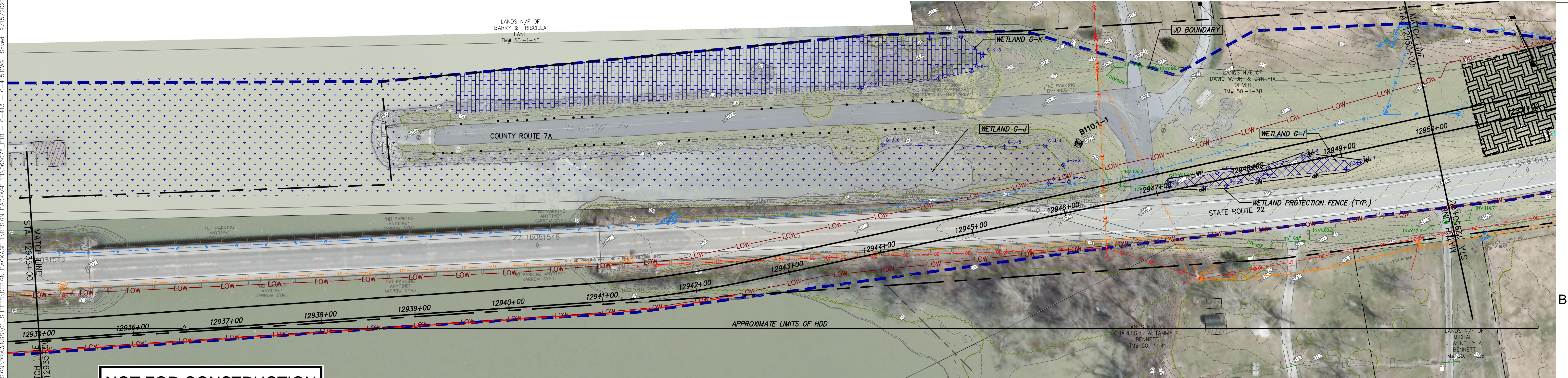



P1B STA. 12920+00 TO STA. 12935+00 PLAN VIEW  
SCALE: 1" = 50'




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STA. 12935+00 TO STA. 12950+00 PLAN VIEW  
SCALE: 1" = 50'




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Power Express



Kiewit



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JOSEPH P. ROMAN  
LICENSED PROFESSIONAL ENGINEER  
081993  
09/18/2022

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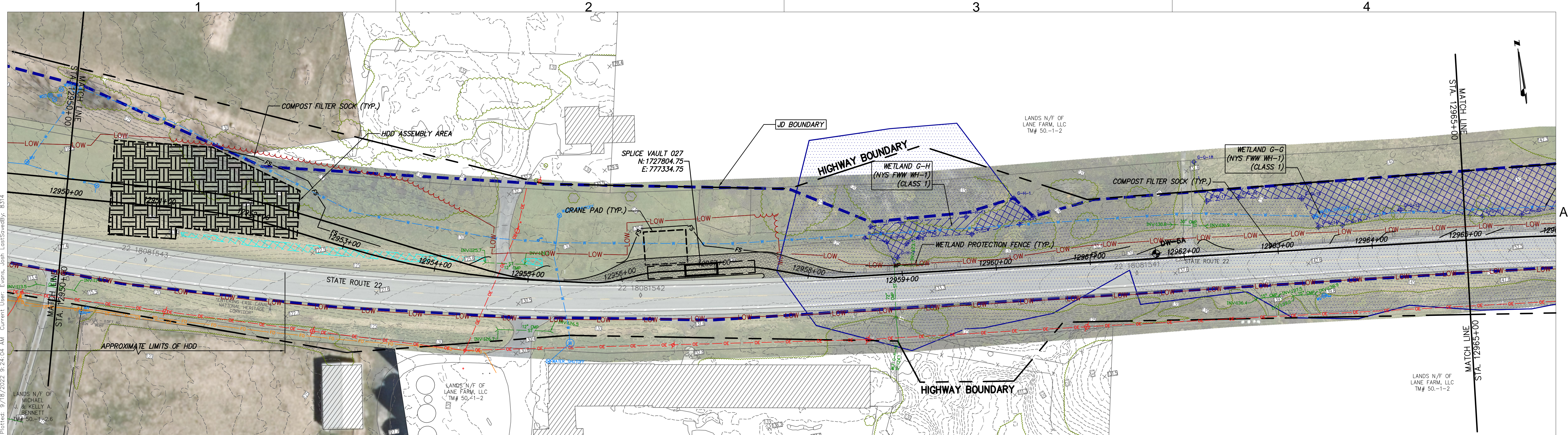
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
STA. 12920+00 TO STA. 12950+00 EROSION AND  
SEDIMENT CONTROL PLAN

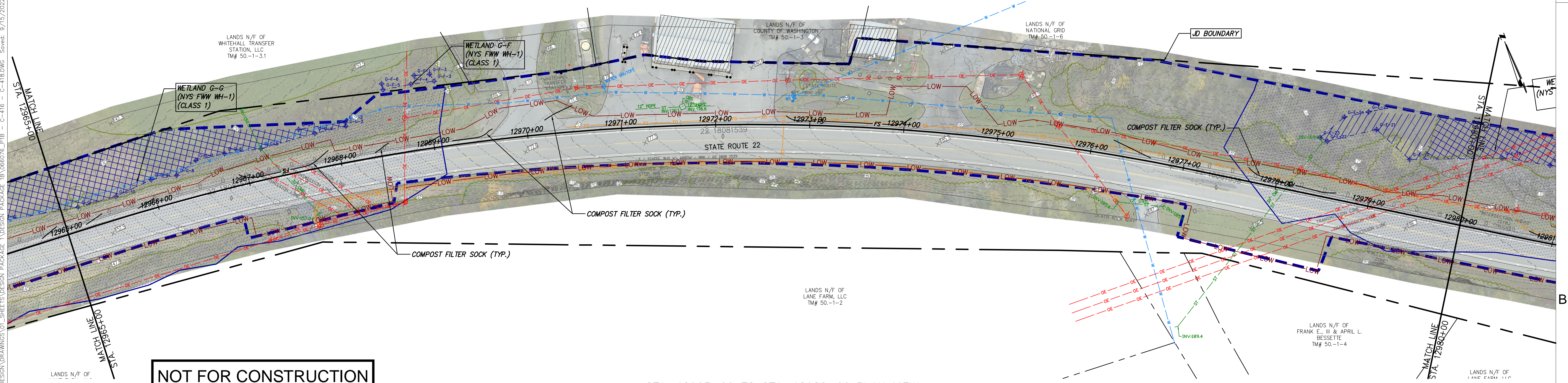
DRAWN BY:	JJE	DESIGNED BY:	JTM	APPROVED BY:	JPR	SCALE:	AS NOTED
						REV. NO.	X

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-415
DATE	09/21/2022
SH.NO.	XXX OF XXX






STA. 12950+00 TO STA. 12965+00 PLAN VIEW  
SCALE: 1" = 50'




STA. 12965+00 TO STA. 12980+00 PLAN VIEW  
SCALE: 1" = 50'


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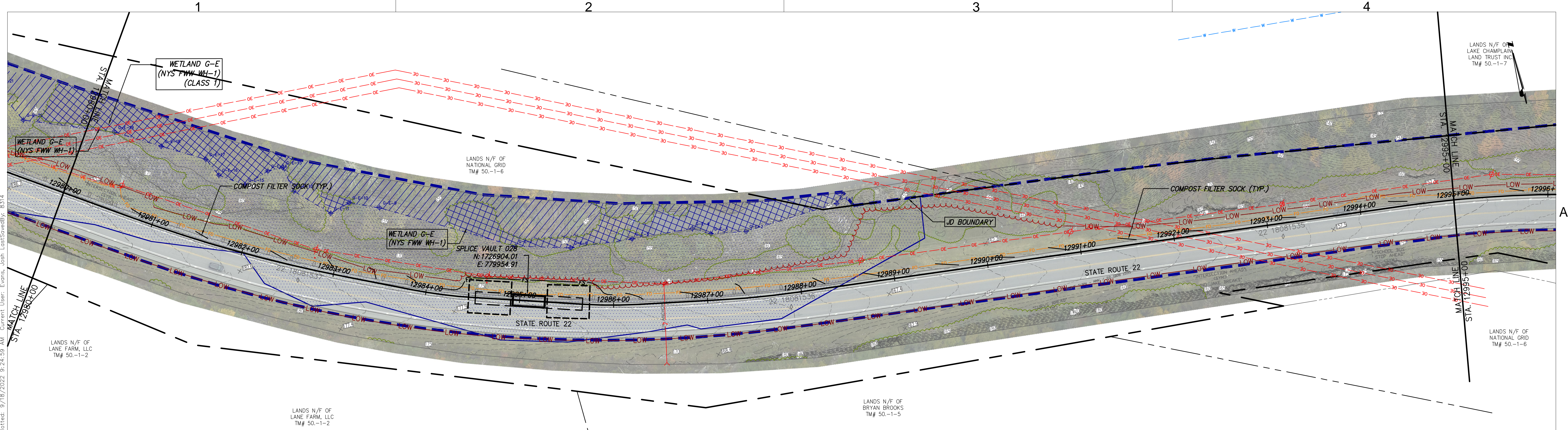
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
STA. 12950+00 TO STA. 12980+00 EROSION AND  
SEDIMENT CONTROL PLAN

DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR SCALE AS NOTED  
REV. NO. X SH.NO.

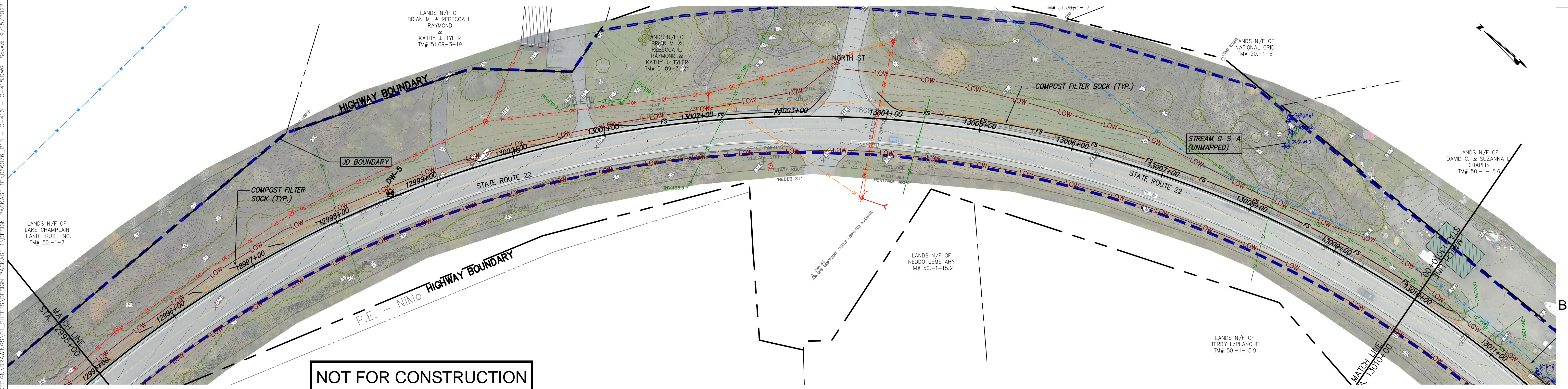
KIEWIT PROJECT NO. 21162  
CHA PROJECT NO. 066076  
DRAWING NO. C-416

DATE 09/21/2022  
XXX OF XXX






STA. 12980+00 TO STA. 12995+00 PLAN VIEW  
SCALE: 1" = 50'




STA. 12995+00 TO STA. 13010+00 PLAN VIEW  
SCALE: 1" = 50'


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08/19/2022

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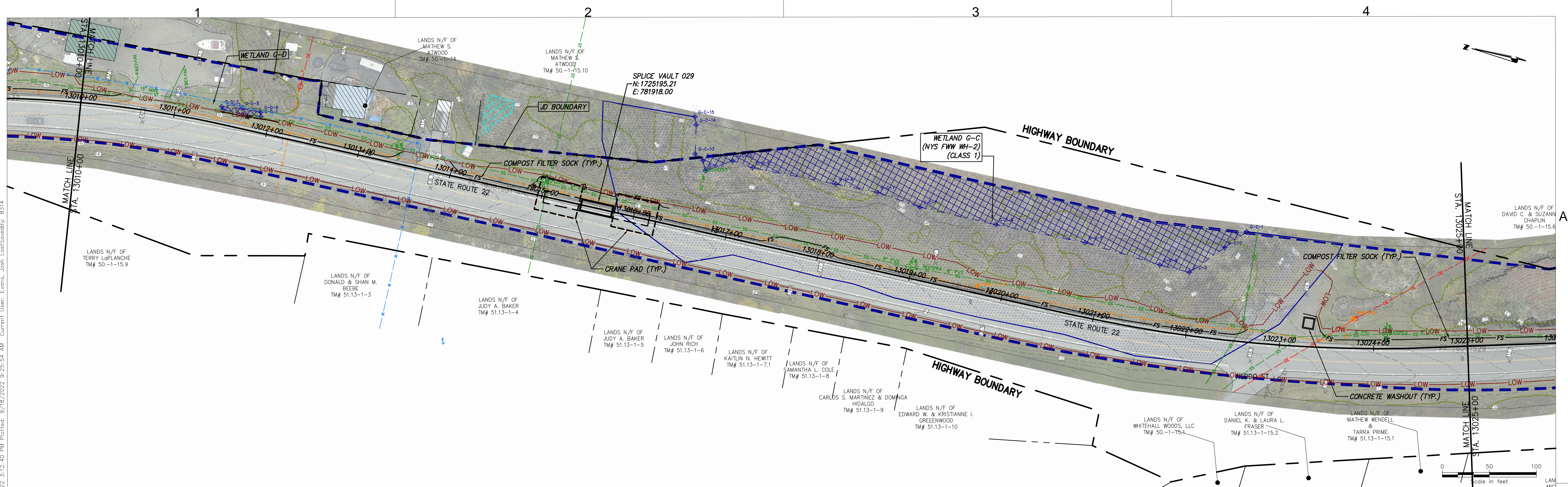
**CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
STA. 12980+00 TO STA. 13010+00 EROSION AND  
SEDIMENT CONTROL PLAN**

DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR SCALE: AS NOTED  
REV. NO. X SH.NO.

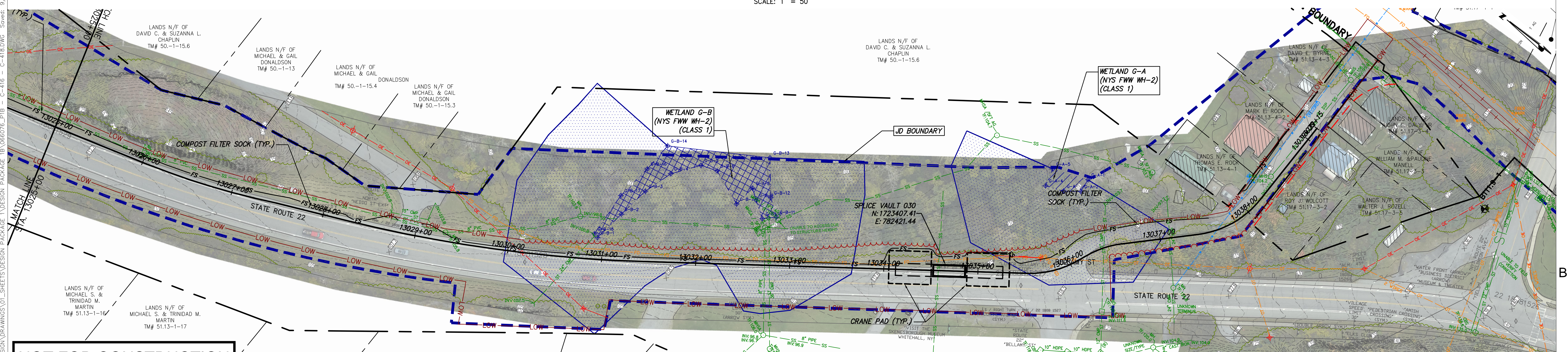
KIEWIT PROJECT NO. 21162  
CHA PROJECT NO. 066076  
DRAWING NO. C-417

DATE 09/21/2022  
XXX OF XXX






STA. 13010+00 TO STA. 13025+00 PLAN VIEW  
SCALE: 1" = 50'




STA. 13025+00 TO STA. 13040+00 PLAN VIEW  
SCALE: 1" = 50'


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JOSEPH P. ROMANO  
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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
STA. 13010+00 TO STA. 13040+00 EROSION AND  
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DRAWN BY:	JJE	DESIGNED BY:	JTM	APPROVED BY:	JPR	SCALE:	AS NOTED	DATE:	09/21/2022
						REV. NO.	X	SH. NO.	XXX OF XXX

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066078
DRAWING NO.	C-418



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NOTES:

1. ALL TRAFFIC CONTROL AND WORK AREA PROTECTIONS DEVICES SHALL BE FABRICATED AND INSTALLED IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION (FHWA) MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS, LATEST EDITION, EXCEPT AS NOTED.

2. ALL TEMPORARY TRAFFIC CONTROL AND WORK AREA PROTECTION DEVICES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED ON THE CONTRACT DRAWINGS AND APPROVED BY THE ENGINEER PRIOR TO COMMENCING WORK.

3. ALL MAINTENANCE AND PROTECTIONS OF TRAFFIC WORK SHALL CONFORM TO THE CONTRACT DRAWINGS, MAINTENANCE AND PROTECTION OF TRAFFIC SCHEMES SHOWN ON THE CONTRACT DRAWINGS SHALL NOT BE CHANGED BY THE CONTRACTOR WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.

4. THE TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION DEVICE LOCATIONS SHOWN ON THE CONTRACT DRAWINGS ARE SCHEMATIC EXCEPT AS NOTED. LOCATION OF TRAFFIC CONTROL DEVICES MAY BE MODIFIED TO MEET FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

5. ALL CLOSURES SHALL BE COORDINATED WITH THE ENGINEER. A SCHEDULE OF WORK SHALL BE SUBMITTED AT LEAST TWO WEEKS IN ADVANCE FOR APPROVAL BY THE ENGINEER.

6. THERE MAY BE ONGOING CONSTRUCTION CONTRACTS WITHIN THE VICINITY OF THE WORK AREA. DO NOT MOVE, MODIFY, OR RELOCATE ANY ITEM ASSOCIATED WITH THESE CONTRACTS WITHOUT PROPER APPROVAL OF AND COORDINATION WITH THE ENGINEER.

7. PERFORM WORK IN SUCH A MANNER AND SEQUENCE AS TO INTERFERE AS LITTLE AS POSSIBLE WITH THE PASSAGE OF VEHICLES, PEDESTRIANS, AND OTHER KINDS OF PUBLIC TRAFFIC.

8. ALL TEMPORARY MAINTENANCE OF TRAFFIC AND WORK AREA PROTECTION SIGN SUPPORTS AND MOUNTING SHALL BE IN CONFORMANCE WITH NYSDOT STANDARD SHEETS AND STANDARD SPECIFICATIONS.

9. ALL CONSTRUCTION SIGNS SHALL BE COVERED WITH THICK PLASTIC WHEN THE WORK THEY ARE INTENDED FOR IS NOT IN PROGRESS.

10. ALL TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF THE WORK TO WHICH THEY APPLY, AND SHALL BE MAINTAINED BY THE CONTRACTOR THEREAFTER. THE DEVICES SHALL REMAIN IN PLACE AS LONG AS THEY ARE APPLICABLE.

11. TRAFFIC CONTROL DEVICES SHALL NOT BE PLACED AT ANY LOCATIONS WHERE THEY MAY OBSCURE OR INTERFERE WITH THE MOTORIST VIEW OF APPROACHING, MERGING, OR INTERSECTING TRAFFIC; OBSTRUCT OTHER TEMPORARY OR PERMANENT TRAFFIC CONTROL DEVICES WHICH ARE STILL APPLICABLE TO ROADWAY CONDITIONS; MISLEAD OR MISDIRECT MOTORISTS, OR ARE BLOCKED BY OTHER TEMPORARY OR PERMANENT OBJECTS.

12. ALL EXISTING ROADWAY ITEMS SUCH AS GUIDE RAILS, PAVEMENT MARKINGS, CURBS, SIGNALS AND SIGNS DAMAGED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.

13. TRAFFIC LANES AND OTHER AREAS CLOSED BY THE CONTRACTOR DURING PERMITTED WORK HOURS SHALL BE CLEARED OF ALL MATERIAL, EQUIPMENT, AND DEBRIS, AND SAFELY REOPENED TO TRAFFIC BY THE END OF THE WORK PERIOD UNLESS OTHERWISE INDICATED ON THE CONTRACT DRAWINGS.

14. IF THE CONTRACTOR REQUIRES A LANE CLOSURE TO PERFORM OPERATIONS, THEY MAY DO SO WITH THE APPROVAL OF THE ENGINEER. ANY LANE CLOSURES WILL BE SHORT TERM AND IN ACCORDANCE WITH NYS STANDARD SHEETS UNLESS OTHERWISE SHOWN IN CONTRACT PLANS.

15. NO WORK ACTIVITY OR STORAGE OF EQUIPMENT, VEHICLES, OR MATERIAL SHOULD OCCUR WITHIN A BUFFER SPACE.

16. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.

17. WHEN THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND WORK IS 2 MILES TO 5 MILES, A SUPPLEMENTAL DISTANCE PLAQUE (W7-30) SHOULD BE USED WITH THE SHOULDER WORK SIGN (W21-5).

18. THE ROAD WORK NEXT XX MILES SIGN (G20-1) MAY BE USED INSTEAD OF THE ROAD WORK AHEAD SIGN (W20-1) IF WORK LOCATIONS OCCUR OVER A DISTANCE OF MORE THAN 2 MILES.

19. ALL ADDITIONAL TEMPORARY SIGNAGE, NOT COVERED IN FHWA MUTCD, SHALL COMPLY WITH NYS SUPPLEMENT TO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

20. DURING NON-WORK HOURS, LEAVE DRUMS AND TCB IN PLACE, OR AS DIRECTED AT THE ENGINEER.

21. DURING WINTER MORATORIUM, COVER EXCAVATION WITH TEMPORARY ASPHALT AND OPEN ALL LANES TO TRAFFIC.

22. THE WZTC DETAILS CONTAINED IN THE CONTRACT PLANS SUPPLEMENT THE CURRENT NYSDOT STANDARD SHEETS. REFERENCE SHALL BE MADE TO THE APPLICABLE NYSDOT STANDARD SHEETS FOR ALL NOTES AND TABLES. THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYSDOT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

23. MAINTAIN SAFE AND ADEQUATE ACCESS FOR INTERSECTING ROADWAYS, HOMES, AND BUSINESSES, AT ALL TIMES, TO THE SATISFACTION OF THE ENGINEER.

24. ACCESS TO RESIDENTIAL AND COMMERCIAL DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES. UNLESS OTHER AGREEMENTS SUITABLE TO THE PROPERTY OWNERS CAN BE MADE, PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. THE CONTRACTOR SHALL MAINTAIN ACCESS TO COMMERCIAL DRIVEWAYS AT ALL TIMES WHEN A FACILITY IS IN USE. FOR MULTIPLE ACCESS PROPERTIES, ONLY ONE DRIVEWAY MAY BE CLOSED AT ANY ONE TIME. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE, OR AS DIRECTED BY THE ENGINEER.

25. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE FOLLOWING HOLIDAY WORK RESTRICTIONS ARE APPLICABLE TO THIS PROJECT. DURING THESE HOLIDAY PERIODS, THE CONTRACTOR WILL NOT BE ALLOWED TO PERFORM ANY WORK THAT WILL BE DISRUPTIVE TO TRAFFIC, INCLUDING BUT NOT LIMITED TO LANE CLOSURES. LANE CLOSURES WILL NOT BE PERMITTED DURING THE FOLLOWING STATE RECOGNIZED HOLIDAYS:

1) LABOR DAY - MONDAY, SEPTEMBER 5, 2022

2) THANKSGIVING DAY - THURSDAY, NOVEMBER 24, 2022

3) CHRISTMAS DAY - SUNDAY, DECEMBER 25, 2022

4) NEW YEAR'S DAY - SUNDAY, JANUARY 1, 2023

5) MEMORIAL DAY - MONDAY, MAY 29, 2023

6) INDEPENDENCE DAY - TUESDAY, JULY 4, 2023

7) LABOR DAY - MONDAY, SEPTEMBER 4, 2023

8) THANKSGIVING DAY - THURSDAY, NOVEMBER 23, 2023

9) CHRISTMAS DAY - MONDAY, DECEMBER 25, 2023

10) NEW YEAR'S DAY - MONDAY, JANUARY 1, 2024

11) MEMORIAL DAY - MONDAY, MAY 27, 2024

12) INDEPENDENCE DAY - THURSDAY, JULY 4, 2024

13) LABOR DAY - MONDAY, SEPTEMBER 2, 2024

14) THANKSGIVING DAY - THURSDAY, NOVEMBER 28, 2024

15) CHRISTMAS DAY - WEDNESDAY, DECEMBER 25, 2024

16) NEW YEAR'S DAY - WEDNESDAY, JANUARY 1, 2025

17) MEMORIAL DAY - MONDAY, MAY 26, 2025

18) INDEPENDENCE DAY - FRIDAY, JULY 4, 2025

19) LABOR DAY - MONDAY, SEPTEMBER 1, 2025

20) THANKSGIVING DAY - THURSDAY, NOVEMBER 27, 2025

21) CHRISTMAS DAY - THURSDAY, DECEMBER 25, 2025

26. LANE CLOSURES SHALL BE SUSPENDED AS FOLLOWS:

- BEGINNING 6AM, FRIDAY SEPTEMBER 2, 2022 AND ENDING 6AM, TUESDAY SEPTEMBER 6, 2022

- BEGINNING 6AM, WEDNESDAY NOVEMBER 23, 2022 AND ENDING 6AM, MONDAY NOVEMBER 28, 2022

- BEGINNING 6AM, FRIDAY DECEMBER 23, 2022 AND ENDING 6AM, TUESDAY DECEMBER 27, 2022

- BEGINNING 6AM, FRIDAY DECEMBER 30, 2022 AND ENDING 6AM, TUESDAY JANUARY 3, 2023

- BEGINNING 6AM, FRIDAY MAY 26, 2023 AND ENDING 6AM, TUESDAY MAY 30, 2023

- BEGINNING 6AM, SATURDAY JULY 1, 2023 AND ENDING 6AM, WEDNESDAY JULY 5, 2023

- BEGINNING 6AM, FRIDAY SEPTEMBER 1, 2023 AND ENDING 6AM, TUESDAY SEPTEMBER 5, 2023

- BEGINNING 6AM, WEDNESDAY NOVEMBER 22, 2023 AND ENDING 6AM, MONDAY NOVEMBER 27, 2023

- BEGINNING 6AM, FRIDAY DECEMBER 22, 2023 AND ENDING 6AM, TUESDAY DECEMBER 26, 2023

- BEGINNING 6AM, SUNDAY DECEMBER 30, 2023 AND ENDING 6AM, WEDNESDAY JANUARY 3, 2024

- BEGINNING 6AM, FRIDAY MAY 24, 2024 AND ENDING 6AM, TUESDAY MAY 28, 2024

- BEGINNING 6AM, MONDAY JULY 1, 2024 AND ENDING 6AM, FRIDAY JULY 5, 2024

- BEGINNING 6AM, FRIDAY AUGUST 30, 2024 AND ENDING 6AM, TUESDAY SEPTEMBER 3, 2024

- BEGINNING 6AM, WEDNESDAY NOVEMBER 27, 2024 AND ENDING 6AM, MONDAY DECEMBER 2, 2024

- BEGINNING 6AM, FRIDAY DECEMBER 20, 2024 AND ENDING 6AM, THURSDAY DECEMBER 26, 2024

- BEGINNING 6AM, TUESDAY DECEMBER 30, 2024 AND ENDING 6AM, FRIDAY JANUARY 3, 2025

- BEGINNING 6AM, FRIDAY MAY 23, 2025 AND ENDING 6AM, TUESDAY MAY 27, 2025

- BEGINNING 6AM, THURSDAY JULY 3, 2025 AND ENDING 6AM, MONDAY JULY 7, 2025

- BEGINNING 6AM, FRIDAY AUGUST 29, 2025 AND ENDING 6AM, TUESDAY SEPTEMBER 2, 2025

- BEGINNING 6AM, WEDNESDAY NOVEMBER 26, 2025 AND ENDING 6AM, MONDAY DECEMBER 1, 2025

- BEGINNING 6AM, WEDNESDAY DECEMBER 24, 2025 AND ENDING 6AM, MONDAY DECEMBER 29, 2025

27. DURING WINTER STORM EVENTS, NO WORK WITHIN THE ROAD RIGHT-OF-WAY IS PERMITTED TO ALLOW FOR SNOW REMOVAL, PLOWING ACTIVITIES.

28. ANY EARTHWORK DONE BETWEEN NOVEMBER 1ST AND APRIL 1ST MUST CONFORM TO NYSDOT STANDARD SPECIFICATION SECTION 203-1.01 P.

29. ALL OPEN TRENCH EXCAVATIONS SHALL BE BACKFILLED OR COVERED BY A STEEL PLATE (HS-20 LOAD RATED) AT THE END OF EACH WORK DAY, OR AS DIRECTED BY THE ENGINEER.

LEGEND

TEMPORARY SIGN

TEMPORARY TRAFFIC SIGNAL UNIT

WORK ZONE

DRUM

TEMPORARY CONCRETE BARRIER (TCB)

FLAGGER

FLAG TREE

WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR

FLASH UNIT (LEFT ARROW)

FLASH UNIT (CAUTION MODE)

TYPE III BARRICADE

TRAILER UNIT

WARNING LIGHT ON SIGN

TEMPORARY TRAFFIC FLOW ARROW

ABBREVIATIONS

AADT	AVERAGE ANNUAL DAILY TRAFFIC
CR	COUNTY ROUTE
DHV	DIRECTIONAL HOURLY VOLUME
FASU	FLASHING ARROW SIGN UNIT
FHWA	FEDERAL HIGHWAY ADMINISTRATION
M.P.	MILE POST
MAX.	MAXIMUM
MIN.	MINIMUM
MPH	MILES PER HOUR
MUTCD	MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
NYS	NEW YORK STATE
NYSDOT	NEW YORK STATE DEPARTMENT OF TRANSPORTATION
RTE	ROUTE
TCB	TEMPORARY CONCRETE BARRIER
STA.	STATION

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

WORK ZONE TRAFFIC CONTROL NOTES  
LEGEND AND ABBREVIATIONS

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-501
DATE SH.NO.	09/21/2022 OF XXX

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REV. NO.			

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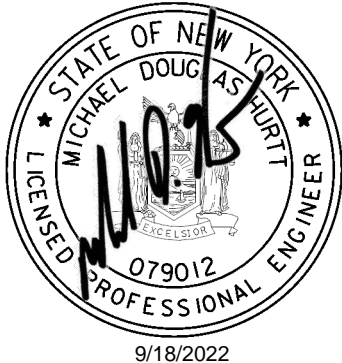
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SEGMENT 2 PACKAGE 1B WORK ZONE TRAFFIC CONTROL MAIN STAGES							
MAIN STAGE	STA. START	STA. END	ROUTE(S)	CLOSURE TYPE	PLAN SHEET	TRAFFIC COUNTS (AADT)	WORK NOTES
1	12500+00	12516+75	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
2	12516+75	12517+60	NYS RTE 22/DRESDEN ROAD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
3	12517+60	12537+50	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
4	12531+45		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	1780 (2019)	SPLICE VAULT INSTALLATION
5	12537+50	12538+00	NYS RTE 22/LECLAIRE RD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
6	12538+00	12555+00	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
7	12555+00	12582+50	NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-505 / C-510	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
8	12557+35		NYS RTE 22	LANE CLOSURE WITH BARRIER	C-510	1780 (2019)	SPLICE VAULT INSTALLATION
9	12582+50	12606+95	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
10	12587+60		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	1780 (2019)	SPLICE VAULT INSTALLATION
11	12606+90	12607+50	NYS RTE 22/OTTENBURGH ROAD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
12	12607+50	12613+80	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
13	12613+80	12614+25	NYS RTE 22/BOSS ROAD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
14	12614+25	12639+50	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
15	12616+50		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	1780 (2019)	SPLICE VAULT INSTALLATION
16	12639+50	12639+90	NYS RTE 22/GILLIS RD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
17	12639+90	12666+90	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
18	12648+45		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	1780 (2019)	SPLICE VAULT INSTALLATION
19	12666+90	12667+70	NYS RTE 22/NORTH RD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
20	12667+70	12671+00	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
21	12671+00	12671+70	NYS RTE 22/CLEMONS CENTER ROAD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
22	12671+70	12691+00	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	1780 (2019)	TRENCHING & CONDUIT INSTALLATION
23	12677+65		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	1780 (2019)	SPLICE VAULT INSTALLATION
24	12691+00	12692+50	NYS RTE 22/CLEMONS CENTER ROAD/CR 6	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
25	12692+50	12716+75	NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-505 / C-511	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
26	12696+85		NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-511	2773 (2019)	SPLICE VAULT INSTALLATION
27	12716+75	12717+50	NYS RTE 22/STEELE ROAD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
28	12717+50	12737+00	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
29	12727+20		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
30	12737+00	12744+00	PARKING AREA/GREEN SPACE	WORK OFF ROAD WZTC	C-509		TRENCHING & CONDUIT INSTALLATION
31	12744+00	12746+10	NYS RTE 22/CRUSHER HILL RD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
32	12746+10	12763+50	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
34	12759+50		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
33	12763+50	12786+70	NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-505 / C-511	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
35	12786+70	12787+40	NYS RTE 22/CATDEN RD	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
36	12787+40	12799+00	NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-505 / C-511	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
37	12789+60		NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-511	2773 (2019)	SPLICE VAULT INSTALLATION
38	12799+00	12886+00	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
39	12821+70		NYS RTE 22	SHOULDER CLOSURE (USING TEMPORARY CONCRETE BARRIER)	C-504	2773 (2019)	SPLICE VAULT INSTALLATION
40	12851+10		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
42	12883+10		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
43	12886+00	12914+00	NYS RTE 22	LANE CLOSURE WITH TWO WAY TRAFFIC	C-505 / C-511	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
44	12914+00	12920+75	GREEN SPACE	WORK OFF ROAD WZTC	C-509	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
45	12914+80		GREEN SPACE	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
46	12920+75	12951+75	CR 7A / GREEN SPACE / NYS RTE 22	WORK OFF ROAD WZTC	C-509	2773 (2019)	HORIZONTAL DIRECTIONAL DRILLING
47	12951+25	12954+50	GREEN SPACE	WORK OFF ROAD WZTC	C-509	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
48	12954+50	12976+25	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
49	12956+85		NYS RTE 22	SHOULDER CLOSURE (USING TEMPORARY CONCRETE BARRIER)	C-504	2773 (2019)	SPLICE VAULT INSTALLATION
50	12976+25	12976+90	NYS RTE 22/NEDDO ST	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
51	12976+90	13003+25	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
52	12985+10		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
53	13003+25	13004+10	NYS RTE 22/NORTH ST	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
54	13004+10	13022+40	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
55	13015+60		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
56	13022+40	13023+05	NYS RTE 22/NEDDO ST	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
57	13023+05	13028+70	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
58	13028+70	13029+15	NYS RTE 22/NEDDO ST	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
59	13029+15	13035+70	NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
60	13034+70		NYS RTE 22	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TEMPORARY SIGNAL	C-507	2773 (2019)	SPLICE VAULT INSTALLATION
61	13035+70	13036+10	NYS RTE 22/BELLAMY STREET	LANE CLOSURE AT OR NEAR INTERSECTION	C-508	2773 (2019)	TRENCHING & CONDUIT INSTALLATION
62	13036+10	13039+00	BELLAMY STREET	LANE CLOSURE WITH ALTERNATING ONE WAY TRAFFIC - TYPICAL SIDE ROAD	C-503 / C-507	2773 (2019)	TRENCHING & CONDUIT INSTALLATION

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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

WORK ZONE TRAFFIC CONTROL

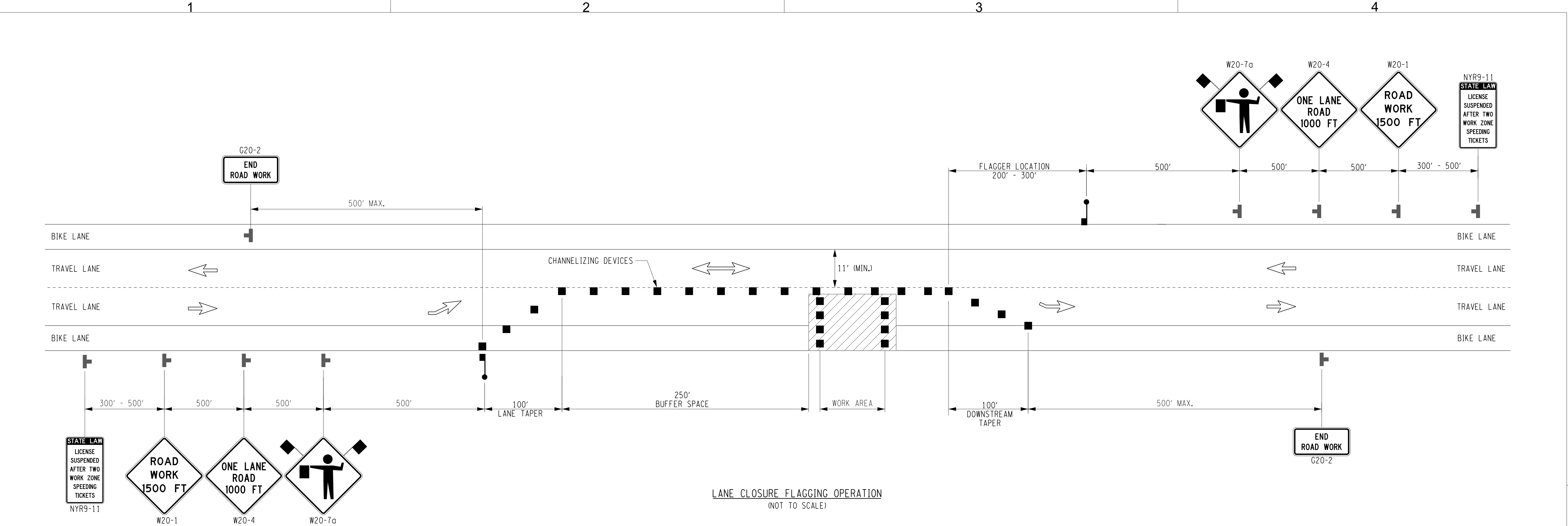
KIEWIT PROJECT NO.  
21162  
CHA PROJECT NO.  
066076  
DRAWING NO.

C-502

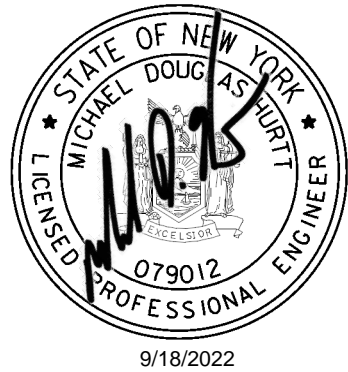
DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV.NO.	AS SHOWN	DATE SH.NO.	09/21/2022 OF XXX
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

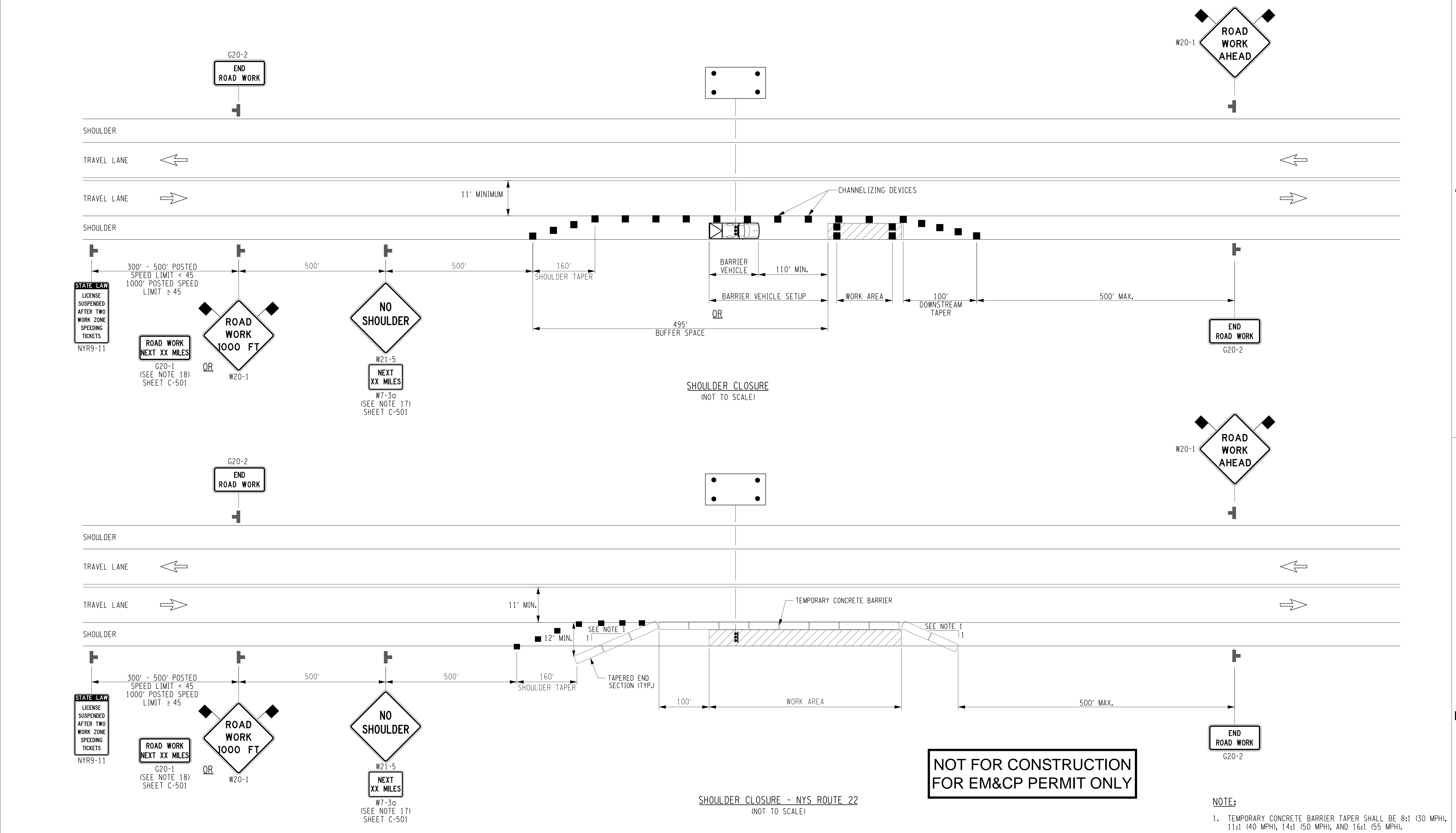
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 1 OF 9


DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE AS SHOWN REV. NO.	DATE 09/21/2022 SH.NO. OF XXX
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KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 066076
DRAWING NO. C-503




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Champlain Hudson  
Power Express

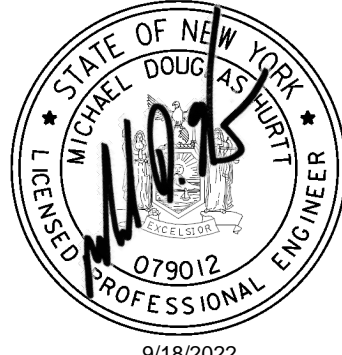


Kiewit



CHA

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9/18/2022

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0	09/21/2022	FINAL EM&CP SUBMISSION	JS	MH

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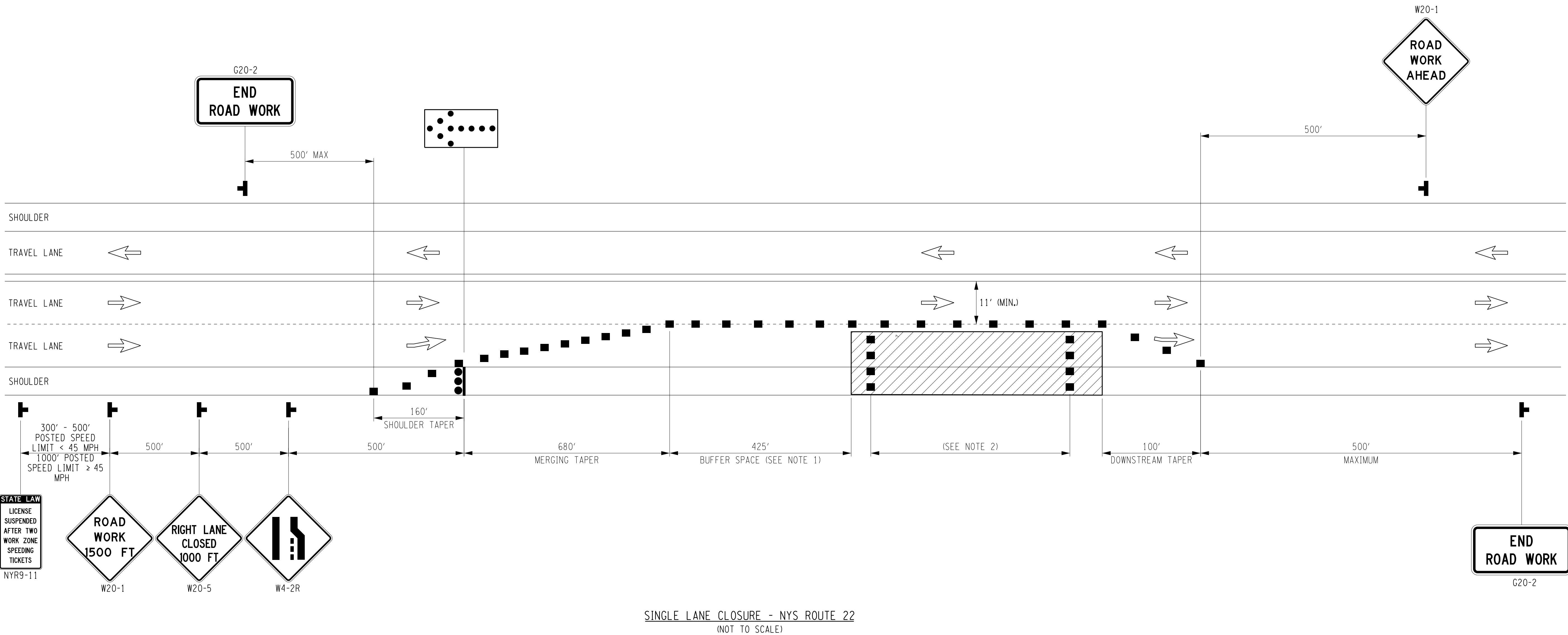
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 2 OF 9

DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV. NO.	AS SHOWN
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KIEWIT PROJECT NO. 21162	
CHA PROJECT NO. 066076	
DRAWING NO. <b>C-504</b>	
DATE SH.NO.	09/21/2022 OF XXX



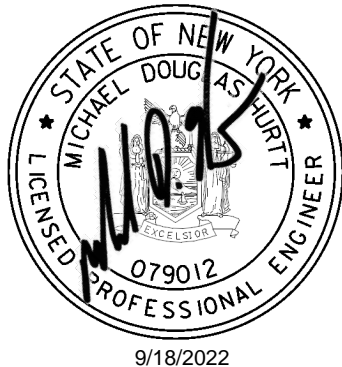
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SINGLE LANE CLOSURE - NYS ROUTE 22  
(NOT TO SCALE)

- NOTES:
1. NO WORK ACTIVITY, EQUIPMENT OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
  2. TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.

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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

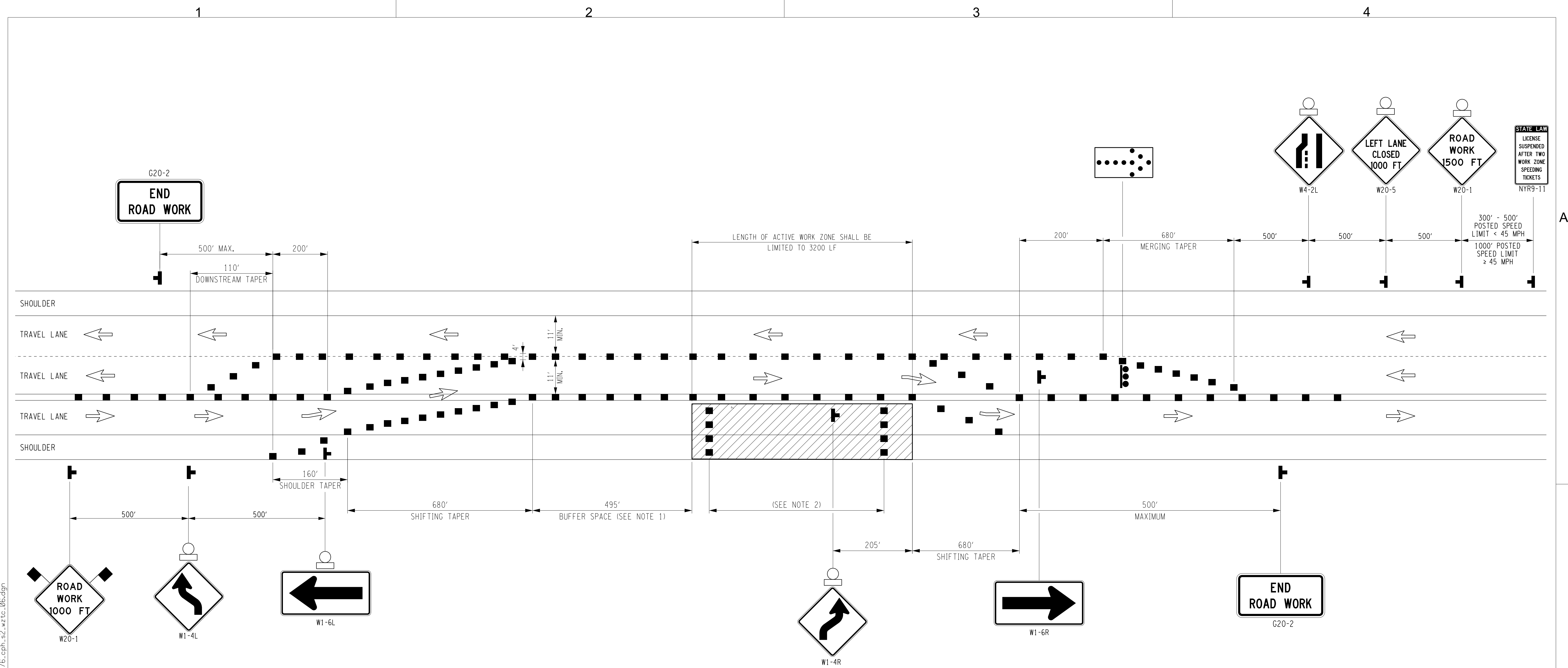
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 3 OF 9

DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE AS SHOWN REV. NO.	DATE 09/21/2022 SH.NO. OF XXX
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KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 066076
DRAWING NO. C-505



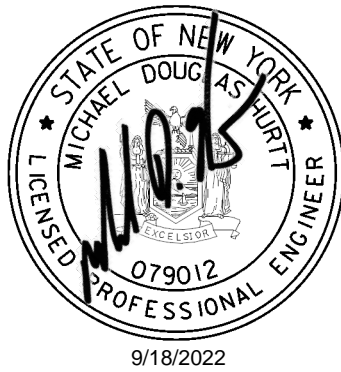
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NOTES:

1. NO WORK ACTIVITY, EQUIPMENT OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.
2. TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 1500'.

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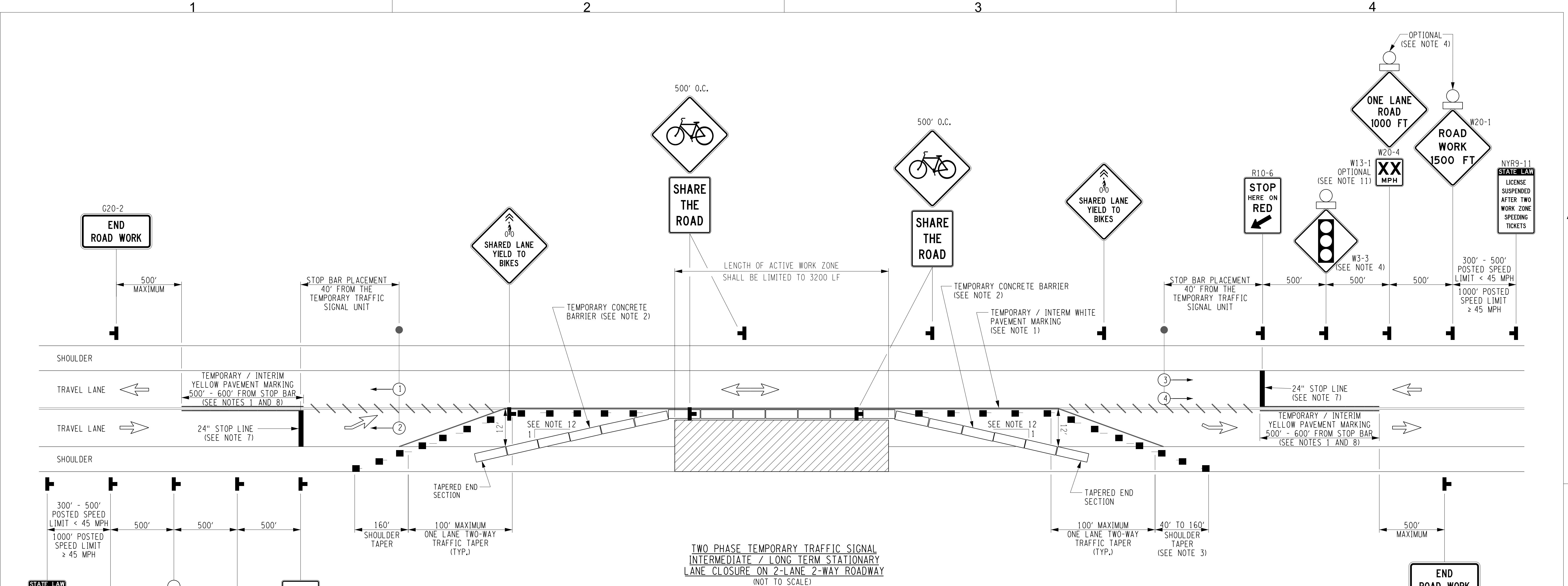
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 4 OF 9

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-506
DATE	09/21/2022
SH.NO.	OF XXX

DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE	AS SHOWN	DATE	09/21/2022
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TWO PHASE TEMPORARY TRAFFIC SIGNAL  
INTERMEDIATE / LONG TERM STATIONARY  
LANE CLOSURE ON 2-LANE 2-WAY ROADWAY  
(NOT TO SCALE)

NOTES:

- FOR LONG DURATION LANE CLOSURES, TEMPORARY / INTERIM PAVEMENT MARKINGS SHALL BE USED IN ACCORDANCE TO THE CONTRACT DOCUMENTS. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.
- THE TEMPORARY CONCRETE BARRIER SHALL NOT BE PLACED ALONG THE MERGING TAPER. THE LANE SHALL BE CLOSED USING CHANNELIZING DEVICES AND PAVEMENT MARKINGS.
- WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.
- REFER TO MUTCD FOR ADDITIONAL TRAFFIC SIGNAL REQUIREMENTS.
- DETECTORS SHALL BE INSTALLED ON ALL APPROACH LANES AND ALL PHASES SHALL BE ACTUATED. DURATION OF RED CLEARANCE INTERVALS SHALL BE ADEQUATE TO CLEAR THE ONE-LANE SECTION OF CONFLICTING TRAFFIC.
- THE TRAFFIC SIGNAL CYCLE SHALL REST IN RED. WHEN THE SIGNAL IS CHANGED TO THE FLASHING MODE, EITHER MANUALLY OR AUTOMATICALLY, RED SIGNAL INDICATIONS SHALL BE FLASHED TO ALL APPROACHES.
- STOP LINES SHALL BE INSTALLED WITH TEMPORARY TRAFFIC SIGNALS FOR INTERMEDIATE AND LONG-TERM CLOSURES. EXISTING, CONFLICTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKINGS, BETWEEN THE ACTIVITY AREA AND THE STOP LINE, SHALL BE REMOVED. AFTER THE TEMPORARY TRAFFIC SIGNAL IS REMOVED, THE STOP LINES AND ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND PERMANENT PAVEMENT MARKINGS RESTORED.
- WHERE NO-PASSING LINES ARE NOT ALREADY IN PLACE, THEY SHALL BE ADDED FOR A DISTANCE OF 500'-600' FROM THE STOP BAR. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.
- ADJUSTMENTS IN THE LOCATION OF ADVANCED WARNING SIGNS SHOULD BE MADE TO ACCOMMODATE THE HORIZONTAL AND VERTICAL ALIGNMENT OF THE ROADWAY, RECOGNIZING THAT THE DISTANCES SHOWN FOR SIGN SPACING ARE MINIMUMS. ADJUSTMENTS IN THE HEIGHT OF THE SIGNAL HEADS SHALL BE MADE AS NEEDED TO CONFORM TO THE VERTICAL ALIGNMENT.
- THE SUPPORTS FOR TEMPORARY TRAFFIC CONTROL SIGNALS SHALL NOT ENROACH INTO THE MINIMUM REQUIRED WIDTH OF A "PEDESTRIAN ACCESS ROUTE" OF 4' OR AN "ALTERNATE CIRCULATION PATH" OF 3'.
- THE ENGINEER MAY REQUIRE THE USE OF AN ADVISORY SPEED PLAQUE (W13-1) TO SUPPLEMENT A WARNING SIGN. THE PLAQUE WILL BE USED TO INDICATE AN ADVISORY SPEED FOR THE WORK ZONE CONDITION. (IE. NARROW LANES, BUMPS, POOR ROADWAY SURFACE, LOW OR NO SHOULDER, DROP-OFFS, GEOMETRIC CONSTRAINTS, AND/OR POOR SIGHT CONDITIONS).
- TEMPORAY CONCRETE BARRIER TAPER SHALL BE 8:1 (30 MPH), 11:1 (40 MPH), 14:1 (50 MPH), AND 16:1 (55 MPH).
- LIMITS OF LONG-TERM LANE CLOSURE WITH TEMPORARY CONCRETE BARRIER SHALL BE ESTABLISHED BETWEEN EXISTING DRIVEWAYS AND SIDEROADS; UNLESS TEMPORARY SIGNALIZATION OF EXISTING DRIVEWAYS AND/OR SIDE ROADS CAN BE PROVIDED AND COORDINATED WITH THE MAINLINE TEMPORARY SIGNALS, AS DIRECTED BY THE ENGINEER.
- LONG-TERM LANE CLOSURES ARE ANTICIPATED TO BE IN PLACE AT ANY ONE LOCATION FOR APPROXIMATELY ONE WEEK ( 7 DAYS).
- WORK SPACE LIMITS SHALL BE ESTABLISHED TO ALLOW FOR STANDARD DEFLECTION OF TEMPORARY CONCRETE BARRIER AS DEFINED IN SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS.

NOTE: TEMPORARY SIGNAL TIMING SHALL BE DETERMINED BY THE REGIONAL TRAFFIC OFFICE, COUNTY, OR MUNICIPALITY

OPERATIONAL SCHEDULE FOR TEMPORARY TRAFFIC SIGNALS					
PHASE	FACE	1	2	3	4
1		G	G	R	R
CLEARANCE		Y	Y	R	R
ALL RED		R	R	R	R
2		R	R	G	G
CLEARANCE		R	R	Y	Y
ALL RED		R	R	R	R
EMERGENCY FLASH		FR	FR	FR	FR

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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

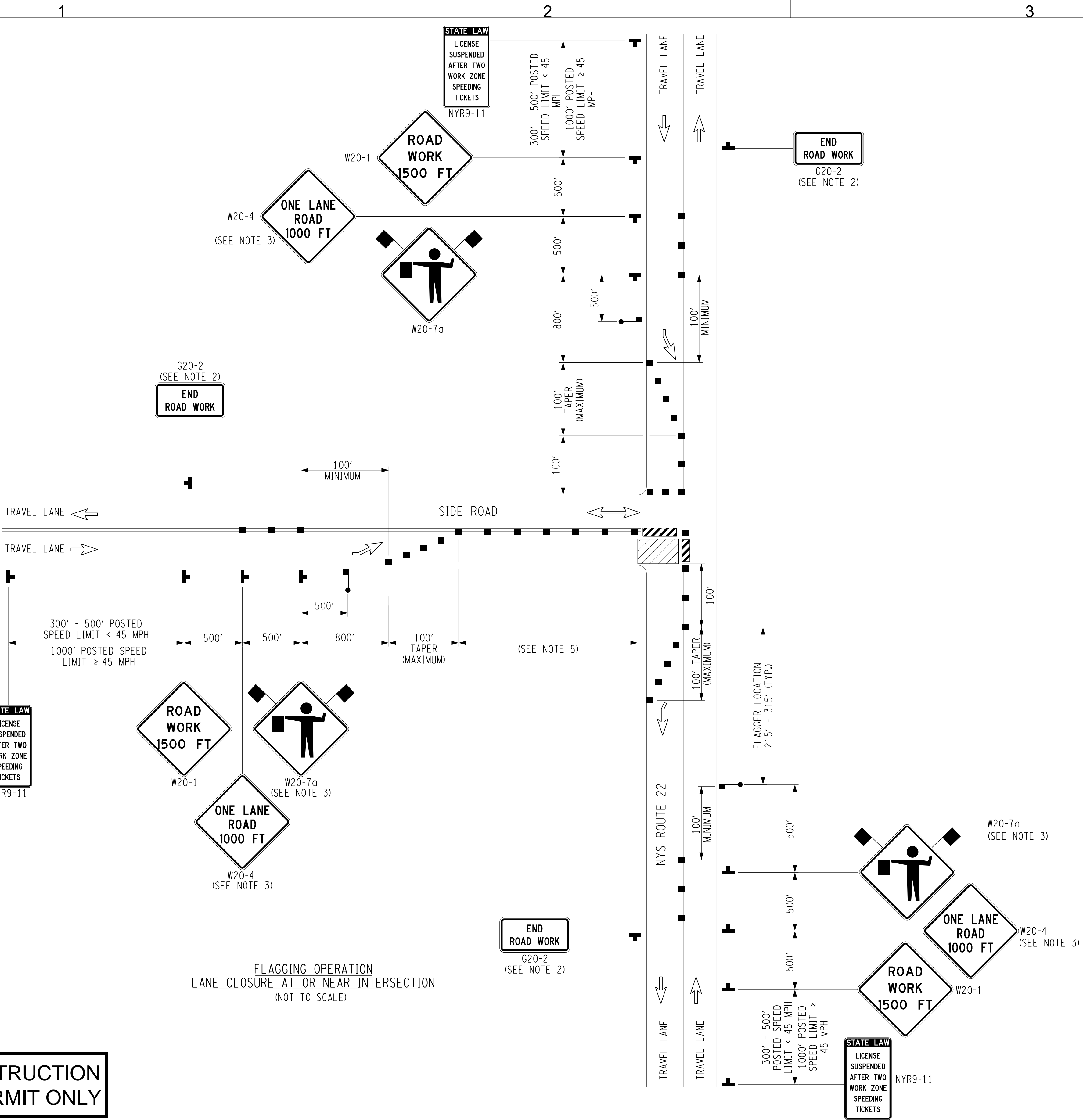
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 5 OF 9

DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE AS SHOWN REV. NO.	DATE 09/21/2022 SH.NO. OF XXX
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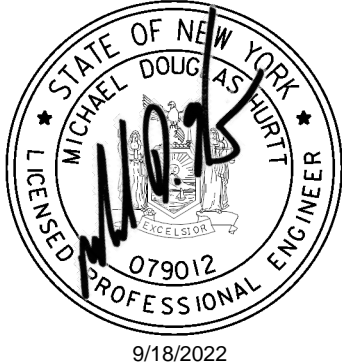
KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 066076
DRAWING NO. C-507



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- NOTES:
1. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 20' IN THE ACTIVE WORK SPACE.
  2. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WORK SPACE.
  3. FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.
  4. ALL FLAGGERS SHALL USE 24" (MIN.) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6" STAFF.
  5. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK AREA.
  6. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLES, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME.



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 6 OF 9

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-508
DATE	09/21/2022
SH.NO.	OF XXX





DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE	AS SHOWN	DATE	09/21/2022
			REV. NO.		SH.NO.	OF XXX



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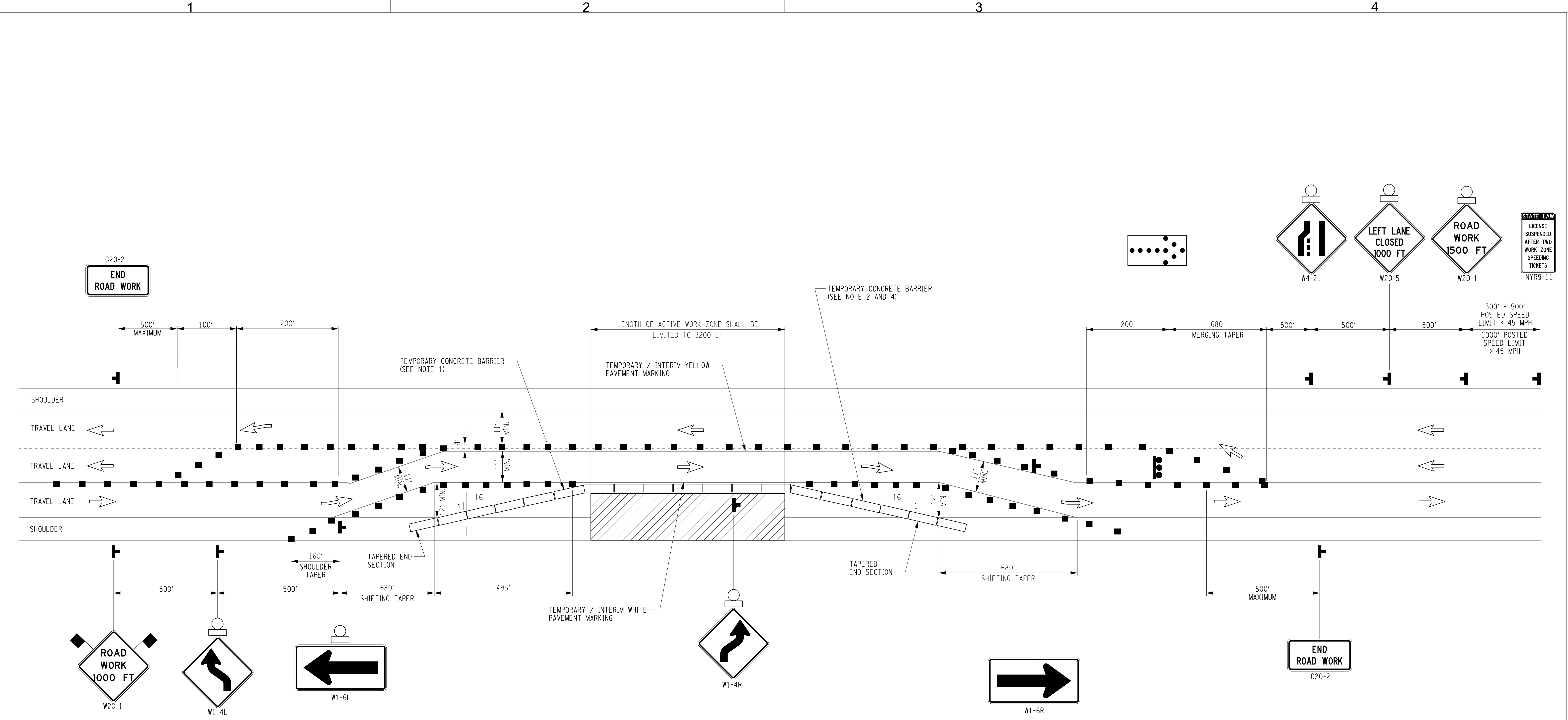


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 <b>CHPE</b> Champlain Hudson Power Express	 <b>Kiewit</b>	 <b>CHA</b> III Winners Circle, PO Box 5269 Albany, NY 12205-0269 518.453.4500 • www.chacompanies.com	 MICHAEL DOUGLAS 079012 LICENSED PROFESSIONAL ENGINEER 9/18/2022	IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.	<table><tr><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>0</td><td>09/21/2022</td><td>FINAL EM&amp;CP SUBMISSION</td><td>JS</td><td>MH</td><td></td></tr><tr><td>No.</td><td>DATE</td><td>SUBMITTAL / REVISION DESCRIPTION</td><td>DB</td><td>APP</td><td></td></tr></table>																																					0	09/21/2022	FINAL EM&CP SUBMISSION	JS	MH		No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP		<table><tr><td colspan="4"><b>CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL</b></td></tr><tr><td colspan="4">WORK ZONE TRAFFIC CONTROL DETAILS SHEET 7 OF 9</td></tr><tr><td>DRAWN BY: JAH</td><td>DESIGNED BY: JPS</td><td>APPROVED BY: MDH</td><td>SCALE AS SHOWN REV. NO.</td></tr></table>	<b>CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL</b>				WORK ZONE TRAFFIC CONTROL DETAILS SHEET 7 OF 9				DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE AS SHOWN REV. NO.	<table><tr><td>KIEWIT PROJECT NO. 21162</td></tr><tr><td>CHA PROJECT NO. 066076</td></tr><tr><td>DRAWING NO. <b>C-509</b></td></tr><tr><td>DATE 09/21/2022 SH.NO. OF XXX</td></tr></table>	KIEWIT PROJECT NO. 21162	CHA PROJECT NO. 066076	DRAWING NO. <b>C-509</b>	DATE 09/21/2022 SH.NO. OF XXX
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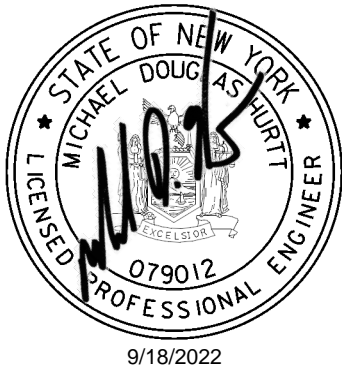
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NYS RTE 22  
INTERMEDIATE / LONG TERM STATIONARY  
LANE CLOSURE ON 3-LANE 2-WAY ROADWAY  
(NOT TO SCALE)

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- NOTES:
1. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.
  2. LIMITS OF LONG-TERM LANE CLOSURE WITH TEMPORARY CONCRETE BARRIER SHALL BE ESTABLISHED BETWEEN EXISTING DRIVEWAYS AND SIDEROADS; UNLESS TEMPORARY SIGNALIZATION OF EXISTING DRIVEWAYS CAN BE PROVIDED AND COORDINATED WITH THE MAINLINE TEMPORARY SIGNALS, AS DIRECTED BY THE ENGINEER.
  3. LONG-TERM LAND CLOSURES ARE ANTICIPATED TO BE IN PLACE AT ANY ONE LOCATION FOR APPROXIMATELY ONE WEEK (7 DAYS).
  4. WORK SPACE LIMITS SHALL BE ESTABLISHED TO ALLOW FOR STANDARD DEFLECTION OF TEMPORARY CONCRETE BARRIER AS DEFINED IN SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS.



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

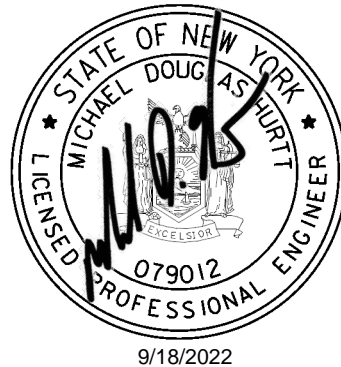
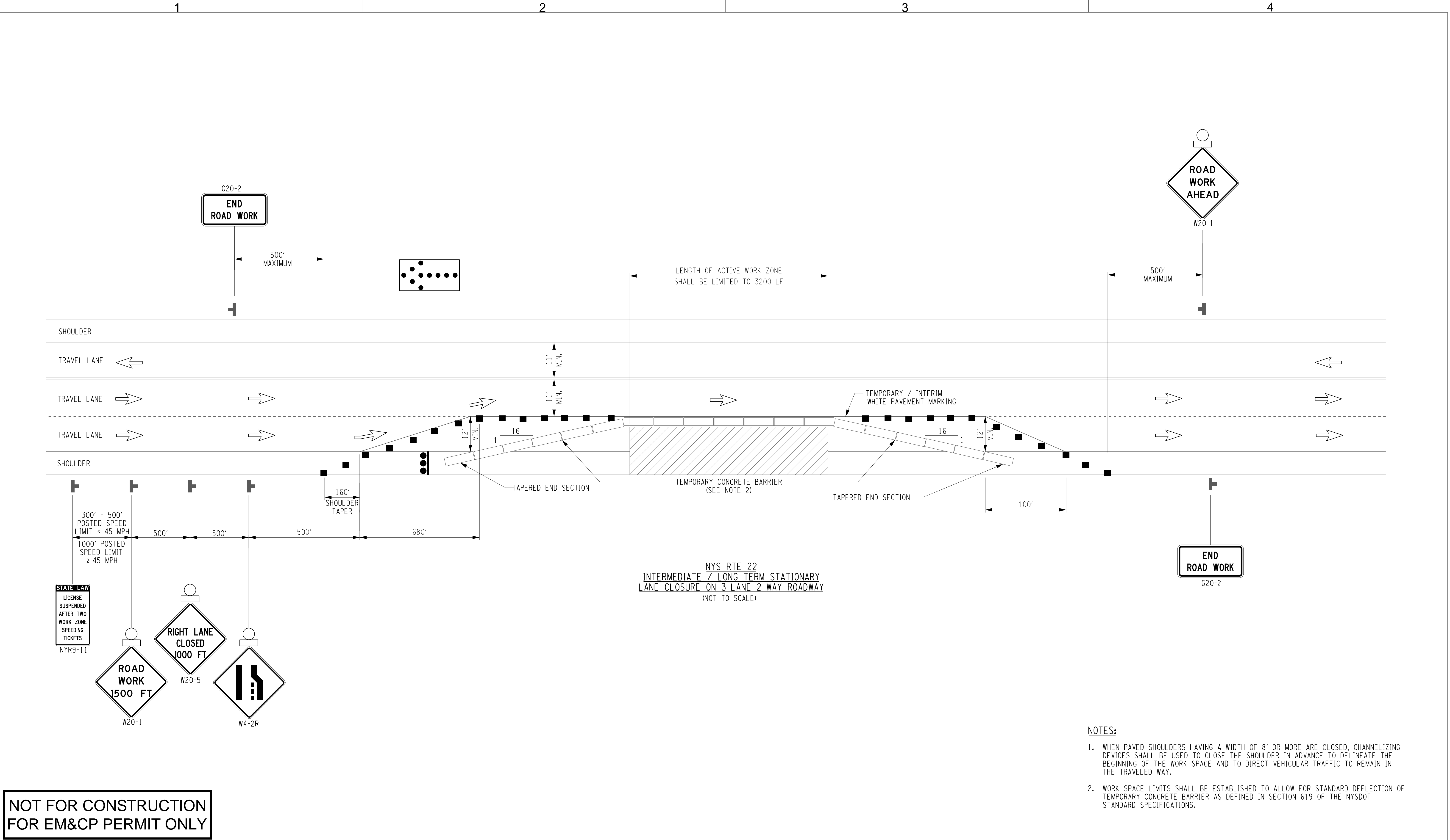
CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 8 OF 9

DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE AS SHOWN REV. NO.	DATE 09/21/2022 SH.NO. OF XXX
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KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 066076
DRAWING NO. C-510



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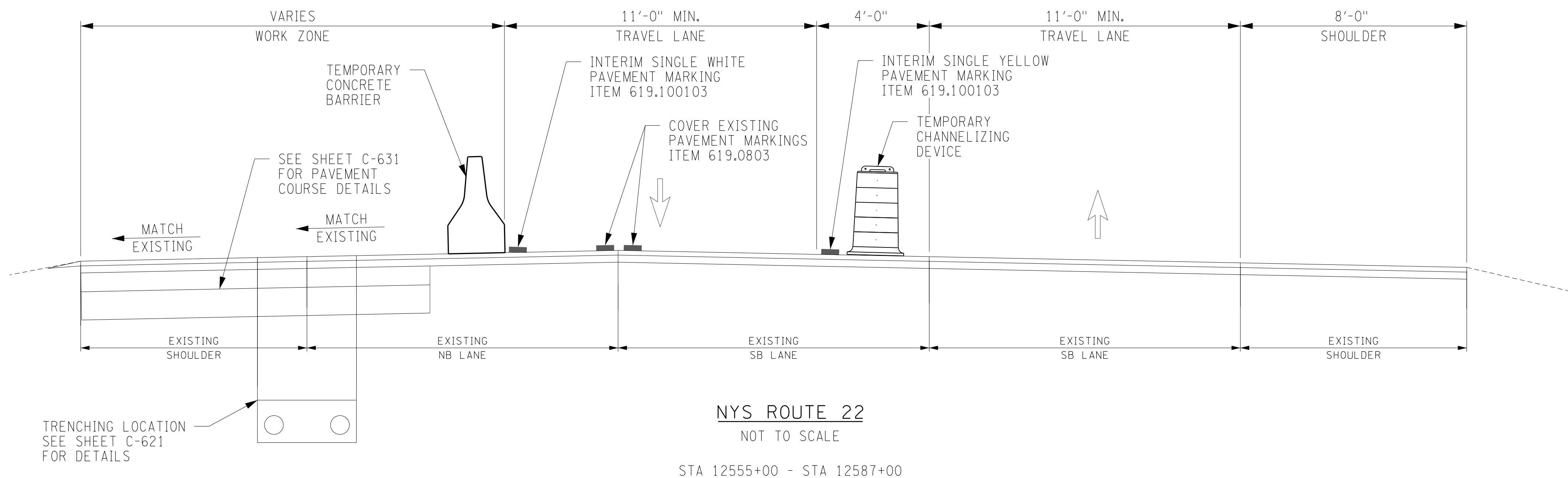
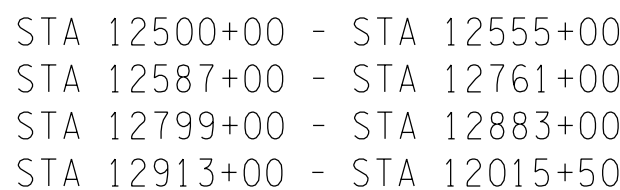
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
WORK ZONE TRAFFIC CONTROL DETAILS  
SHEET 9 OF 9

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-511
DATE	09/21/2022
SH.NO.	OF XXX

- NOTES:
- WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.
  - WORK SPACE LIMITS SHALL BE ESTABLISHED TO ALLOW FOR STANDARD DEFLECTION OF TEMPORARY CONCRETE BARRIER AS DEFINED IN SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS.



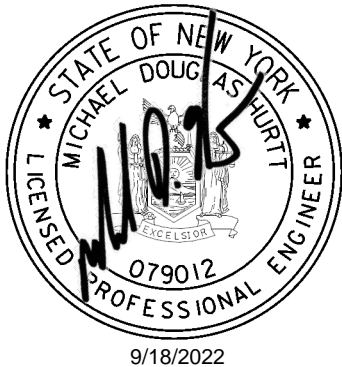


KIEWIT PROJECT NO.  
21162  
CHA PROJECT NO.  
066076  
DRAWING NO.  
**C-512**  
DATE 09/21/2022  
SH.NO. OF XXX



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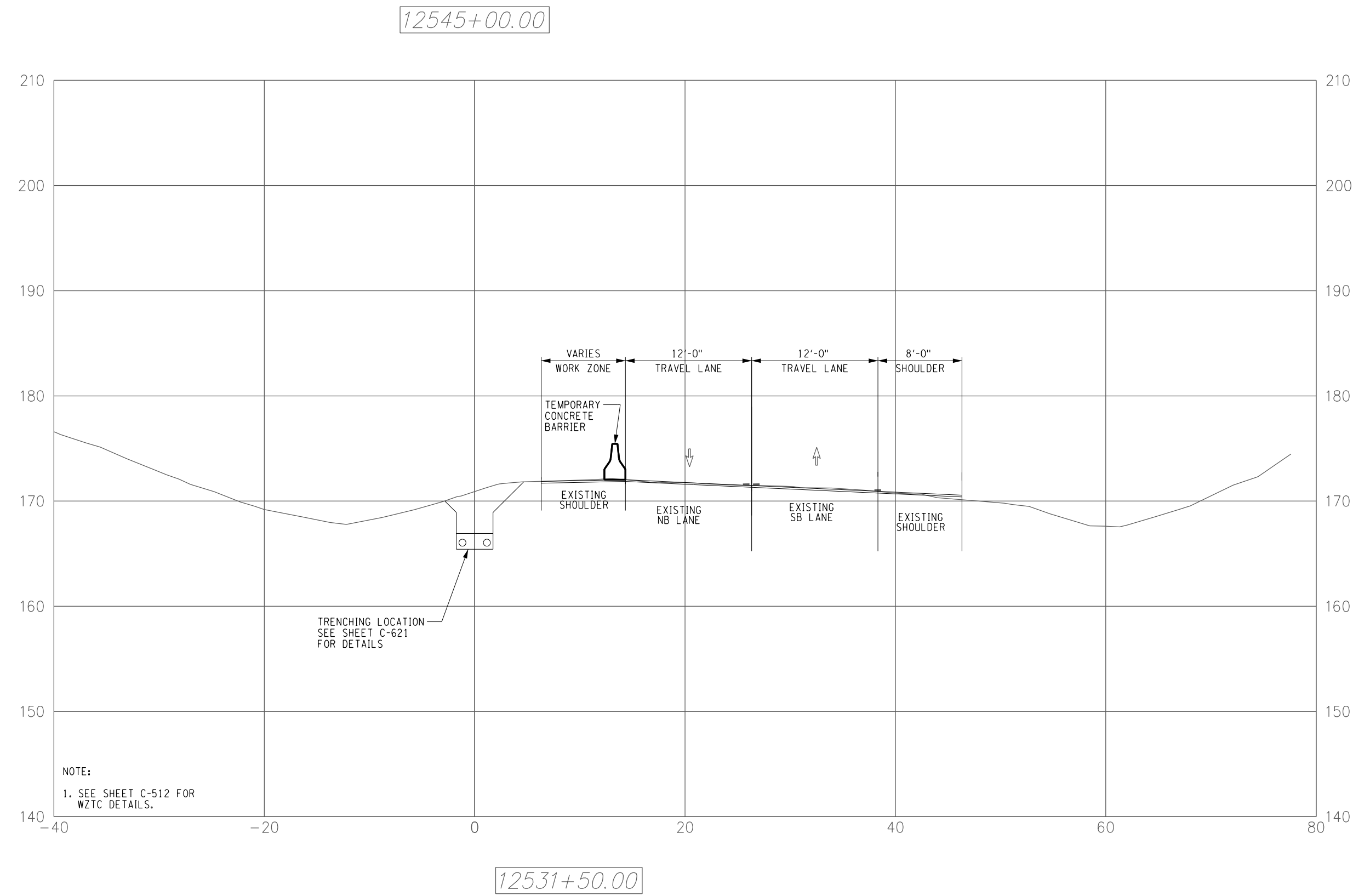
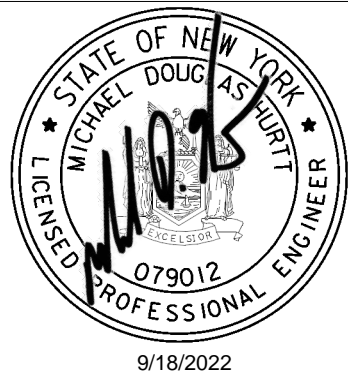
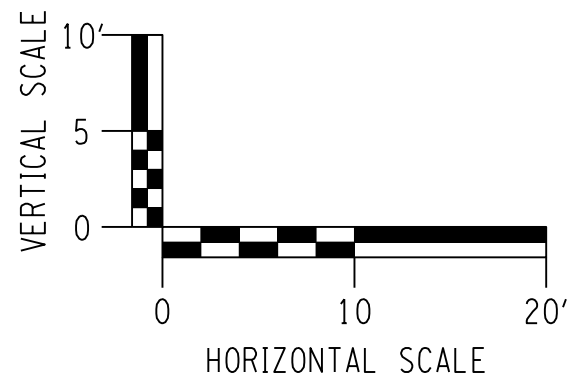
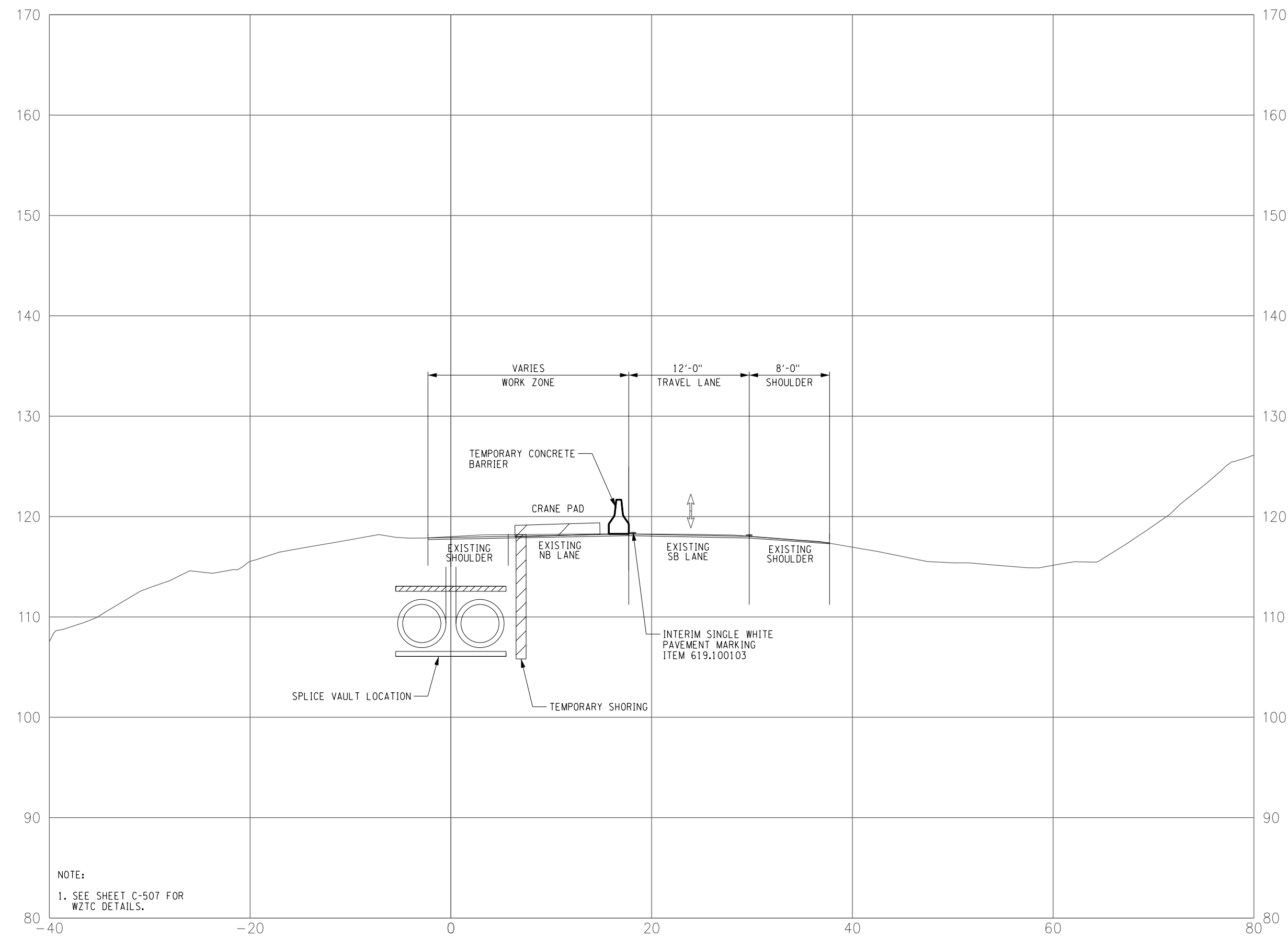
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
WORK ZONE TRAFFIC CONTROL  
TYPICAL SECTIONS  
SHEET 2 OF 2

DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE AS SHOWN	DATE 09/21/2022
			REV. NO.	SH.NO. OF XXX

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-513

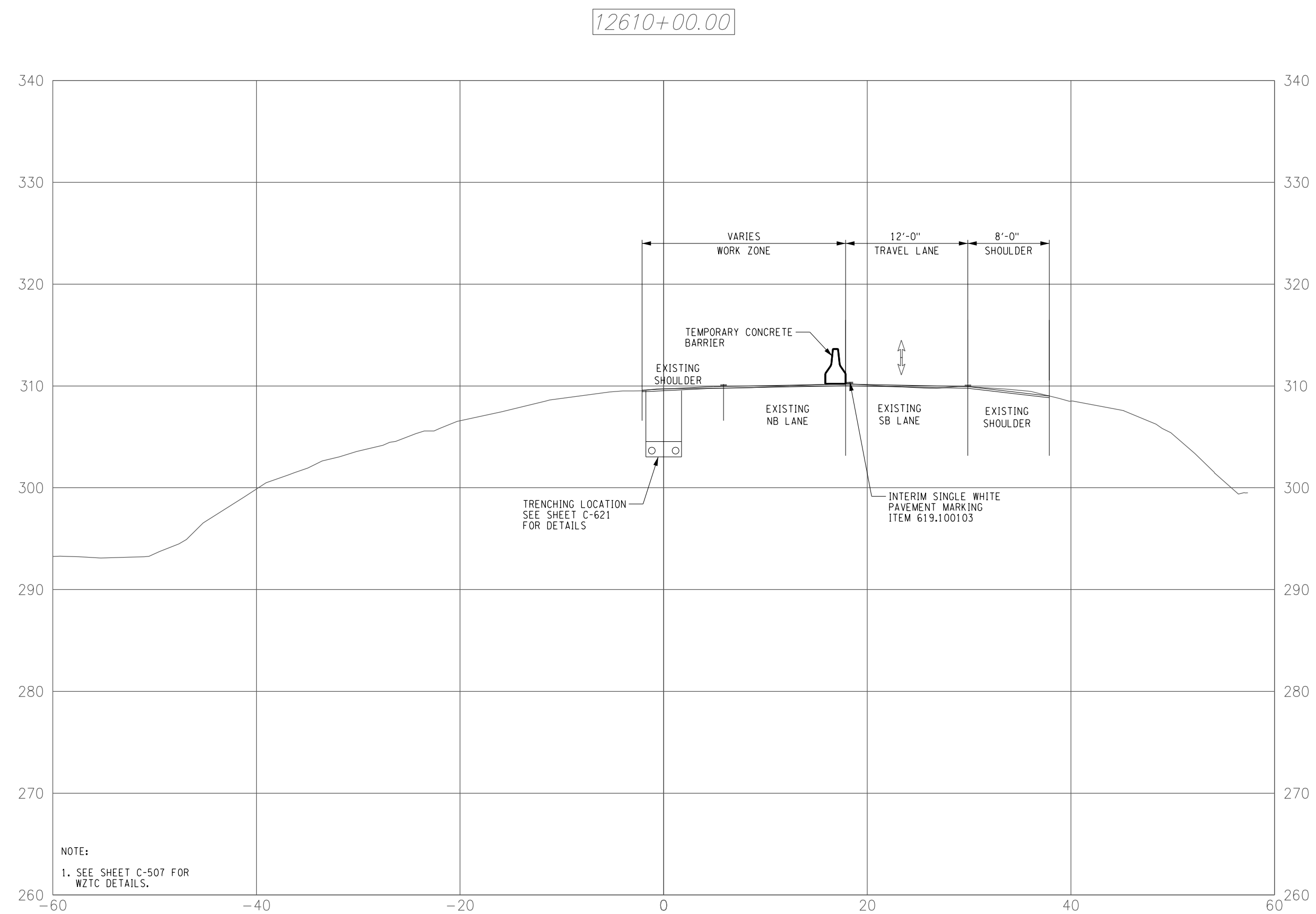


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						<div>CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL WORK ZONE TRAFFIC CONTROL CROSS SECTIONS SHEET 1 OF 9</div>				KIEWIT PROJECT NO. 21162	
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12589+00.00

390

380

370

360

350

340

330

320

VARIES  
WORK ZONE

11'-0" MIN.  
TRAVEL LANE

4'-0"

11'-0" MIN.  
TRAVEL LANE

8'-0"

SHOULDER

TEMPORARY CONCRETE  
BARRIER

TEMPORARY CHANNELIZING  
DEVICE

EXISTING NB LANE

EXISTING SB LANE

EXISTING NB LANE

EXISTING SB LANE

EXISTING SHOULDER

EXISTING SHOULDER

INTERIM SINGLE YELLOW  
PAVEMENT MARKING  
ITEM 619.100103

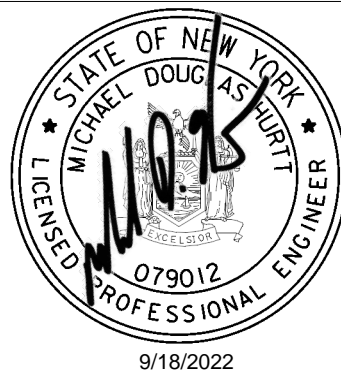
COVER EXISTING  
PAVEMENT MARKINGS  
ITEM 619.08003

INTERIM SINGLE WHITE  
PAVEMENT MARKING  
ITEM 619.100103

TRENCHING LOCATION  
SEE SHEET C-621  
FOR DETAILS

NOTE:  
1. SEE SHEET C-510 FOR  
WZTC DETAILS.

-60 -40 -20 0 20 40 60



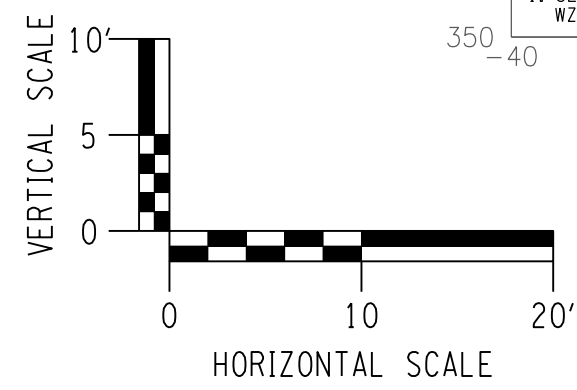
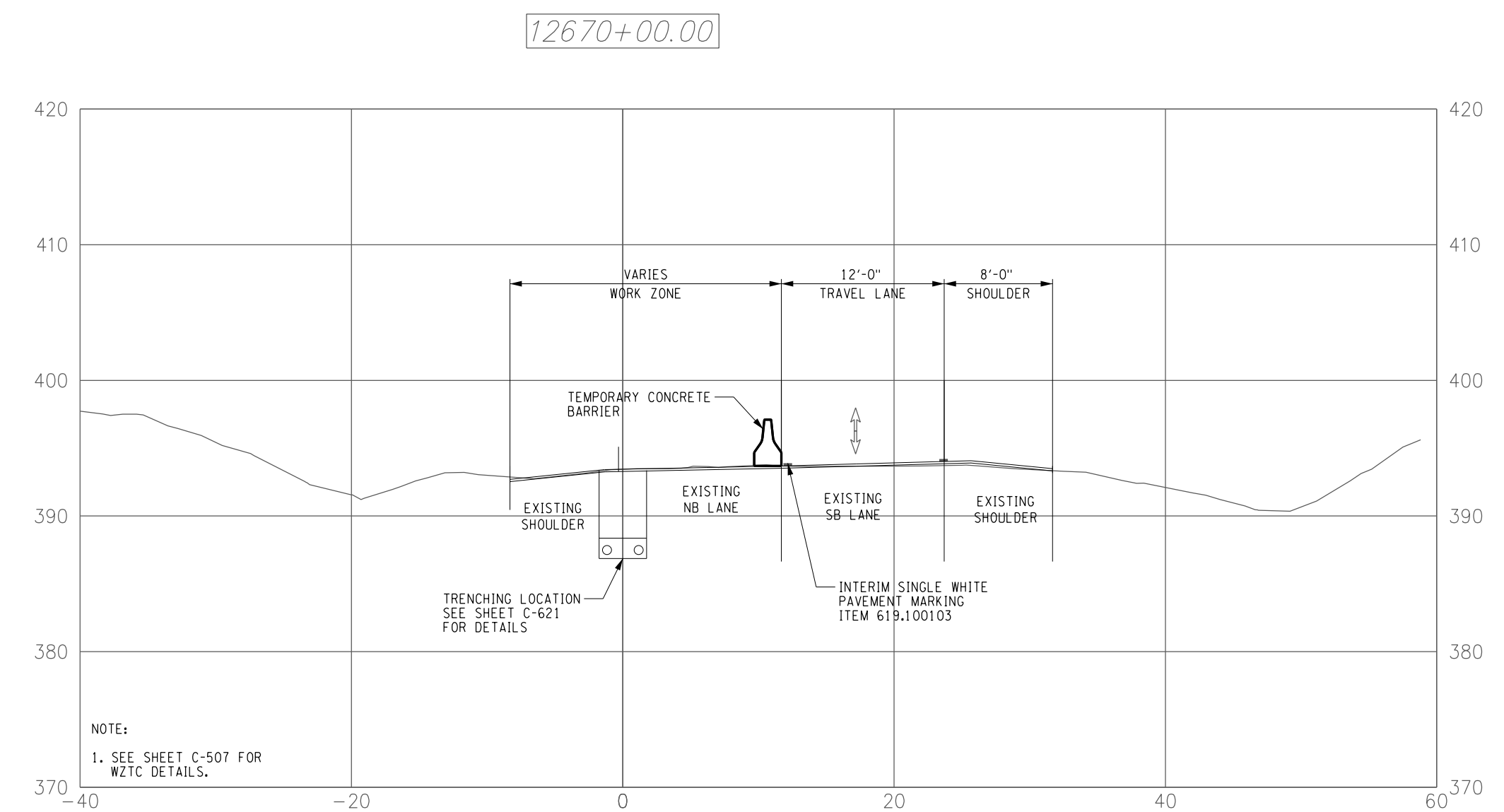
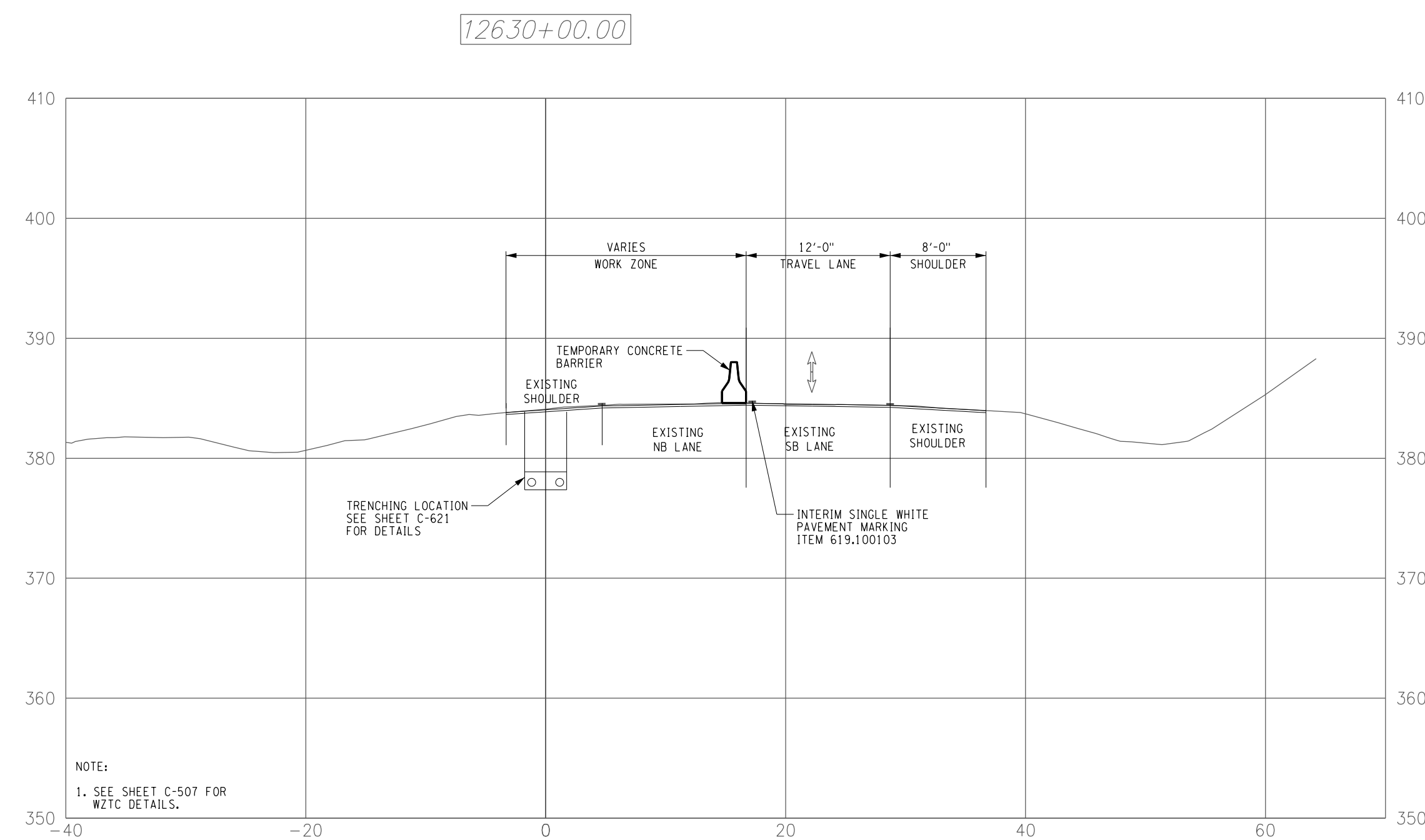
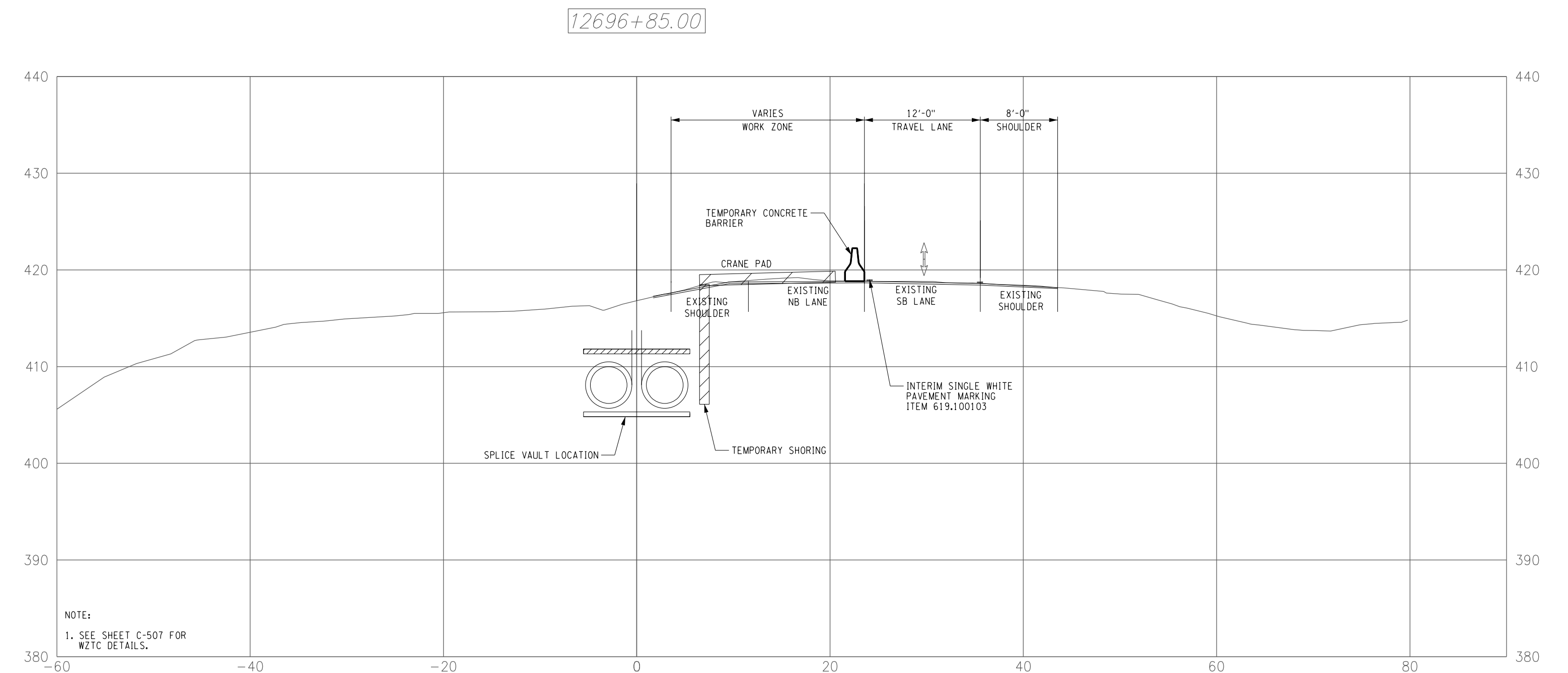
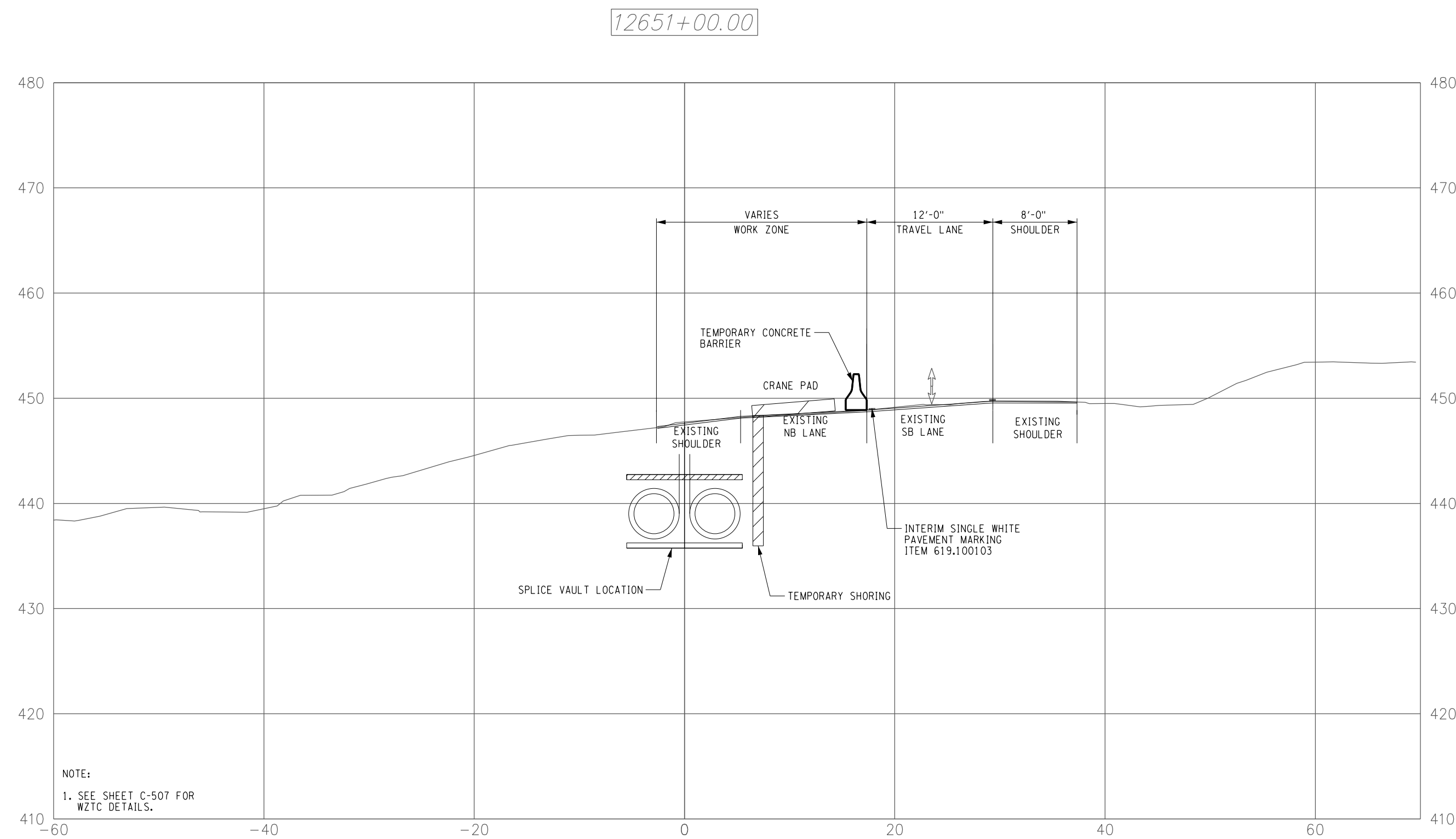
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						<div>CHAMPLAIN HUDSON POWER EXPRESS</div> <div>SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL</div> <div>WORK ZONE TRAFFIC CONTROL</div> <div>CROSS SECTIONS</div> <div>SHEET 2 OF 9</div>					KIEWIT PROJECT NO. 21162	
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											<b>C-515</b>	
0	09/21/2022	FINAL EM&CP SUBMISSION		JS	MH							
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**CHAMPLAIN HUDSON POWER EXPRESS**  
**SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL**  
 WORK ZONE TRAFFIC CONTROL  
 CROSS SECTIONS  
 SHEET 2 OF 9

KIEWIT PROJECT NO.	
21162	
CHA PROJECT NO.	
066076	
DRAWING NO.	
<b>C-515</b>	
DATE	09/21/2022
SH.NO.	OF XXX





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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

# CHAMPLAIN HUDSON POWER EXPRESS

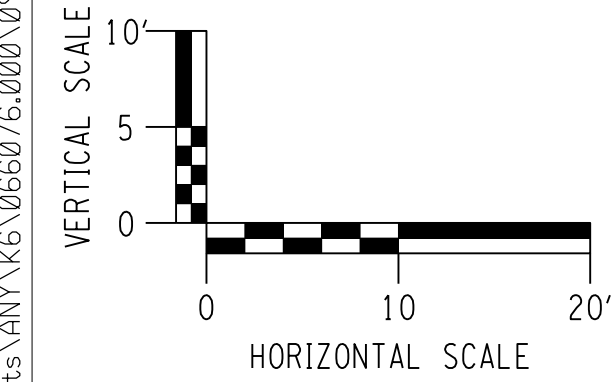
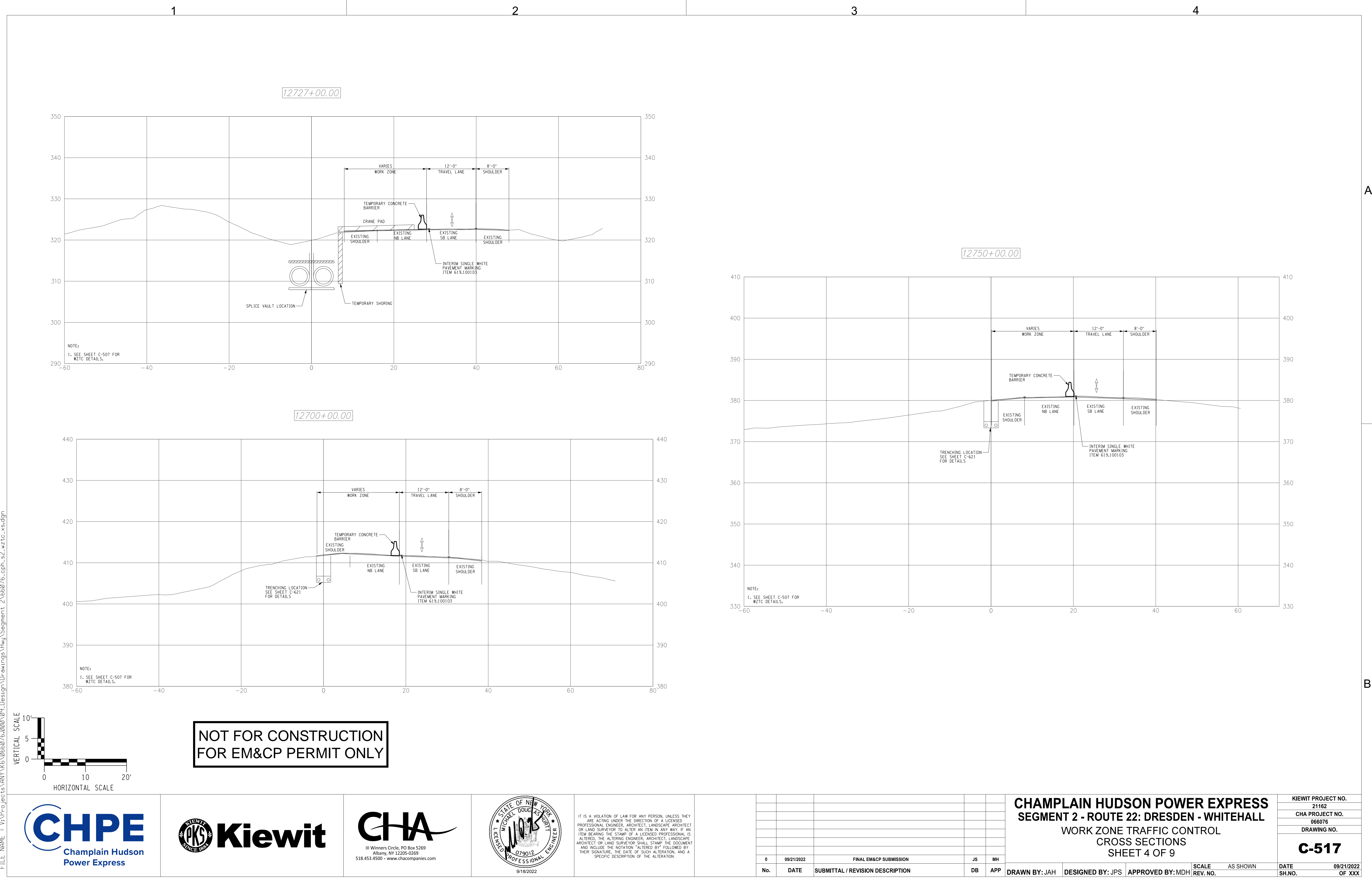
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### WORK ZONE TRAFFIC CONTROL CROSS SECTIONS SHEET 3 OF 9

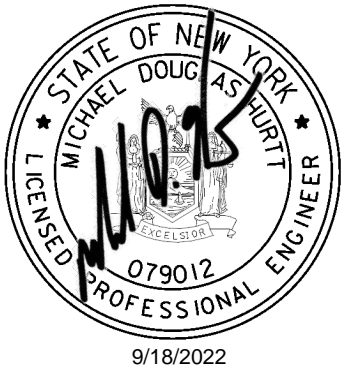
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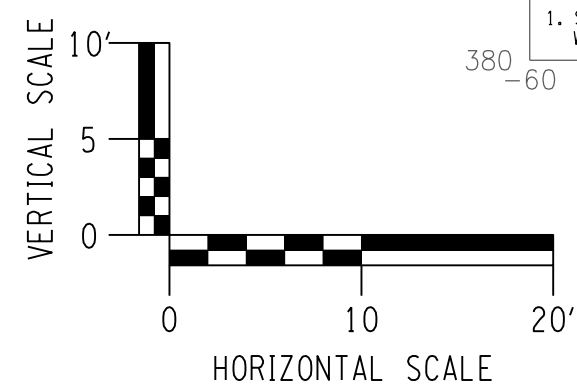
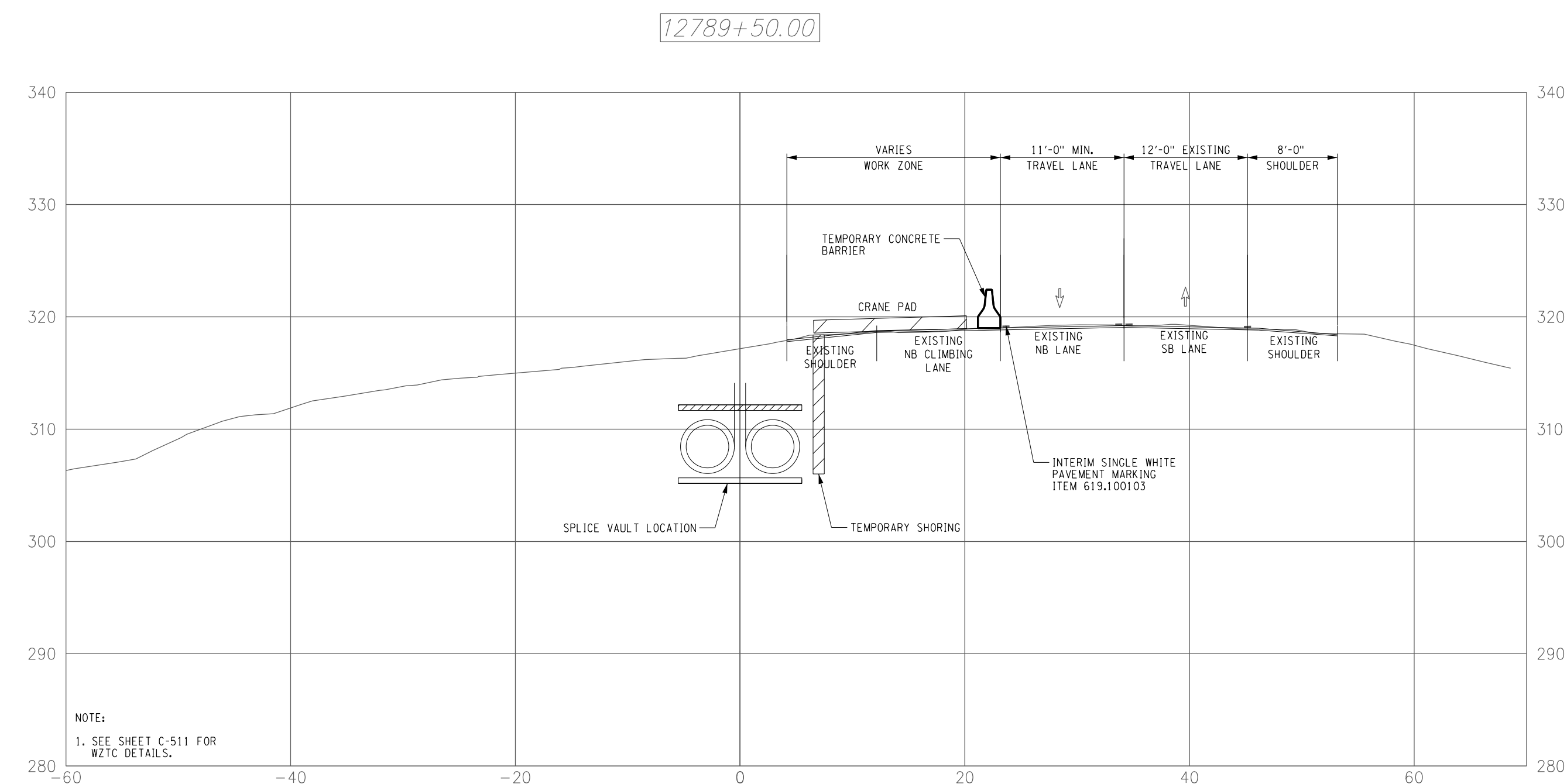
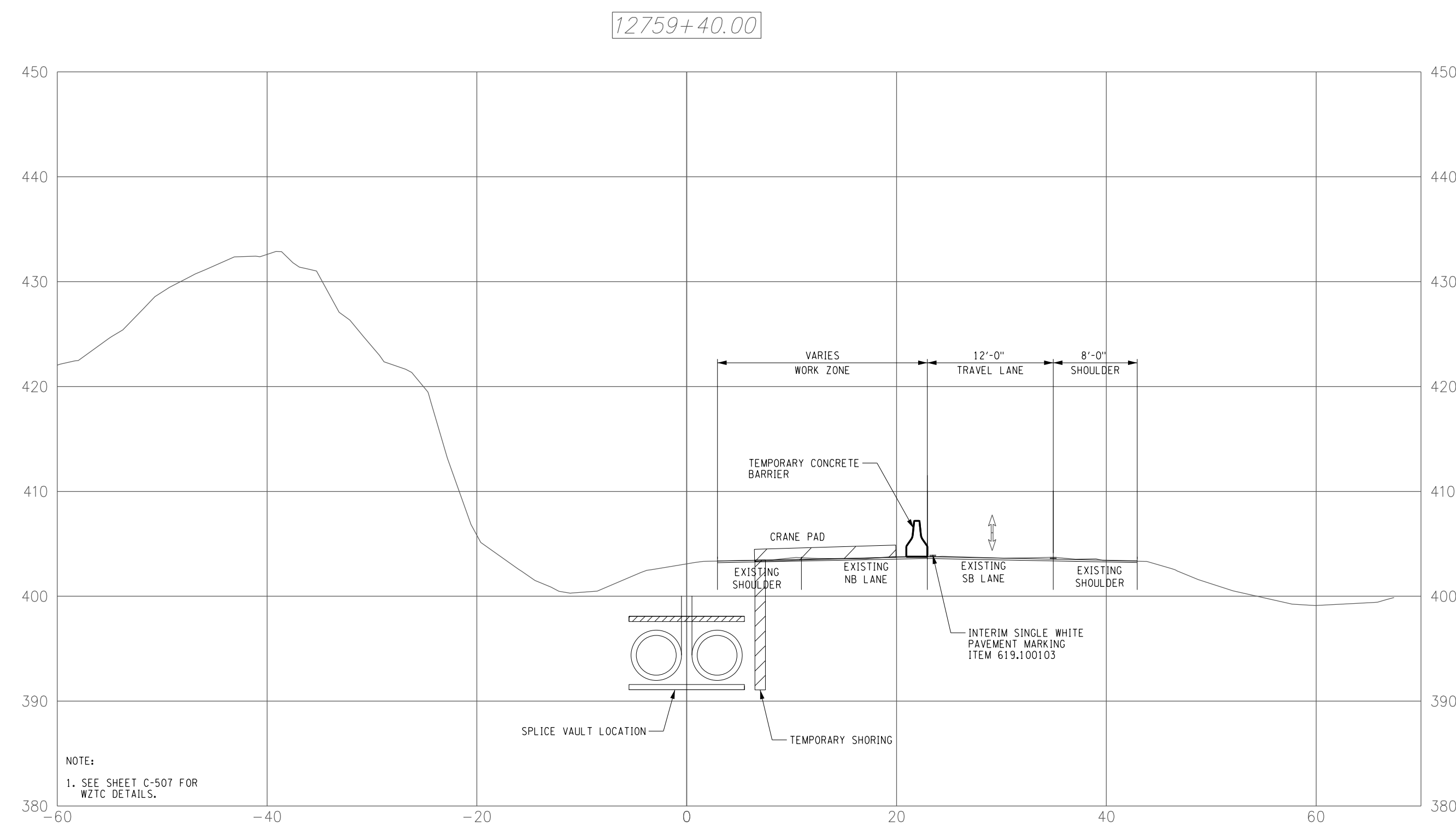
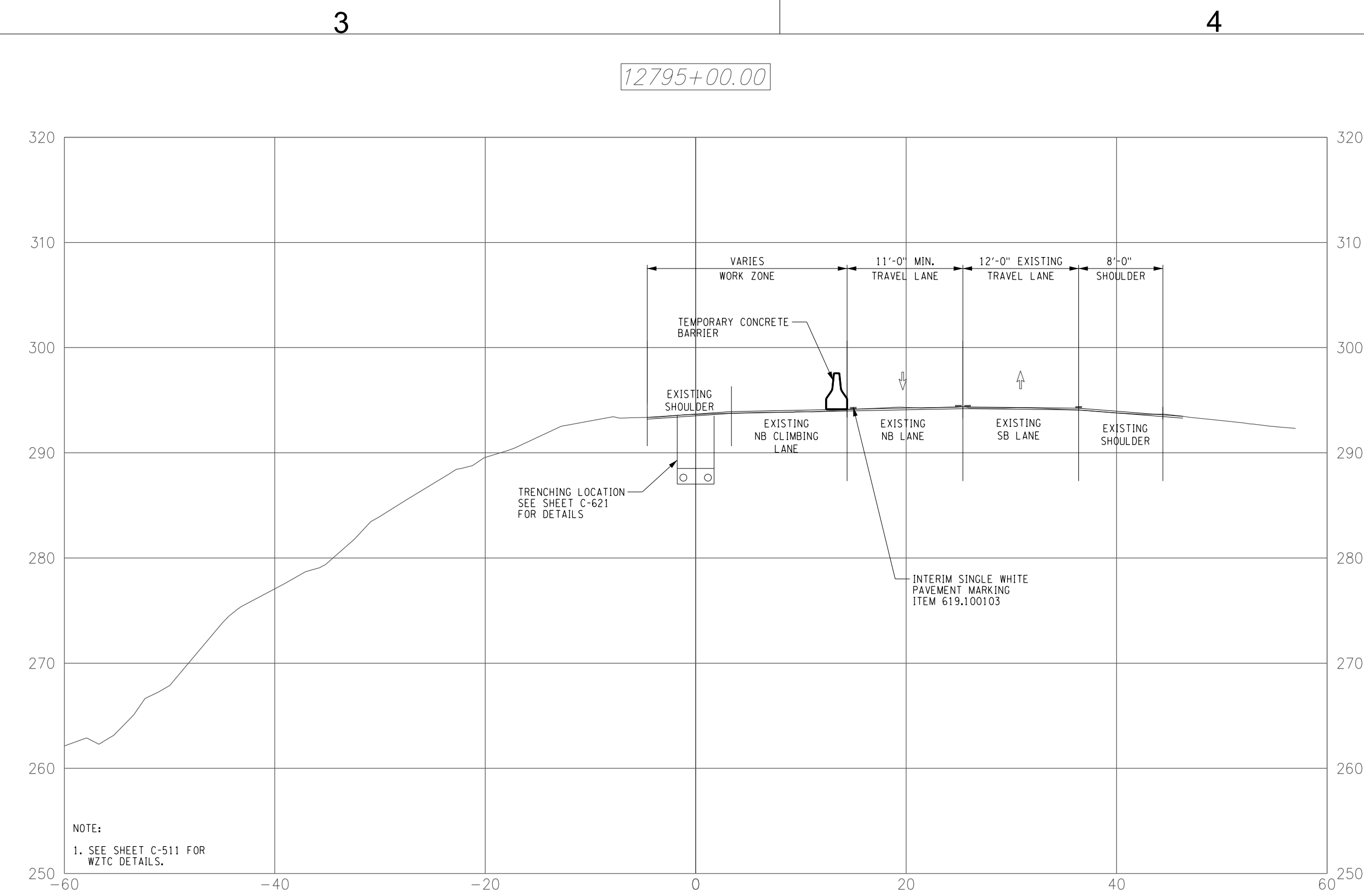
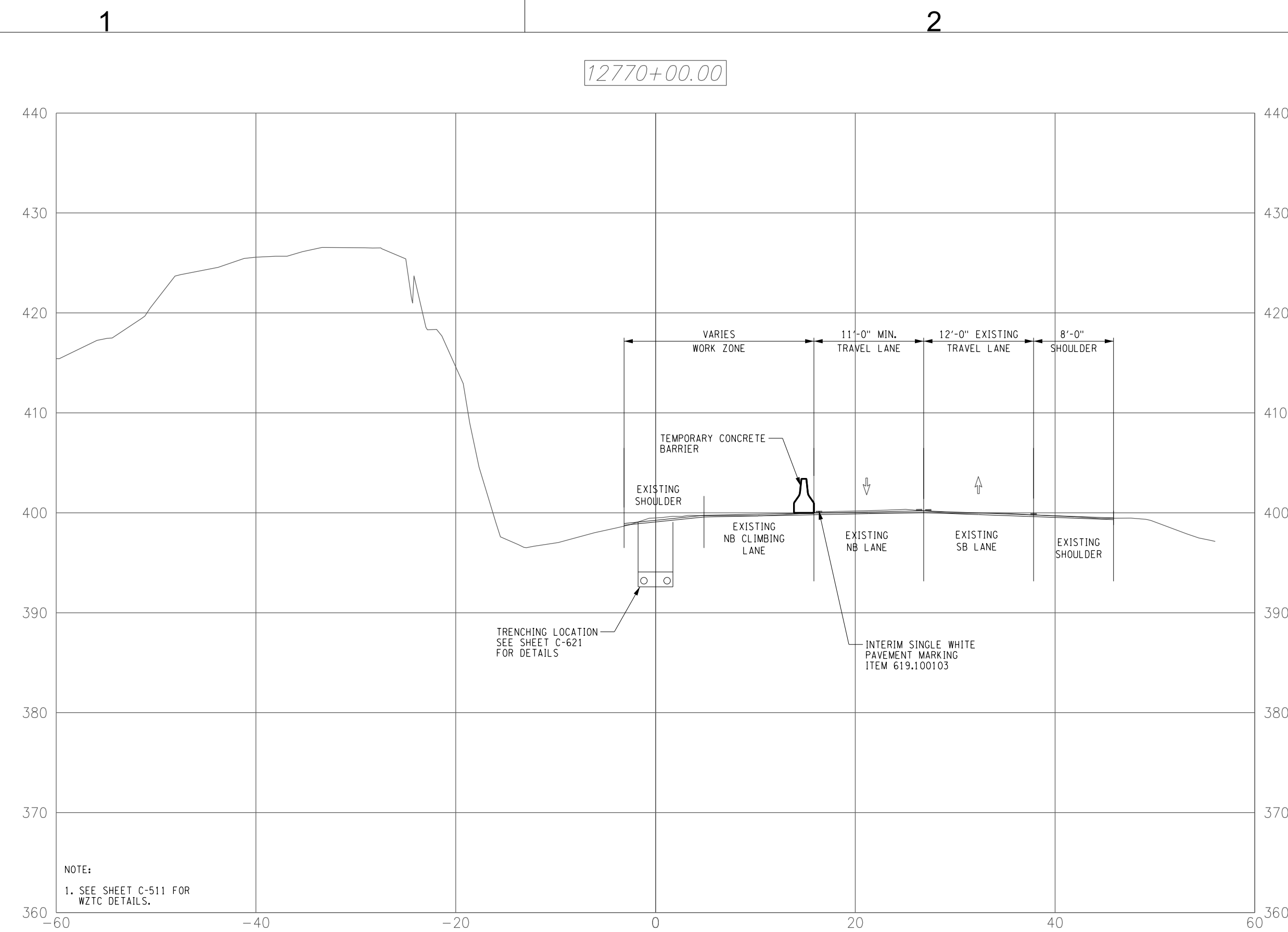
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CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
WORK ZONE TRAFFIC CONTROL  
CROSS SECTIONS  
SHEET 4 OF 9

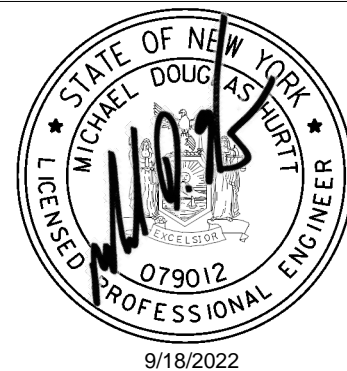
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CHA PROJECT NO.	066076
DRAWING NO.	C-517
DATE	09/21/2022
SH.NO.	OF XXX

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			REV. NO.		SH.NO.	OF XXX





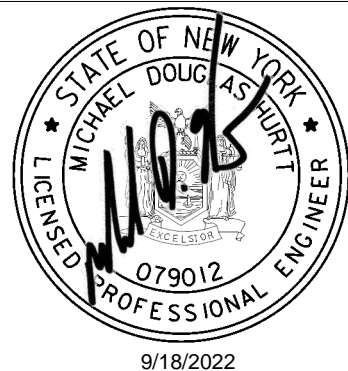
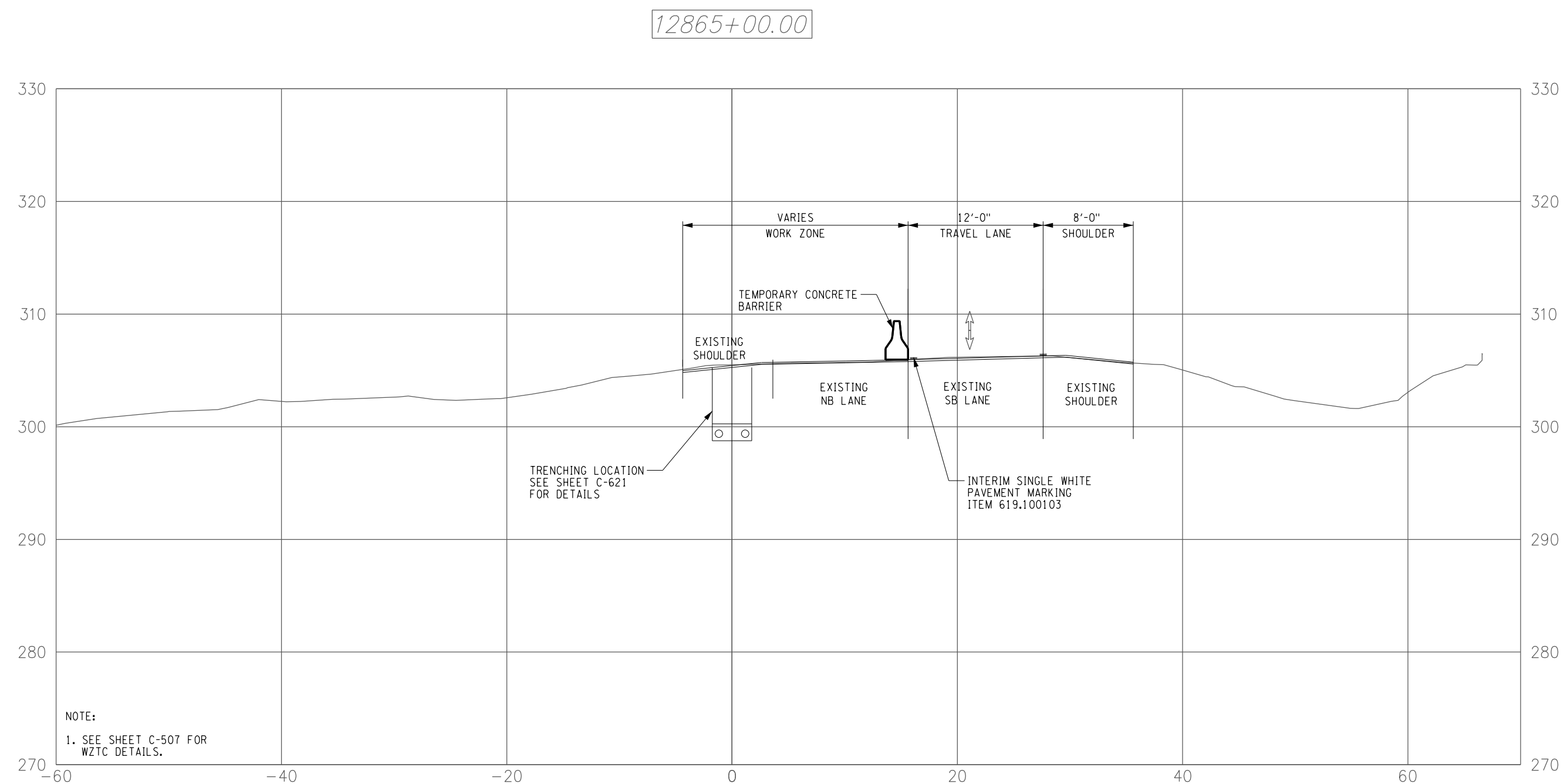
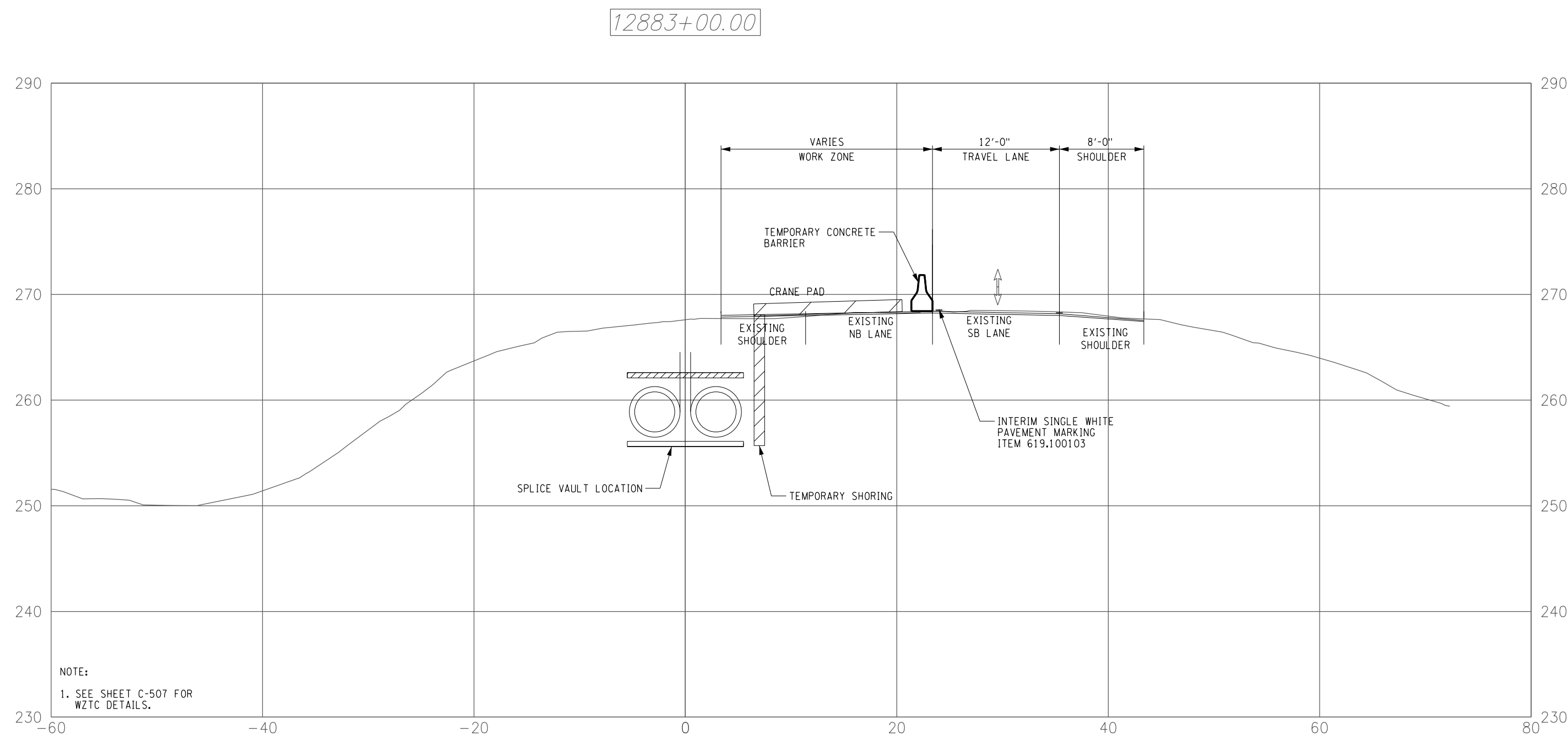
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						<div>CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL WORK ZONE TRAFFIC CONTROL CROSS SECTIONS SHEET 5 OF 9</div>				KIEWIT PROJECT NO. 21162	
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV. NO.	AS SHOWN	DATE SH.NO.	09/21/2022 OF XXX

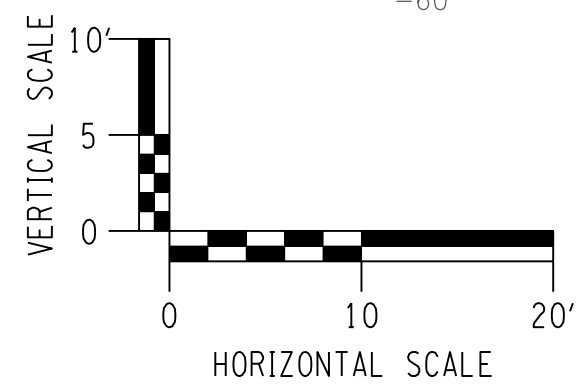
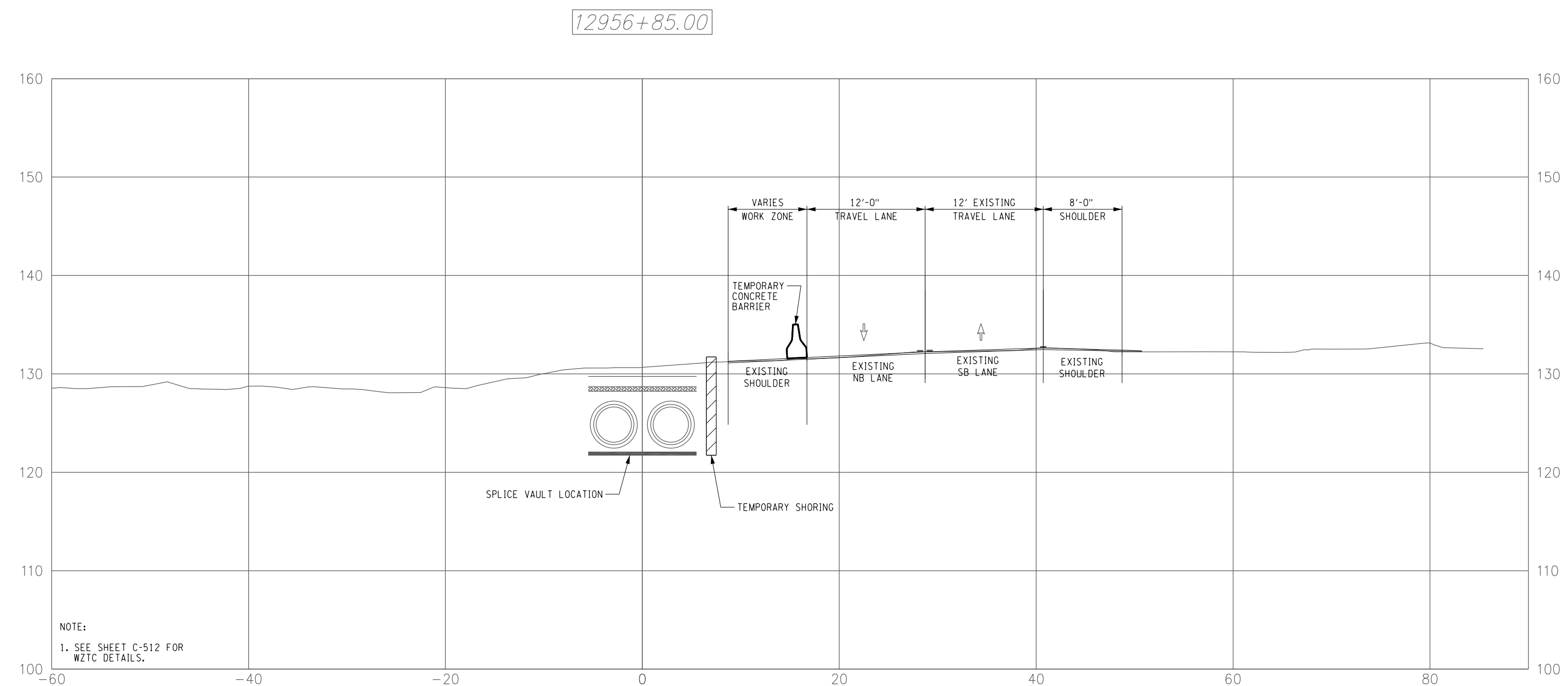
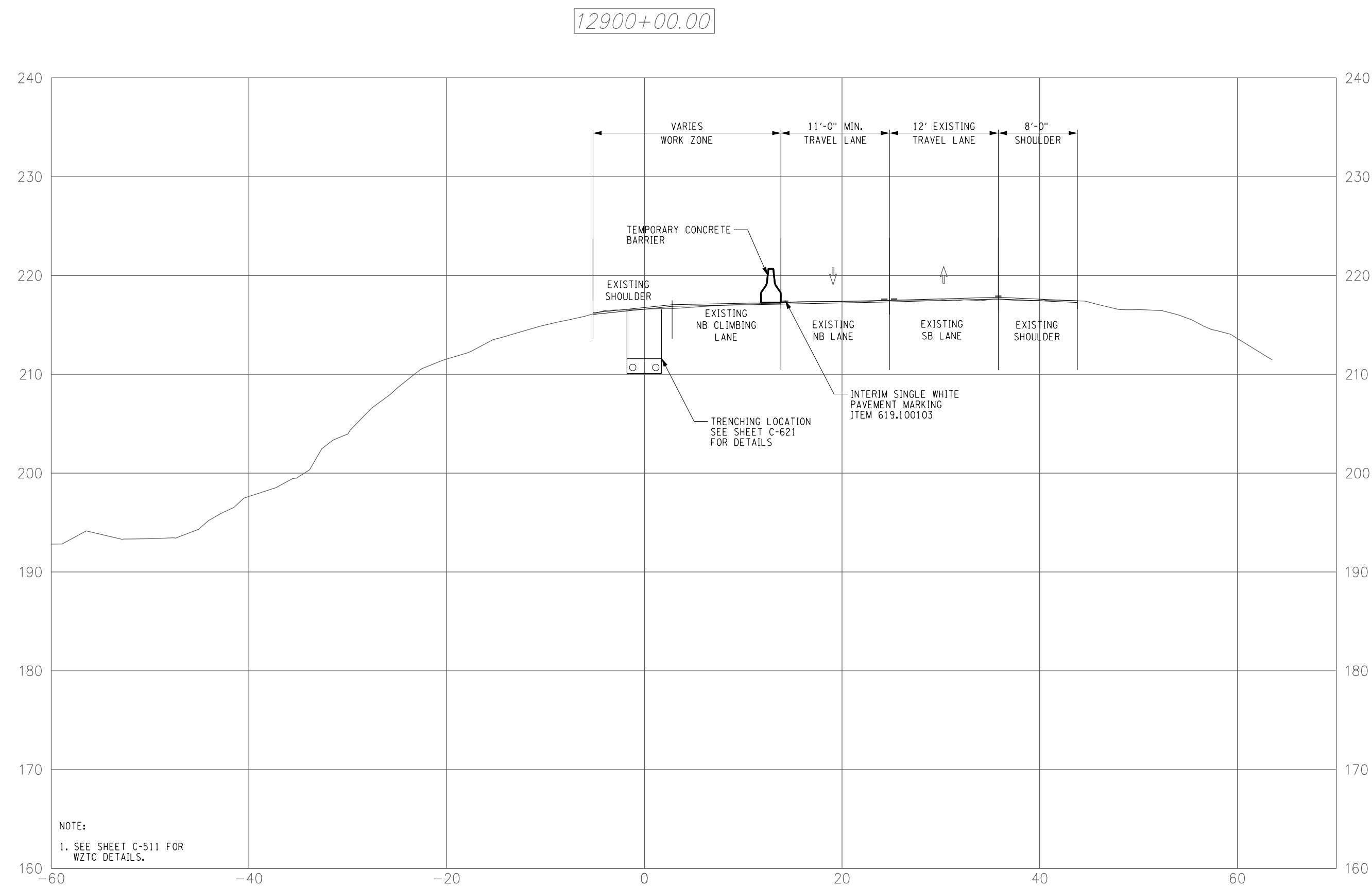
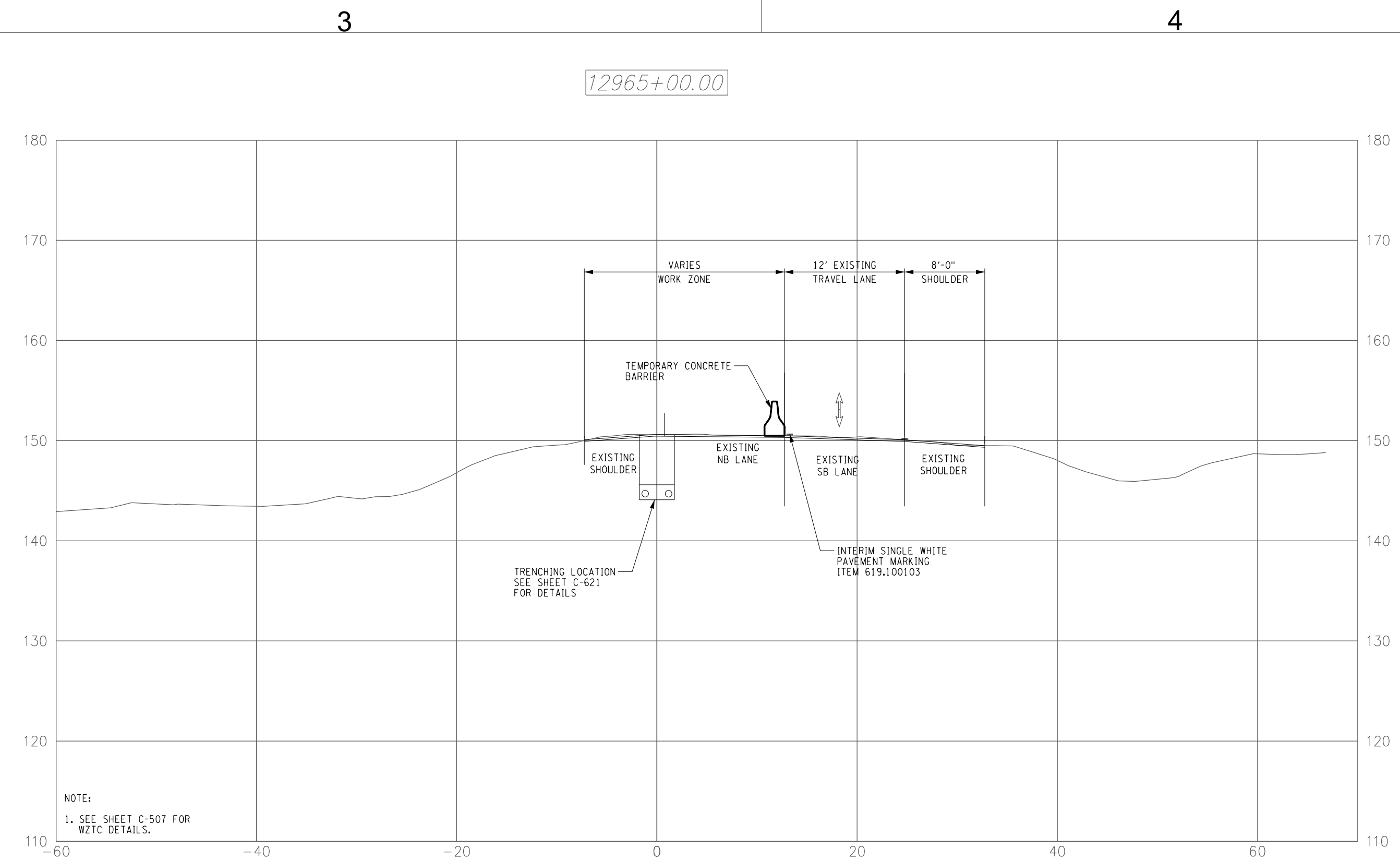
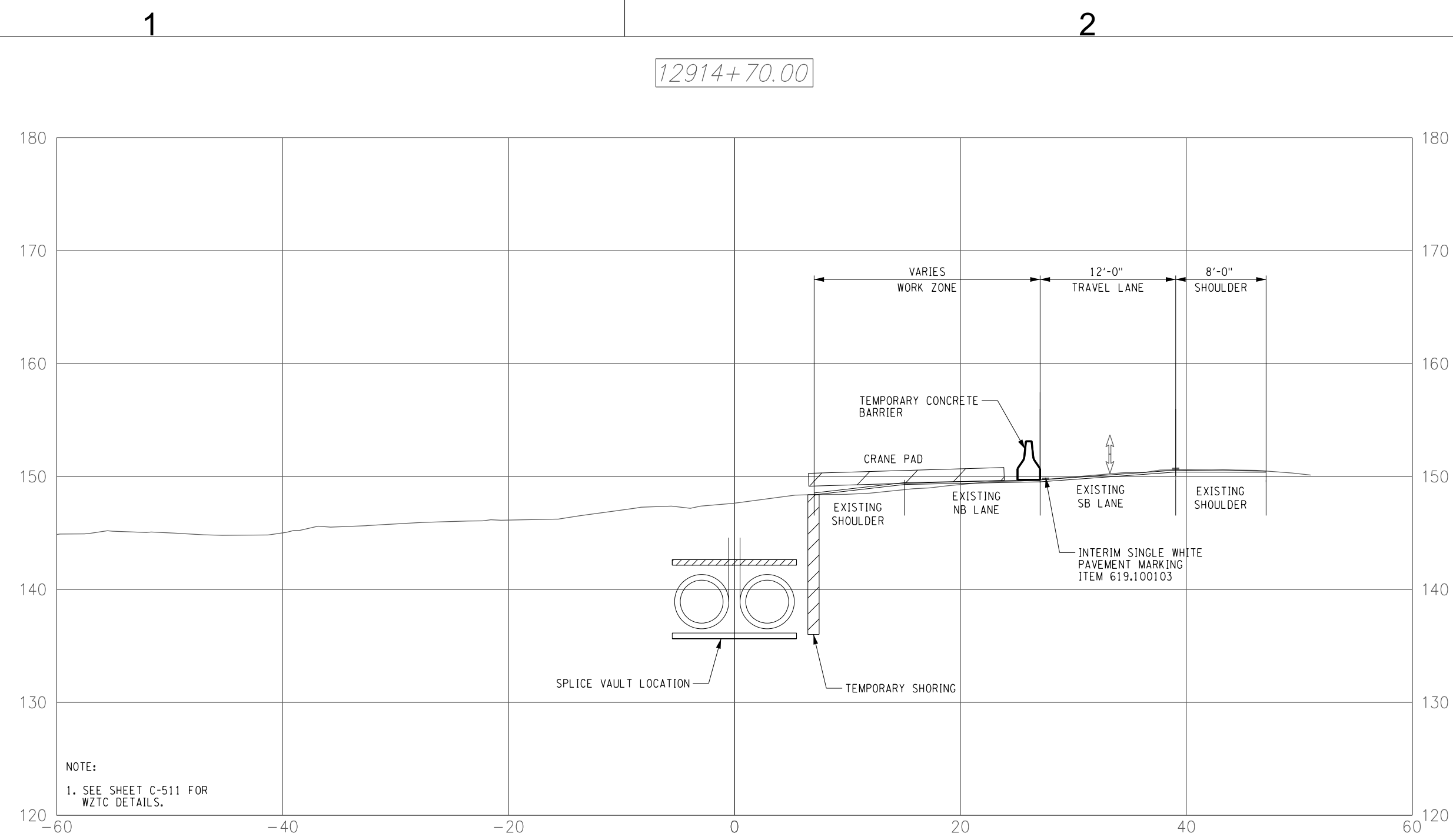




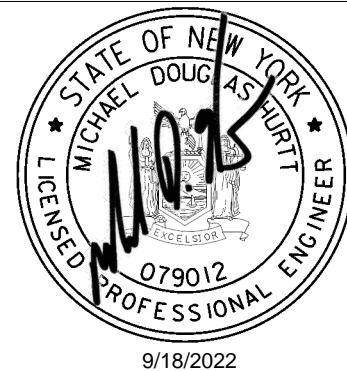
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

						<div>CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL WORK ZONE TRAFFIC CONTROL CROSS SECTIONS SHEET 6 OF 9</div>				KIEWIT PROJECT NO. 21162	
										CHA PROJECT NO. 066076	
										DRAWING NO. <div>C-519</div>	
0	09/21/2022	FINAL EM&CP SUBMISSION		JS	MH						
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV. NO.	AS SHOWN	DATE SH.NO.	09/21/2022 OF XXX





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**CHAMPLAIN HUDSON POWER EXPRESS**  
**SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL**  
 WORK ZONE TRAFFIC CONTROL  
 CROSS SECTIONS  
 SHEET 7 OF 9

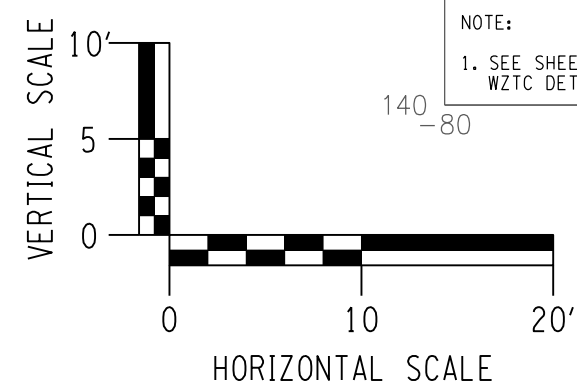
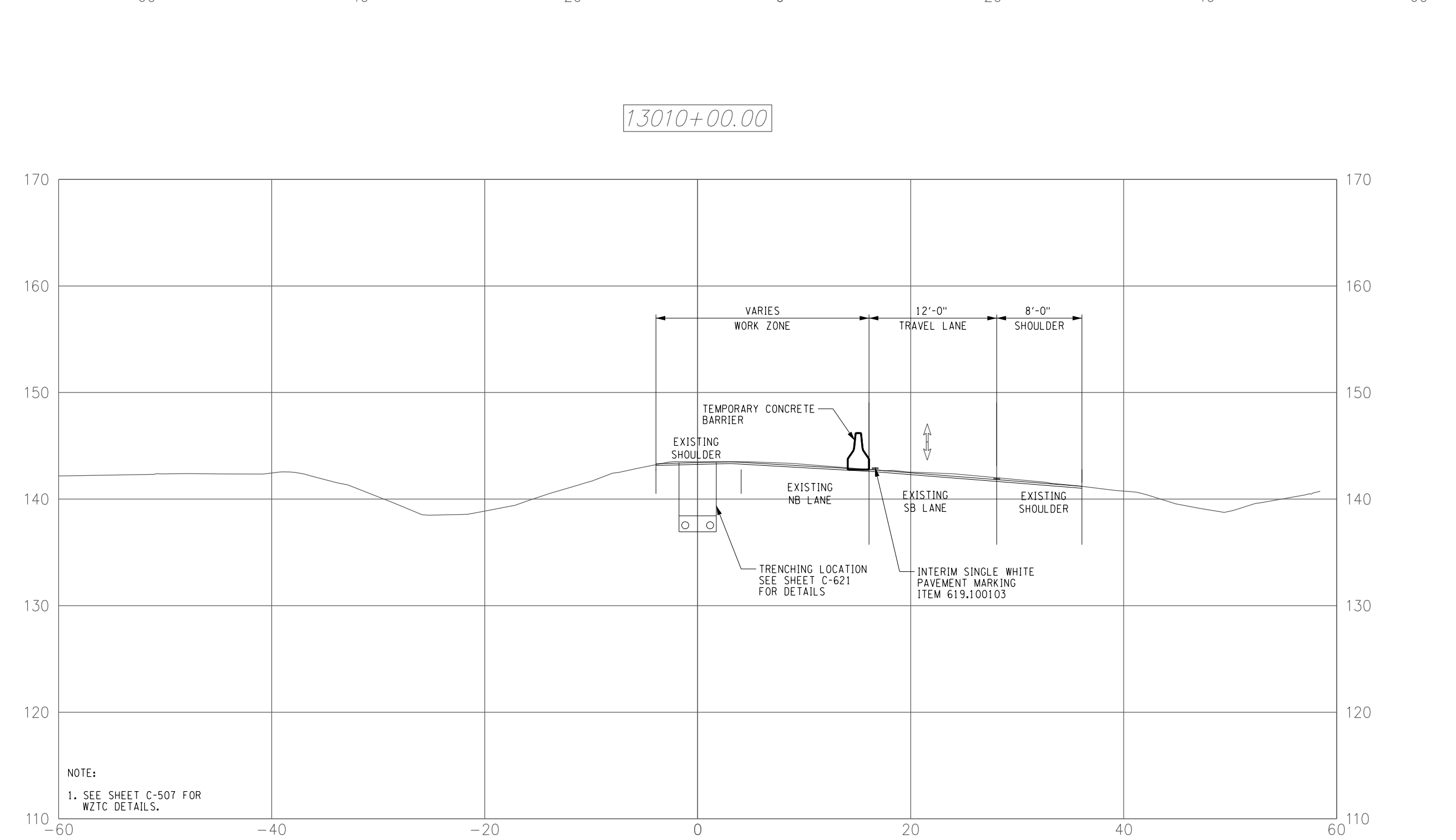
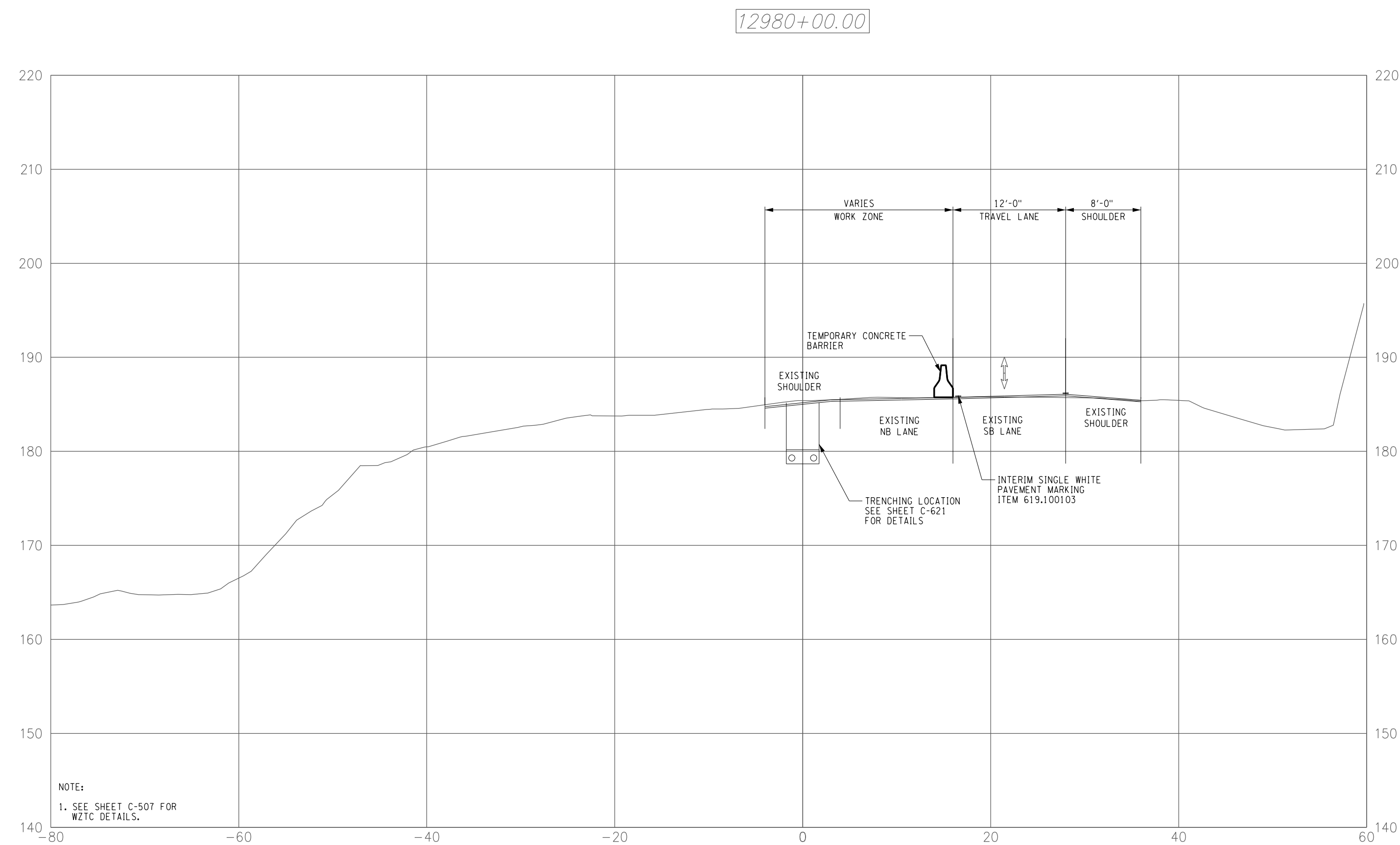
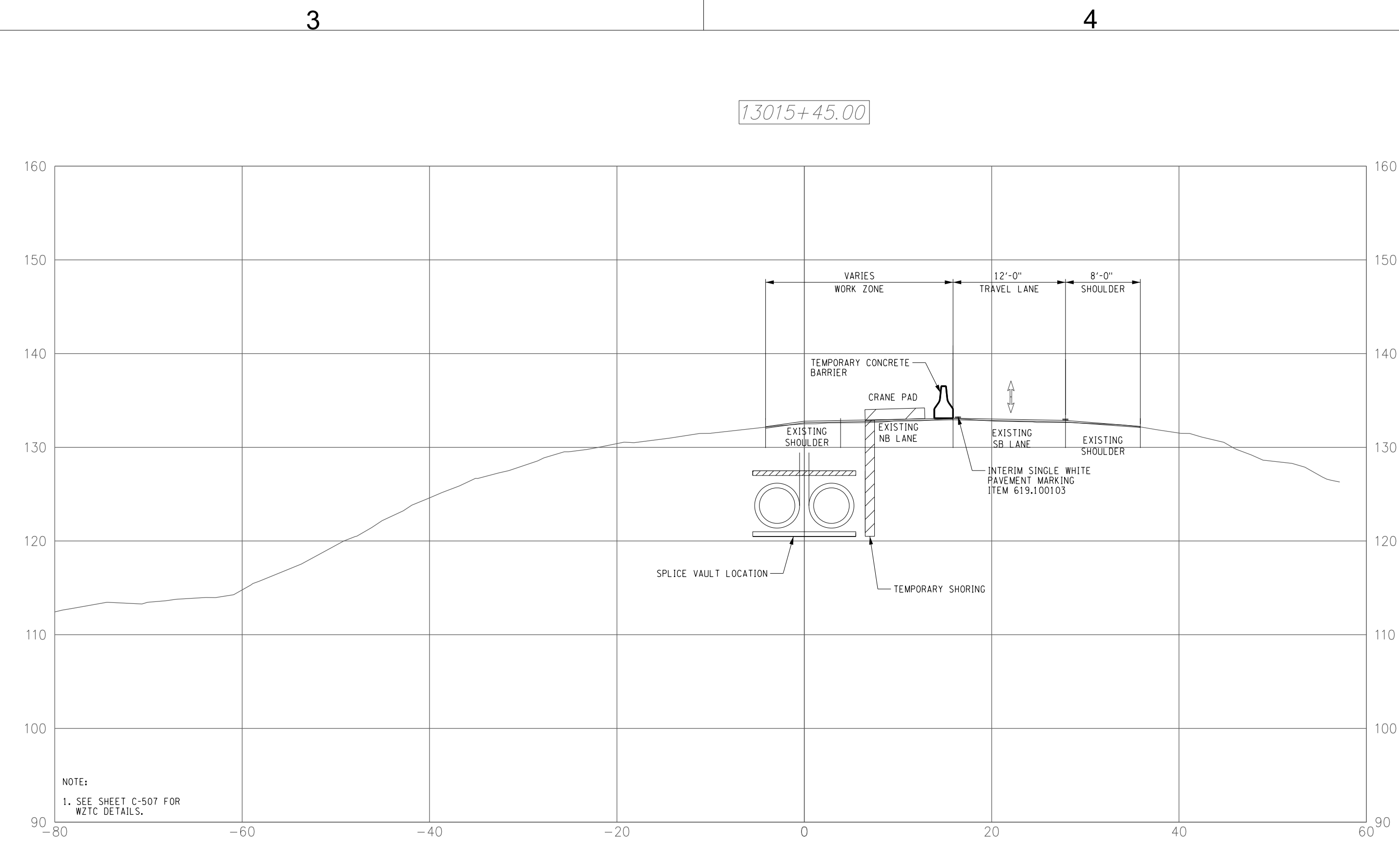
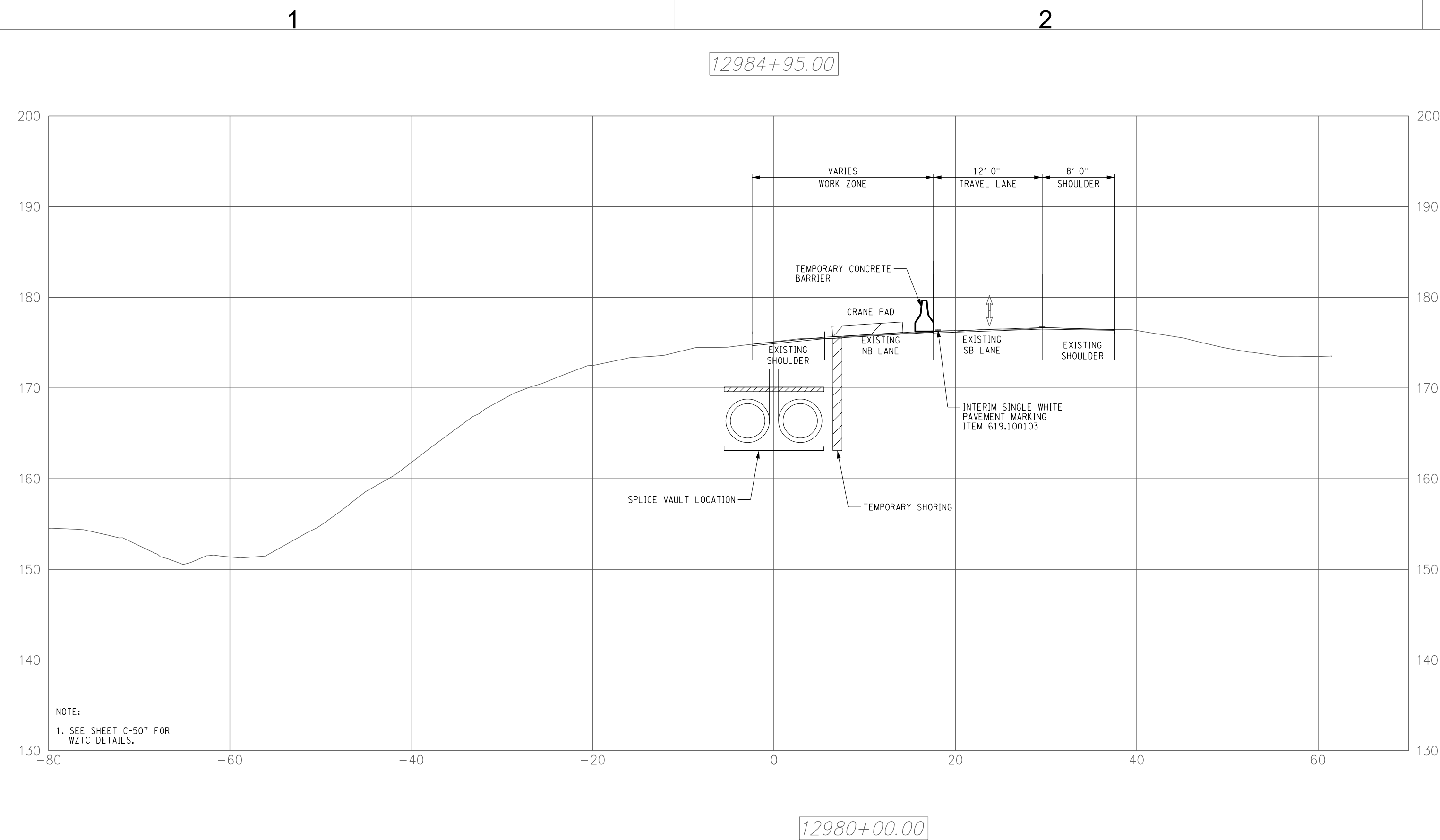
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21162	
CHA PROJECT NO.	
066076	
DRAWING NO.	
<b>C-520</b>	
DATE	09/21/2022
SH NO	OF XXX

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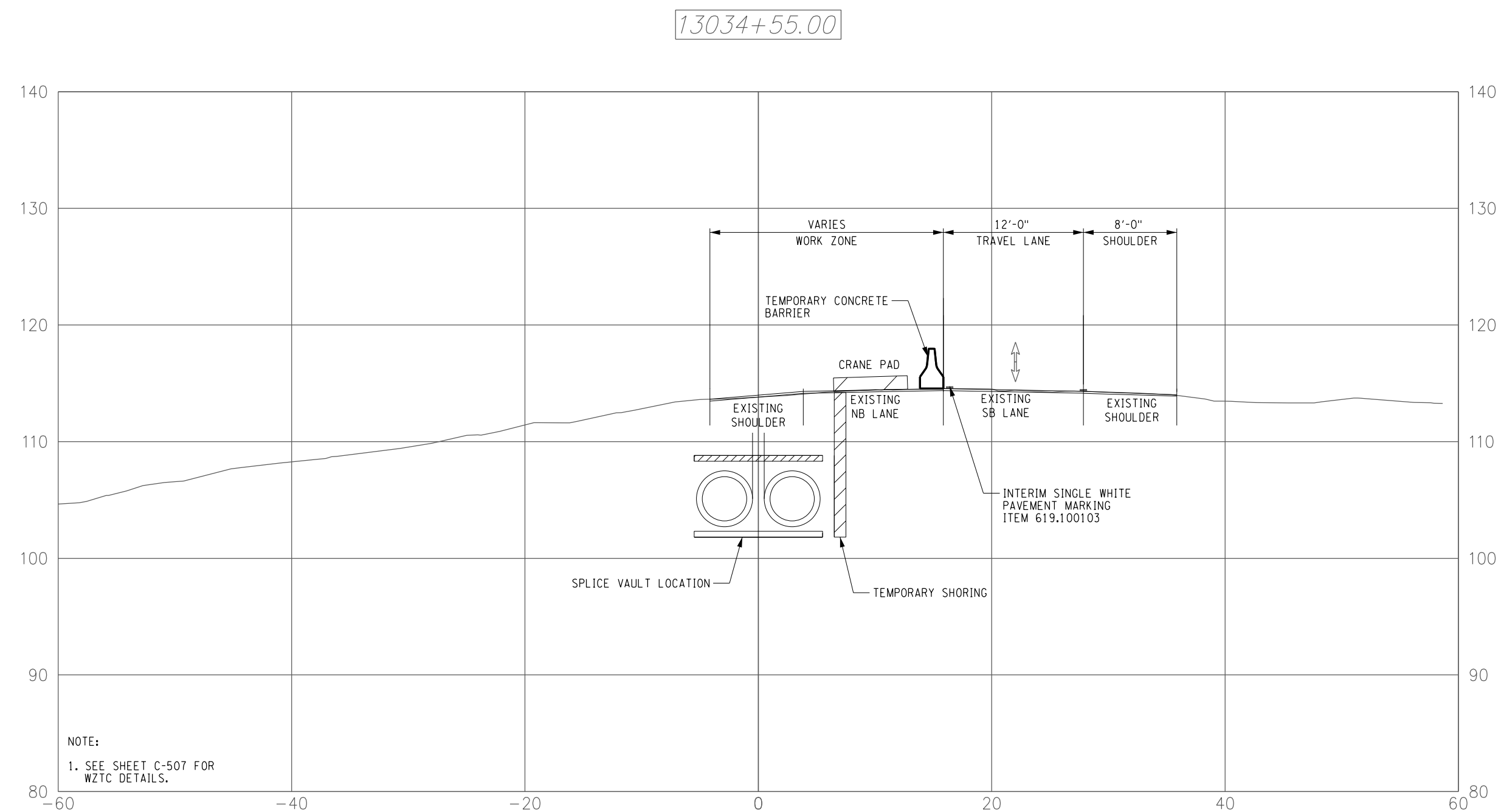
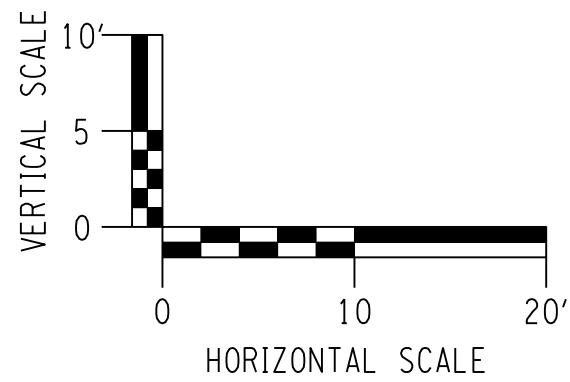
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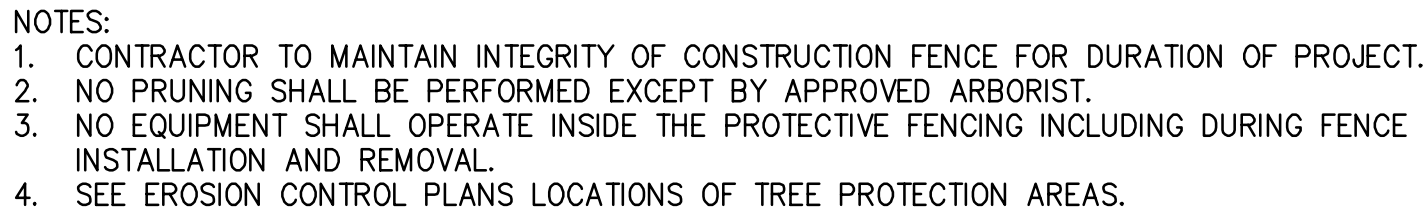




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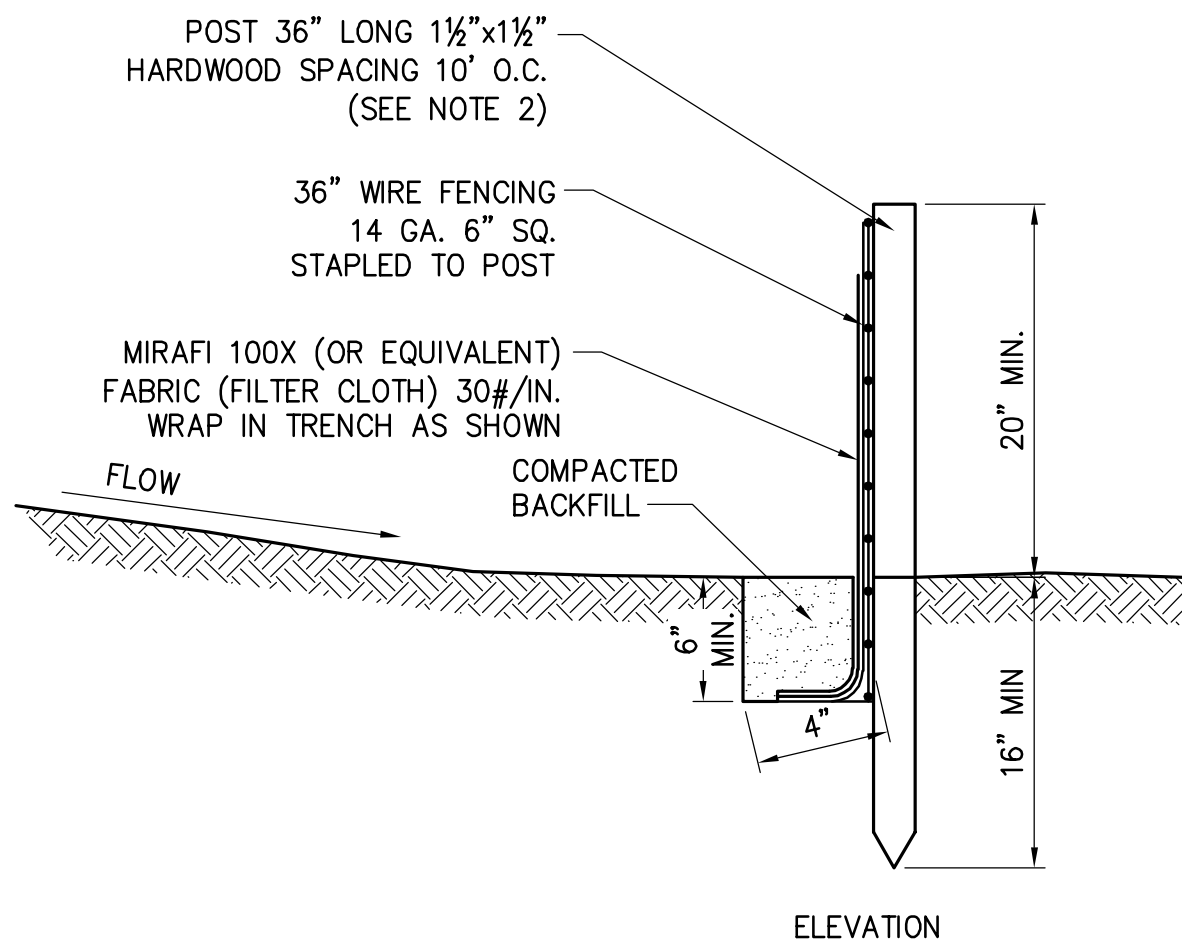
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										CHA PROJECT NO. 066076	
										DRAWING NO.	
										<div>C-522</div>	
0	09/21/2022	FINAL EM&CP SUBMISSION		JS	MH						
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JAH	DESIGNED BY: JPS	APPROVED BY: MDH	SCALE REV. NO.	AS SHOWN	DATE SH.NO.	09/21/2022 OF XXX





NOT TO SCALE

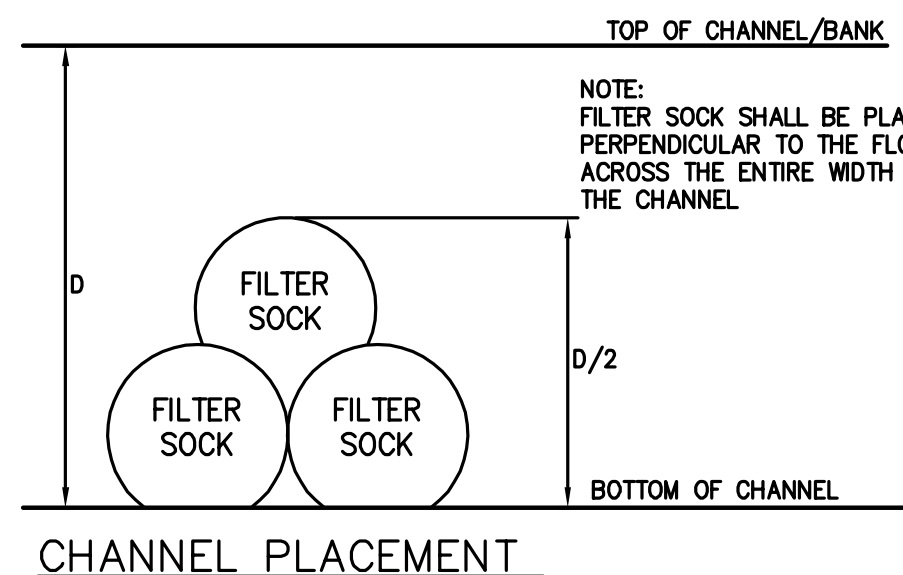
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SCALE: N.T.S.

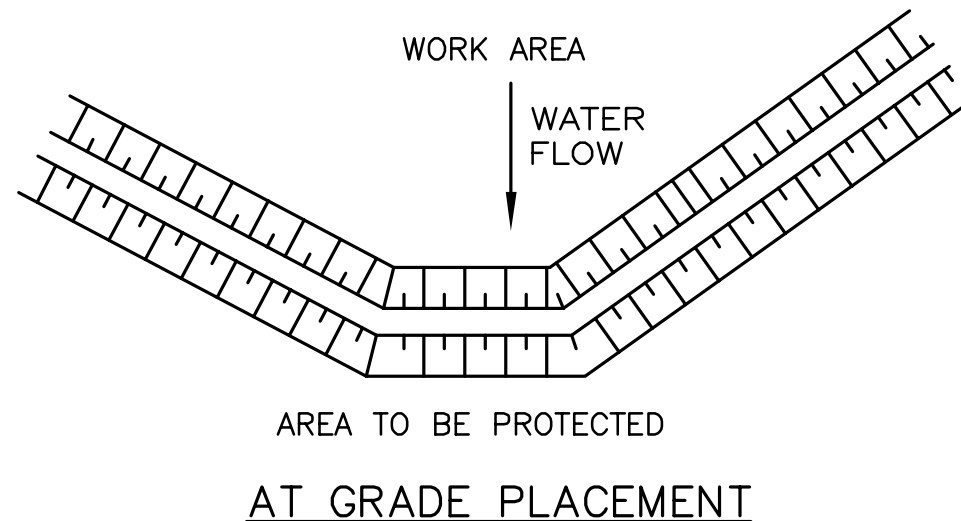


1. ALL MATERIAL TO MEET MANUFACTURER SPECIFICATIONS.
2. ALL FILTER SOCKS SHALL BE 12" DIAMETER OR LARGER.
3. THE CONTRACTOR SHALL MAINTAIN THE COMPOST FILTER BERM IN A FUNCTIONAL CONDITION AT ALL TIMES AND IT SHALL BE ROUTINELY INSPECTED.
4. WHERE THE BERM REQUIRES REPAIR, IT WILL BE ROUTINELY REPAIRED.
5. THE CONTRACTOR SHALL REMOVE SEDIMENTS COLLECTED AT THE BASE OF THE BERM WHEN THEY REACH 1/3 OF THE EXPOSED HEIGHT OF THE BERM, OR AS DIRECTED BY THE OWNERS.
6. THE COMPOST FILTER BERM WILL BE REMOVED ON SITE WHEN NO LONGER REQUIRED, AS DETERMINED BY THE OWNERS.
7. INSTALL PERPENDICULAR TO FLOW.

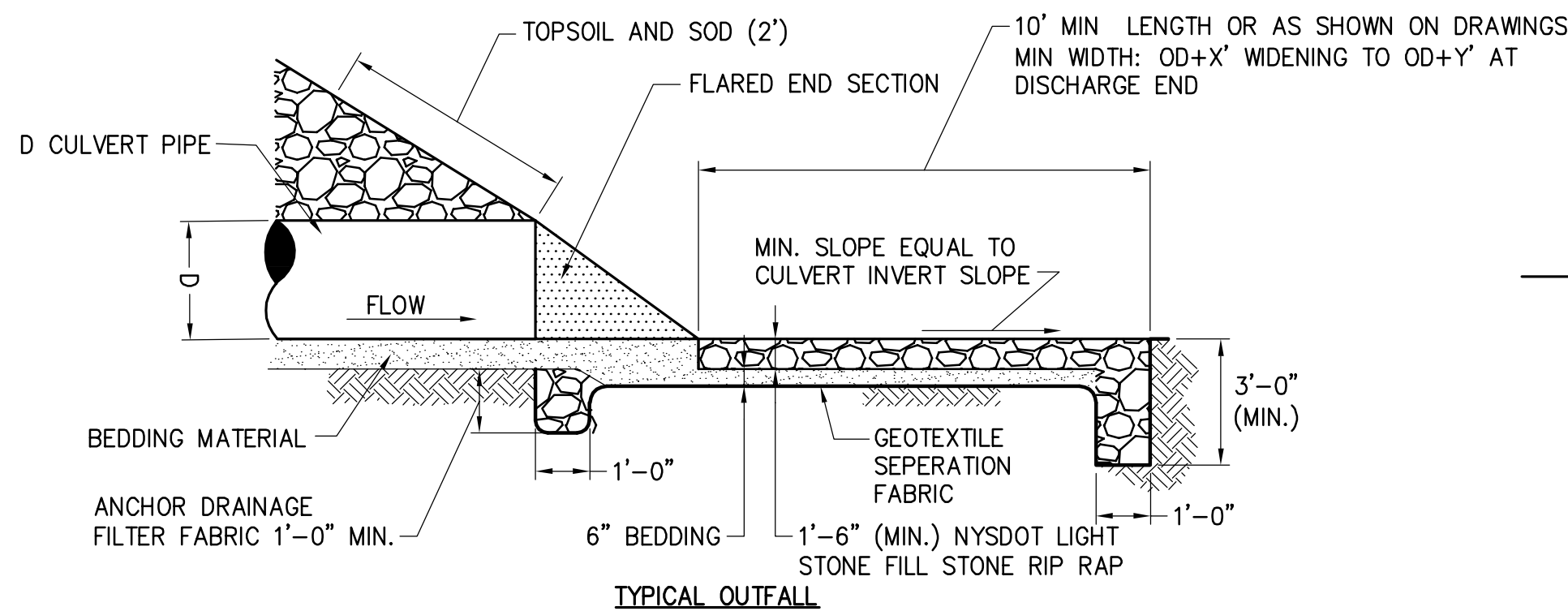


MAINTENANCE NOTES:

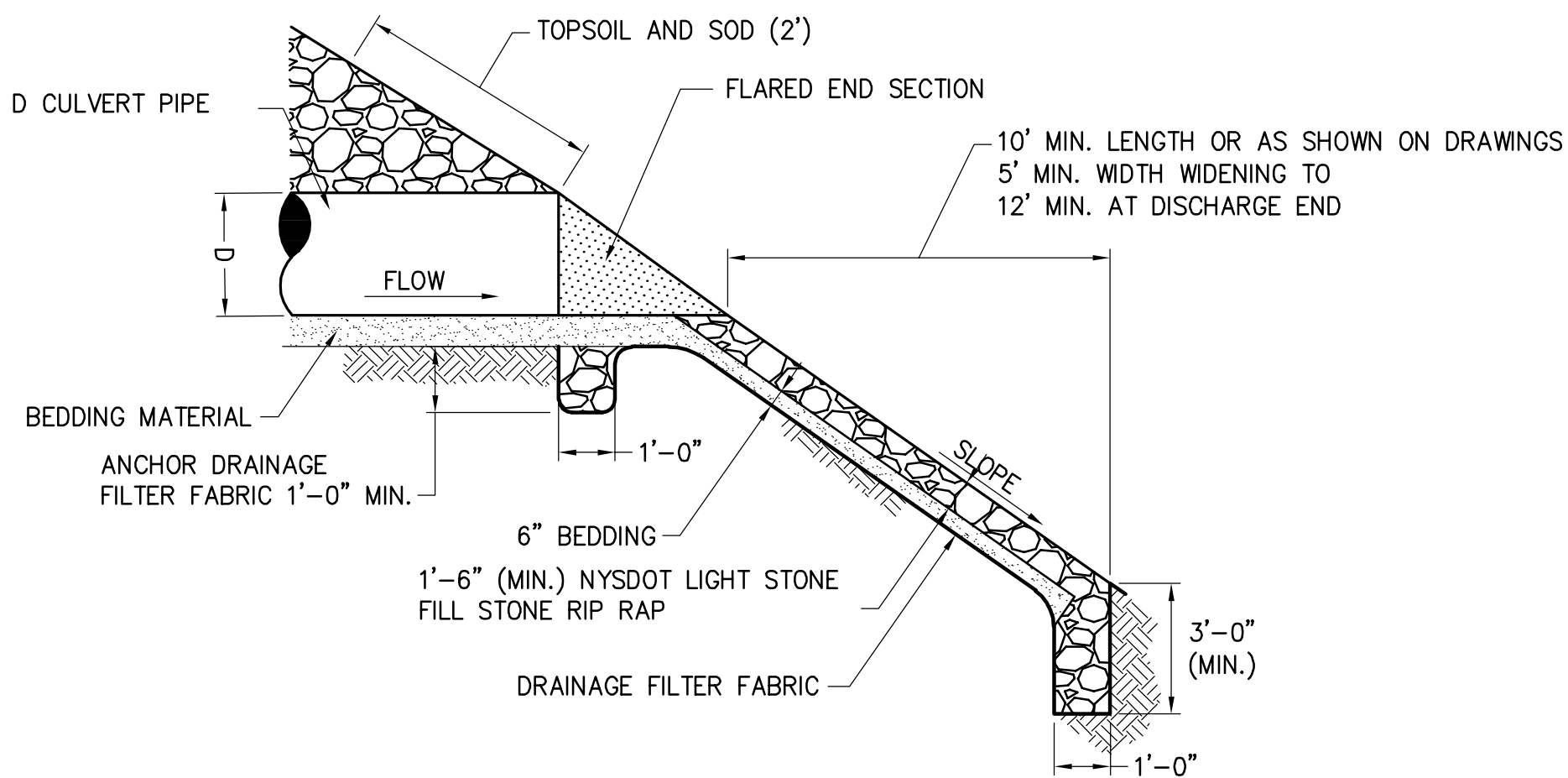
1. TRAFFIC SHALL NOT BE PERMITTED TO CROSS FILTER SOCKS.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES  $\frac{1}{2}$  OF THE EXPOSED HEIGHT OF THE PRACTICE AND DISPOSED OF IN ACCORDANCE WITH THE SWPPP.
3. SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED IN THE MANNER REQUIRED BY THE MANUFACTURER OR REPLACED WITHIN 24 HOURS OF INSPECTION NOTIFICATION.
4. BIODEGRADABLE FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTO-DEGRADABLE FILTER SOCKS AFTER 1 YEAR. POLY-PROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.
5. UPON STABILIZATION OF THE AREA CONTRIBUTORY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK SHALL BE REMOVED. FOR REMOVAL THE MESH CAN BE CUT AND COMPOST SPREAD AS AN ADDITIONAL MULCH TO ACT AS A SOIL SUPPLEMENT.



SCALE: N.T.S.

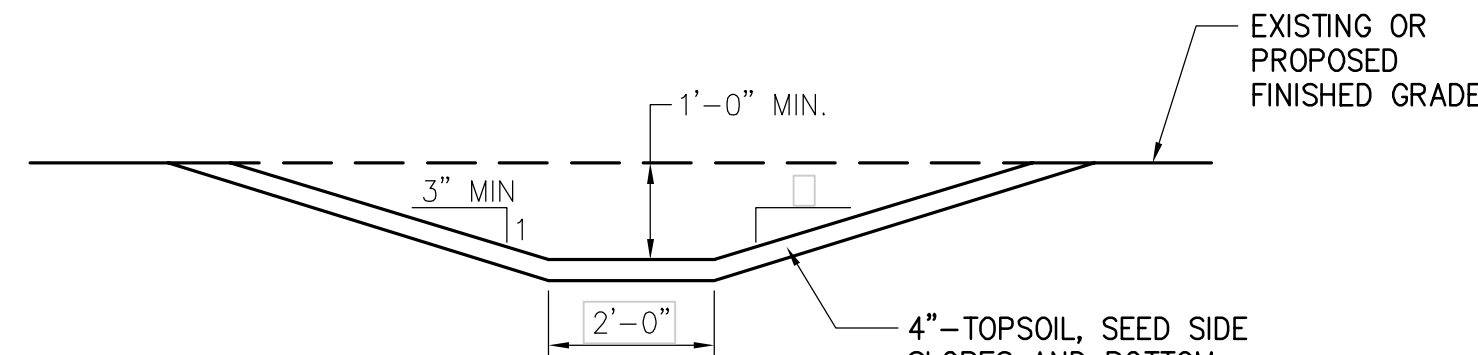


TYPICAL OUTFALL

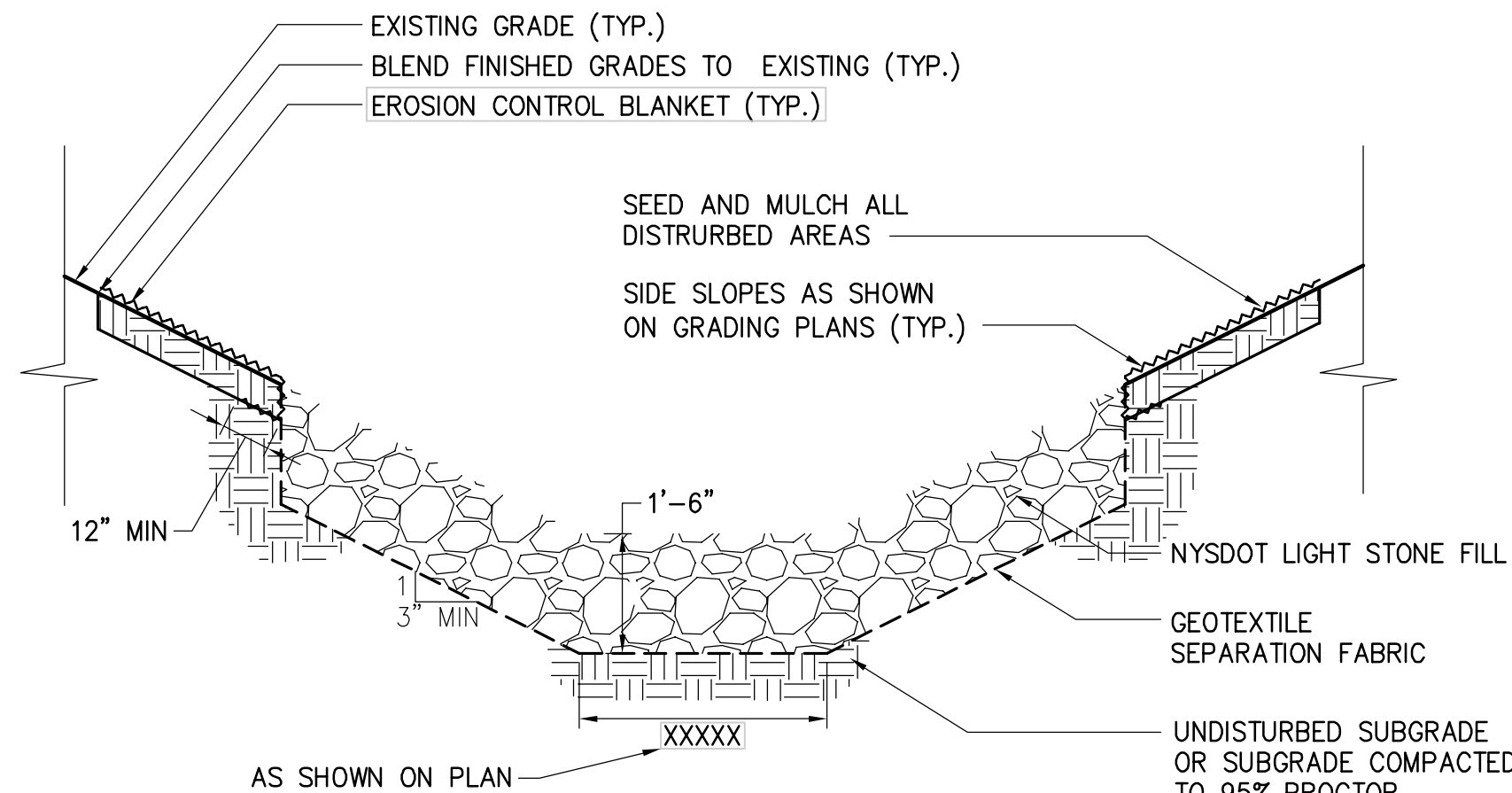


## TYPICAL OUTFALL ON SLOPE

SCALE: N.T.S.



SCALE: N.T.S.



SCALE: N.T.S.

# CHAMPLAIN HUDSON POWER EXPRESS

## SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

### EROSION AND SEDIMENT CONTROL DETAILS

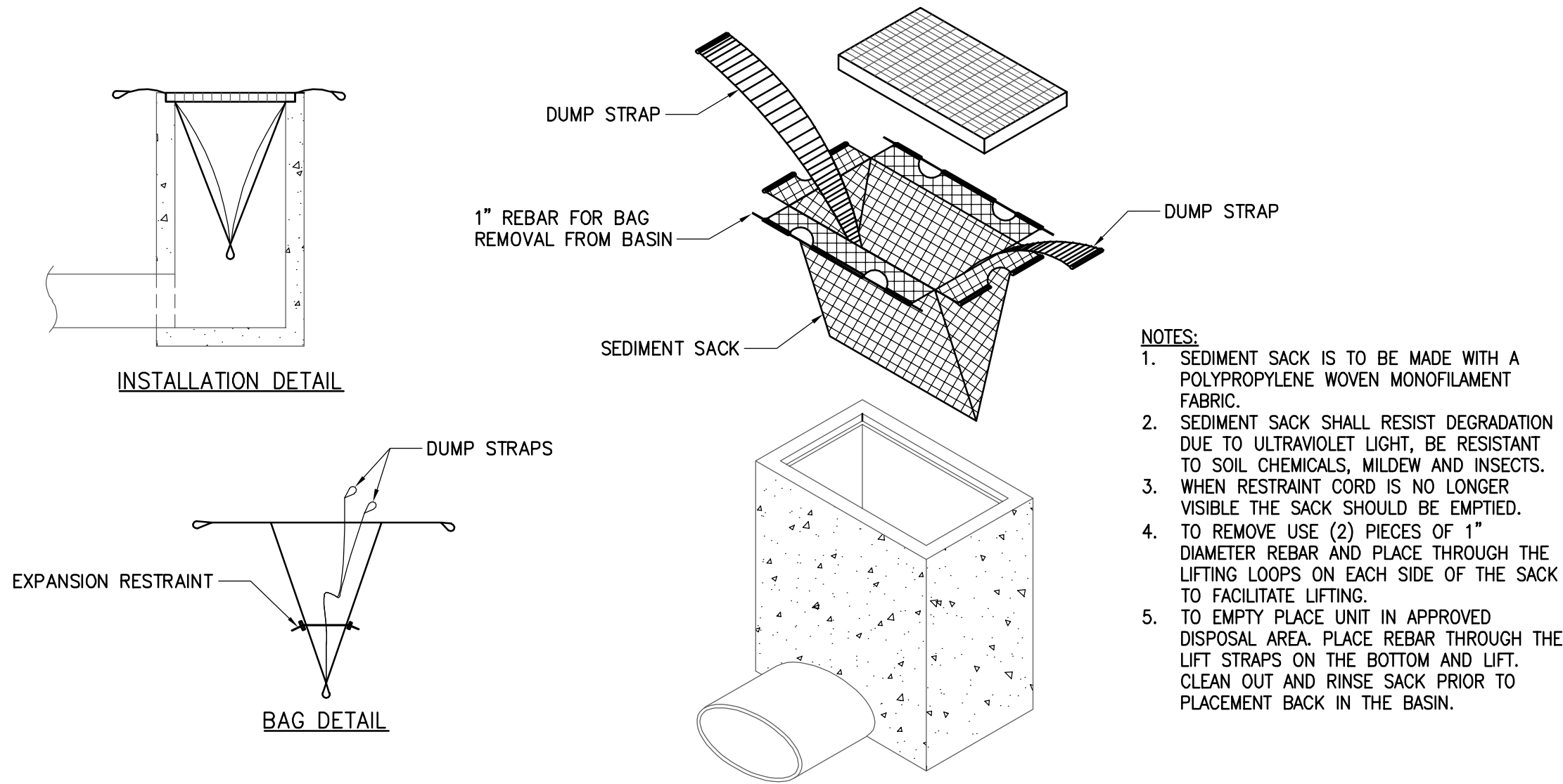
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21162
CHA PROJECT NO.
066076
DRAWING NO.

C-601

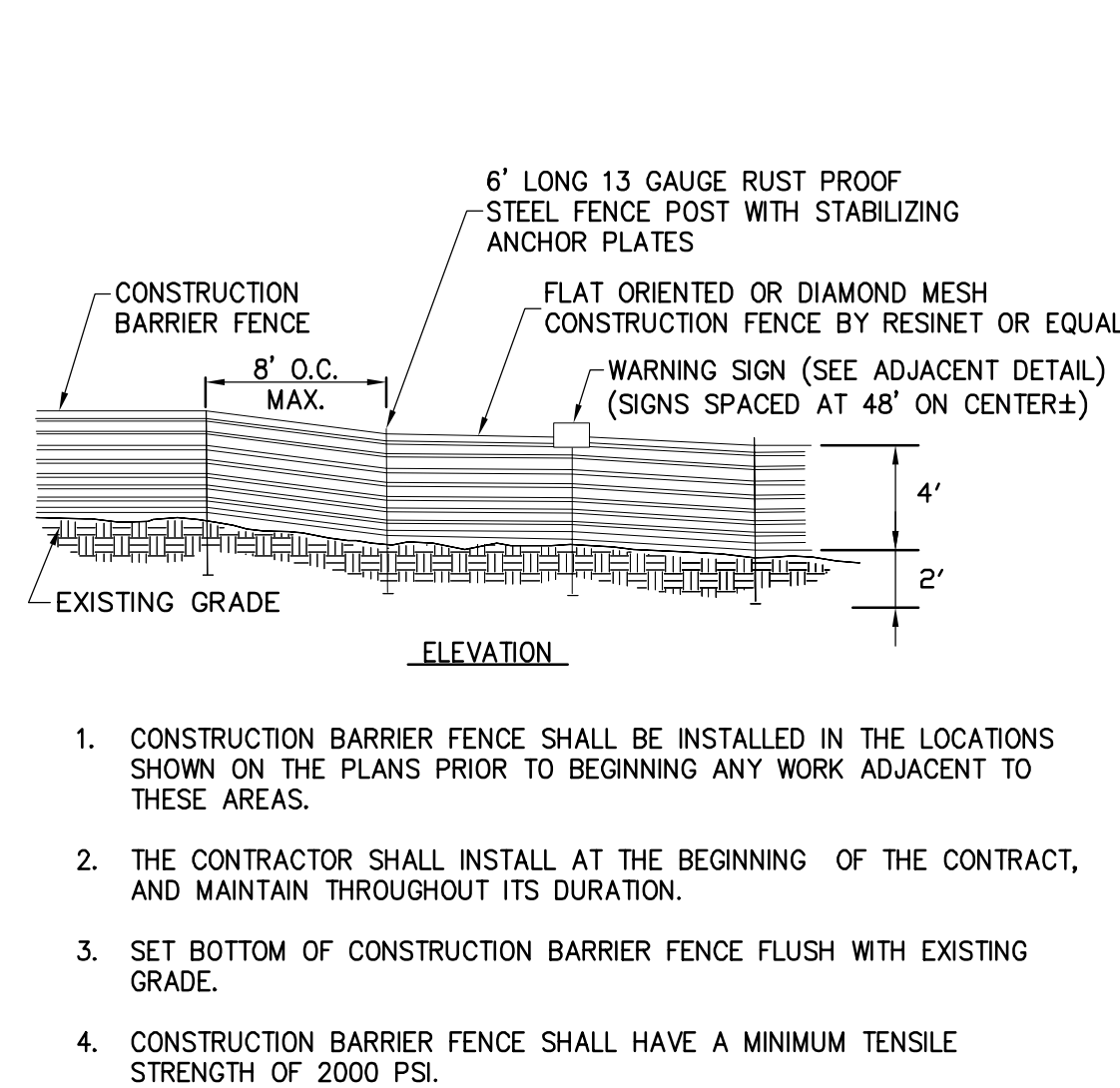
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SH.NO.	XXX OF



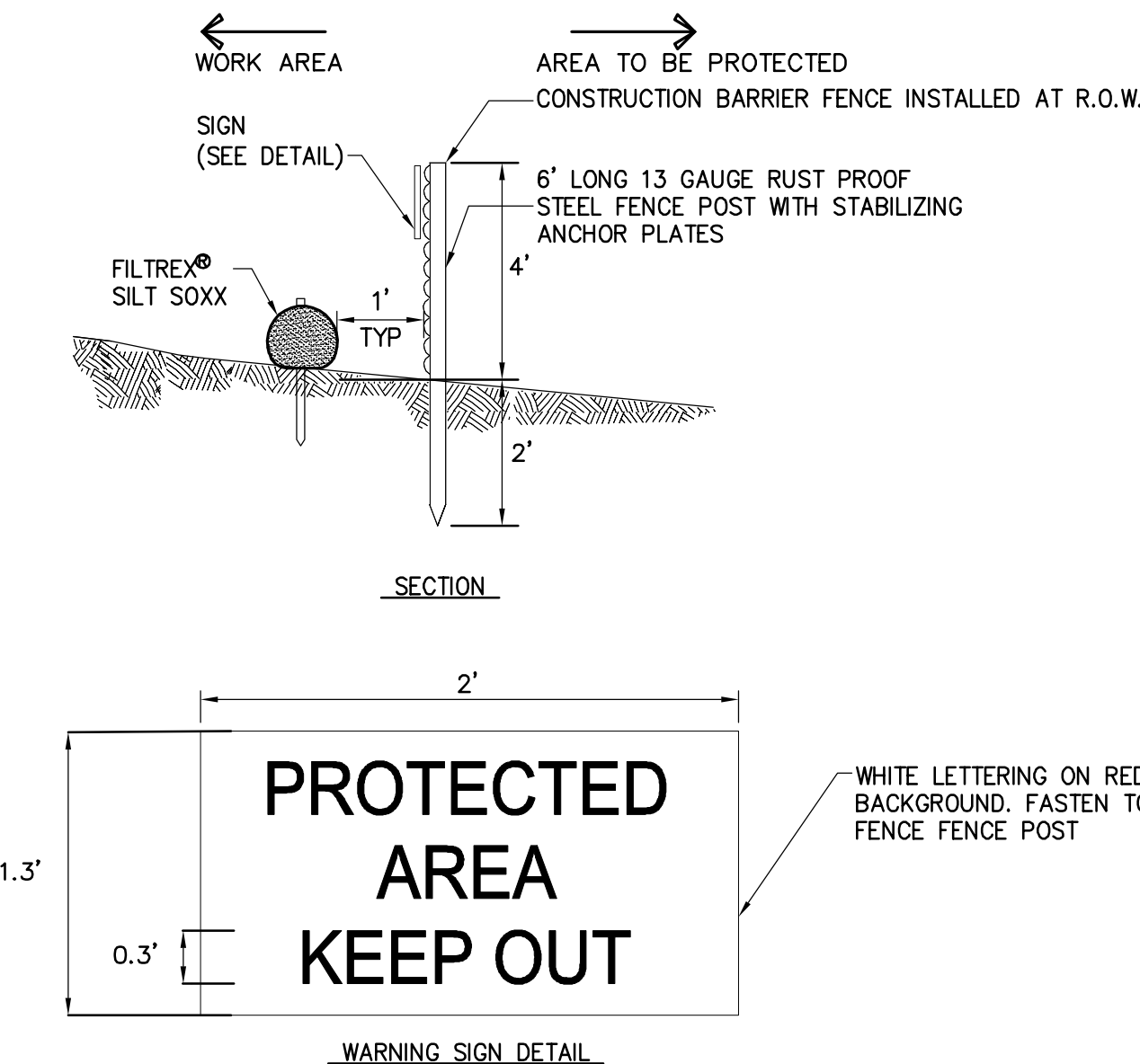
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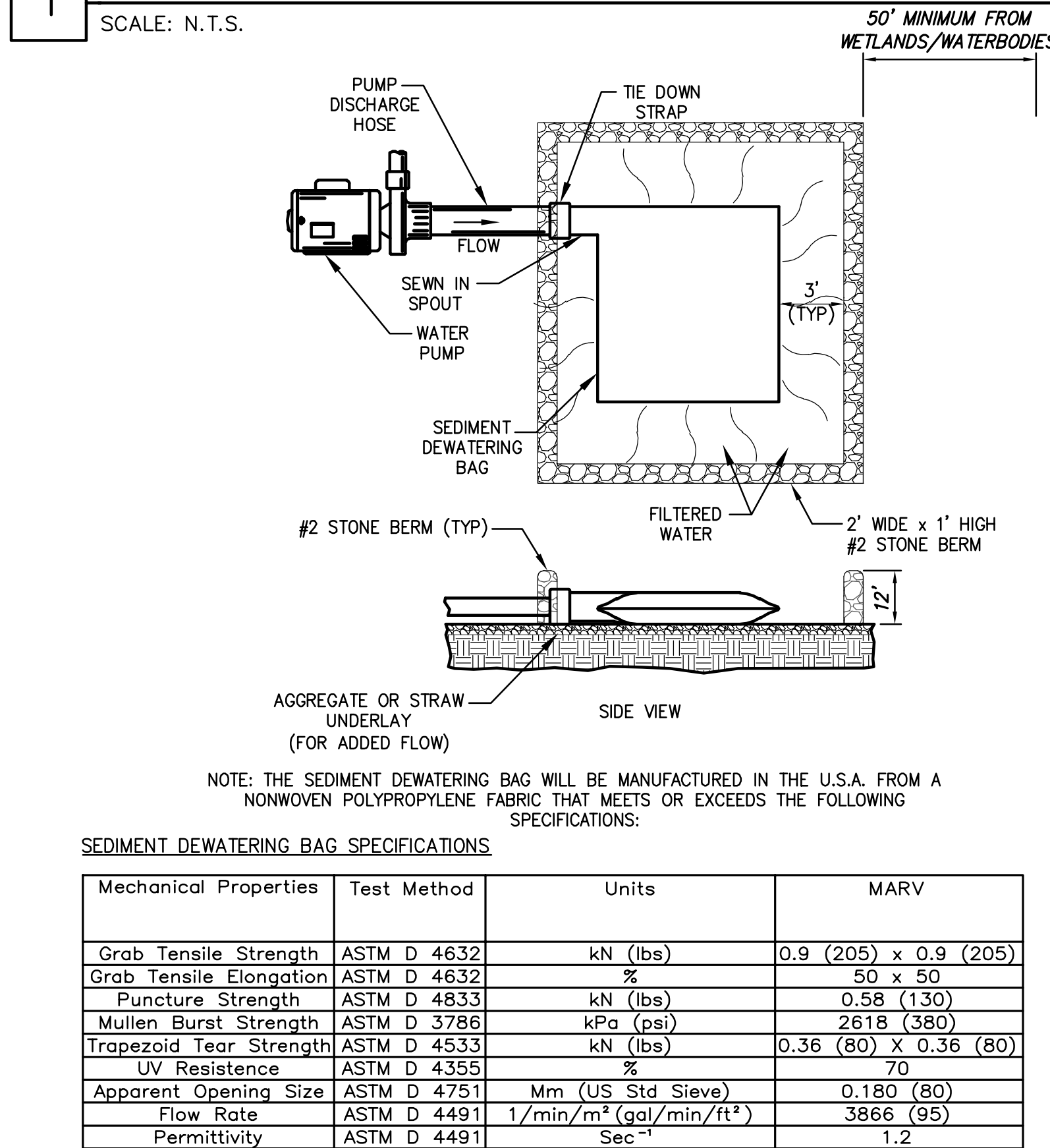
1 INLET PROTECTION  
SCALE: N.T.S.



2 WETLAND PROTECTION FENCE  
SCALE: N.T.S.

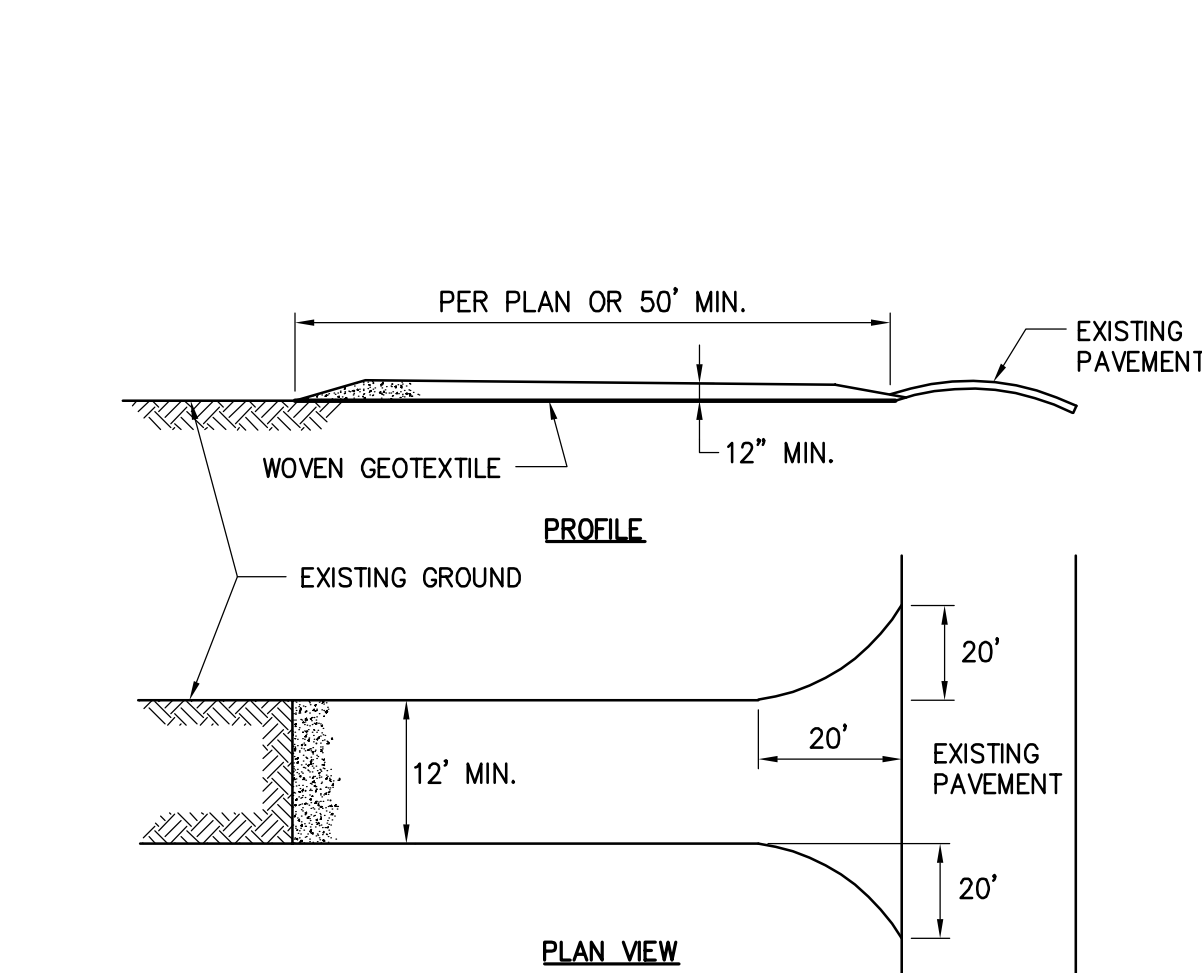


3 EROSION CONTROL BANK STABILIZATION DETAIL  
SCALE: N.T.S.



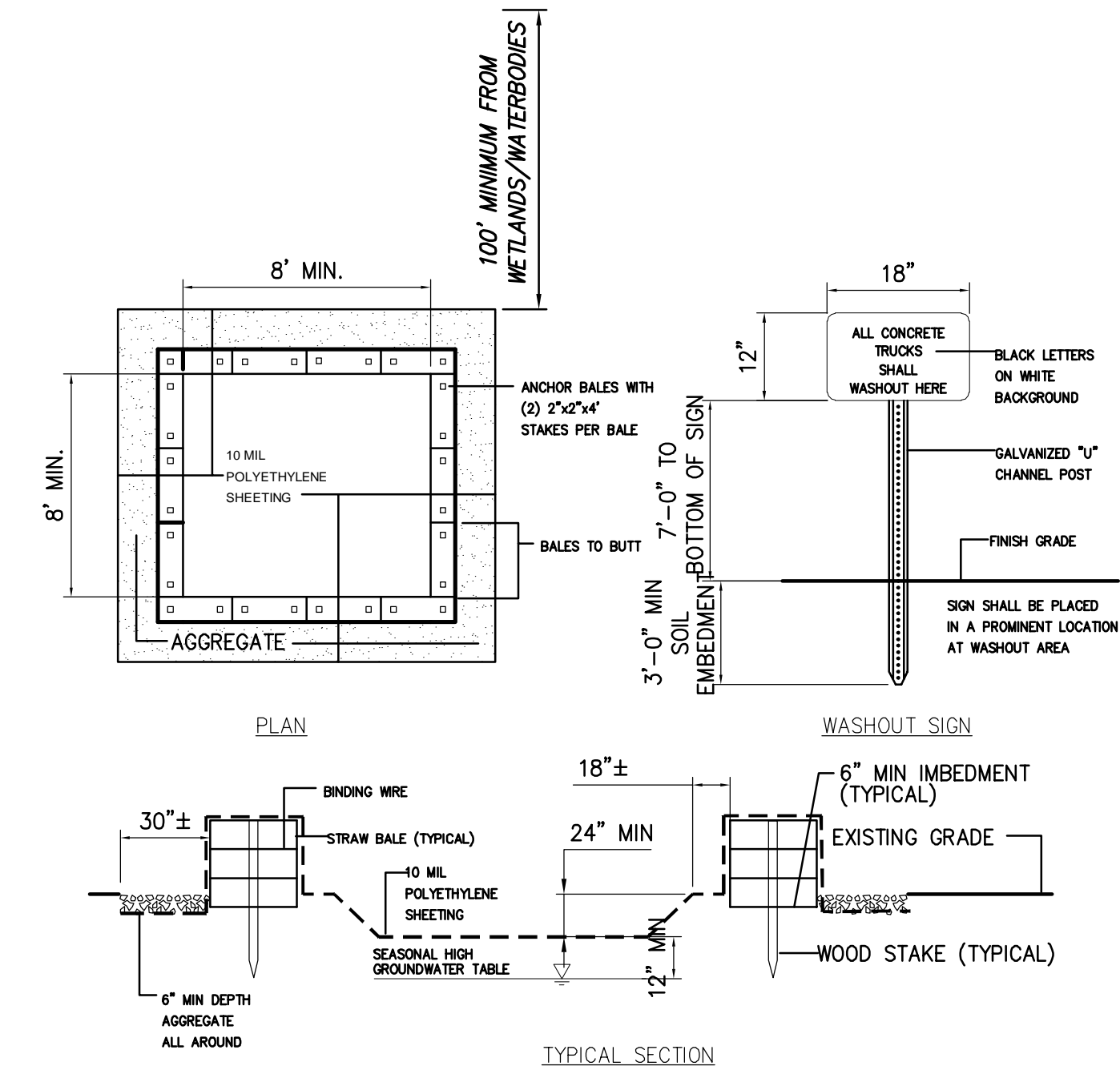
4 SEDIMENT DEWATERING BAG  
SCALE: N.T.S.

NOT FOR CONSTRUCTION  
FOR EM&CP PERMIT ONLY



5 STABILIZED CONSTRUCTION ACCESS  
SCALE: N.T.S.

- STONE SIZE—USE AASHTO M43 SIZE 3 COARSE AGGREGATE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
- LENGTH — NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY).
- THICKNESS — NOT LESS THAN 12".
- WIDTH — TWELVE (12) FOOT MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. TWENTY-FOUR (24) FOOT IF SINGLE ACCESS TO SITE.
- WOVEN GEOTEXTILE FABRIC WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE.
- EXISTING ROAD SIDE DRAINAGE SHALL BE MAINTAINED.
- SURFACE WATER — ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- MAINTENANCE—THE ACCESS SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT OR STONE SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.
- WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON A AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
- PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.



6 CONCRETE WASHOUT AREA  
SCALE: N.T.S.

- MAINTENANCE NOTES:
- ALL CONCRETE WASHOUT FACILITIES SHALL BE INSPECTED DAILY. DAMAGED OR LEAKING FACILITIES SHALL BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY. EXCESS RAINWATER THAT HAS ACCUMULATED OVER HARDENED CONCRETE SHALL BE PUMPED TO A STABILIZED AREA SUCH AS A GRASS FILTER STRIP.
  - ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE CAPACITY OF THE STRUCTURE IS FILLED. ANY EXCESS WASH WATER SHALL BE PUMPED INTO A CONTAINMENT VESSEL AND PROPERLY DISPOSED OF OFF SITE.
  - DISPOSE OF THE HARDENED MATERIAL OFF-SITE IN A CONSTRUCTION/DEMOLITION LANDFILL.
  - THE PLASTIC LINER SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.
  - INSPECT THE PROJECT SITE FREQUENTLY TO ENSURE THAT NO CONCRETE DISCHARGES ARE TAKING PLACE IN NON-DESIGNATED AREAS.
  - LOCATION(S) TO BE DETERMINED IN THE FIELD BY THE OWNER'S REPRESENTATIVE



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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
EROSION AND SEDIMENT CONTROL DETAILS

DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR SCALE: AS NOTED  
REV. NO. X

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-602
DATE	09/21/2022
SH.NO.	XXX OF XXX



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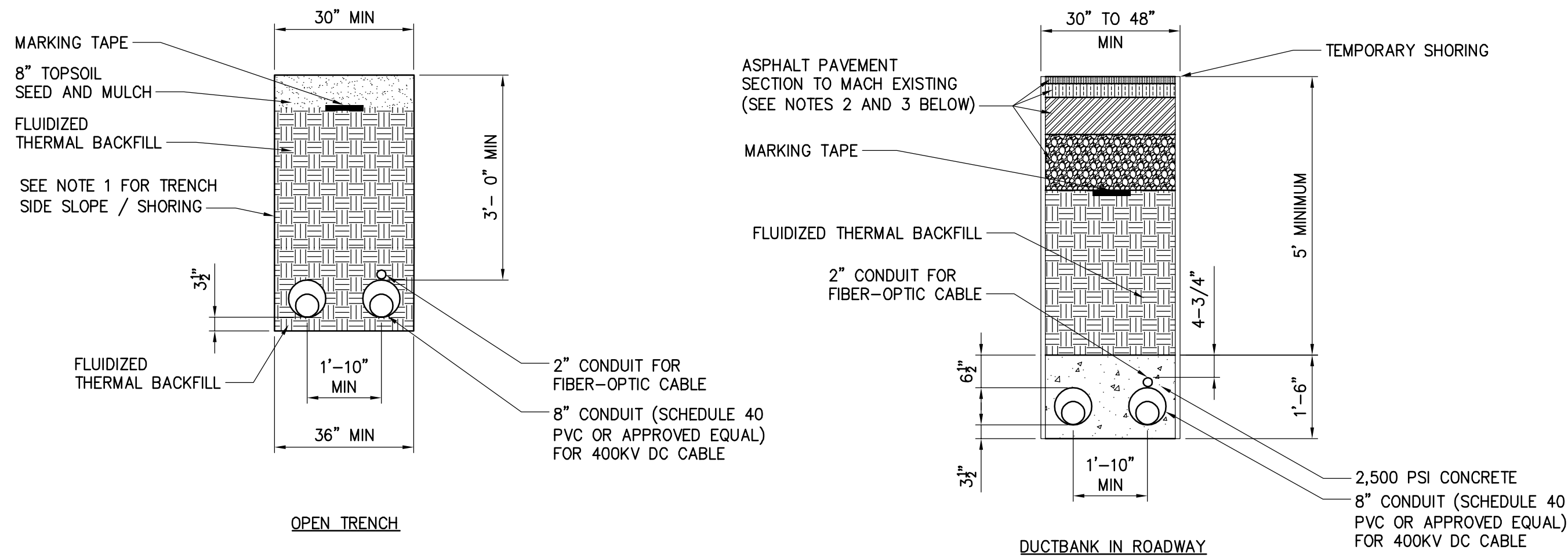


- B

## SCALE: N.T.S.







NOTE:

1. SLOPE SHALL BE BASED ON OSHA SHEETING AND SHORING REQUIREMENTS TO BE DETERMINED BY THE COMPETENT PERSON IN THE FIELD.
2. SEE DETAIL 4 ON DETAIL SHEET C-631 FOR PAVEMENT TRANSITION DETAIL.
3. SEE SHEET C-631 FOR SURFACE RESTORATION DETAILS.

**2 36" WIDTH. 22" CENTER TO CENTER SPACING**  
NOT TO SCALE



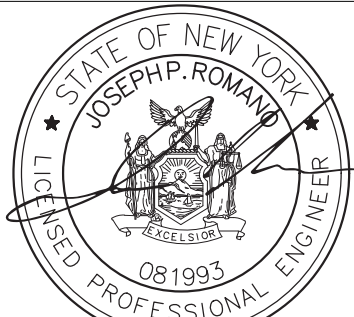
4 8"-10" PVC/HDPE TRANSITION COUPLING  
NOT TO SCALE



# Kiewit



III Winners Circle, PO Box 5269  
Albany, NY 12205-0269  
518.453.4500 • [www.chacompanies.com](http://www.chacompanies.com)



09/18/2022

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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

# CHAMPLAIN HUDSON POWER EXPRESS

## SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

### TRENCHING DETAILS

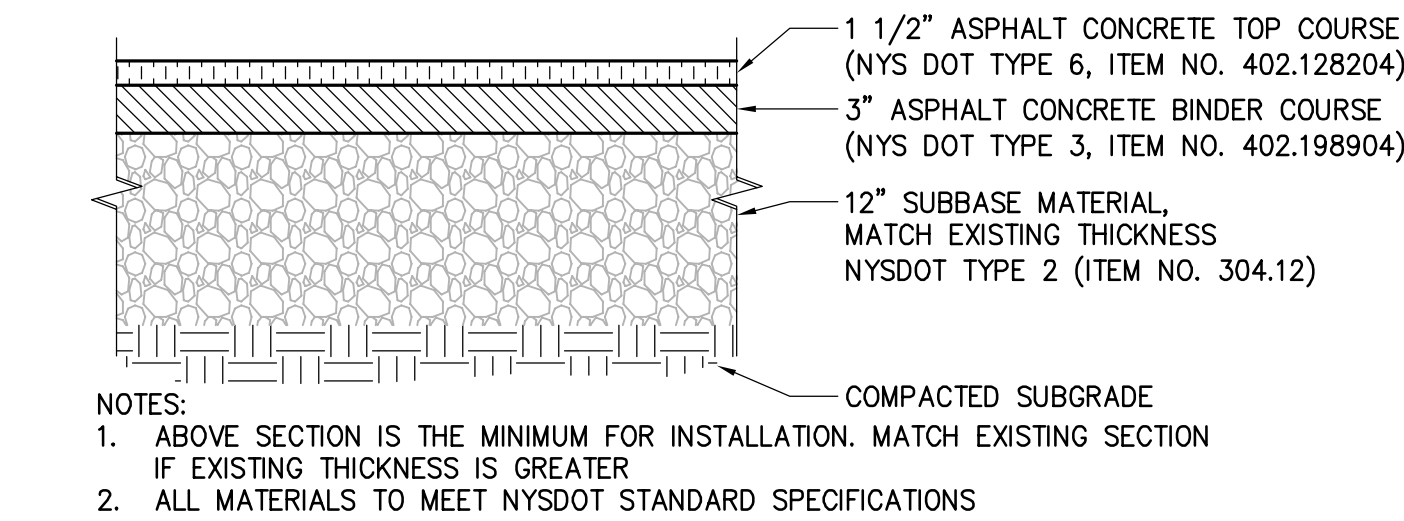
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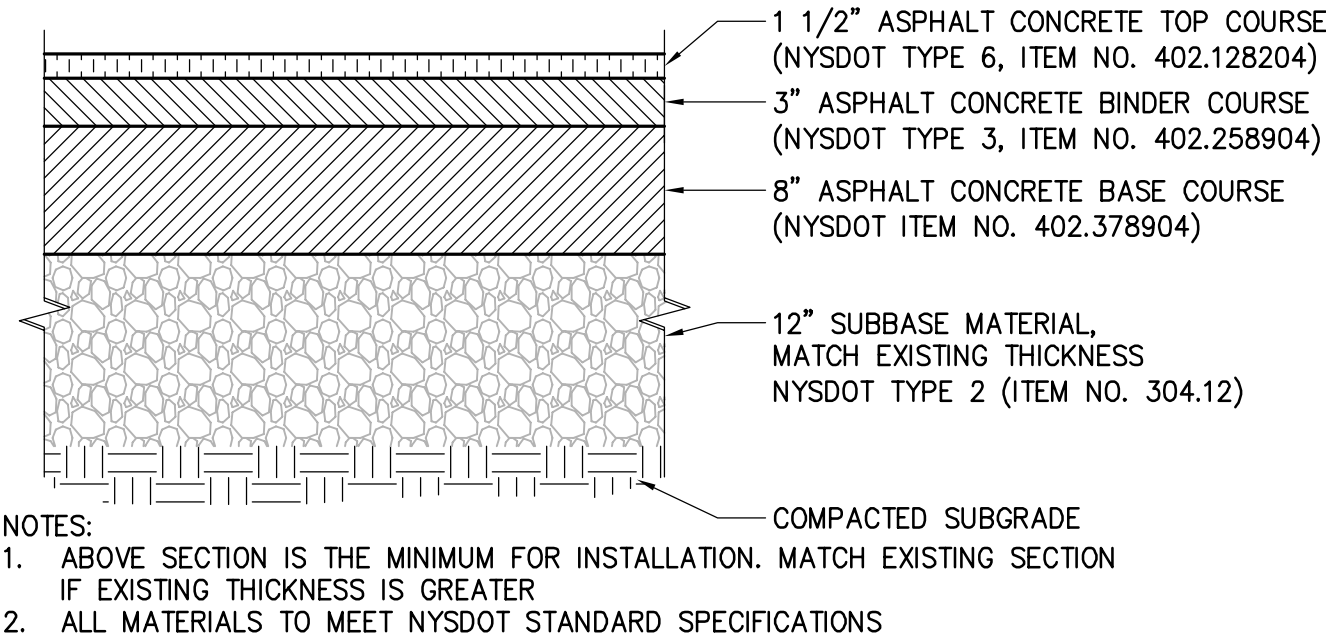
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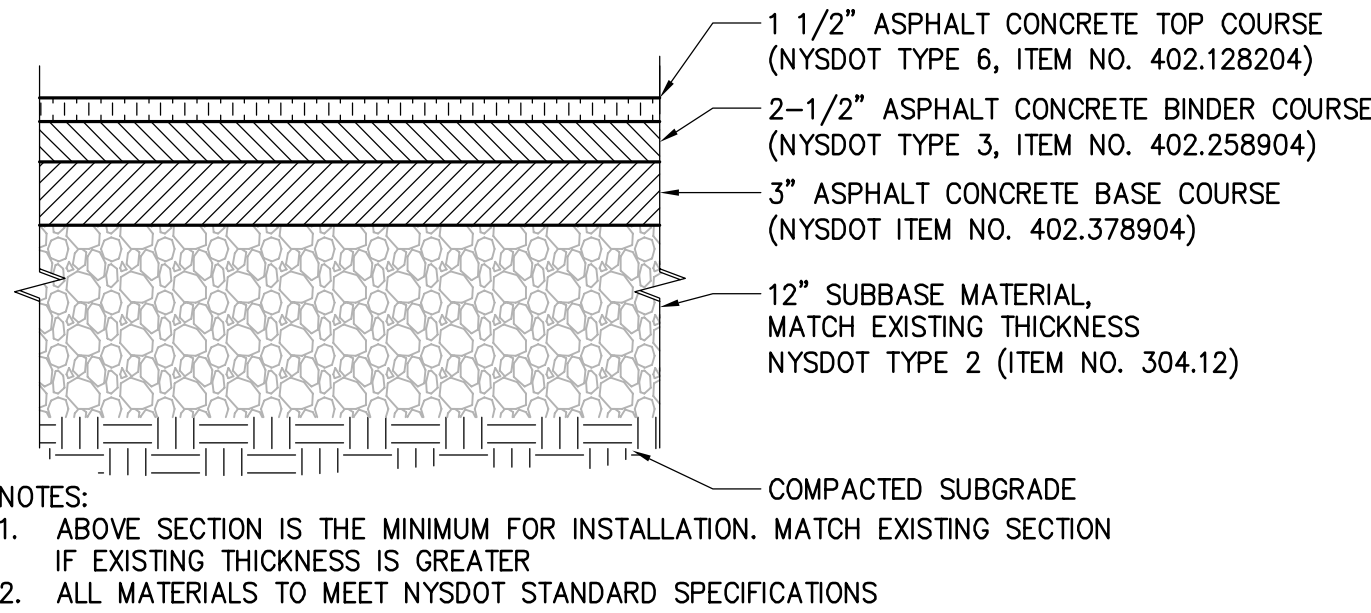
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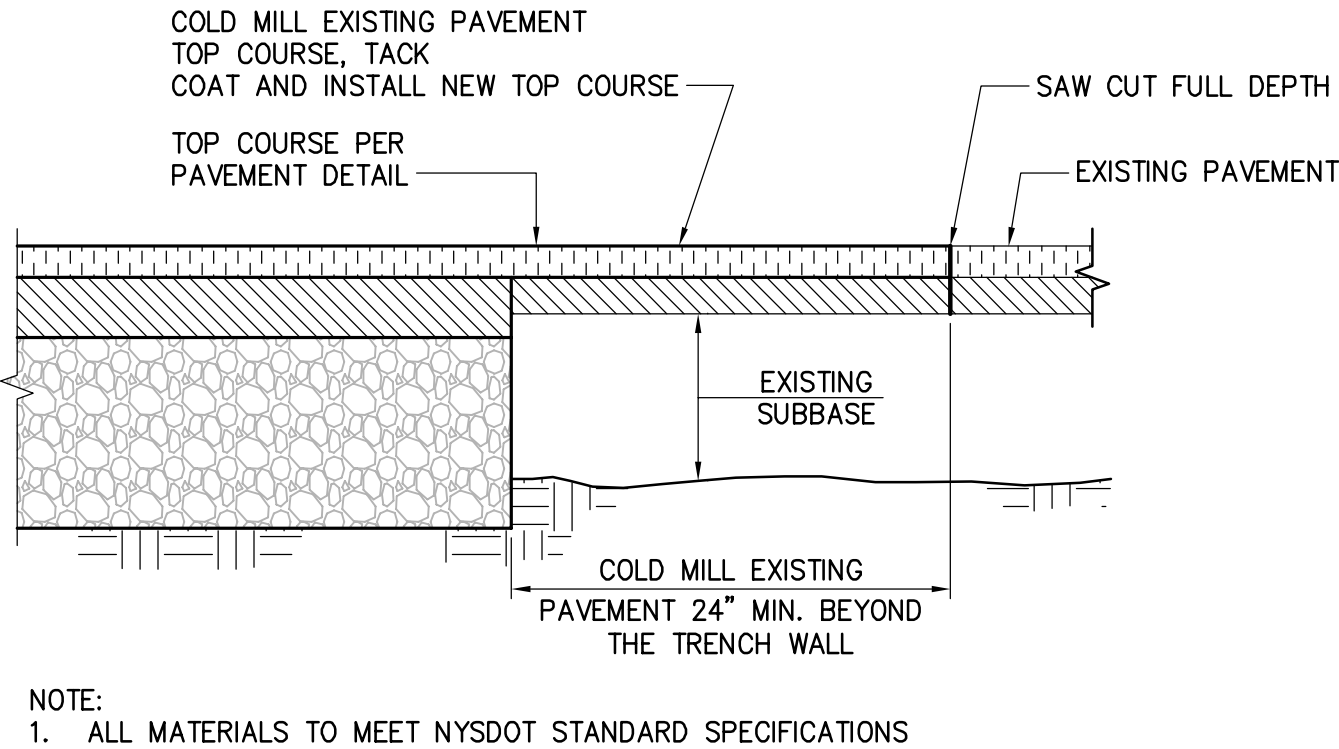
1 ASPHALT CONCRETE PAVEMENT DETAIL  
(PRIVATE DRIVEWAY)  
SCALE: N.T.S.



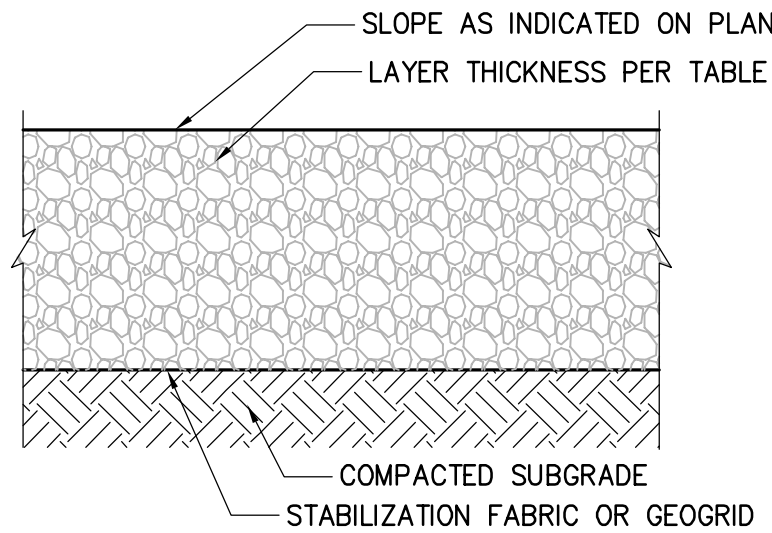
2 ASPHALT CONCRETE PAVEMENT  
(WITHIN NYSDOT ROADWAYS)  
SCALE: N.T.S.



3 ASPHALT CONCRETE PAVEMENT  
(WITHIN COUNTY OR TOWN ROADWAYS)  
SCALE: N.T.S.



4 PAVEMENT TRANSITION DETAIL  
SCALE: N.T.S.

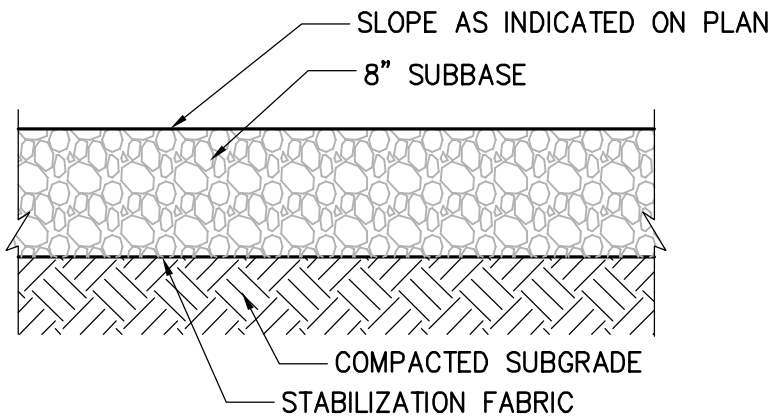


5 TEMPORARY ACCESS ROAD  
SCALE: N.T.S.

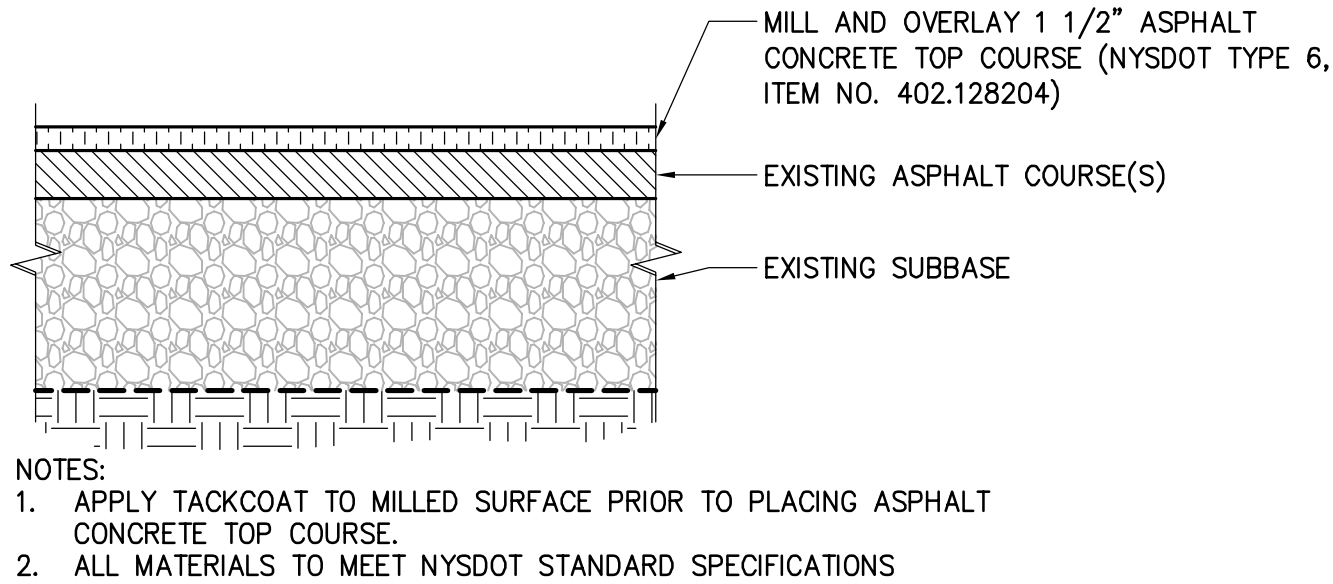
TEMPORARY ACCESS ROAD SECTION			
CBR	180N GEOTEXTILE	BX1200 GEOGRID <sup>2</sup>	RD580I GEOTEXTILE
0.5	24 INCH RIP RAP <sup>3</sup> + 6 INCH AGGREGATE <sup>4</sup>	--	24 INCH RIP RAP + 4 INCH AGGREGATE
1.0	20 INCH AGGREGATE	12 INCH AGGREGATE	9 INCH AGGREGATE
1.5	15 INCH AGGREGATE	9 INCH AGGREGATE	--
2.0	12 INCH AGGREGATE	6 INCH AGGREGATE	--
3.0+	9 INCH AGGREGATE	6 INCH AGGREGATE	--

NOTES:  
<sup>1</sup> ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER.  
<sup>2</sup> SEPARATOR GEOTEXTILE FABRIC SUCH AS MIRAFI 140N SHALL BE INCLUDED BENEATH GEOGRID LAYER ON COHESIVE SUBGRADE  
<sup>3</sup> RIP RAP TO BE NYSDOT LIGHT STONE FALL  
<sup>4</sup> A LAYER OF #57 STONE MAY BE REQUIRED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE FABRIC.  
<sup>5</sup> GEOGRID AND GEOTEXTILES SHALL BE INSTALLED PER MANUFACTURES SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS

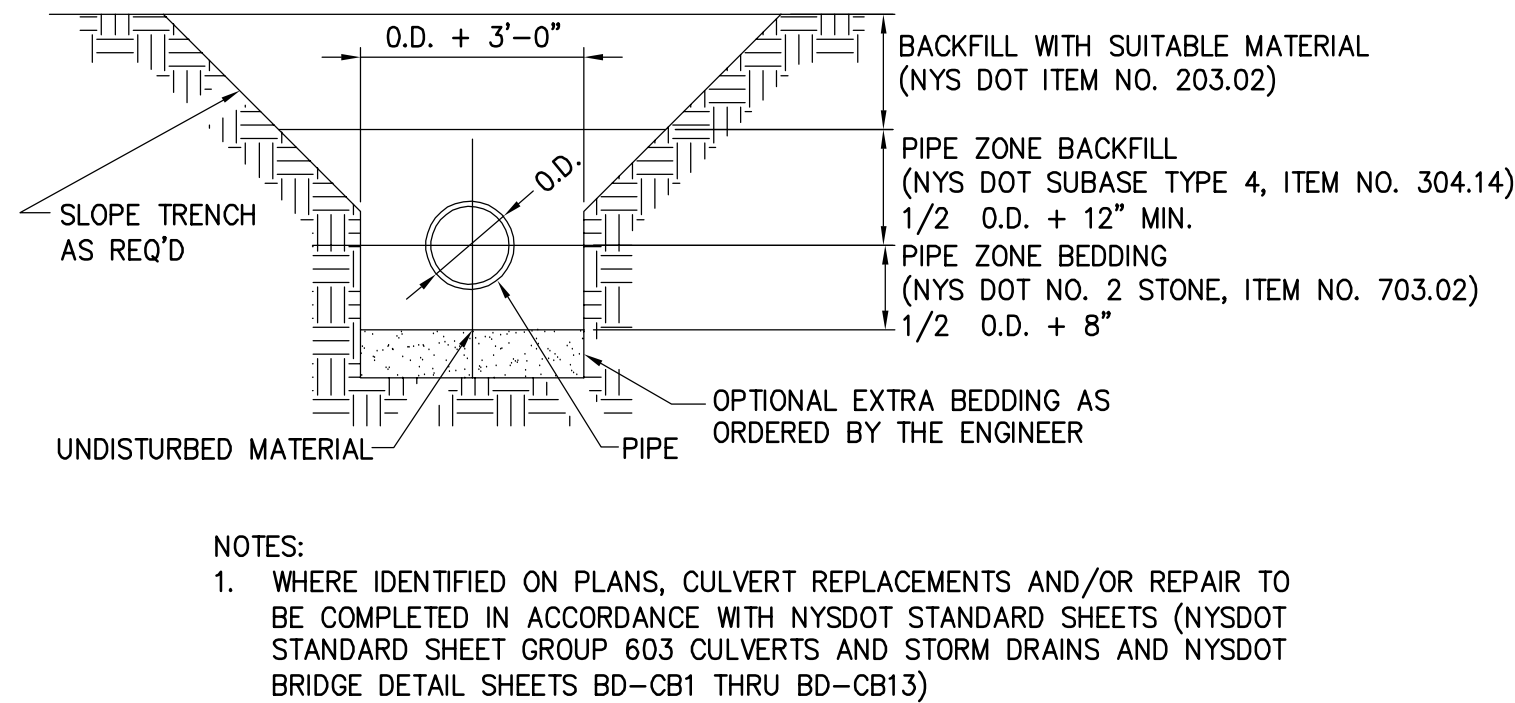
NOTE:  
ACCESS ROAD SECTIONS PER KIEWIT GEOTECHNICAL



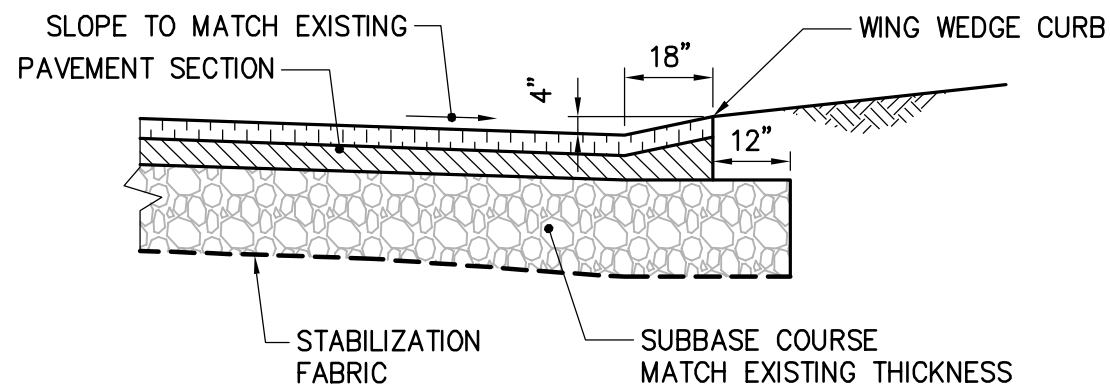
6 GRAVEL PAVEMENT  
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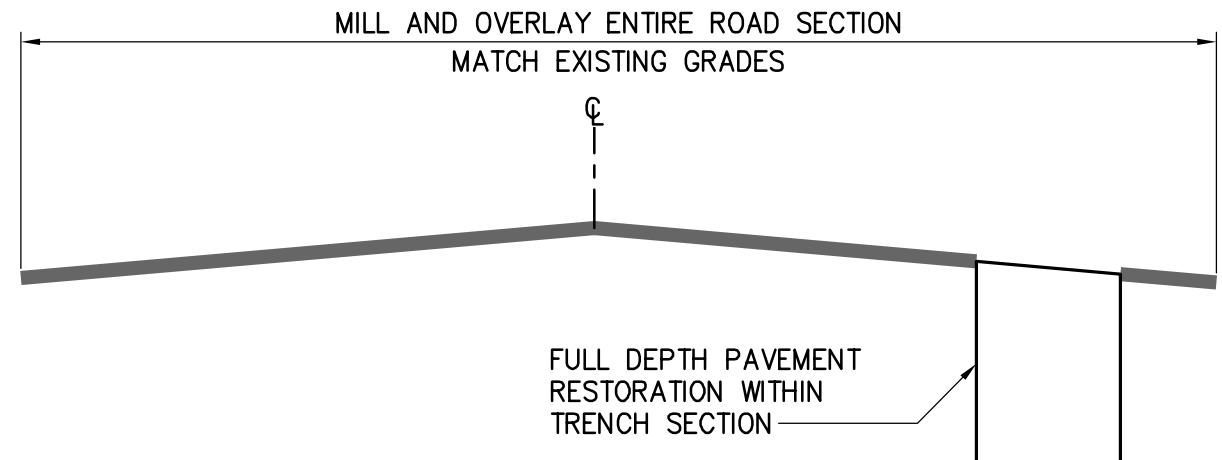
7 MILL AND OVERLAY ASPHALT CONCRETE PAVEMENT DETAIL  
SCALE: N.T.S.



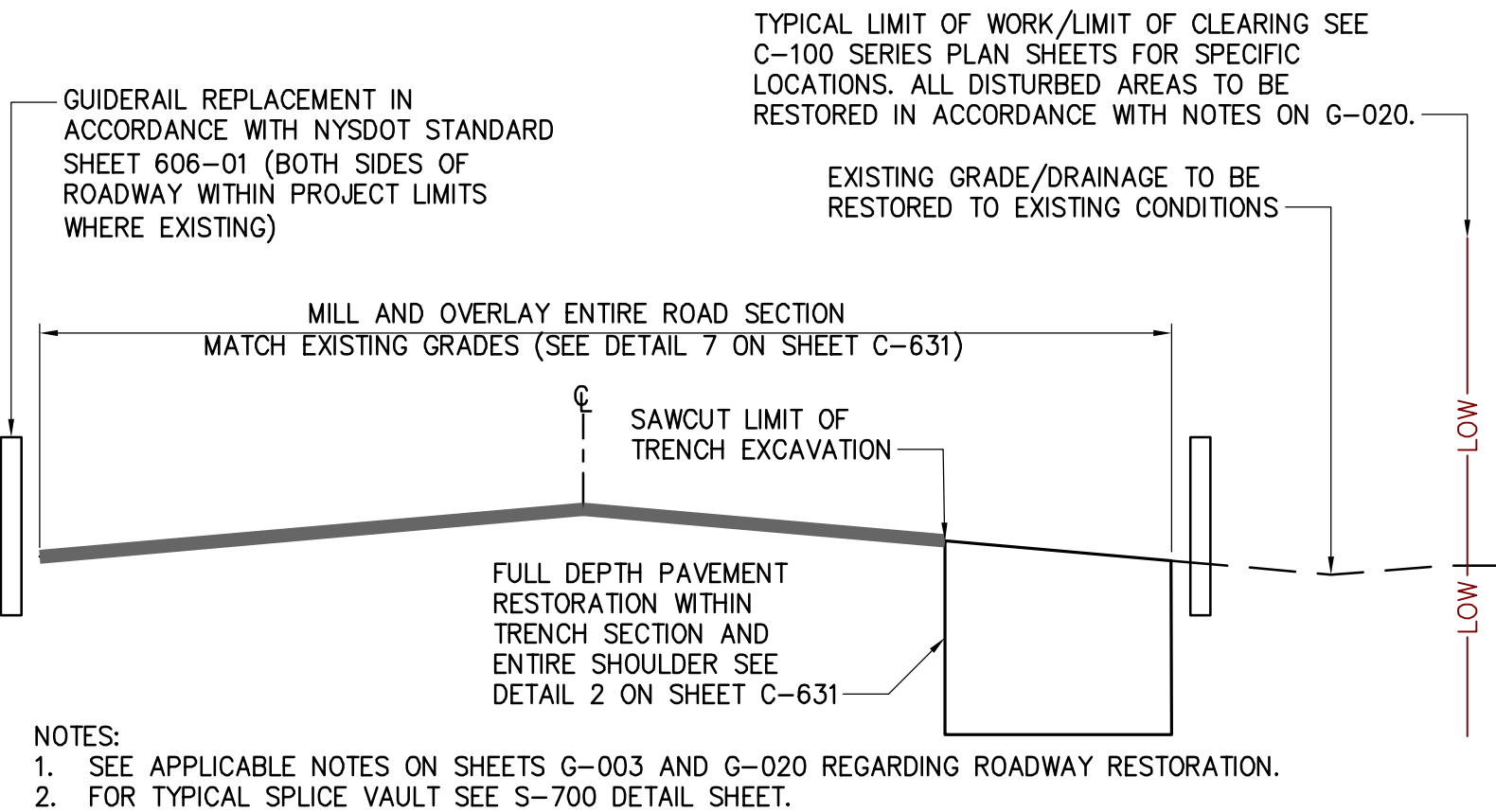
8 CULVERT REPLACEMENT  
SCALE: N.T.S.



9 WING WEDGE CURB DETAIL  
SCALE: N.T.S.

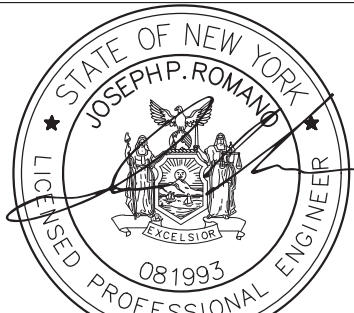


10 BELLAMY ST  
TYPICAL ROADWAY RESTORATION  
SCALE: N.T.S.



11 NYSDOT ROUTE 22  
TYPICAL ROADWAY RESTORATION  
SCALE: N.T.S.

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09/18/2022

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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
0	09/21/2022	FINAL EM&CP SUBMISSION	JM	JR

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
SURFACE RESTORATION DETAILS

DRAWN BY: JJE DESIGNED BY: JTM APPROVED BY: JPR SCALE AS NOTED DATE 09/21/2022  
REV. NO. X SH.NO. XXX OF XXX

KIEWIT PROJECT NO.  
21162  
CHA PROJECT NO.  
068076  
DRAWING NO.

C-631





1. INSTALL ALL EROSION AND SEDIMENT CONTROL MEASURES.
2. INSTALL MAINTENANCE AND PROTECTION OF TRAFFIC MEASURES.
3. INSTALL A SANDBAG COFFERDAM AT THE UPSTREAM END TO SUPPORT BY-PASS PUMPING.
4. INSTALL A RIPRAP APRON AT THE DOWNSTREAM END TO SUPPORT BY-PASS PUMPING.
5. INSTALL A TRENCH ACROSS THE ROAD TO RUN BY-PASS PUMPING PIPE AND BACKFILL OVER TOP OF THE PIPE WITH CRUSHED STONE TO MATCH THE GRADES OF THE EXISTING DRIVING SURFACE.
6. EXCAVATE THE DOWNSTREAM PORTION OF THE ROADWAY (FIRST HALF OF ROAD).
7. PREPARE SUBGRADE AND SUBBASE AND INSTALL HALF OF THE NEW CULVERT INLINE WITH THE EXISTING CULVERT. MATCH THE DOWNSTREAM INVERT TO THE TO THAT OF THE PREVIOUS DOWNSTREAM INVERT OF THE CULVERT REPLACED. (REMOVE A MINIMUM 100' OF PAVEMENT IN EACH DIRECTION FROM THE CULVERT)
8. INSTALL THE HEADWALL SECTION AND RIPRAP SECTION ALONG THE DOWNSTREAM SIDE (IF REQUIRED)
9. BACKFILL AND COMPACT IN LIFTS AROUND THE HALF SECTION OF NEWLY INSTALLED CULVERT AND INSTALL A COMPACTED CRUSHED STONE DRIVING SURFACE OVER TOP OF THE BACKFILLED AREA.
10. ADJUST MAINTENANCE AND PROTECTION OF TRAFFIC MEASURES AS NECESSARY TO INSTALL THE SECOND HALF OF THE CULVERT.
11. EXCAVATE THE UPSTREAM PORTION OF ROADWAY (SECOND HALF OF ROAD).
12. PREPARE SUBGRADE AND SUBBASE AND INSTALL HALF OF THE CULVERT INLINE WITH THE NEWLY INSTALLED DOWNSTREAM PORTION OF CULVERT. MATCH THE UPSTREAM INVERT TO THE TO THAT OF THE PREVIOUS UPSTREAM INVERT OF THE CULVERT REPLACED. (REMOVE A MINIMUM 100' OF PAVEMENT IN EACH DIRECTION FROM THE CULVERT)
13. INSTALL THE HEADWALL SECTION AND RIPRAP SECTION ALONG THE UPSTREAM SIDE (IF REQUIRED)
14. ALLOW FLOW THROUGH THE NEWLY INSTALLED CULVERT BY REMOVING THE SANDBAG COFFERDAM.
15. BACKFILL AND COMPACT IN LIFTS THE EXCAVATED AREAS AROUND THE NEW CULVERT SECTION AND INSTALL A CRUSHED STONE DRIVING SURFACE.
16. ROUGH GRADE THE EMBANKMENT BACKFILL AND COMPACT IN LIFTS.
17. FINE GRADE THE ROADWAY SUBBASE AND BASE COURSES TO MATCH THE EXISTING GRADES.
18. INSTALL THE PORTION OF ROADWAY SECTION ABOVE THE INSTALLED CULVERT TO MATCH EX. GRADE.
19. TOPSOIL SEED AND MULCH THE GRADED EMBANKMENT AREA.
20. REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES AT THE SUBSTANTIAL COMPLETION OF THE CULVERT REPLACEMENT

**BY-PASS PUMPING NOTE:**

1. CONTRACTOR SHALL PROVIDE 24-HOUR BY-PASS PUMPING OF THE STREAM DURING THE REMOVAL OF EXISTING CULVERT THROUGH THE INSTALLATION OF THE NEW CULVERT.

X	TYPICAL
	SCALE: N.T.S.



1. SAND BAGS SHALL BE FILTER FABRIC TYPE AND BE DOUBLE BAGGED.
2. PORTADAM, BY PORTADAM, INC. SHALL BE CONSIDERED ACCEPTABLE SUBSTITUTE TO SAND BAGS.

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X SCALE: N.T.S



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# CHAMPLAIN HUDSON POWER EXPRESS

## SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

### TYPICAL CULVERT REPLACEMENT SEQUENCING DETAILS

KIEWIT PROJECT NO.
21162
CHA PROJECT NO.
066076
DRAWING NO.

C-632

DRAWN BY: JJE	DESIGNED BY: JTM	APPROVED BY: JPR	SCALE	AS NOTED	DATE	09/21/2022
			REV. NO.	X	SH. NO.	XXX OF XXX



FILE NAME =  
DATE/TIME = 14-MAR-2019 12:39FILE NAME =  
DATE/TIME = 14-MAR-2019 12:48FILE NAME =  
DATE/TIME = 14-MAR-2019 12:42  
USER = nvdhansen

RELINING OF EXISTING ROADWAY CULVERTS SHALL BE IN ACCORDANCE WITH NYSDOT  
STANDARD SPECIFICATIONS – SECTION 706-09 CURED IN PLACE PIPE (CIPP) LINER

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# Kiewit



III Winners Circle, PO Box 5269  
Albany, NY 12205-0269  
518.453.4500 • [www.chacompanies.com](http://www.chacompanies.com)



09/18/2022

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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP

# CHAMPLAIN HUDSON POWER EXPRESS

## SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

### NYSDOT CULVERT REPAIR DETAILS

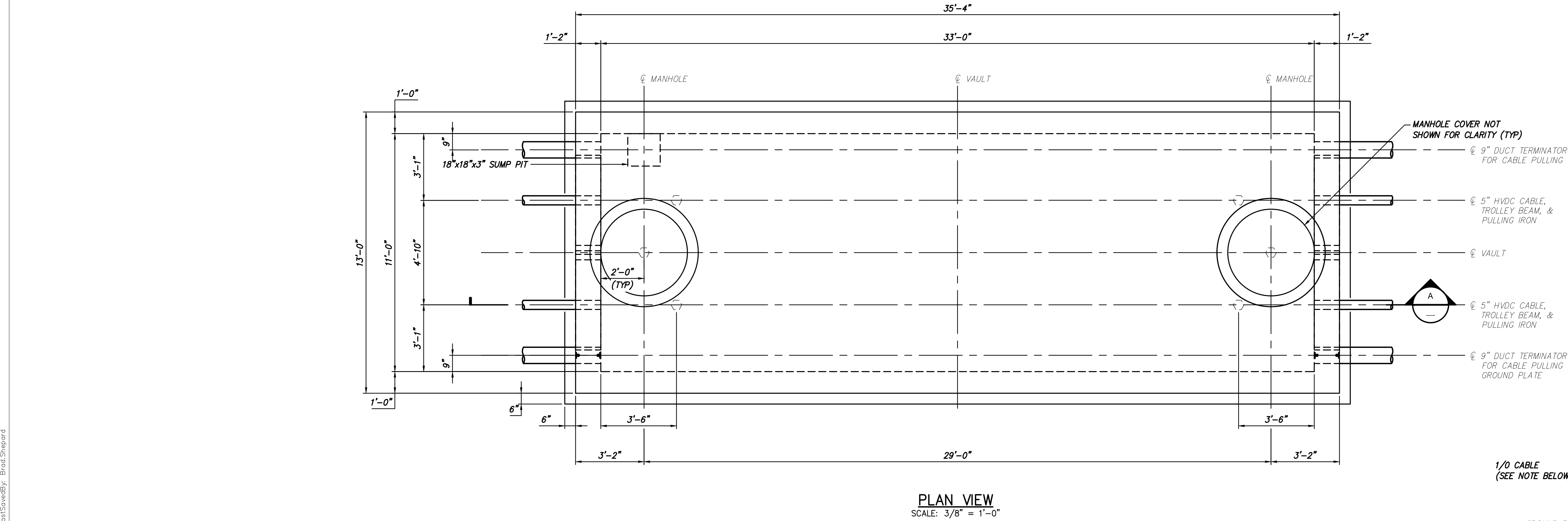
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DATE	09/21/2022
SH.NO.	XXX OF XXX

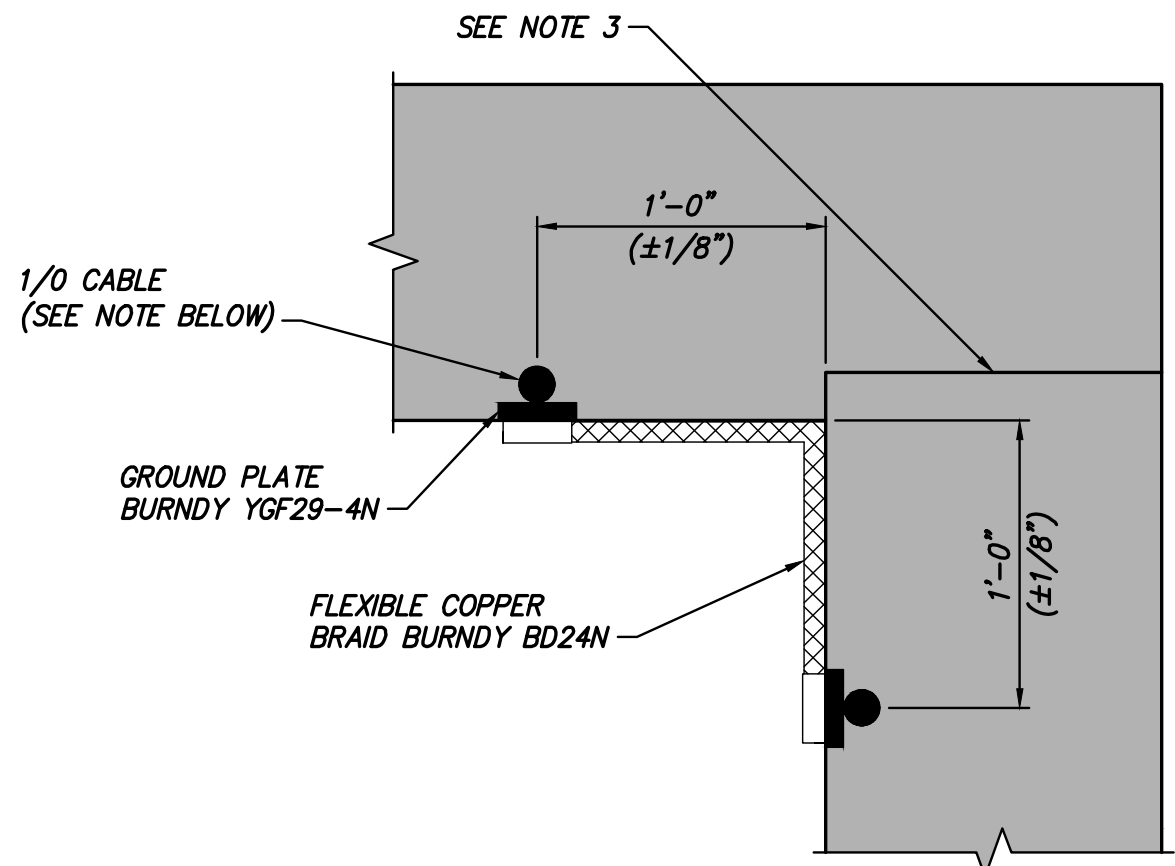


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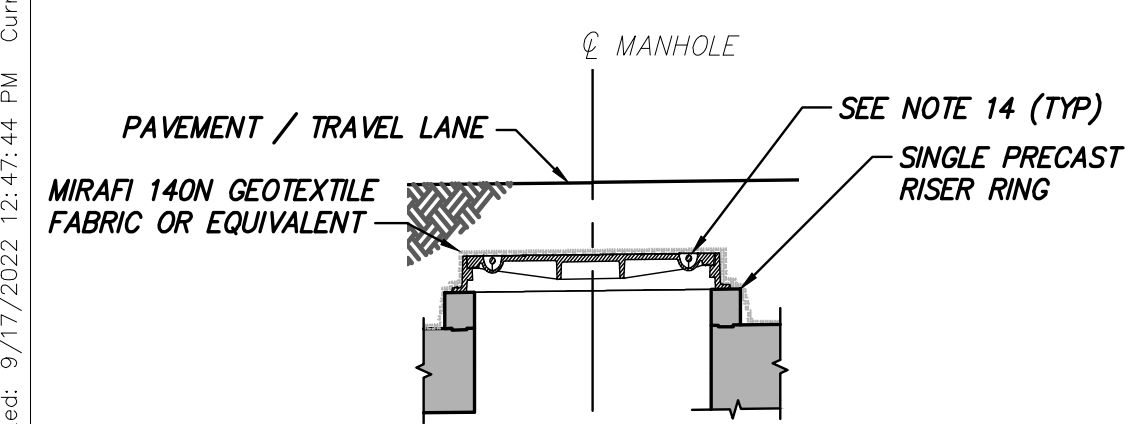
PLAN VIEW  
SCALE: 3/8" = 1'-0"

- NOTES:
- EACH ENDWALL (8.5 FT x 11 FT) IS DESIGNED FOR A SINGLE 10,000 LB PULLING IRON FORCE. THE FLOOR SLAB IS DESIGN FOR A SINGLE 51,000 LB PULLING IRON FORCE. CABLE PULLING CREW SHALL NOT USE MORE THAN ONE PULLING IRON ON FLOOR CONCURRENTLY.
  - DESIGN LIVE LOAD: HS-20
  - EXTERIOR COATING & JOINT SEALERS/WATER STOPS TO BE USED BETWEEN PRECAST JOINTS, AS SPECIFIED.
  - MAXIMUM PRECAST PIECE PICK WEIGHT LIMITED TO 50,000 LB.
  - SEE ELECTRICAL DRAWINGS FOR CABLE RACKING DETAILS & GROUND WIRE DETAILS.
  - WALL THICKNESSES TO BE FINALIZED PER APPROVED VENDOR'S DESIGN. REFER TO APPROVED VENDOR SHOP DRAWINGS FOR WEIGHTS AND PICK POINTS.
  - THREADED ANCHOR WORKING LOAD SHALL BE 1,100 LB MINIMUM.
  - LINK SEAL TO BE USED BETWEEN CABLE CONDUIT AND PENETRATION SLEEVE, AS SPECIFIED.
  - ELECTRIC SUMP PUMP TO BE PROVIDED BY OTHERS.
  - SEE SHEET S-702 FOR ANCHOR AND EMBED LOCATIONS.
  - ESTABLISH STABLE SUBGRADE CONDITIONS AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE.
  - A MINIMUM BEDDING SECTION CONSISTING OF A 4-INCH THICK MUDMAT OR 4-INCH THICK SELECT GRANULAR FILL SHALL BE PLACED ON TOP OF PREPARED SUBGRADE. ADDITIONAL BEDDING MAY BE REQUIRED AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE BASED ON IN-SITU CONDITIONS.
  - PRECAST SUPPLIER TO COORDINATE WITH MANHOLE COVER SUPPLIER FOR FIT-UP ISSUES.
  - WHERE A MANHOLE IS LOCATED UNDER TRAVEL LANES, AFTER CABLE COMMISSIONING, ADDITIONAL RISERS TO BE REMOVED, MANHOLE FRAME AND COVER TO BE LOWERED TO FINAL POSITION. MANHOLE FRAME AND COVER TO BE COVERED WITH 8' X 8' MIRAFI 140N GEOTEXTILE FABRIC OR EQUIVALENT. FULL DEPTH PAVEMENT WILL BE RESTORED PRIOR TO MILL AND OVERLAY AFTER CABLE COMMISSIONING.







DETAIL 1  
SCALE: 1 1/2" = 1'-0"

NOTE: USED TO ELECTRICALLY JOIN PRECAST CONCRETE SECTIONS TOGETHER, BY MEANS OF REBAR CONNECTIONS, TO BE APPLIED AT EACH PRECAST SECTION, SUCH THAT ALL SECTIONS ARE JOINED TOGETHER.



FINAL POSITION OF MANHOLE UNDER PAVEMENT  
DETAIL 2  
SCALE: 3/8" = 1'-0"

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0	9/21/2022	FINAL EM&CP SUBMISSION	JNK	OO

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
SPLICE VAULT PLAN \_ ELEVATION

DRAWN BY: DRH

DESIGNED BY: JNK

APPROVED BY: OO

SCALE AS SHOWN

DATE 06/09/2022

SH.NO. XX OF XXX

KIEWIT PROJECT NO. 21162

CHA PROJECT NO. 066076

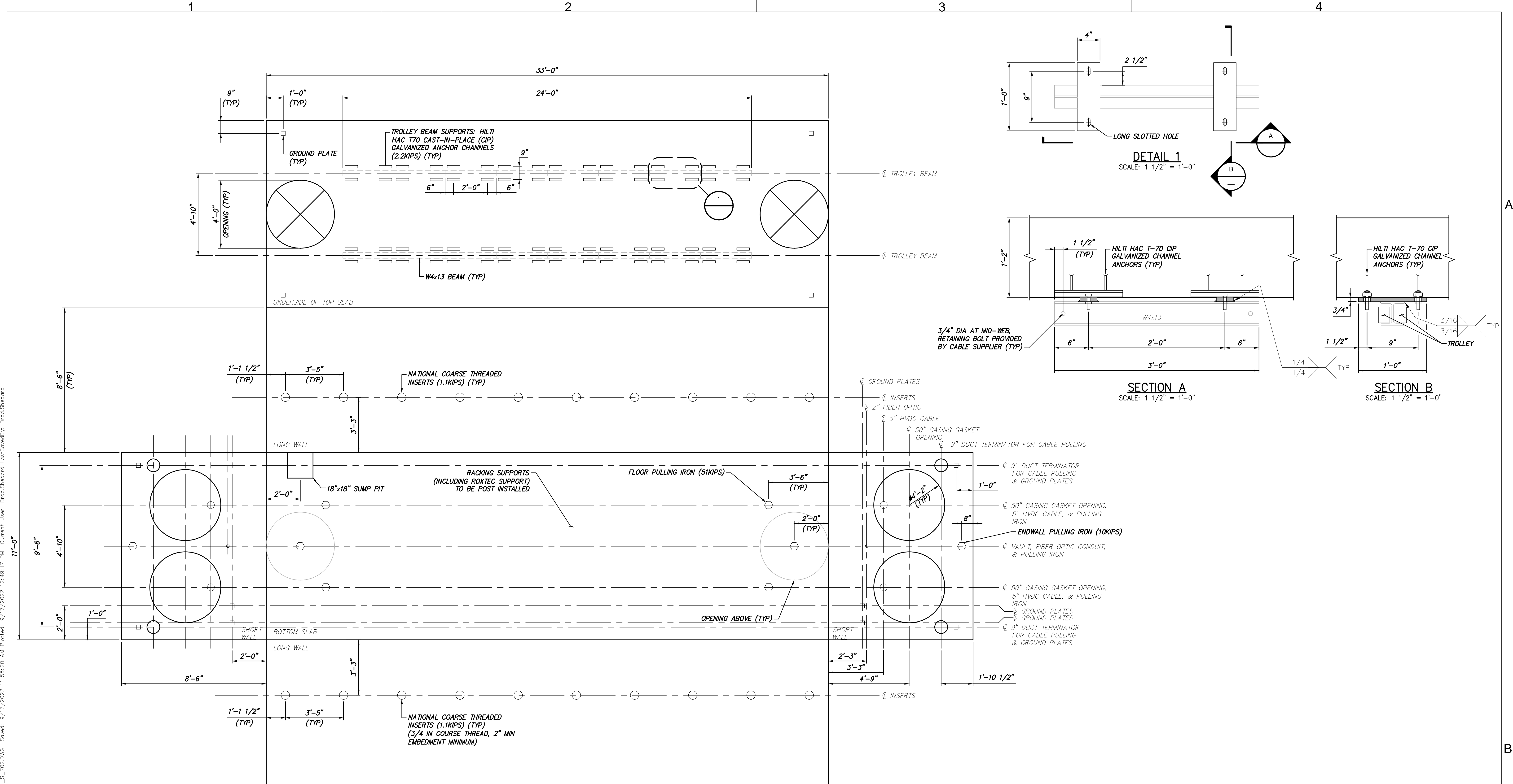
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UNFOLDED INTERIOR VIEW  
SCALE: 3/8" = 1'-0"

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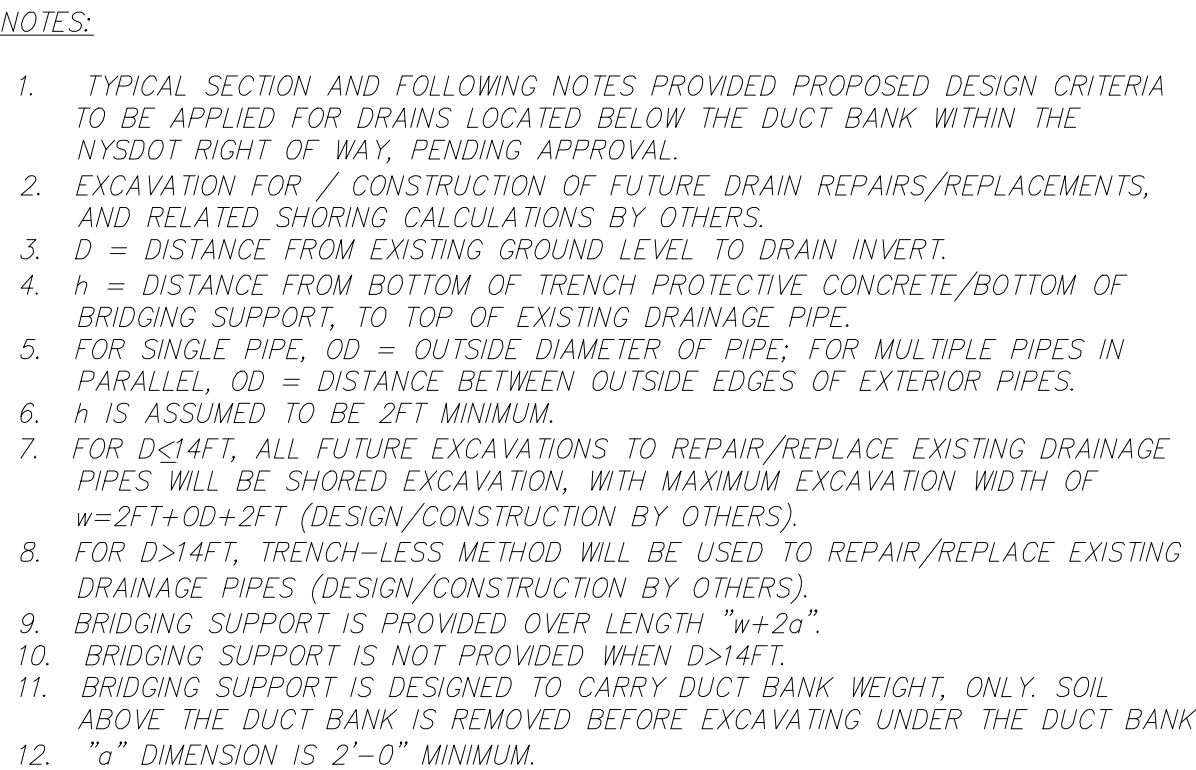
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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP					

CHAMPLAIN HUDSON POWER EXPRESS  
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL  
SPLICE VAULT ANCHOR \_ EMBED DETAILS

DRAWN BY:	DRH	DESIGNED BY:	JNK	APPROVED BY:	OO	SCALE	AS SHOWN	DATE	06/09/2022
						REV. NO.		SH.NO.	XX OF XXX

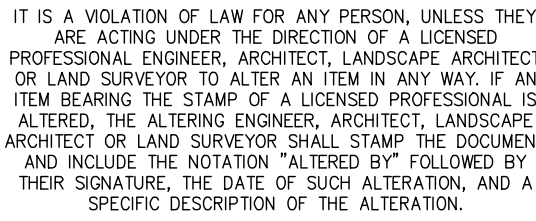
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CHA PROJECT NO.	066076
DRAWING NO.	S-702
DATE	06/09/2022
SH.NO.	XX OF XXX





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						CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL SELF-SUPPORTING STRUCTURE OVER UTILITIES						KIEWIT PROJECT NO.		
												21162		
												CHA PROJECT NO.		
												066076		
												DRAWING NO.		
												S-720		
0	9/21/2022	FINAL EM&CP SUBMISSION	JNK	OO										
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY:	DRH	DESIGNED BY:	JNK	APPROVED BY:	OO	SCALE REV. NO.	AS SHOWN	DATE SH.NO.	06/09/2022 XX OF XX