONS 619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS.

9. BICYCLISTS AND PEDESTRIANS:

- A. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN AND BICYCLE TRAFFIC THROUGH OR AROUND EACH WORK AREA FOR THE DURATION OF THE CONSTRUCTION. MATERIAL, EQUIPMENT OR OTHER SUCH BARRIERS SHALL NOT BE PLACED OR PARKED IN SUCH A MANNER AS TO OBSTRUCT PEDESTRIAN OR BICYCLE TRAFFIC OR TO PRESENT A SAFETY HAZARD TO THE NON-MOTORIZED PUBLIC. WHERE PEDESTRIAN TRAFFIC MUST BE RELOCATED OFF THE EXISTING FACILITY, WALKWAYS SHALL BE CLEARLY MARKED AND HAVE LOGICAL START AND TERMINUS. SEE NYSDOT STANDARD SHEET 619-519 FOR DETAILS.
- B. THE CONTRACTOR SHALL PROVIDE, DELINEATE AND MAINTAIN A USABLE TRAVEL PATH FOR PEDESTRIANS EITHER THROUGH OR AROUND WORK ZONES FOR THE DURATION OF THIS PROJECT. THE TRAVEL PATH SHALL COMPLY WITH THE CONTINUOUS PASSAGE PROVISIONS OF THE CURRENT AMERICANS WITH DISABILITIES ACT (ADA) GUIDELINES. SEE NYSDOT STANDARD SHEET 619-519 FOR DETAILS.
- C. TEMPORARY SAFETY FENCING SHALL BE PLACED ALONG ANY SIDEWALK ADJACENT TO ANY EXCAVATION OR VERTICAL DROP-OFF OVER SIX (6) INCHES. SAFETY FENCING SHALL BE CONTINUOUS PLASTIC FLUORESCENT ORANGE SUPPORTED BY WOOD POSTS. THE FENCING SHALL BE A MINIMUM OF FOUR (4) FEET IN HEIGHT AND MAY INCLUDE A TOP AND BOTTOM RAIL TO PROVIDE ADDITIONAL SUPPORT. ABOVE THE SAFETY FENCE, SHALL CONFORM TO SECTION 107-05 OF THE STANDARD SPECIFICATIONS. ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO INSTALL AND REMOVE TEMPORARY SAFETY FENCING SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.
- D. THE CONTRACTOR SHALL PROVIDE A USABLE TRAVEL PATH AT ALL BUS STOP LOCATIONS TO PROVIDE SAFE ACCESS TO AND FROM THE BUS.

LEGEND

- TEMPORARY SIGN
- CHANGEABLE MESSAGE SIGN (PWMS)
- WORK ZONE
- CHANNELIZING DEVICE
- TCB (UNPINNED)
- FLAGGER
- FLAG TREE
- 24,000 LB PROTECTIVE VEHICLE WITH TRUCK/TRAILER MOUNTED ATTENUATOR (TMA)
- ARROW PANEL
- ARROW PANEL (CAUTION MODE)
- TYPE III BARRICADE
- ARROW PANEL TRAILER OR SUPPORT
- WARNING LIGHT ON SIGN
- TEMPORARY TRAFFIC FLOW ARROW
- TEMPORARY IMPACT ATTENUATOR

ABBREVIATIONS

AADT	AVERAGE ANNUAL DAILY TRAFFIC
CR	COUNTY ROUTE
DHV	DIRECTIONAL HOURLY VOLUME
FASU	FLASHING ARROW SIGN UNIT
FHWA	FEDERAL HIGHWAY ADMINISTRATION
M.P.	MILE POST
MAX.	MAXIMUM
MIN.	MINIMUM
MPH	MILES PER HOUR
MUTCD	MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
NYS	NEW YORK STATE
NYSDOT	NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RTE	ROUTE
TCB	TEMPORARY CONCRETE BARRIER
STA.	STATION

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WORK ZONE TRAFFIC CONTROL  
GENERAL NOTES & ABBREVIATIONS

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	O-001
DATE	04/12/2023
SH.NO.	XX OF XXX



Kiewit



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ISSUED FOR  
PERMITTING

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E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MS	SB	
D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB	
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB	
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB	
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DRAWN BY:	DESIGNED BY: JM	APPROVED BY: SB	SCALE	AS SHOWN	
			REV. NO.		



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STAGE	START STATION DESCRIPTION	START STATION	END STATION DESCRIPTION	END STATION	CLOSURE TYPE	NYSDOT STANDARD DETAIL SHEET	DETOUR PLAN SHEET	WORK NOTES	PVMS LOCATION / PARCEL OWNER		TRAFFIC COUNTS (AADT (pd))	TRAFFIC COUNTS (AADT (pd))	DHV (p)	ROUTE(S)
1	3,800 feet north of Lake Road	72899+00	NY Route 303	72847+00	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	619-407 619-421	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		11508 (COMBINED) 2019	5248 (NB) 2019 6058 (SB) 2019	444 (16:00-17:00) (NB) 2019 487 (07:00-08:00) (SB) 2019	NYS RTE 9W
2	NY Route 303	72847+00	NY Route 304	72815+50	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	619-407 619-421	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		16450 (COMBINED) 2019	8277 (NB) 2011 8660 (SB) 2011	963 (16:00-17:00) (NB) 2011 473 (07:00-08:00) (SB) 2011	NYS RTE 9W
3	NY Route 304	72815+50	Short Clove RD CR 90	72765+50	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	619-407 619-421 619-422	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		17493 (COMBINED) 2019	17493 (COMBINED) 2019	N/A	NYS RTE 9W
4	Short Clove RD CR 90	72765+50	Westside Ave.	72685+00	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	619-407 619-421 619-422	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		17493 (COMBINED) 2019	17493 (COMBINED) 2019	N/A	NYS RTE 9W
5	Westside Avenue	72685+00	Railroad Avenue CR 94	72624+00	LANE CLOSURE WITH ONE WAY TRAFFIC AND DETOUR (LOCAL)	619-407 619-421 619-422 619-520	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		17493 (COMBINED) 2019	17493 (COMBINED) 2019	N/A	NYS RTE 9W / US RTE 202
6	Railroad Avenue CR 94	72624+00	Filors Lane CR 98	72592+50	LANE CLOSURE WITH ONE WAY TRAFFIC AND DETOUR (LOCAL)	619-407 619-421 619-520	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		13946 (COMBINED) 2019	7121 (NB) 2017 6926 (SB) 2017	648 (16:00-17:00) (NB) 2017 513 (17:00-18:00) (SB) 2017	NYS RTE 9W / US RTE 202
7	Filors Lane CR 98	72592+50	Central Drive CR 106	72559+00	LANE CLOSURE WITH ONE WAY TRAFFIC AND DETOUR (LOCAL)	619-407 619-421 619-520	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		13946 (COMBINED) 2019	7121 (NB) 2017 6926 (SB) 2017	648 (16:00-17:00) (NB) 2017 513 (17:00-18:00) (SB) 2017	NYS RTE 9W / US RTE 202
8	Central Drive CR 106	72559+00	Main Street CR 108	72547+50	LANE CLOSURE WITH ONE WAY TRAFFIC AND DETOUR (LOCAL)	619-407 619-421 619-503	O-401	TRENCHING & CONDUIT INSTALLATION	650' SOUTH OF ROUTE 9W / LOWLAND HILL RD INTERSECTION	LANDS OF SRISHIVA LLC TM#20.02-11-32	13946 (COMBINED) 2019	7121 (NB) 2017 6926 (SB) 2017	648 (16:00-17:00) (NB) 2017 513 (17:00-18:00) (SB) 2017	NYS RTE 9W / US RTE 202
9	Main Street CR 108	72547+50	Park Road	72515+50	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	619-407 619-421 619-520	N/A	TRENCHING & CONDUIT INSTALLATION	N/A		8190 (COMBINED) 2019	4224 (NB) 2018 3996 (SB) 2018	438 (16:00-17:00) (NB) 2018 361 (07:00-08:00) (SB) 2018	NYS RTE 9W / US RTE 202
10	Park Road	72515+50	Park Rd. at Battlefield Rd.	72495+32	FULL ROAD CLOSURE WITH DETOUR	619-407 619-421 619-503	O-402	TRENCHING & CONDUIT INSTALLATION	N/A		368 (COMBINED) 2019	227 (NB) 2015 145 (SB) 2015	20 (16:00-17:00) (NB) 2015 14 (16:00-17:00) (SB) 2015	PARK RD

PACKAGE 7B WZTC WITH DETOUR INFORMATION AND WORK DESCRIPTIONS																
STAGE	DETOUR INFORMATION					Construction Work Hours			WORK TO BE COMPLETED							
	9W Length (Miles)	Detour Length (Miles)	EXISTING TRAVEL LANES	TRAFFIC TO BE MAINTAINED ON 9W DURING CONSTRUCTION	TRAFFIC TO BE DETOURED	Detour Description	Day Time	Night Time	Approx. Length of Trench (ft.)	ESTIMATED TIMEFRAME TO COMPLETE TRENCH*	Transition, Splice, and Link vaults	HDD #	HDD Start Sta.	HDD End Sta.	Total length of HDD (ft.)	ESTIMATED TIMEFRAME TO COMPLETE HDD (DAYS)
STAGE 1	1	N/A	1 NB 1 SB	BI-DIRECTIONAL	N/A	N/A	7AM-3PM	7PM-5AM	5200	104	Transition Vault 4 (72899+15) Splice Vault 251A (72867+28)	N/A	N/A	N/A	N/A	N/A
STAGE 2	0.6	N/A	1 NB 1 SB	BI-DIRECTIONAL	N/A	N/A	9AM-3PM	7PM-5AM	3300	66	Splice Vault 251 (72843+58)	N/A	N/A	N/A	N/A	N/A
STAGE 3	0.96	N/A	1 NB 1 SB	BI-DIRECTIONAL	N/A	N/A	9AM-3PM	7PM-5AM	5000	100	Splice Vault 250A (72811+08) Splice Vault 250 (72785+36) Splice Vault 249A (72766+56)	N/A	N/A	N/A	N/A	N/A
STAGE 4	1.87	N/A	1 NB 1 SB 1 TL	BI-DIRECTIONAL	N/A	N/A	N/A	7PM-5AM	6900	138	Splice Vault 249 (72730+20) Splice Vault 248A (72712+54)	HDD 132	72678+86	72697+21	1842	75
STAGE 5	1.15	N/A	1 NB 1 SB 1 TL	BI-DIRECTIONAL	N/A	N/A	9AM-3PM	7PM-5AM	1500	28	Splice Vault 248 (72677+18) Splice Vault 247 (72631+66)	HDD 131 HDD 129	72655+17 72637+71	72675+66 72654+26	2055 2011	82 81
STAGE 6	0.6	N/A	1 NB 1 SB 1 TL	BI-DIRECTIONAL	N/A	N/A	9AM-3PM	7PM-5AM	1870	34	Splice Vault 246A (72600+93)	HDD 127	72610+99	72625+39	1603	66
STAGE 7	0.63	N/A	1 NB 1 SB 1 TL	BI-DIRECTIONAL	N/A	N/A	9AM-3PM	7PM-5AM	1000	19	Splice Vault 246 (72560+03) Splice Vault 246BB (72584+05)	HDD 126	72561+10	72582+80	2177	86
STAGE 8	0.22	0.48	1 NB 1 SB 1 TL	SB TRAFFIC	NB	Detour permanent during duration of stage	9AM-3PM	7PM-5AM	1200	22	N/A	N/A	N/A	N/A	N/A	N/A
STAGE 9	0.6	N/A	1 NB 1 SB	BI-DIRECTIONAL	N/A	N/A	7AM-5PM (Including Saturday's)	7PM-5AM (including Saturday)	3200	59	Splice Vault 245 (72527+57) Splice Vault 245A (72538+41)	N/A	N/A	N/A	N/A	N/A
STAGE 10	0.26	0.54	1 WB 1 EB	PARTIALLY CLOSED	NB & SB LOCAL	Detour permanent during duration of stage	7AM-5PM (Including Saturday's)	7PM-5AM (including Saturday)	1300	24	Transition Vault 3 (72495+67)	HDD 124	72496+78	72504+16	738	36


\*ASSUMING 50 FEET OF TRENCH PER SHIFT, AT 1 SHIFT PER DAY. PER NYSDOT CORRESPONDENCE DATED 04/07/2021  
\*\*FOR DRIVEWAY CLOSURES REFER TO NOTE 1-K ON O-001.

WZTC Standards for Mill and Inlay (M&I) Operation on route 9W			
Stage	NYSDOT Standard sheets	M&I Start	M&I End
Stage 10	619-407, 619-421, 402-01	72499+00	72575+00
Stage 9	619-407, 619-421, 402-01	72575+00	72547+00
Stage 8	619-407, 619-421, 619-422, 402-01	72547+00	72559+50
Stage 7	619-421, 619-422, 402-01	72583+00	72592+50
Stage 6	619-407, 619-421, 619-422, 402-01	72592+50	72610+00
Stage 5	619-421, 619-422, 402-01	72625+50	72637+50
Stage 4	619-407, 619-421, 402-01	72698+00	72766+00
Stage 3	619-407, 619-421, 402-01	72766+00	72815+00
Stage 2	619-407, 619-421, 402-01	72815+00	72841+00
Stage 1	619-407, 619-421, 402-01	72847+00	72898+00
All Intersection	619-421		

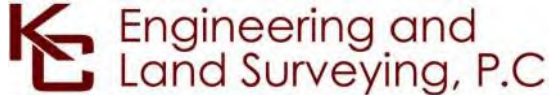
- MILL AND INLAY NOTES:
- MILL AND INLAY TO BE PLACED/REPLACED AS APPROVED BY ENGINEER OF RECORD
  - LEVEL II ILLUMINATIONATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEING PAVING OR MILLING MACHINE
  - NO MILLING AND INLAYING IS REQUIRED OVER BRIDGE, UNLESS OTHERWISE NOTED
  - WORK ZONE TRAFFIC CONTROL FOR INTERSECTIONS SHALL FOLLOW NYSDOT STANDARD SHEET 619-421
  - THIS TABLE SHALL ALSO APPLY FOR PERMANENT RESTRIPIING OPERATIONS



Champlain Hudson Power Express



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D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB
A	02/18/2022	PRELIMINARY PROGRESS	MS	SB
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WORK ZONE TRAFFIC CONTROL TABLES

DRAWN BY: BM

DESIGNED BY: BM

APPROVED BY: SB

SCALE AS SHOWN  
REV. NO.

KIEWIT PROJECT NO.  
21162  
KC PROJECT NO.  
120174  
DRAWING NO.  
**O-002**

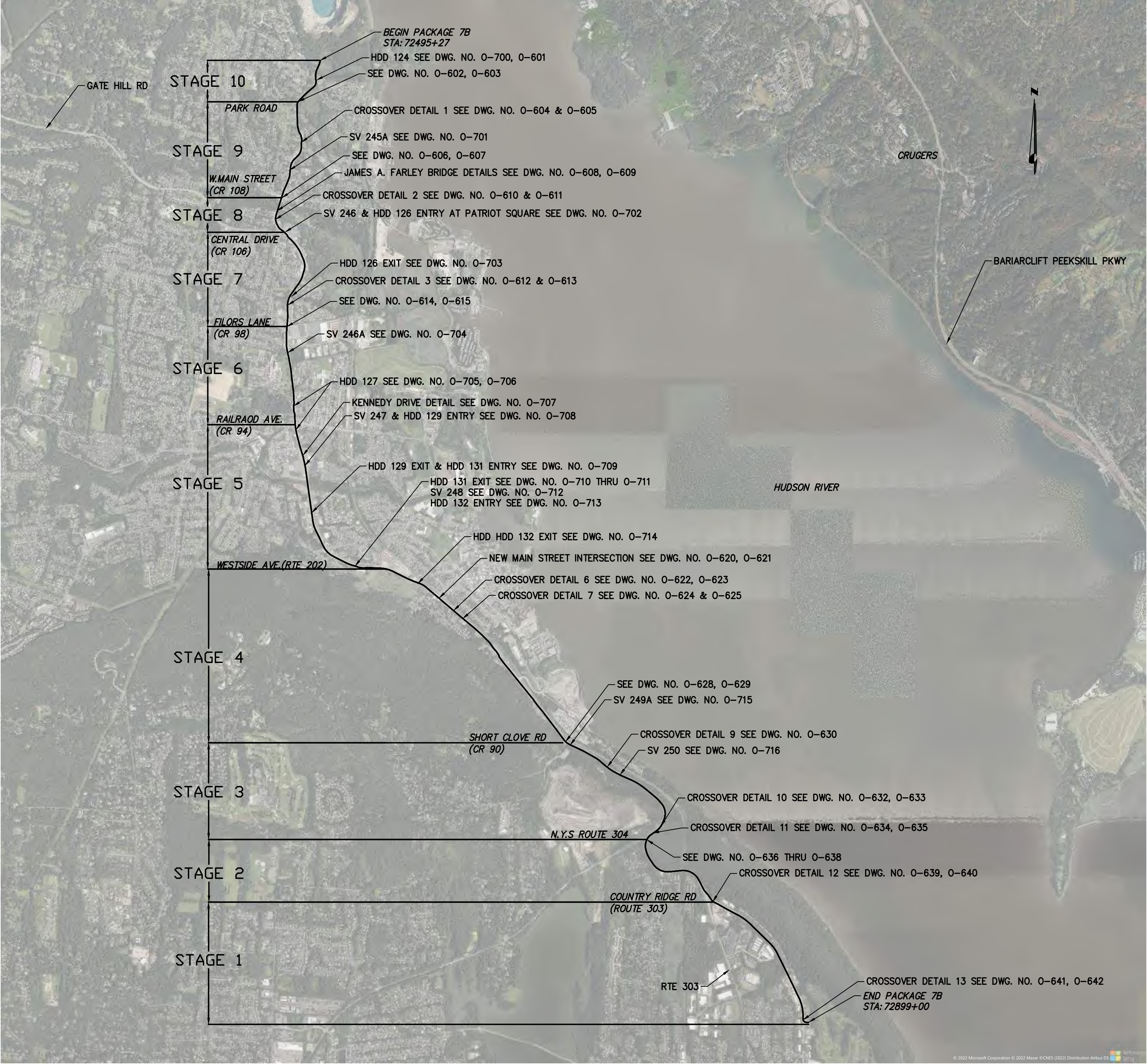
DATE 04/12/2023  
SH.NO. XX OF XXX

B-002 7B WORK ZONE TRAFFIC CONTROL STAGES

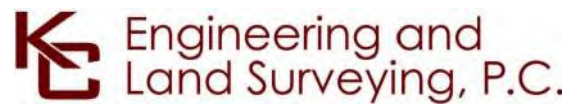


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	NYSDOT STANDARD SHEET / SHEET NUMBER
245	619-080 WORK BEYOND SHOULDER
246BB	619-080 WORK BEYOND SHOULDER
248	619-080 WORK BEYOND SHOULDER
248A	619-080 WORK BEYOND SHOULDER
249	619-080 WORK BEYOND SHOULDER
250A	619-080 WORK BEYOND SHOULDER
251	619-080 WORK BEYOND SHOULDER
251A	619-080 WORK BEYOND SHOULDER



WZTC KEY PLAN  
SCALE: 1" = 2000'



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-E-	03/04/2023	DRAFT FINAL REV 1 SUBMISSION	MS	SB
D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB
A	02/18/2022	PRELIMINARY PROGRESS	MS	SB

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND

WORK ZONE TRAFFIC CONTROL  
KEY PLAN












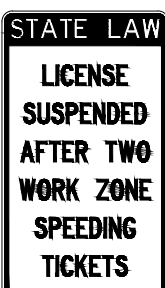



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						REV. NO.	X	SH. NO.	X OF XXX









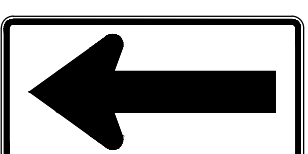
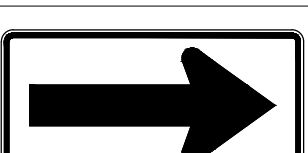
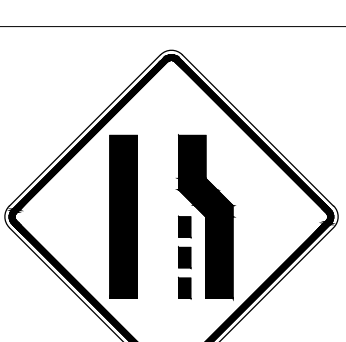
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DRAWING NO.	

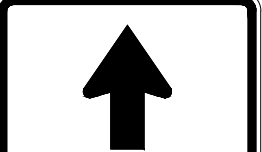
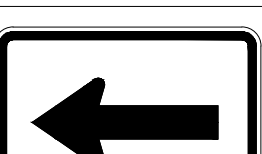
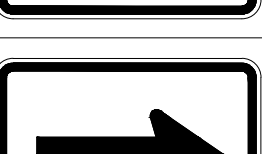



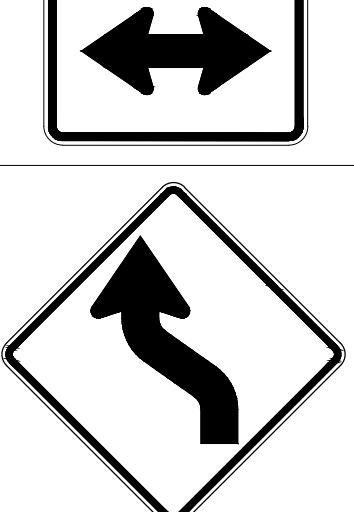
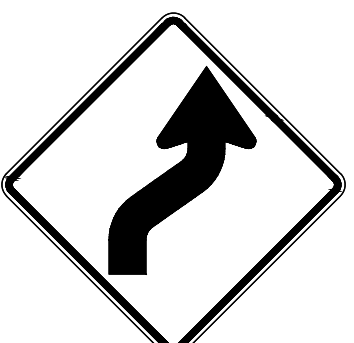
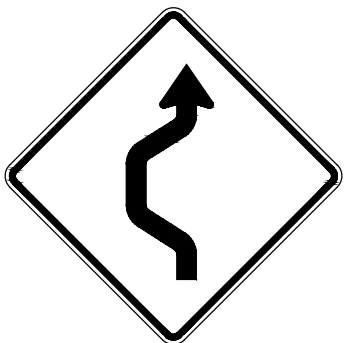

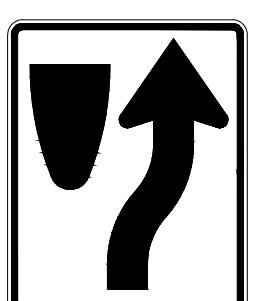
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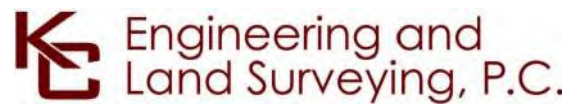


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LOCATION	SIGN	MUTCD_NO.	COLOR	SIZE(S)
A		M4-8	ORANGE AND BLACK	24"x12"
B		M4-8a	ORANGE AND BLACK	24"x18"
C		M3-1	BLACK AND WHITE	24"x12"
D		M3-3	BLACK AND WHITE	24"x12"
E		M3-2	BLACK AND WHITE	24"x12"
F		M3-4	BLACK AND WHITE	24"x12"
G		M4-4	BLACK AND WHITE	24"x12"
H		M1-4	BLACK AND WHITE	24"x24"
I		M1-4t	BLACK AND WHITE	30"x24"
J		D1-2	BLACK AND WHITE	24"x30"
K		D1-2	BLACK AND WHITE	24"x30"
L		NYR9-11	BLACK AND WHITE	24"x42"
M		G20-2	ORANGE AND BLACK	36"x18"
N		W20-1	ORANGE AND BLACK	36"x36"
O		W3-4	ORANGE AND BLACK	36"x36"

LOCATION	SIGN	MUTCD_NO.	COLOR	SIZE(S)
P		W20-4	ORANGE AND BLACK	36"x36"
Q		W20-7	ORANGE AND BLACK	36"x36"
R		W20-2	ORANGE AND BLACK	36"x36"
S		W20-3	ORANGE AND BLACK	36"x36"
T		R3-2	BLACK AND WHITE	24"x24"
U		R3-1	BLACK AND WHITE	24"x24"
V		R11-2	BLACK AND WHITE	48"x30"
W		NYW8-33	ORANGE AND BLACK	48"x24"
X		W1-6L	ORANGE AND BLACK	48"x24"
Y		W1-6R	ORANGE AND BLACK	48"x24"
Z		W4-R2	ORANGE AND BLACK	36"x36"

LOCATION	SIGN	MUTCD_NO.	COLOR	SIZE(S)
1		M6-3	BLACK AND WHITE	21"x15"
2		M6-1L	BLACK AND WHITE	21"x15"
3		M6-1R	BLACK AND WHITE	21"x15"
6		M5-1L	BLACK AND WHITE	21"x15"
7		M5-1R	BLACK AND WHITE	21"x15"
8		M6-4	BLACK AND WHITE	21"x15"
9		W1-4L	ORANGE AND BLACK	36"x36"
10		W1-4R	ORANGE AND BLACK	36"x36"
11		W24-1L	ORANGE AND BLACK	36"x36"
12		W20-5	ORANGE AND BLACK	36"x36"
13		R4-7	BLACK AND WHITE	24"x30"



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ISSUED FOR PERMITTING

F	04/14/2023	FINAL SUBMISSION	MS	SB	
E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MS	SB	
D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB	
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB	
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB	
A	02/18/2022	PRELIMINARY PROGRESS	MS	SB	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WORK ZONE TRAFFIC CONTROL  
SIGNAGE TABLES  
SHEET 1 OF 2

DRAWN BY:DESIGNED BY: MSAPPROVED BY: SBSCALE AS SHOWN  
REV.NO.

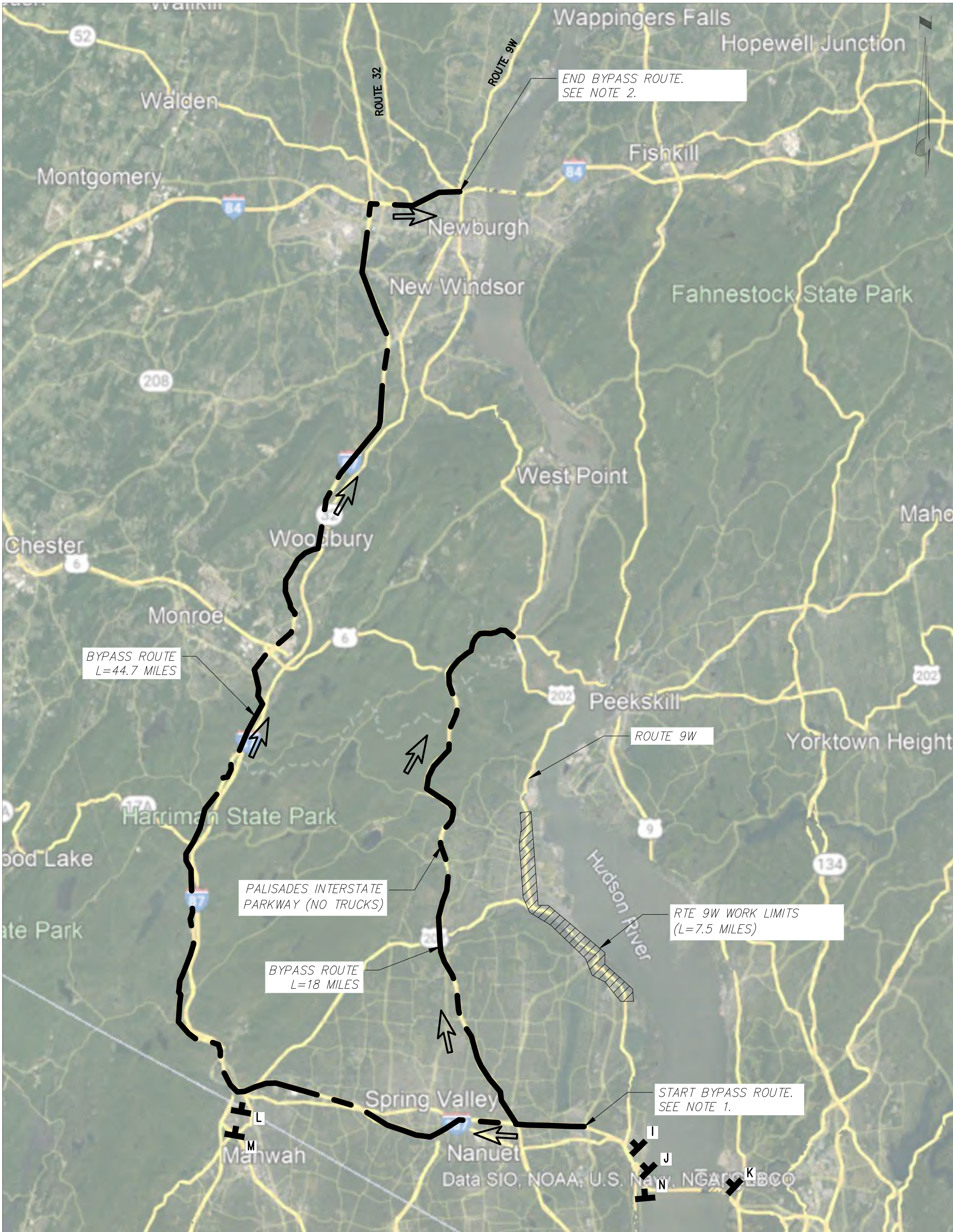
KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	O-004
DATE SH.NO.	04/12/2023 XX OF XXX







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- NOTES:
1. START BYPASS ROUTE AT INTERSECTION OF 9W AND NY RTE 52/ I-84
  2. END SB 9W BYPASS ROUTE AT INTERSECTION OF I-287 AND ROUTE 303.
  3. INSTALL PWMS AT THE FOLLOWING LOCATIONS:
    - I. I-287 WB: 1,200 FEET UPSTREAM OF EXIT 11 (RIGHT SIDE)
    - J. I-287 WB: 1,200 FEET UPSTREAM IF EXIT 10 (RIGHT SIDE)
    - K. I-287 WB: ENTRANCE RAMP GORE FROM ROUTE 9 (EAST OF MARIO CUOMO BRIDGE) (RIGHT SIDE)
    - L. I-287 EB: 1,300 FEET SOUTH OF ENTRANCE RAMP FROM ROUTE 17
    - M. I-287 EB: 2,300 FEET SOUTH OF ENTRANCE RAMP FROM ROUTE 17
    - N. 9W NB: 200 FEET SOUTH OF INTERSECTION OF 9W AND SHADYSIDE AVE.

SCREEN 1	SCREEN 2	SCREEN 3
RD WRK RTE 9W BYPASS	USE ALT RTES I-87, 287	& PALISADES PKWY

LEGEND	
	CONSTRUCTION SIGNS
	PROPOSED SIGN LOCATION ID (SIGNS STACKED PER MUTCD)
	DETOUR ROUTE
	DIRECTION OF TRAFFIC

RTE 9W NB  
BYPASS ROUTES  
SCALE: N.T.S.



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ISSUED FOR PERMITTING

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
F	04/14/2023	FINAL SUBMISSION	MS	SB
E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MS	SB
D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB
A	02/18/2022	PRELIMINARY PROGRESS	MS	SB

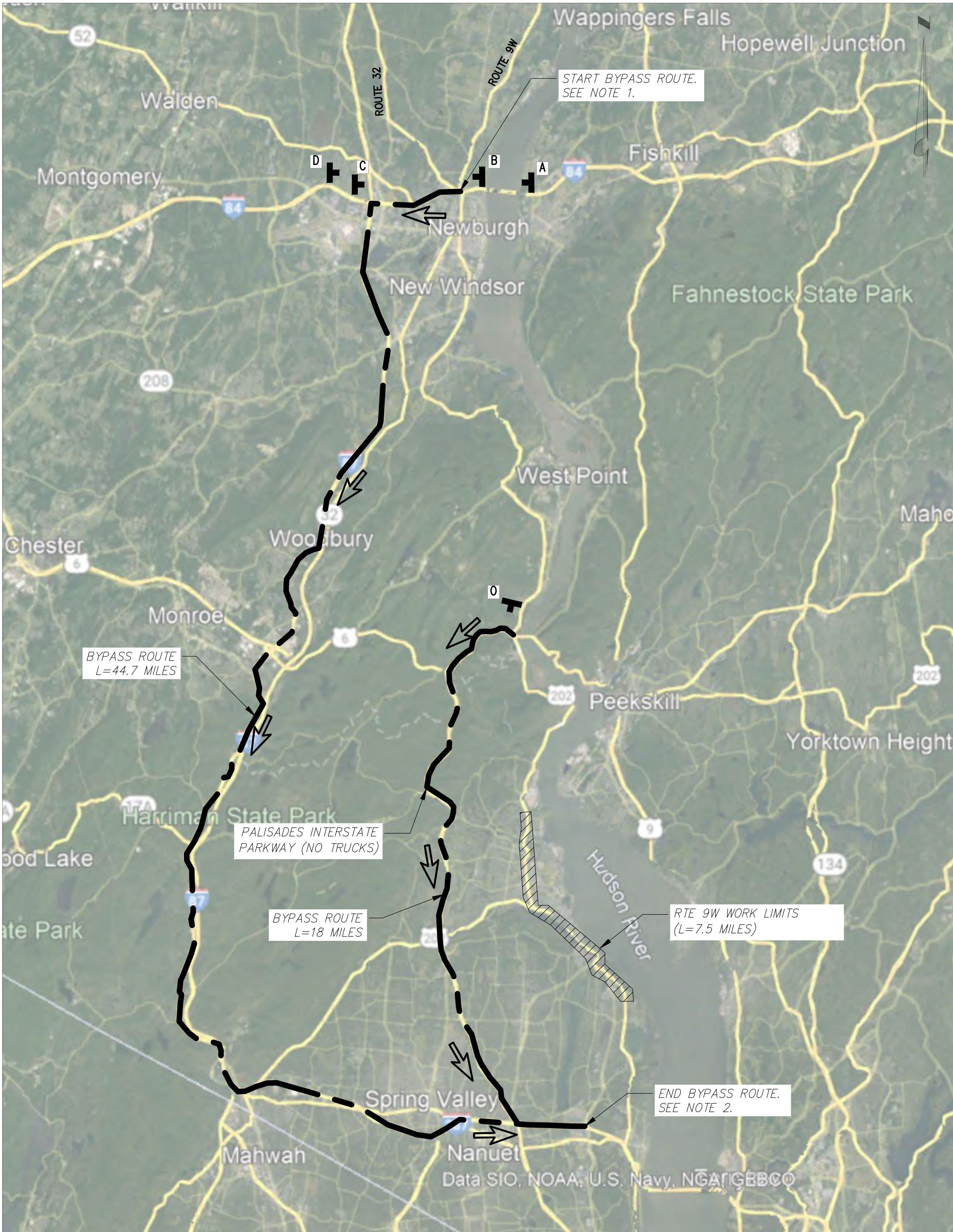
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WORK ZONE TRAFFIC CONTROL  
NB BYPASS  
ALTERNATE TRAFFIC ROUTES

DRAWN BY:	MS	DESIGNED BY:	MS	APPROVED BY:	SB	SCALE	AS SHOWN	DATE	04/12/2023
						REV. NO.		SH.NO.	XX OF XXX

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	O-006



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RTE 9W SB  
BYPASS ROUTES  
SCALE: N.T.S.

NOTES:

1. START SB 9W BYPASS ROUTE AT INTERSECTION OF 9W AND NY RTE 52/ I-84
2. END SB 9W BYPASS ROUTE AT INTERSECTION OF I-287 AND ROUTE 303.
3. INSTALL PWMS AT THE FOLLOWING LOCATIONS
  - A. I-84 WB: 570 FEET DOWNSTREAM FROM ENTRANCE RAMP FROM RTE 9D (RIGHT SIDE)
  - B. I-84 WB: 750 FEET UPSTREAM OF EXIT 39B (RIGHT SIDE)
  - C. I-84 EB: 1300 FEET UPSTREAM OF EXIT 36A (RIGHT SIDE)
  - D. I-84 EB: 1.4 MILES UPSTREAM OF EXIT 36A (RIGHT SIDE)
  - O. ROUTE 9W SB: 500 FEET NORTH OF ON ROUTE 9W AND FIREFIGHTERS MEMORIAL DRIVE

SCREEN 1  
RD WRK  
RTE 9W  
BYPASS

SCREEN 2  
USE  
ALT RTES  
I-87, 287

SCREEN 3  
&  
PALISADES  
PKWY

LEGEND

- CONSTRUCTION SIGNS
- PROPOSED SIGN LOCATION ID  
(SIGNS STACKED PER MUTCD)
- DETOUR ROUTE
- DIRECTION OF TRAFFIC



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ISSUED FOR  
PERMITTING

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
F	04/14/2023	FINAL SUBMISSION	MS	SB
E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MS	SB
D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB
A	02/18/2022	PRELIMINARY PROGRESS	MS	SB







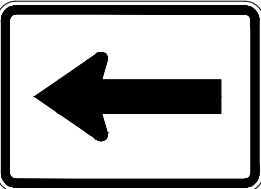
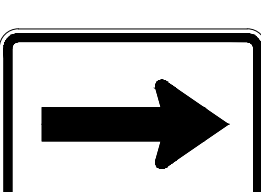
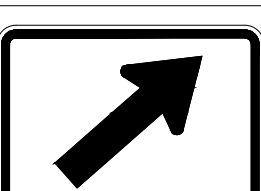
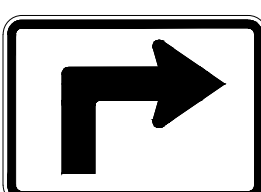

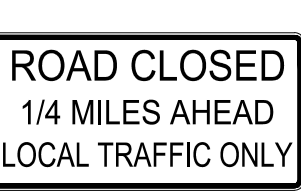
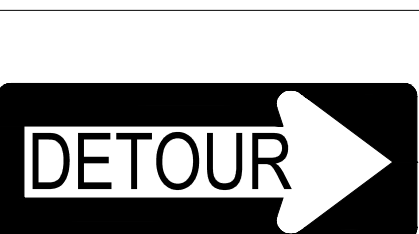


CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WORK ZONE TRAFFIC CONTROL  
SB BYPASS  
ALTERNATE TRAFFIC ROUTES

DRAWN BY:	MS	DESIGNED BY:	MS	APPROVED BY:	SB	SCALE	AS SHOWN	DATE	04/12/2023
						REV. NO.		SH.NO.	XX OF XXX

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	O-007



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LOCATION	SIGN	MUTCD NO.	COLOR	SIZE(S)
A		M4-8	ORANGE AND BLACK	24"x12"
B		M4-8a	ORANGE AND BLACK	24"x18"
D		M3-3	BLACK AND WHITE	24"x12"
F		M3-4	BLACK AND WHITE	24"x12"
H		M1-4	BLACK AND WHITE	24"x24"
I		M1-4	BLACK AND WHITE	30"x24"
2		M6-1L	BLACK AND WHITE	21"x15"
3		M6-1R	BLACK AND WHITE	21"x15"
4		M6-2R	BLACK AND WHITE	21"x15"
7		M5-1R	BLACK AND WHITE	21"x15"
J		W20-2	ORANGE AND BLACK	36"x36"
K		R11-3A	WHITE AND BLACK	60"x30"
M		M4-10R	ORANGE AND BLACK	48"x18"
N		R11-2	BLACK AND WHITE	48"x30"
S		W20-3	ORANGE AND BLACK	36"x36"

NOTES:

- INSTALL PVMS AT 650' FEET SOUTH OF ROUTE 9W/LOWLAND HILL RD INTERSECTION, FACING NB RTE 9W.

SCREEN #1



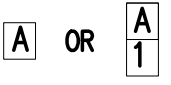

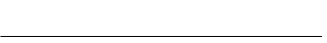

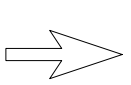
RTE 9W  
NORTH  
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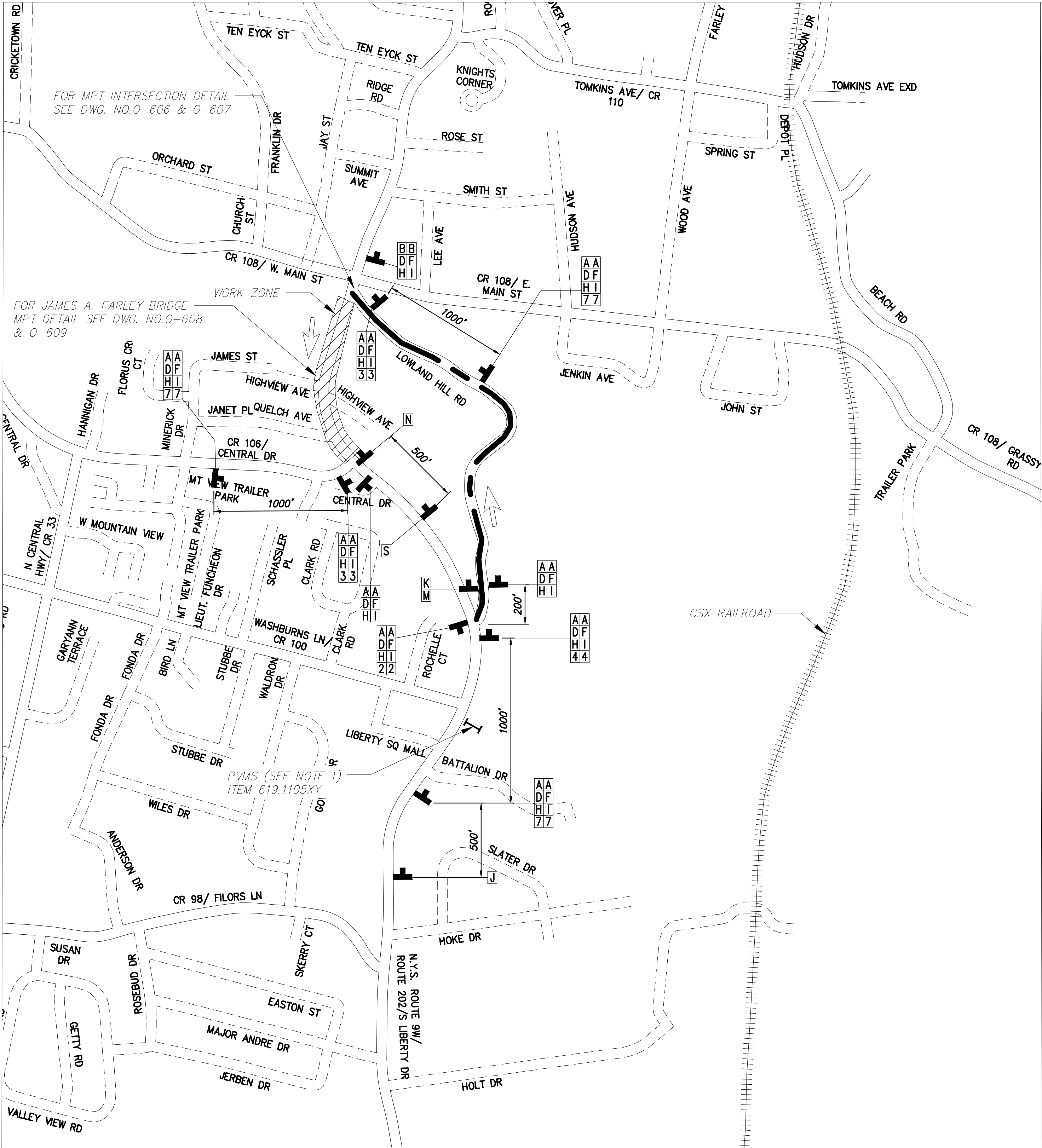
SCREEN #2

USE  
DETOUR
- THE CONTRACTOR SHALL COVER CONFLICTING SIGNAGE WITH THICK PLASTIC. REMOVE PRIOR TO END OF STAGE.
- DURING WINTER MORATORIUM, COVER EXCAVATION WITH TEMPORARY ASPHALT AND OPEN ALL LANES TO TRAFFIC.
- DRUMS SHALL BE LEFT IN PLACE DURING NON-WORKING HOURS
- MINIMUM LANE WIDTHS FOR THRU TRAFFIC ON ROUTE 9W/202 SHALL BE 11'

SUMMARY OF TRAFFIC CONTROL:




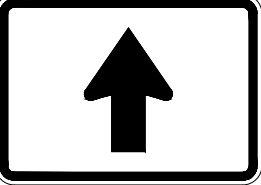
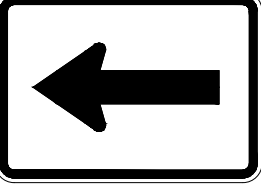
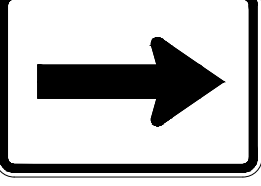
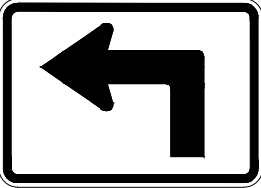
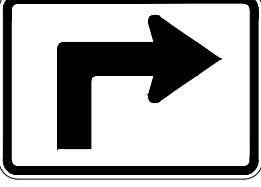

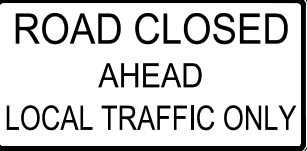
- NIGHTTIME WORK
- SB ROUTE 9W TRAFFIC REMAINS OPEN AT ALL TIMES
- ACCESS/EGRESS TO LOCAL COMMERCIAL AN RESIDENTIAL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES
- DETOUR ROUTE LENGTH: 0.48 MILE
- NB ROUTE 9W IS CLOSED FROM RT 210 (CENTRAL DRIVE) TO CR 108 (WEST MAIN ST.). LENGTH: 0.22 MILE

LEGEND	
	PVMS
	CONSTRUCTION SIGNS
	PROPOSED SIGN LOCATION ID (SIGNS STACKED PER MUTCD)
	LOCAL ROAD
	STATE OR COUNTY ROUTE
	DETOUR ROUTE
	DIRECTION OF TRAFFIC





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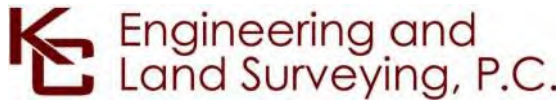
LOCATION	SIGN	MUTCD NO.	COLOR	SIZE(S)
A		M4-8	ORANGE AND BLACK	24"x12"
B		M4-8a	ORANGE AND BLACK	24"x18"
J		D1-2	BLACK AND WHITE	24"x30"
1		M6-3	BLACK AND WHITE	21"x15"
2		M6-1L	BLACK AND WHITE	21"x15"
3		M6-1R	BLACK AND WHITE	21"x15"
6		M5-1L	BLACK AND WHITE	21"x15"
7		M5-1R	BLACK AND WHITE	21"x15"
S		W20-2	ORANGE AND BLACK	36"x36"
K		R11-3A	WHITE AND BLACK	60"x30"

SUMMARY OF TRAFFIC CONTROL:

- 1. DAYTIME WORK
- 2. DIVERT PARK RD TRAFFIC TO DETOUR
- 3. ACCESS/EGRESS RESIDENTIAL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES
- 4. DETOUR ROUTE LENGTH: 0.54 MILE
- 5. PARK ROAD CLOSE FROM ROUTE 9W TO BATTLEFIELD RD (LENGTH: 0.26 MILE)

NOTES:

- 1. THE CONTRACTOR SHALL COVER CONFLICTING SIGNAGE WITH THICK PLASTIC. REMOVE PRIOR TO END OF STAGE.
- 2. DURING WINTER MORATORIUM, COVER EXCAVATION WITH TEMPORARY ASPHALT AND OPEN ALL LANES TO TRAFFIC.
- 3. DURING NON-WORK HOURS, RELOCATE DRUMS TO EDGE OF WORK ZONE AND OPEN TRAFFIC TO PARK ROAD SOUTHBOUND. COVER PARK RD DETOUR SIGN PANELS.
- 4. MINIMUM LANE WIDTHS FOR THRU TRAFFIC ON ROUTE 9W/202 SHALL BE 11'
- 5. PARK ROAD IS A NARROW LOCAL ROAD WITH SEVERAL RESIDENCIES. THE CONTRACTOR SHALL STAGE THEIR CONSTRUCTION ACTIVITIES SO THAT ACCESS TO ALL RESIDENCES AND LOCAL ROADS (GEORGIAN DRIVE AND BATTLEFIELD ROAD), INCLUDING ACCESS TO BIKE WAY, EMERGENCY VEHICLES AND FIRE TRUCKS SHALL BE PROVIDED AT ALL TIMES.



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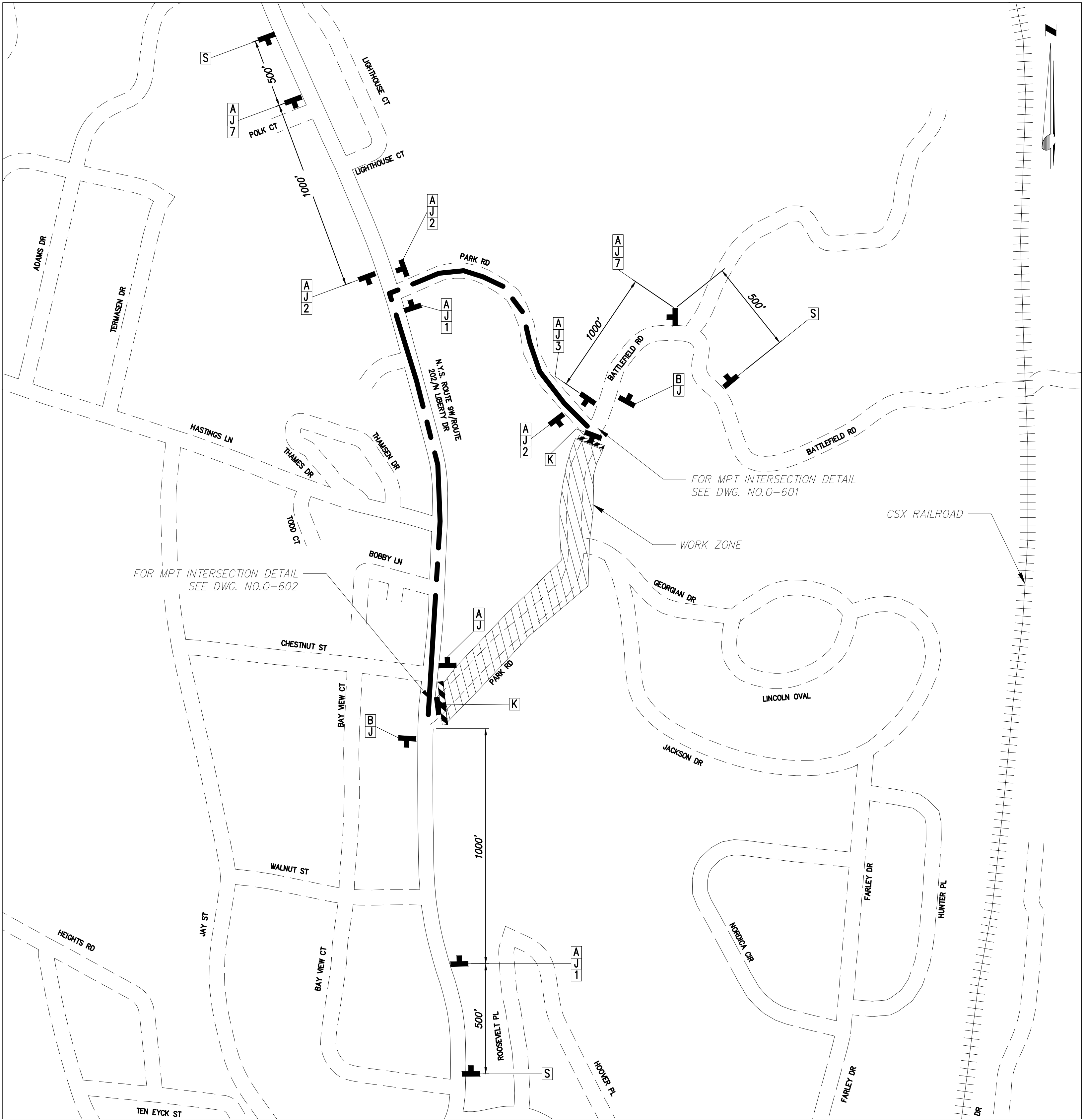
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E	03/01/2023	DRAFT FINAL REV 1 SUBMISSION	MS	SB	
D	11/16/2022	DRAFT FINAL SUBMISSION	MS	SB	
C	05/20/2022	60% DESIGN SUBMISSION	MS	SB	
B	03/23/2022	PRELIMINARY DESIGN DEVELOPMENT	MS	SB	
A	02/18/2022	PRELIMINARY PROGRESS	MS	SB	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 12 (PACKAGE 7B) - ROUTE 9W: ROCKLAND  
WORK ZONE TRAFFIC CONTROL  
STAGE 10 DETOUR PLAN

DRAWN BY:	MS	DESIGNED BY:	MS	APPROVED BY:	SB	SCALE	AS SHOWN	DATE	04/12/2023
						REV. NO.		SH.NO.	XX OF XXX

KIEWIT PROJECT NO.	21162
KC PROJECT NO.	120174
DRAWING NO.	
<b>O-402</b>	



STAGE 10  
PARK ROAD DETOUR (LOCAL) PLAN VIEW  
SCALE: NTS



