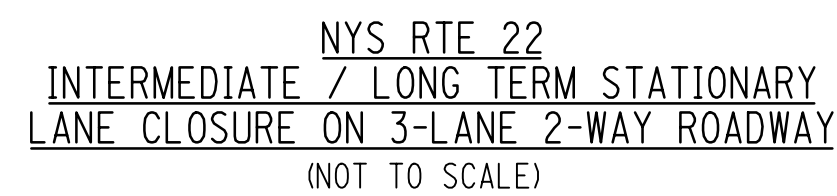


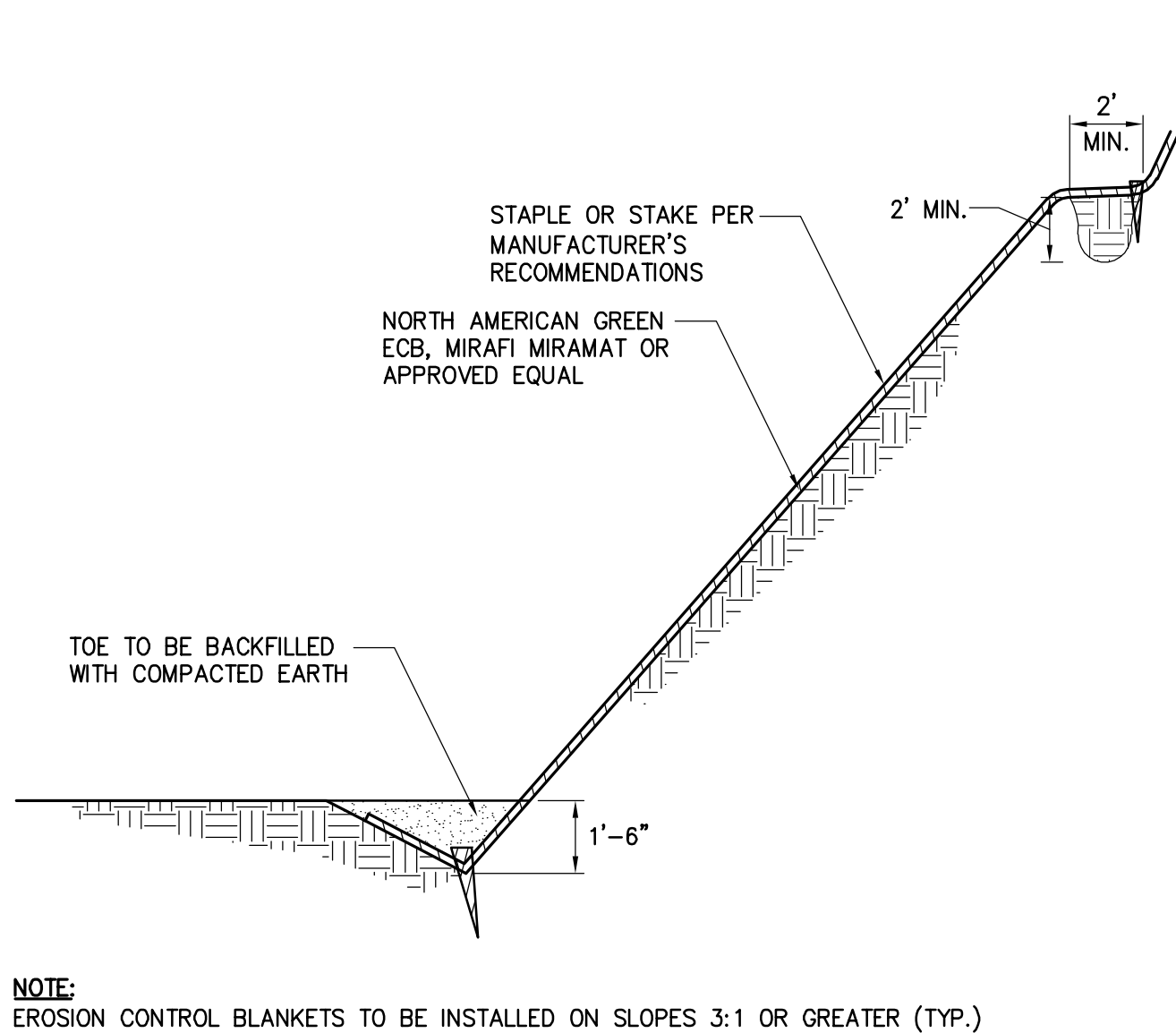
1. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.
2. LIMITS OF LONG-TERM LANE CLOSURE WITH TEMPORARY CONCRETE BARRIER SHALL BE ESTABLISHED BETWEEN EXISTING DRIVEWAYS AND SIDEROADS; UNLESS TEMPORARY SIGNALIZATION OF EXISTING DRIVEWAYS CAN BE PROVIDED AND COORDINATED WITH THE MAINLINE TEMPORARY SIGNALS, AS DIRECTED BY THE ENGINEER.
3. LONG-TERM LANE CLOSURES ARE ANTICIPATED TO BE IN PLACE AT ANY ONE LOCATION FOR APPROXIMATELY ONE WEEK (7 DAYS).
4. WORK SPACE LIMITS SHALL BE ESTABLISHED TO ALLOW FOR STANDARD DEFLECTION OF TEMPORARY CONCRETE BARRIER AS DEFINED IN SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS.

KIEWIT PROJECT NO.	
21162	
CHA PROJECT NO.	
066076	
DRAWING NO.	
C-511	
DATE	XX/XX/XXXX
SH.NO.	OF XXX

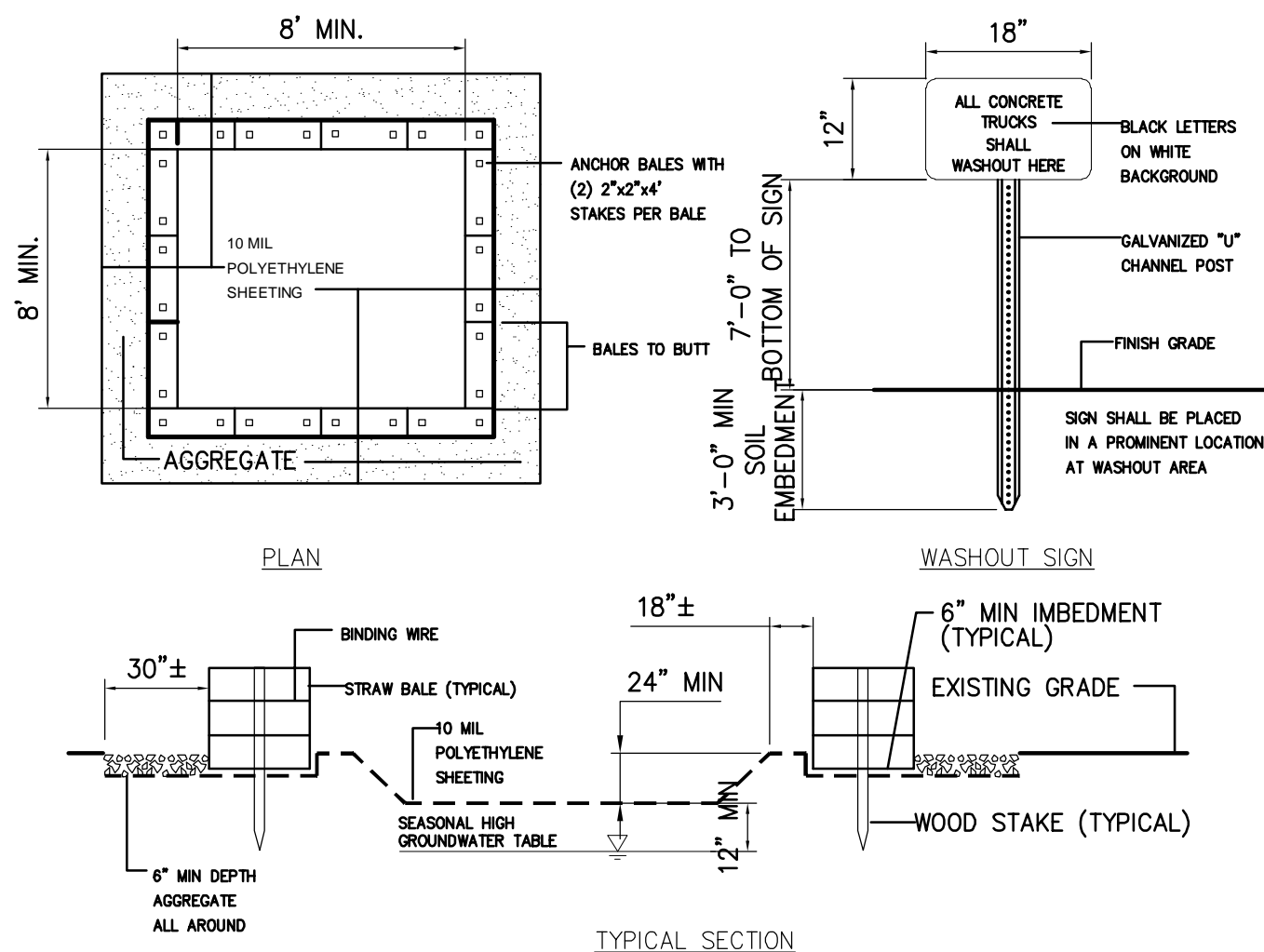


1. WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZING DEVICES SHALL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK SPACE AND TO DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVELED WAY.
2. WORK SPACE LIMITS SHALL BE ESTABLISHED TO ALLOW FOR STANDARD DEFLECTION OF TEMPORARY CONCRETE BARRIER AS DEFINED IN SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS.

KIEWIT PROJECT NO.	
21162	
CHA PROJECT NO.	
066076	
DRAWING NO.	
C-601	
DATE	04/15/2022
SH NO.	XXX OF XXX



3 EROSION CONTROL BANK STABILIZATION DETAIL



6	CONCRETE WASHOUT AREA
	SCALE: N.T.S.

Mechanical Properties	Test Method	Units	MARV
Grab Tensile Strength	ASTM D 4632	kN (lbs)	0.9 (205) x 0.9 (205)
Grab Tensile Elongation	ASTM D 4632	%	50 x 50
Puncture Strength	ASTM D 4833	kN (lbs)	0.58 (130)
Mullen Burst Strength	ASTM D 3786	kPa (psi)	2618 (380)
Trapezoid Tear Strength	ASTM D 4533	kN (lbs)	0.36 (80) X 0.36 (80)
UV Resistance	ASTM D 4355	%	70
Apparent Opening Size	ASTM D 4751	Mm (US Std Sieve)	0.180 (80)
Flow Rate	ASTM D 4491	l/min/m ² (gal/min/ft ²)	3866 (95)
Permittivity	ASTM D 4491	Sec ⁻¹	1.2

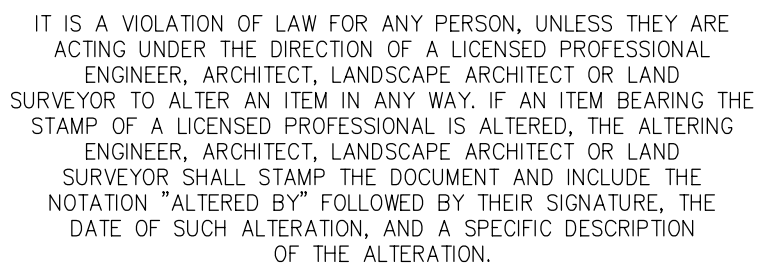
MAINTENANCE NOTES:

1. ALL CONCRETE WASHOUT FACILITIES SHALL BE INSPECTED DAILY. DAMAGED OR LEAKING FACILITIES SHALL BE DEACTIVATED AND REPAIRED OR REPLACED IMMEDIATELY. EXCESS RAINWATER THAT HAS ACCUMULATED OVER HARDENED CONCRETE SHALL BE PUMPED TO A STABILIZED AREA SUCH AS A GRASS FILTER STRIP.
2. ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE CAPACITY OF THE STRUCTURE IS FILLED. ANY EXCESS WASH WATER SHALL BE PUMPED INTO A CONTAINMENT VESSEL AND PROPERLY DISPOSED OF OFF SITE.
3. DISPOSE OF THE HARDENED MATERIAL OFF-SITE IN A CONSTRUCTION/DEMOLITION LANDFILL.
4. THE PLASTIC LINER SHALL BE REPLACED WITH EACH CLEANING OF THE WASHOUT FACILITY.
5. INSPECT THE PROJECT SITE FREQUENTLY TO ENSURE THAT NO CONCRETE DISCHARGES ARE TAKING PLACE IN NON-DESIGNATED AREAS.
6. LOCATION(S) TO BE DETERMINED IN THE FIELD BY THE OWNER'S REPRESENTATIVE



1. IN-STREAM EXCAVATION SHOULD BE COMPLETED IN ACCORDANCE WITH "TEMPORARY ACCESS WATERWAY CROSSING" ON PAGE 2.32 OF THE 2016 NYSDEC STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (OR NEWEST VERSION).
2. THE CONSTRUCTION OF ANY CROSSING SHOULD NOT CAUSE A SIGNIFICANT WATER LEVEL DIFFERENCE BETWEEN THE UPSTREAM AND DOWNSTREAM WATER SURFACE ELEVATIONS. FISH SPAWNING OR MIGRATION DATES CAN VARY ACROSS NEW YORK, AND RESTRICTIONS IMPOSED BY THE NYSDEC MAY VARY AND MUST BE VERIFIED.
3. THE TEMPORARY WATERWAY CROSSING SHOULD BE AT RIGHT ANGLES TO THE STREAM WHERE APPROACH CONDITIONS DICTATE, THE CROSSINGS MAY VARY 15 DEGREES FROM A LINE DRAWN PERPENDICULAR TO THE CENTERLINE OF THE STREAM AT THE INTENDED CROSSING LOCATION.
4. ALL FILL MATERIALS ASSOCIATED WITH THE ROADWAY APPROACH SHOULD BE LIMITED TO A MAXIMUM HEIGHT OF 2 FEET ABOVE THE EXISTING FLOOD PLAIN ELEVATION.
5. A WATER DIVERTING STRUCTURE SUCH AS A SWALE OR WARE BAR SHOULD BE CONSTRUCTED (ACROSS THE ROADWAY ON BOTH ROADWAY APPROACHES) 50 FEET (MAXIMUM) ON EITHER SIDE OF THE WATERWAY CROSSING. THIS WILL PREVENT ROADWAY SURFACE RUNOFF FROM DIRECTLY ENTERING THE WATERWAY. THE 50 FEET MEASURED IS MEASURED FROM THE TOP OF THE WATERWAY BANK. IF THE ROADWAY APPROACH IS CONSTRUCTED WITH A REVERSE GRADE AWAY FROM THE WATERWAY, A SEPARATE DIVERTING STRUCTURE IS NOT REQUIRED.
6. ALL CROSSINGS SHOULD HAVE ONE TRAFFIC LANE. THE MINIMUM WIDTH SHOULD BE 12 FEET WITH A MAXIMUM WIDTH OF 20 FEET.
7. ANCHORS: TIMBER MATS SHOULD BE SECURELY ANCHORED AT ONLY ONE END, USING STEEL CABLE OR CHAIN, ANCHORING AT ONLY ONE END WILL PREVENT CHANNEL OBSTRUCTION IN THE EVENT THAT FLOODWATERS FLOAT THE BRIDGE. ACCEPTABLE ANCHORS ARE LARGE TREES, LARGE BOULDERS, OR DRIVEN STEEL ANCHORS. ANCHORING SHOULD BE SUFFICIENT TO PREVENT THE TIMBER MATS FROM FLOATING DOWNSTREAM AND POSSIBLY CAUSING AN OBSTRUCTION TO THE FLOW.

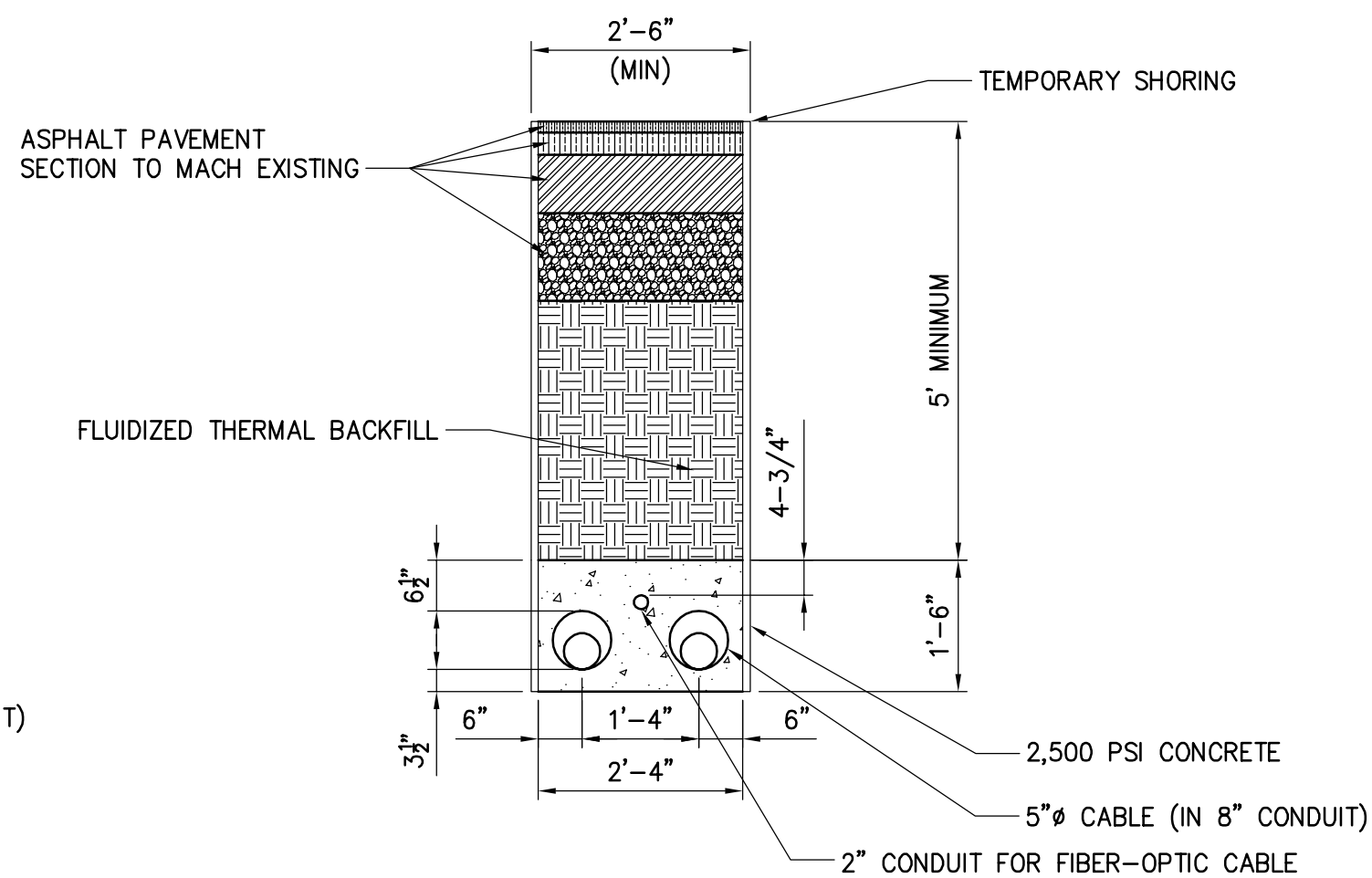
SCALE: N.T.S.



						CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN WETLAND CROSSING DETAILS				KIEWIT PROJECT NO. 21162		
										CHA PROJECT NO. 066076		
										DRAWING NO.		
										C-611		
A	04/15/2022	EM&CP REGULATORY SUBMISSION		JM	JR							
No.	DATE	SUBMITTAL / REVISION DESCRIPTION		DB	APP	DRAWN BY: XXX	DESIGNED BY: XXX	APPROVED BY: XXX	SCALE	AS NOTED	DATE	04/15/2022
									REV. NO.	X	SH.NO.	XXX OF XXX



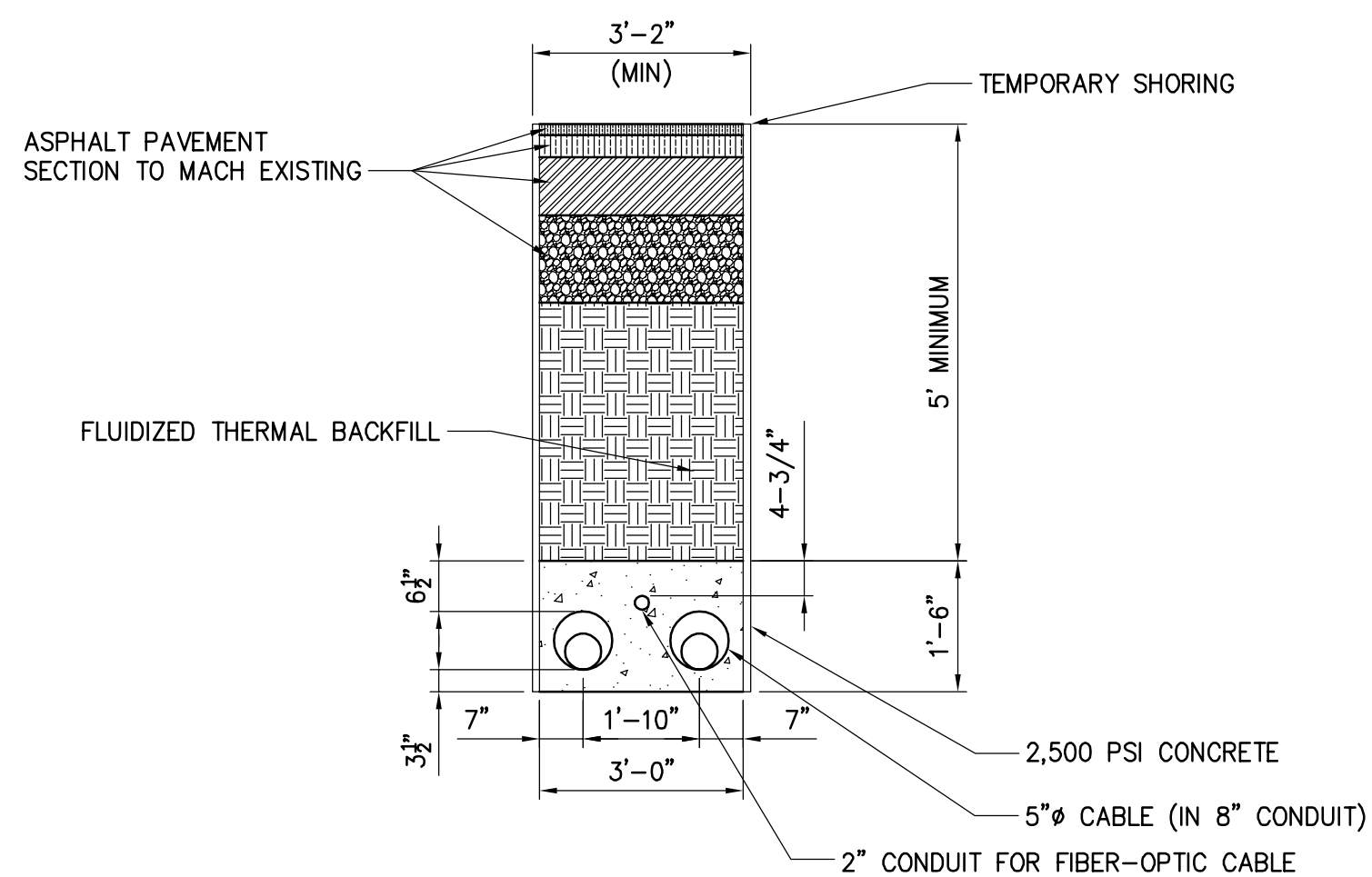
1 OPEN TRENCH (30" WIDTH, 16" CENTER TO CENTER SPACING)
NOT TO SCALE



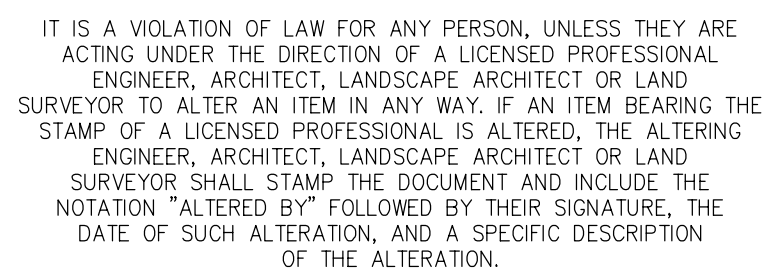
2 TYPICAL DUCTBANK IN ROADWAY (30" WIDTH, 16" CENTER TO CENTER SPACING)



3 OPEN TRENCH (36" WIDTH, 22" CENTER TO CENTER SPACING)
NOT TO SCALE

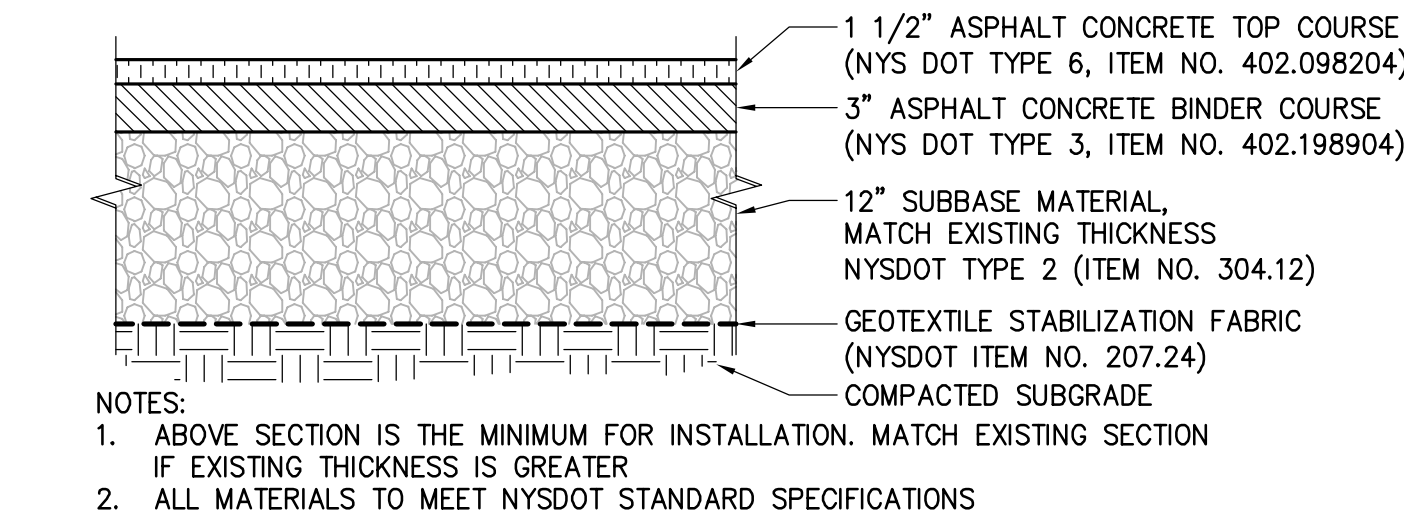


4 TYPICAL DUCTBANK IN ROADWAY (36" WIDTH, 22" CENTER TO CENTER SPACING)
NOT TO SCALE

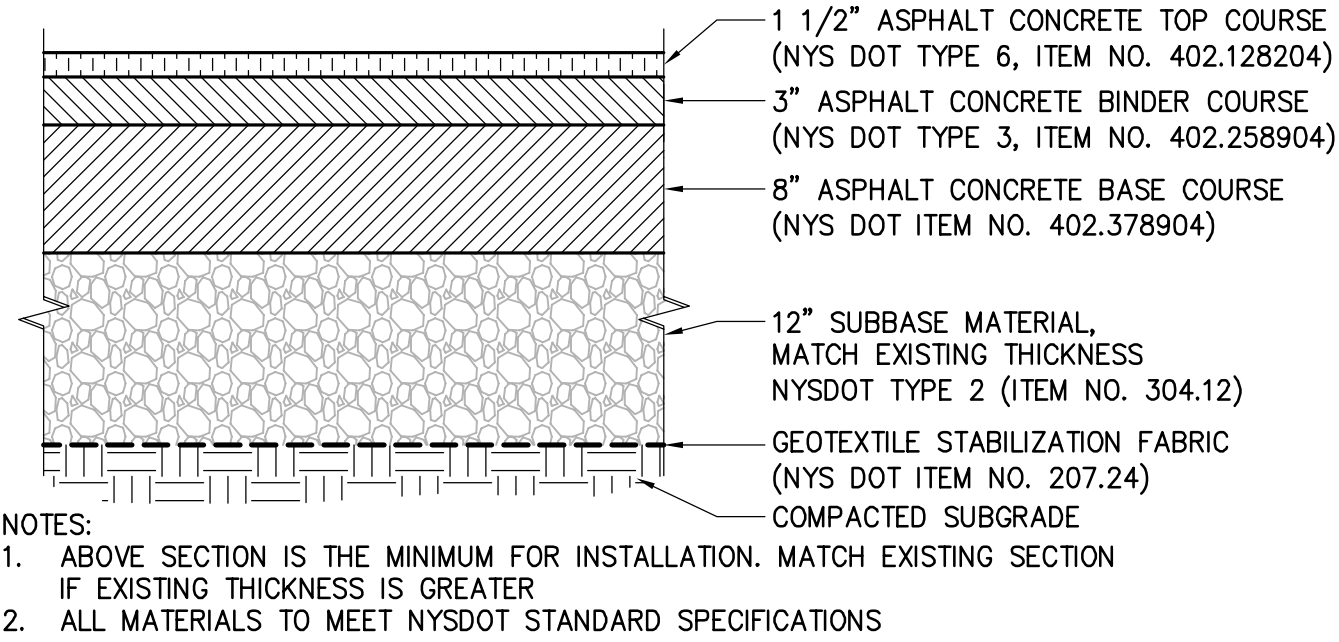


						CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN TRENCHING DETAILS				KIEWIT PROJECT NO. 21162	
										CHA PROJECT NO. 068076	
										DRAWING NO.	
										C-621	
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR							
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: XXX	DESIGNED BY: XXX	APPROVED BY: XXX	SCALE	AS NOTED	DATE	04/15/2022
					REV. NO.				X	SH.NO.	XXX OF XXX

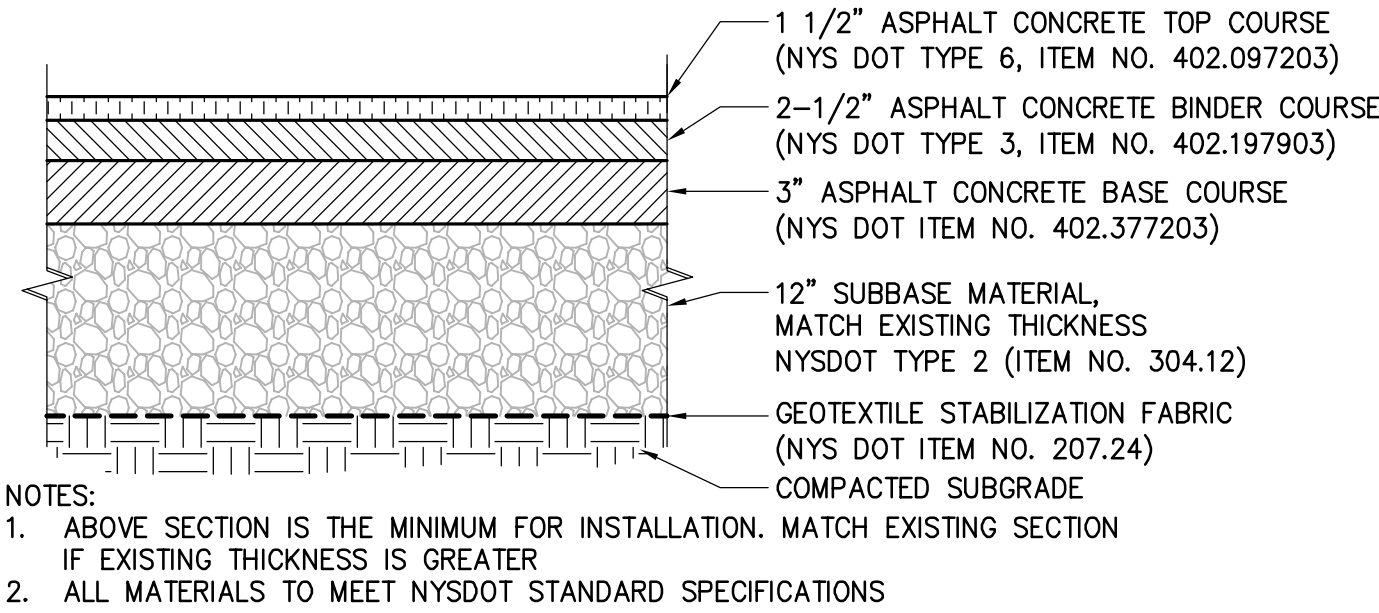
File: V:\PROJECTS\ANY\6\066076.000\09_Design\Drawings\01_Sheets\Design Package 1A\066076_PHA - C-631.DWG Saved: 4/13/2022 11:29:07 AM Plotted: 4/13/2022 11:39:41 AM Current User: Evans, Josh LastSavedBy: 4894



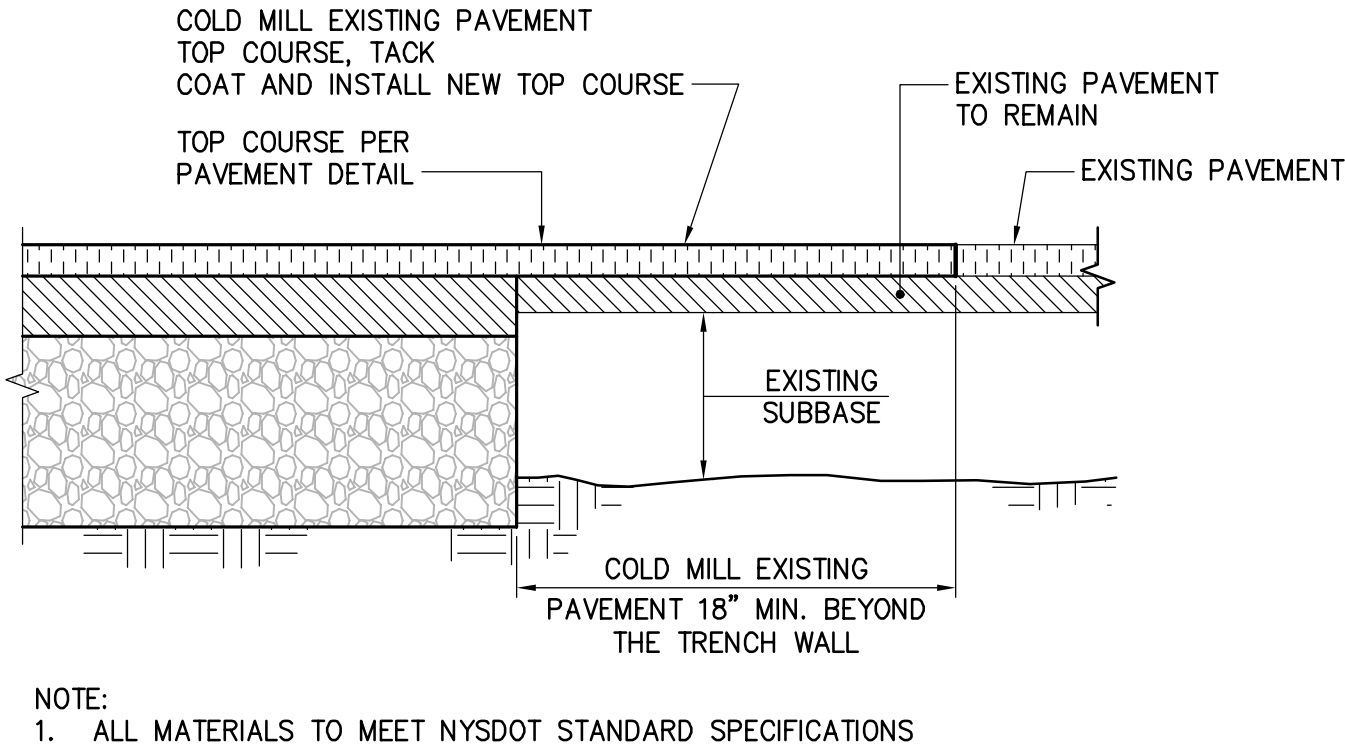
1 ASPHALT CONCRETE PAVEMENT DETAIL
(PRIVATE DRIVEWAY)
SCALE: N.T.S.



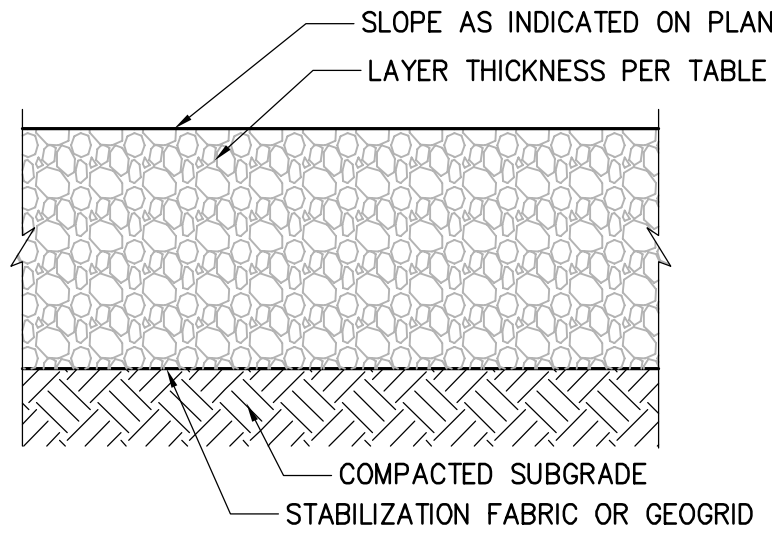
2 ASPHALT CONCRETE PAVEMENT
(WITHIN NYSDOT ROADWAYS)
SCALE: N.T.S.



3 ASPHALT CONCRETE PAVEMENT
(WITHIN COUNTY OR TOWN ROADWAYS)
SCALE: N.T.S.



4 PAVEMENT TRANSITION DETAIL
SCALE: N.T.S.

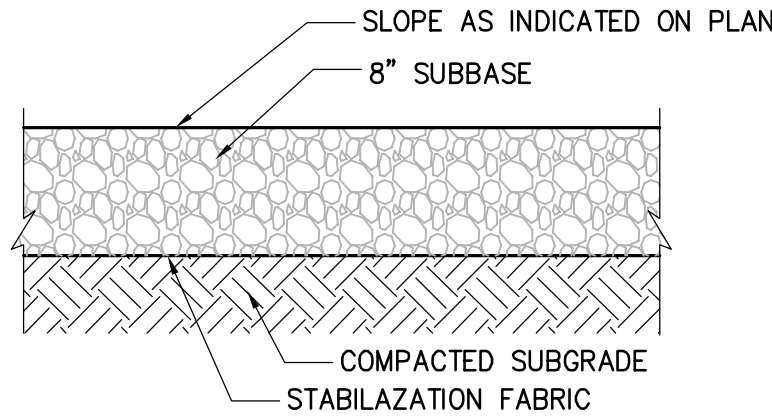


TEMPORARY ACCESS ROAD SECTION			
CBR	180N GEOTEXTILE	BX1200 GEOGRID ²	RD580I GEOTEXTILE
0.5	24 INCH RIP RAP ³ + 6 INCH AGGREGATE ⁴	--	24 INCH RIP RAP + 4 INCH AGGREGATE
1.0	20 INCH AGGREGATE	12 INCH AGGREGATE	9 INCH AGGREGATE
1.5	15 INCH AGGREGATE	9 INCH AGGREGATE	--
2.0	12 INCH AGGREGATE	6 INCH AGGREGATE	--
3.0+	9 INCH AGGREGATE	6 INCH AGGREGATE	--

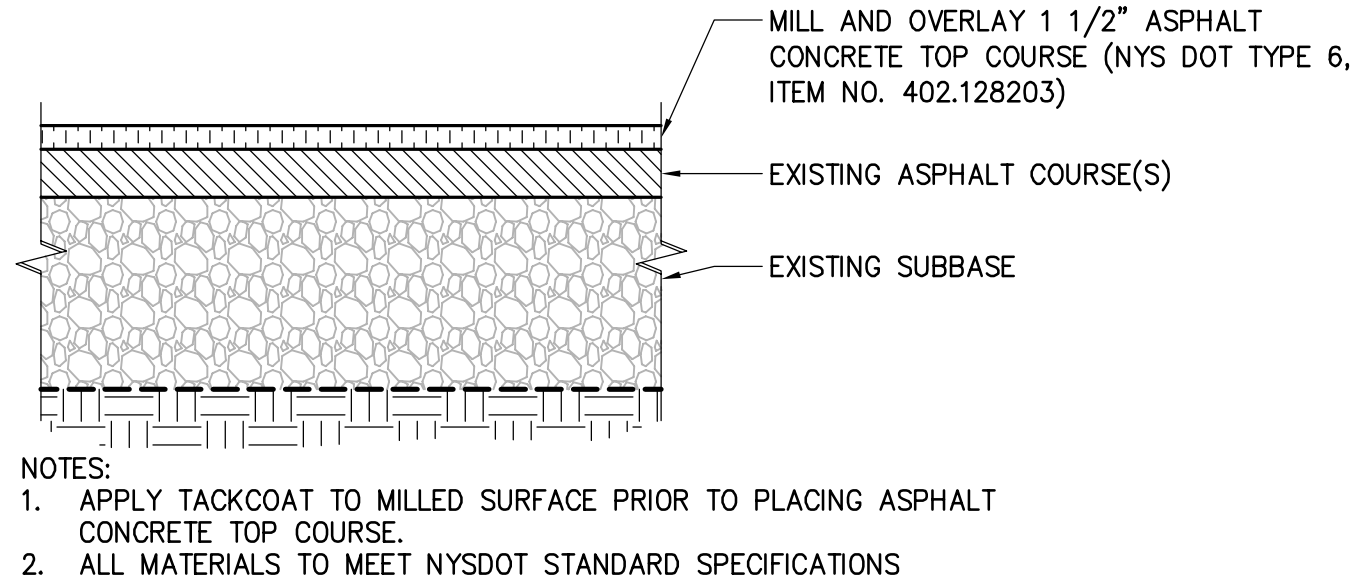
NOTES:
¹ ESTIMATE CBR IN THE FIELD USING A DYNAMIC CONE PENETROMETER.
² SEPARATOR GEOTEXTILE FABRIC SUCH AS MIRAFI 140N SHOULD BE INCLUDED BENEATH GEOGRID LAYER ON COHESIVE SUBGRADE
³ RIP RAP TO BE NYSDOT LIGHT STONE FALL
⁴ A LAYER OF #57 STONE MAY BE REQUIRED ON TOP OF GEOTEXTILE TO PREVENT DAMAGING OR PUNCHING OF THE FABRIC.
⁵ GEOGRID AND GEOTEXTILES SHOULD BE INSTALLED PER MANUFACTURES SPECIFICATION INCLUDING OVERLAP AND EDGE DETAILS

NOTE:
ACCESS ROAD SECTIONS PER KIEWIT GEOTECHNICAL

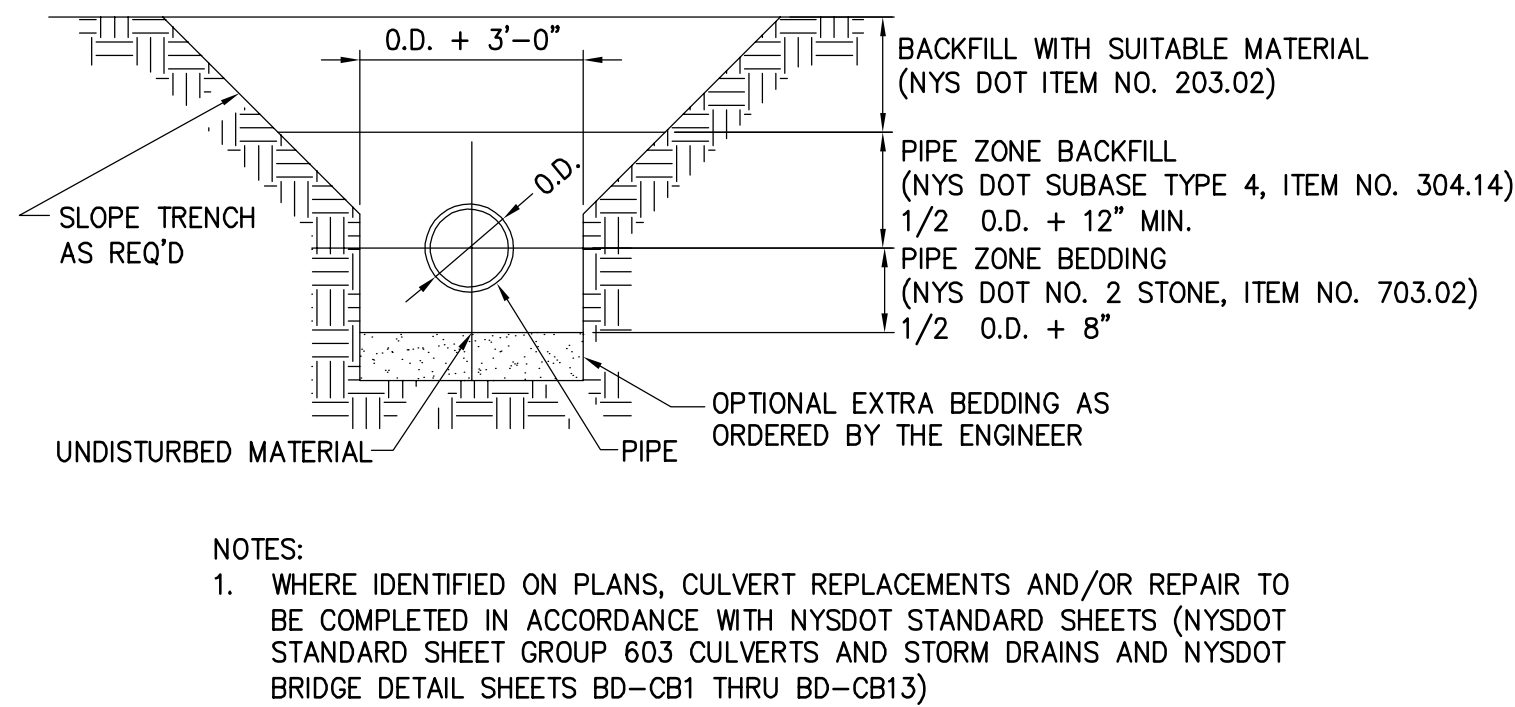
5 TEMPORARY ACCESS ROAD
SCALE: N.T.S.



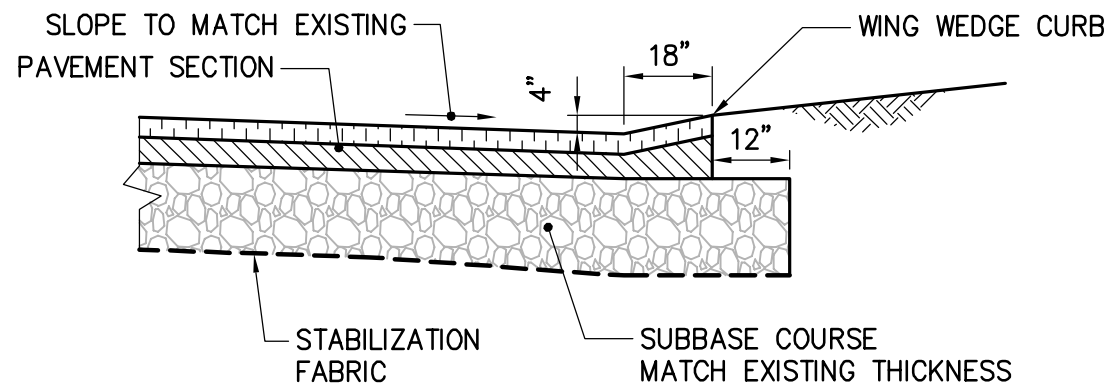
6 GRAVEL PAVEMENT
SCALE: N.T.S.



7 MILL AND OVERLAY ASPHALT CONCRETE PAVEMENT DETAIL
SCALE: N.T.S.

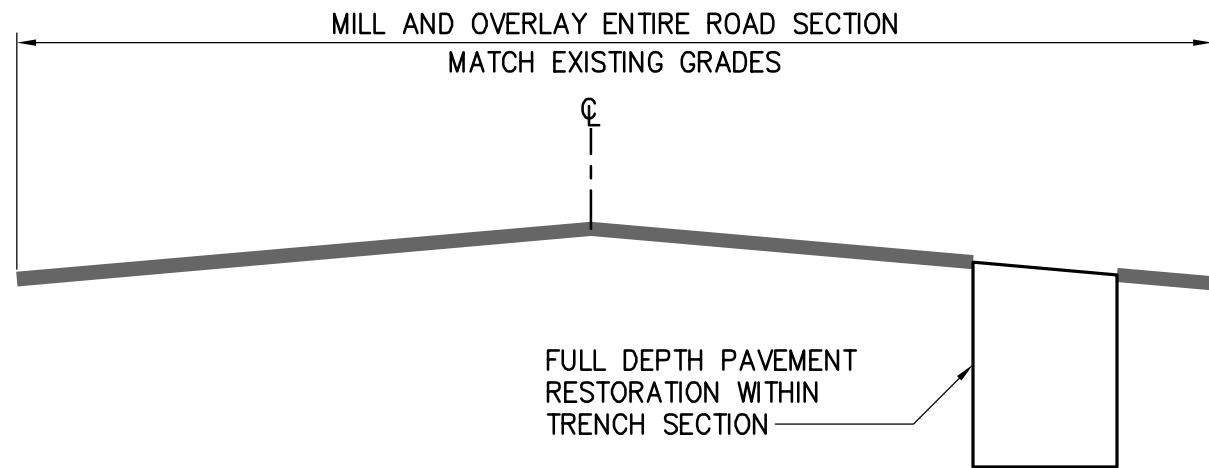


8 CULVERT REPLACEMENT
SCALE: N.T.S.

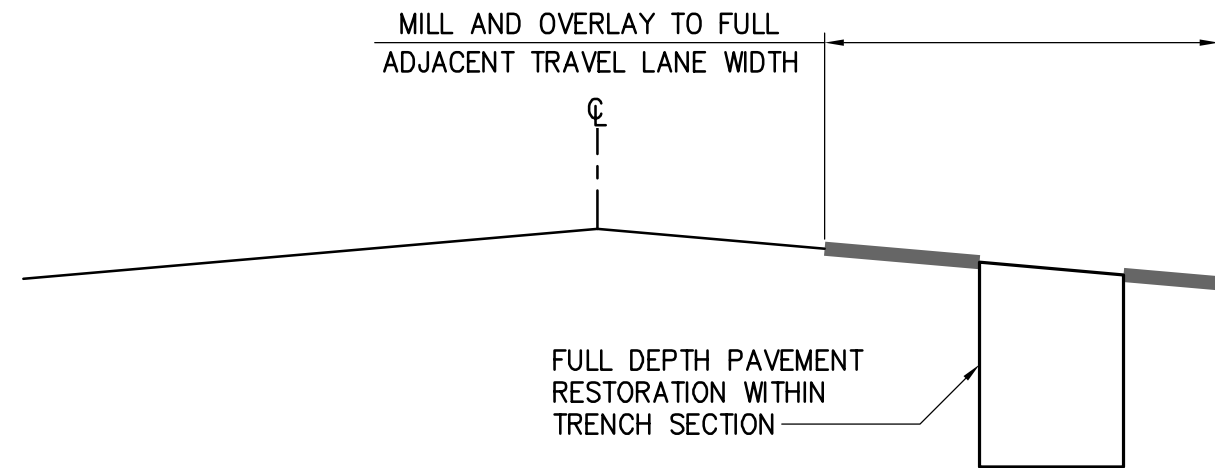


NOTE:
1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

9 WING WEDGE CURB DETAIL
SCALE: N.T.S.



10 LAKE ROAD & COUNTY
TYPICAL ROADWAY RESTORATION
SCALE: N.T.S.



NOTE: TWO LANE SECTION TO RECEIVE MILL AND OVERLAY TO CENTER LINE

11 NYSDOT ROUTE 22
TYPICAL ROADWAY RESTORATION
SCALE: N.T.S.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 1 - PUTNAM TO DRESDEN
SURFACE RESTORATION DETAILS

DRAWN BY: xxx DESIGNED BY: xxx APPROVED BY: xxx SCALE AS NOTED DATE 04/15/2022
REV. NO. X SH. NO. xxx OF xxx

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-631
DATE	04/15/2022
SH. NO.	xxx OF xxx

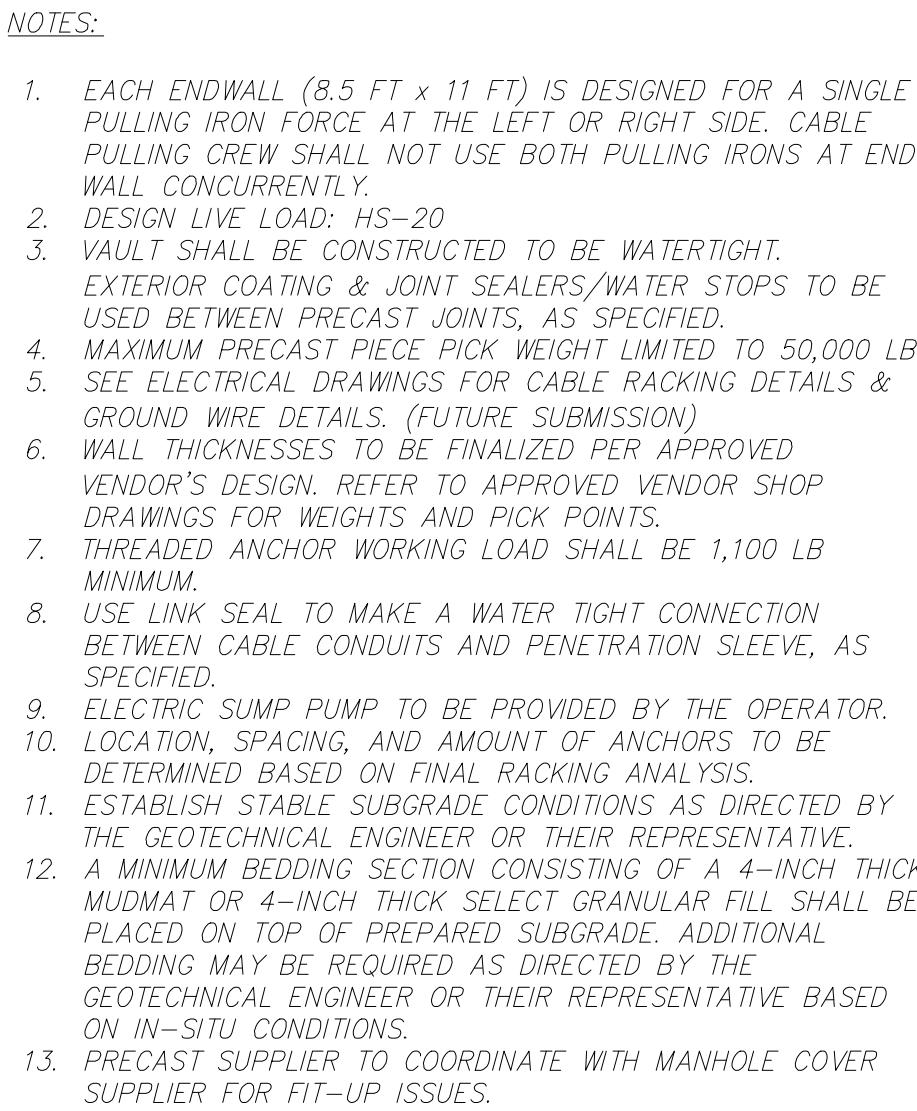


Diagram illustrating the installation of a cable tray system. The diagram shows a cross-section of a wall and a cable tray. A 1/0 cable is shown entering the tray from the left. The cable is supported by a ground plate (BURNDY YGF29-4N) and a flexible copper braid (BURNDY BD24N). The distance from the wall to the cable support is 1'-0" (±1/8"). The distance from the cable support to the wall is 1'-0" (±1/8").

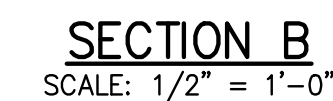
Technical drawing of a manhole vault assembly, showing cross-section and plan views with dimensions and labels.

Labels and Dimensions:

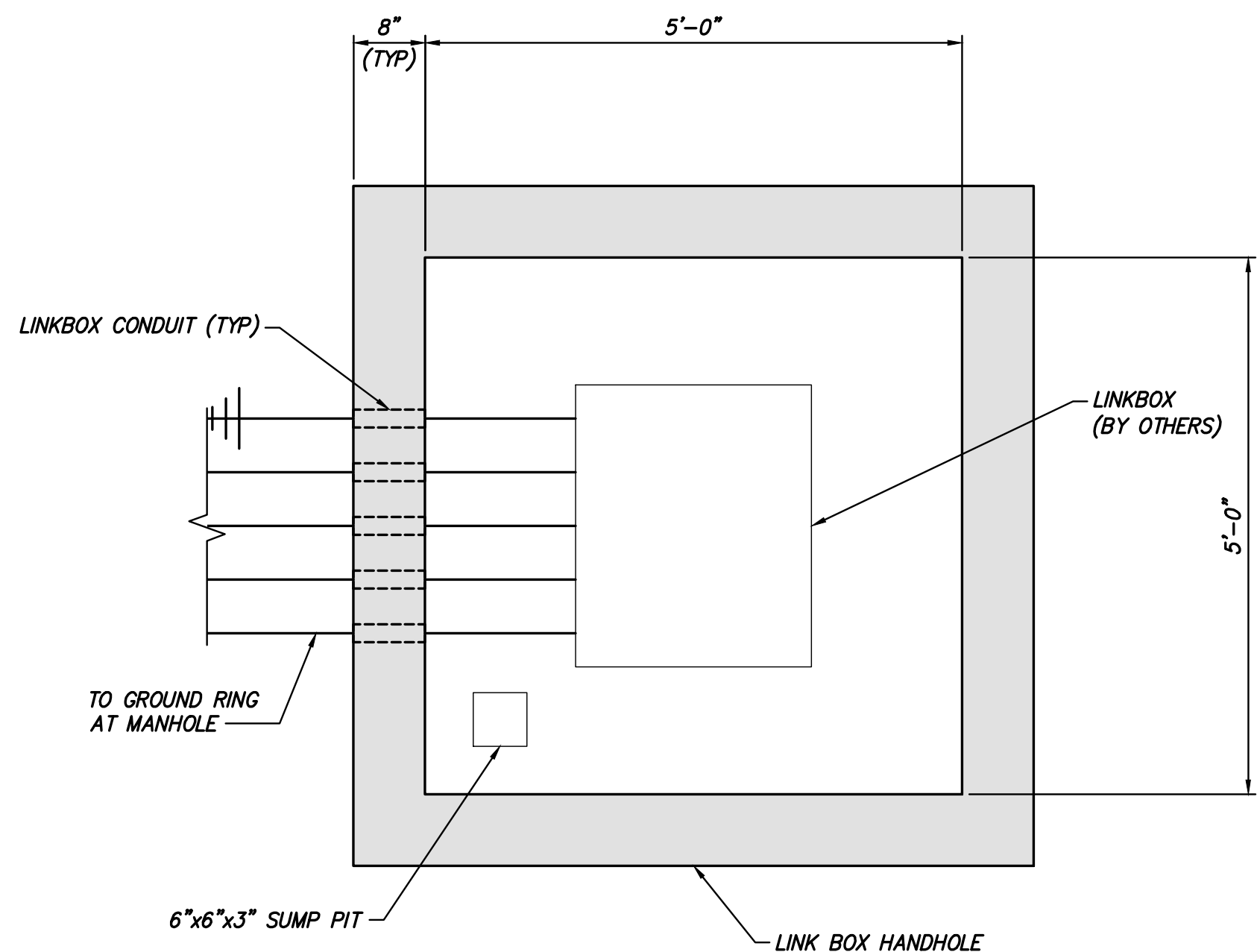
- Top View (Plan View):**
 - Manhole opening: 48" diameter (indicated by circle 1).
 - Access for retractable ladder, provided by others.
 - (5 PER BEAM) TROLLEY SUPPORT @ EQ. SPA PENDING VENDOR DESIGN (SEE NOTE 6).
 - TOP OF PAVEMENT.
 - VARIES 2'-0" MIN.
 - ELEVATION @ FINISHED GRADE.
 - Manhole opening: 48" diameter (indicated by circle 1).
 - 1'-2" (typical width of top section).
 - 8" DUCT TERMINATOR FORMEX (TYP).
 - 1'-1 1/2" ± (TYP) (width of top section).
 - 3'-5" ± (TYP) (width of top section).
 - THREADED ANCHOR INSERTS ON LONG WALLS (SEE NOTE 7) (TYP).
 - 2 EA W4x13 TROLLEY BEAM OVER CONDUIT PATH.
 - LINKBOX CONDUIT (TYP).
 - 9" (typical height of top section).
 - 4'-0" (TYP) (width of top section).
 - 1'-4" (typical width of top section).
 - 8'-6" (typical height of top section).
 - 11'-0" (typical height of top section).
 - 1'-4" (typical width of top section).
 - CONSTRUCTION JOINT (TYP).
 - CAST-IN-PLACE OVERHANG AT SITE (TYP ALL AROUND).
 - 2" (typical width of top section).
 - 4'-0" (TYP) (width of top section).
 - 2'-0" (TYP) (width of top section).
 - 1'-0" (typical width of top section).
 - 3'-3" (typical width of top section).
 - 4'-0" (typical width of top section).
 - 1'-4" (typical width of top section).
 - 6" (typical width of top section).
 - 18"x18"x3" SUMP (LOCATE UNDER 48" MANWAY).
 - 2" GROUNDING HALO CONDUIT (TYP).
 - PRECAST VAULT (SEE NOTE 6).
 - SEE NOTE 10.
 - PULLING IRON (TYP) APPLIED FORCE AND LOCATION TBD.
 - GROUNDING PAD (TYP).
 - 8" CABLE CONDUIT.
 - PULLING IRONS, SEE NOTE 1.
 - 1'-2" (typical width of top section).
- Side View (Cross-section):**
 - Manhole opening: 48" diameter (indicated by circle 1).
 - Access for retractable ladder, provided by others.
 - (5 PER BEAM) TROLLEY SUPPORT @ EQ. SPA PENDING VENDOR DESIGN (SEE NOTE 6).
 - TOP OF PAVEMENT.
 - VARIES 2'-0" MIN.
 - ELEVATION @ FINISHED GRADE.
 - Manhole opening: 48" diameter (indicated by circle 1).
 - 1'-2" (typical width of top section).
 - 8'-6" (typical height of top section).
 - 11'-0" (typical height of top section).
 - 1'-4" (typical width of top section).
 - CONSTRUCTION JOINT (TYP).
 - CAST-IN-PLACE OVERHANG AT SITE (TYP ALL AROUND).
 - 2" (typical width of top section).
 - 4'-0" (TYP) (width of top section).
 - 2'-0" (TYP) (width of top section).
 - 1'-0" (typical width of top section).
 - 3'-3" (typical width of top section).
 - 4'-0" (typical width of top section).
 - 1'-4" (typical width of top section).
 - 6" (typical width of top section).
 - 18"x18"x3" SUMP (LOCATE UNDER 48" MANWAY).
 - 2" GROUNDING HALO CONDUIT (TYP).
 - PRECAST VAULT (SEE NOTE 6).
 - SEE NOTE 10.
 - PULLING IRON (TYP) APPLIED FORCE AND LOCATION TBD.
 - GROUNDING PAD (TYP).
 - 8" CABLE CONDUIT.
 - PULLING IRONS, SEE NOTE 1.
 - 1'-2" (typical width of top section).

A cross-sectional diagram of a wall formwork system. It shows a concrete wall with a wavy internal reinforcement pattern. A vertical section of the wall is highlighted with a dashed line, indicating the location of the mild reinforcement. A label 'MILD REINFORCEMENT (TYP)' points to this section. Another label 'FORM SAVER (TYP)' points to a vertical component on the right side of the wall, which appears to be a formwork panel or a protective layer.

DATE	04/15/2022
SH NO	XX OF XXX



B

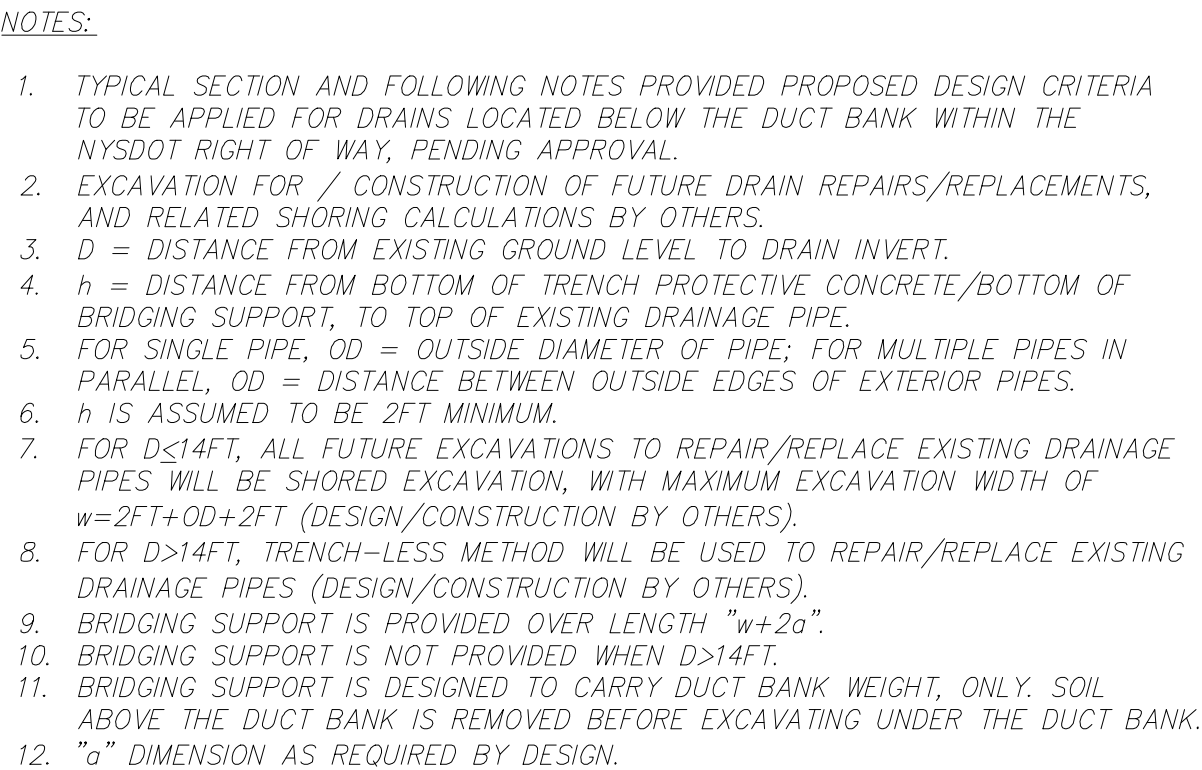


SECTION B
SCALE: 3/4" = 1'-0"

- NOTES:
1. CLEAR SPACE BETWEEN SPLICE VAULT AND LINKBOX HANDHOLE TO BE 5 FT MAXIMUM.
 2. FOR LINK BOX DETAILS SEE ELECTRICAL DRAWINGS.
 3. DESIGN LIVE LOAD: HS-20.
 4. WALL THICKNESSES TO BE FINALIZED PER APPROVED VENDOR'S DESIGN. REFER TO APPROVED VENDOR SHOP DRAWINGS FOR WEIGHTS AND PICK POINTS.
 5. ELECTRIC SUMP PUMP TO BE PROVIDED BY OTHERS.
 6. SEE S-700 FOR VAULT DETAILS.
 7. ESTABLISH STABLE SUBGRADE CONDITIONS AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE.
 8. A MINIMUM BEDDING SECTION CONSISTING OF A 4-INCH THICK MUDMAT OR 4-INCH THICK SELECT GRANULAR FILL SHALL BE PLACED ON TOP OF PREPARED SUBGRADE. ADDITIONAL BEDDING MAY BE REQUIRED AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE BASED ON IN-SITU CONDITIONS.

SECTION A
SCALE: 3/8" = 1'-0"

						CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN LINK BOX HANDHOLE PLAN ELEVATION & SECTION				KIEWIT PROJECT NO. 21162	
										CHA PROJECT NO. 066076	
										DRAWING NO.	
										S-710	
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR							
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: DRH	DESIGNED BY: JNK	APPROVED BY: OO	SCALE REV. NO.	AS SHOWN	DATE SH.NO.	04/15/2022 XX OF XXX



TYPICAL DUCT BANK SECTION OVER UTILITIES
(FOR $D < 14\text{FT}$)
SCALE: NTS

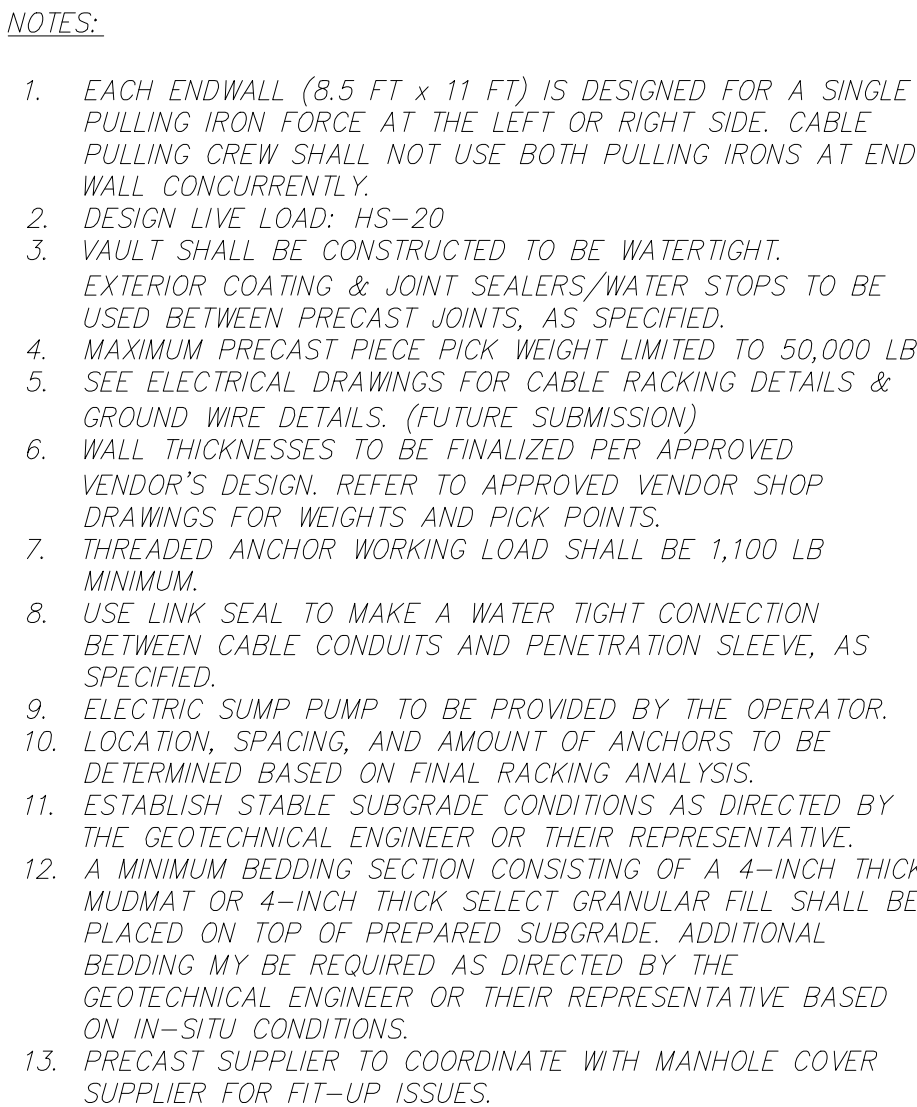


Diagram illustrating the connection of a cable to a ground plane. The cable is labeled **1/0 CABLE**. The ground plane is labeled **GROUND PLATE BURNDY YGF29-4N**. The connection is made using a **FLEXIBLE COPPER BRAID BURNDY BD24N**. The distance from the cable to the ground plane is indicated as **1'-0" ($\pm 1/8"$)**. The distance from the ground plane to the cable is indicated as **1'-0" ($\pm 1/8"$)**.

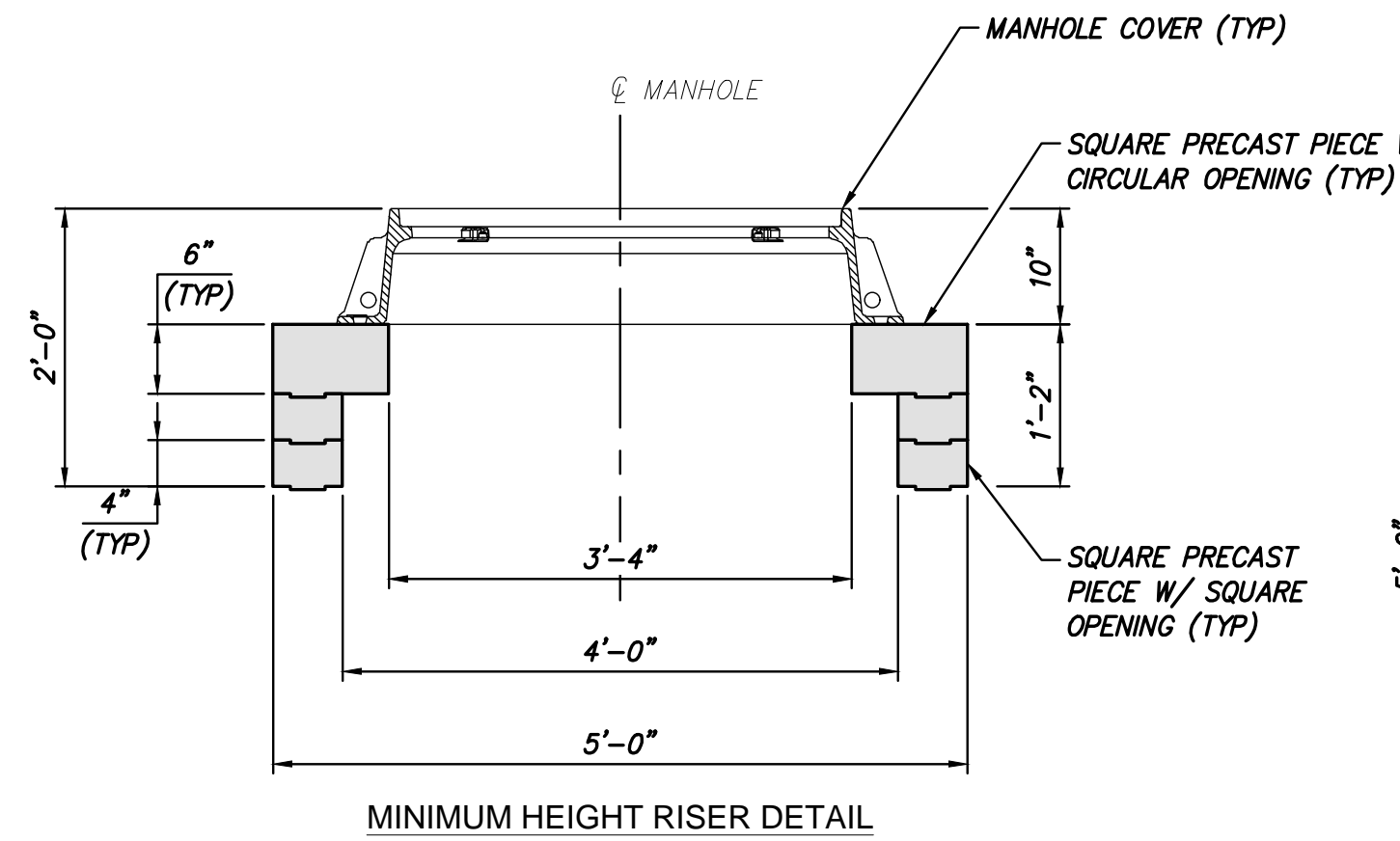
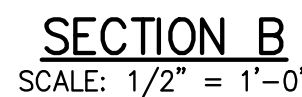
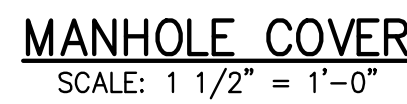
Technical drawing of a vault structure, showing a cross-section and plan view. The drawing includes dimensions and labels for various components.

Labels and Dimensions:

- UNDERGROUND CABLE END** (Left side)
- SUBMARINE CABLE END** (Right side)
- STA @ V** (Center line)
- MANHOLE** (Top center)
- ACCESS WITH RETRACTABLE LADDER** (Top left)
- TROLLEY SUPPORTS, PENDING VENDOR DESIGN (SEE NOTE 6)** (Top center)
- VARIES 2'-0" MIN** (Top center)
- TOP OF PAVEMENT** (Top center)
- ELEVATION @ FINISHED GRADE** (Top right)
- 8" DUCT TERMINATOR, FORMEX (TYP)** (Left side)
- 1'-11"± (TYP)** (Left side)
- 3'-5"± (TYP)** (Left side)
- THREADED ANCHOR INSERTS ON LONG WALLS (SEE NOTE 7) (TYP)** (Left side)
- 2 EA W4x13 TROLLEY BEAM OVER CONDUIT PATH** (Center)
- LINKBOX CONDUIT (TYP)** (Center)
- GROUNDING PAD (TYP)** (Center)
- PULLING IRON (TYP) APPLIED FORCE AND LOCATION TBD** (Center)
- CONSTRUCTION JOINT (TYP)** (Right side)
- CAST-IN-PLACE OVERHANG AT SITE (TYP ALL AROUND)** (Right side)
- 1'-2"** (Right side)
- 8'-6"** (Right side)
- 11'-0"** (Right side)
- 1'-4"** (Right side)
- 4'-9"** (Left side)
- 4'-0"** (Left side)
- 3'-3"** (Left side)
- 1'-0"** (Left side)
- 1'-4"** (Left side)
- 6"** (Left side)
- 18"x18"x3" SUMP (LOCATE UNDER 48" MANWAY)** (Bottom left)
- 2" GROUNDING HALO CONDUIT (TYP)** (Bottom left)
- PRECAST VAULT (SEE NOTE 6)** (Bottom center)
- ELEVATION @ BOTTOM OF VAULT** (Bottom center)
- PREPARED SUBGRADE AND BEDDING (SEE NOTES 11 AND 12)** (Bottom center)
- 4'-0" (TYP)** (Bottom right)

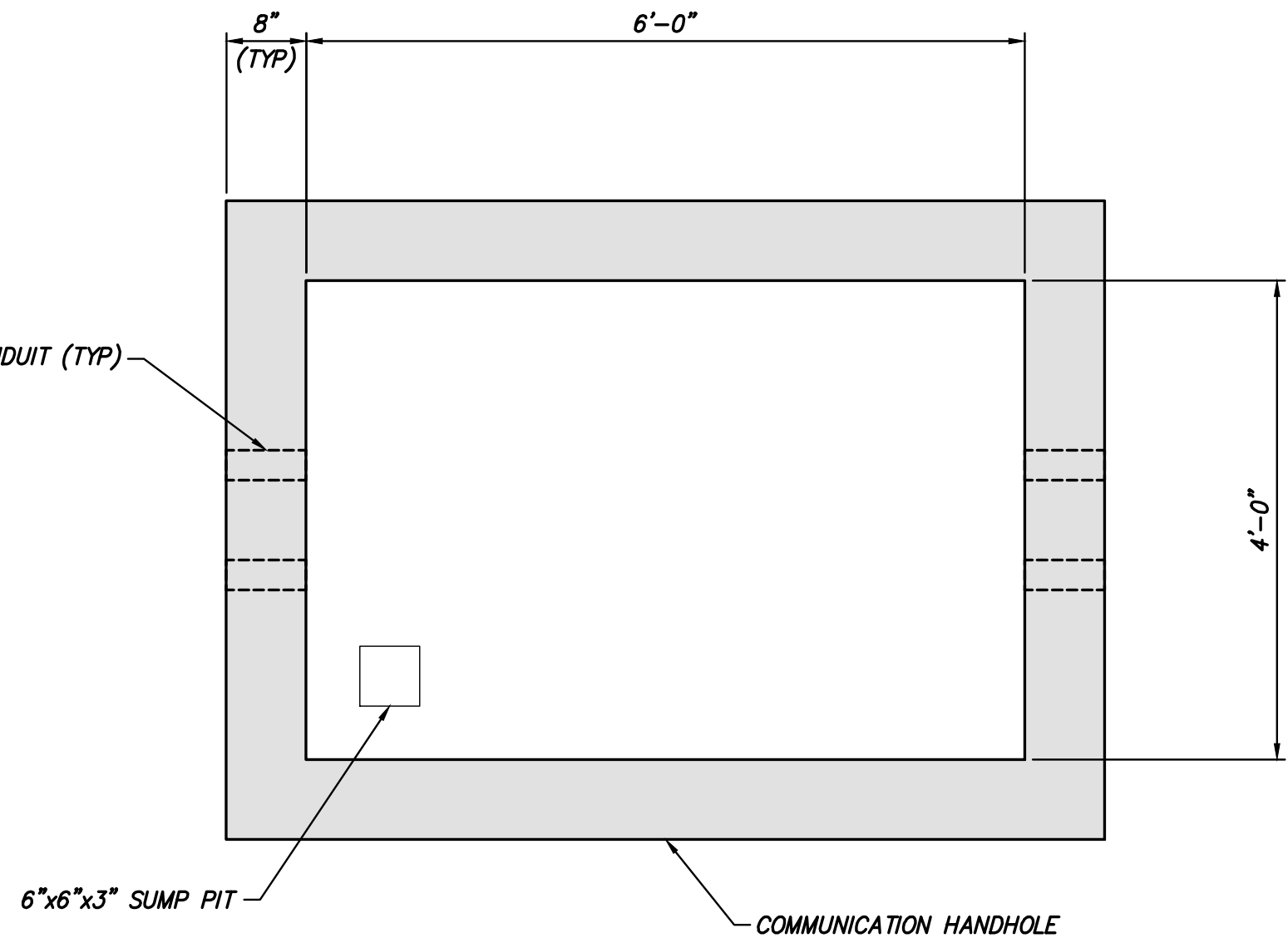
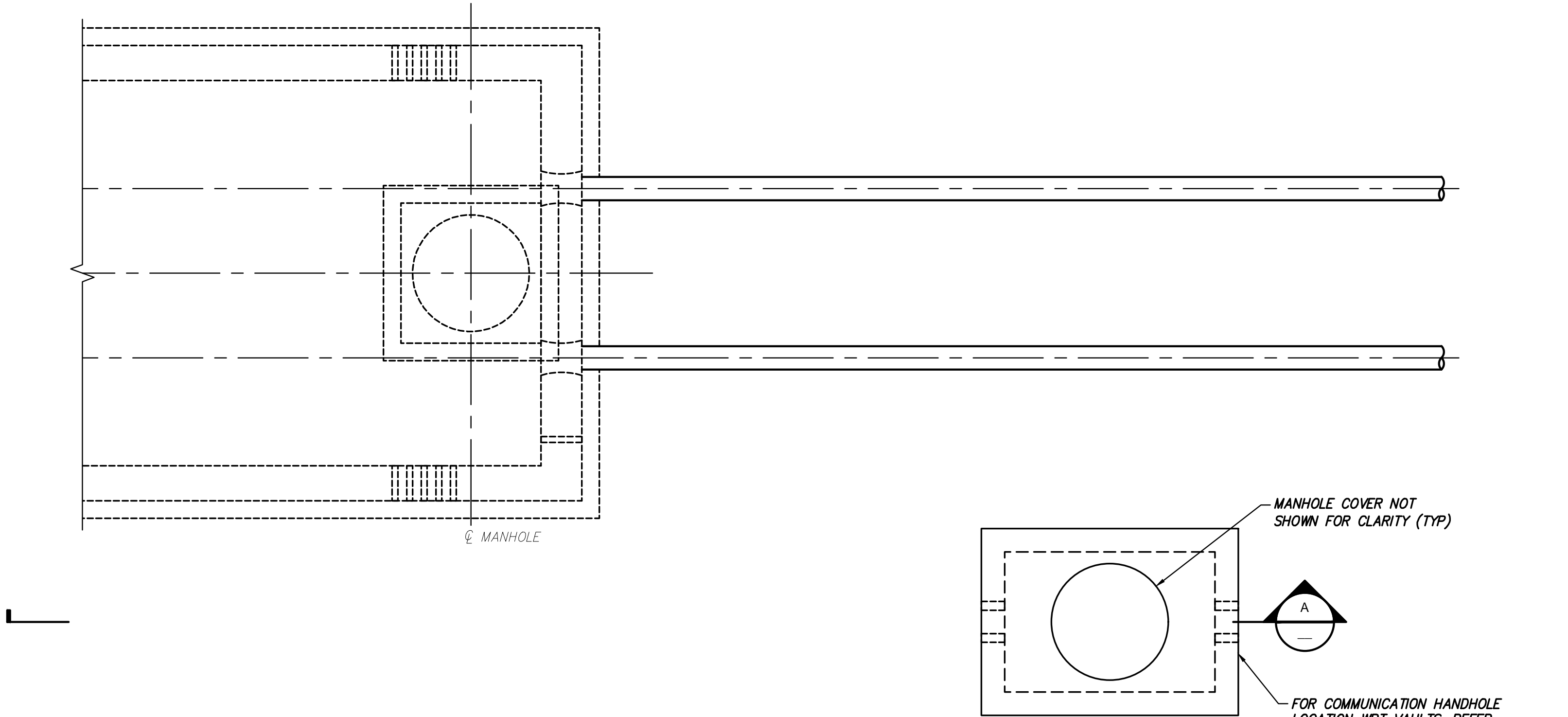
A cross-sectional diagram of a concrete wall corner. The wall is shown in grey. On the right side, there is a vertical section of the wall. Inside this section, there is a wavy line representing a form saver. The text "FORM SAVER (TYP)" has a line pointing to this wavy line. Above the wavy line, there is a label "MILD REINFORCEMENT (TYP)" with a line pointing to a horizontal bar that passes through the wall and the form saver.

KIEWIT PROJECT NO.	
21162	
CHA PROJECT NO.	
066076	
DRAWING NO.	
S-730	
DATE	04/15/
SH NO	XX OF

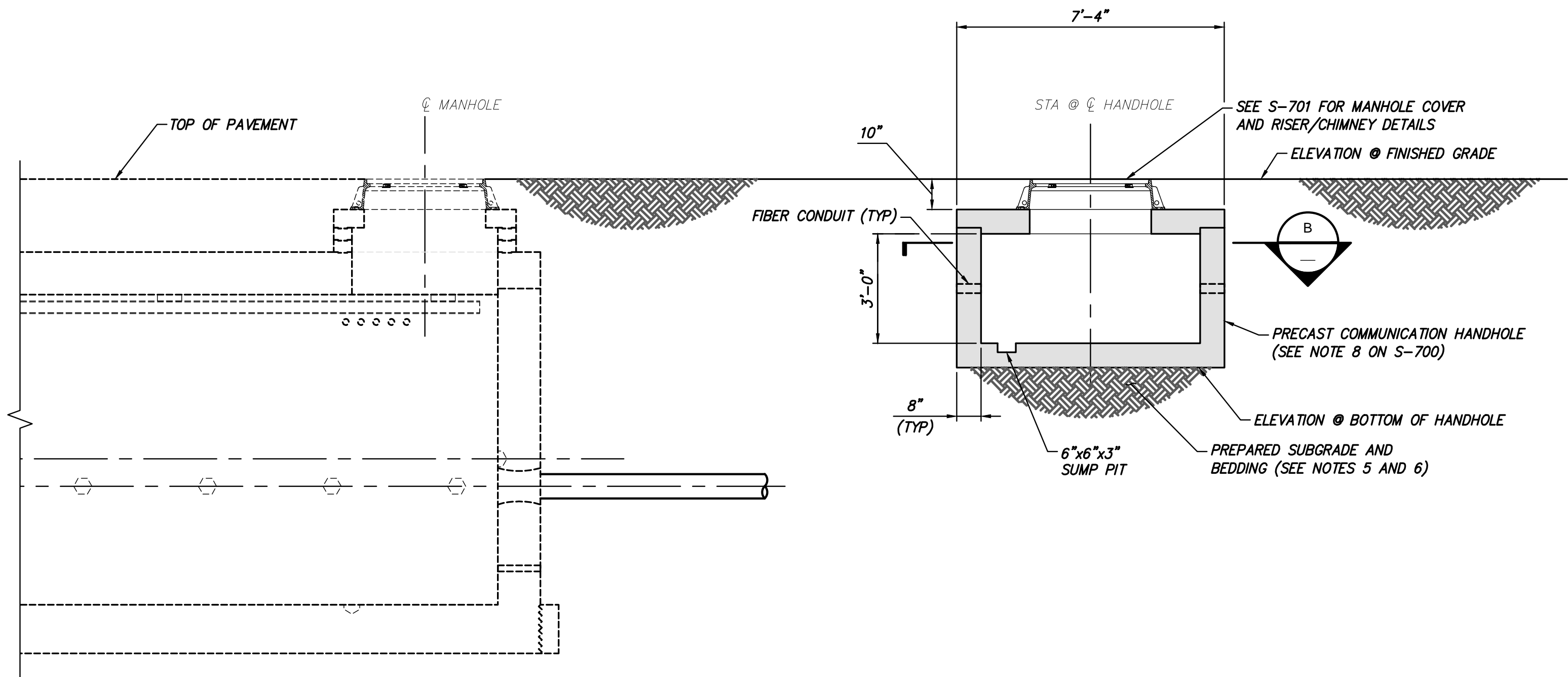


1. REFER TO NOTES ON SHEET S-730.

File: P:\KIE-CHPE\3-DES\07 STR\SHTS\21162_1A_S-770.DWG Saved: 4/8/2022 1:24:31 PM Plotted: 4/13/2022 10:54:35 AM Current User: Dietrich.Henderson LastSavedBy: Dietrich.Henderson



- NOTES:
- DESIGN LIVE LOAD: HS-20.
 - WALL THICKNESSES TO BE FINALIZED PER APPROVED VENDOR'S DESIGN. REFER TO APPROVED VENDOR SHOP DRAWINGS FOR WEIGHTS AND PICK POINTS.
 - ELECTRIC SUMP PUMP TO BE PROVIDED BY OTHERS.
 - SEE S-700 FOR VAULT DETAILS.
 - ESTABLISH STABLE SUBGRADE CONDITIONS AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE.
 - A MINIMUM BEDDING SECTION CONSISTING OF A 4-INCH THICK MUDMAT OR 4-INCH THICK SELECT GRANULAR FILL SHALL BE PLACED ON TOP OF PREPARED SUBGRADE. ADDITIONAL BEDDING MAY BE REQUIRED AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE BASED ON IN-SITU CONDITIONS.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

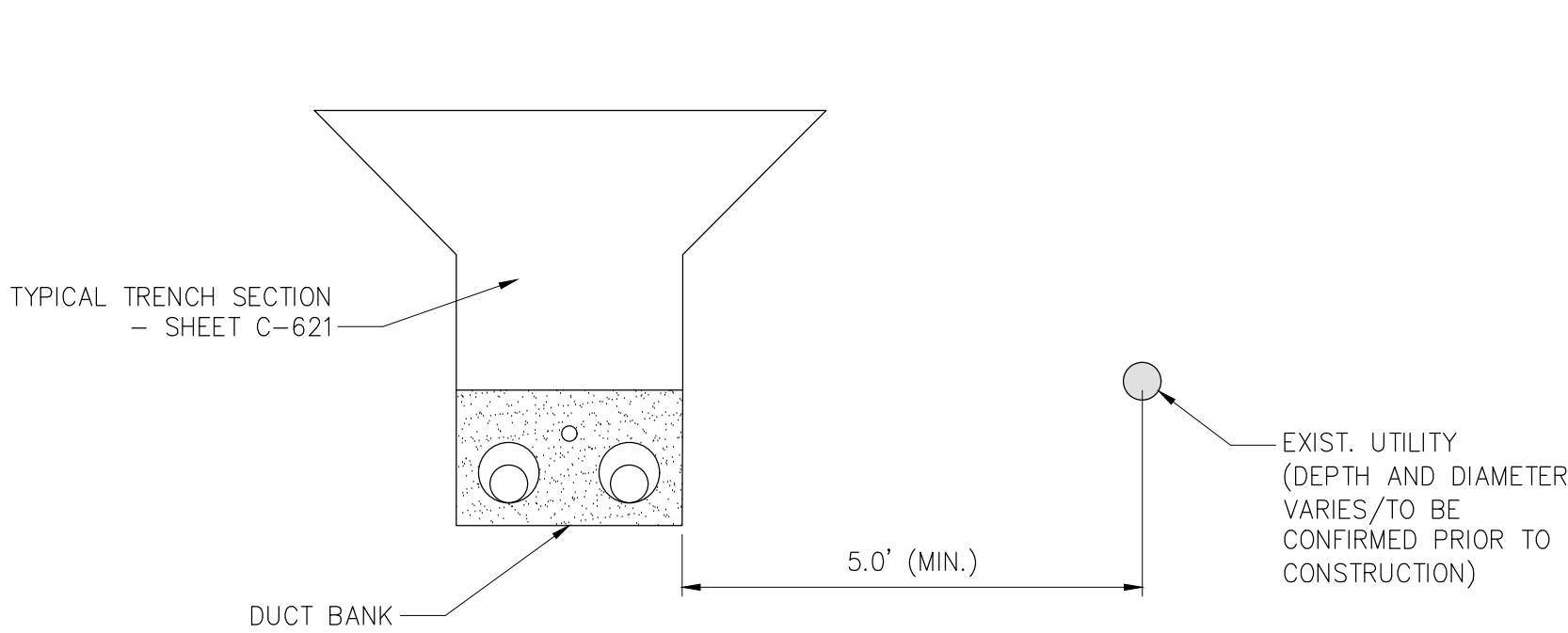
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 1 - PUTNAM TO DRESDEN
COMMUNICATION HANDHOLE PLAN ELEVATION & SECTION

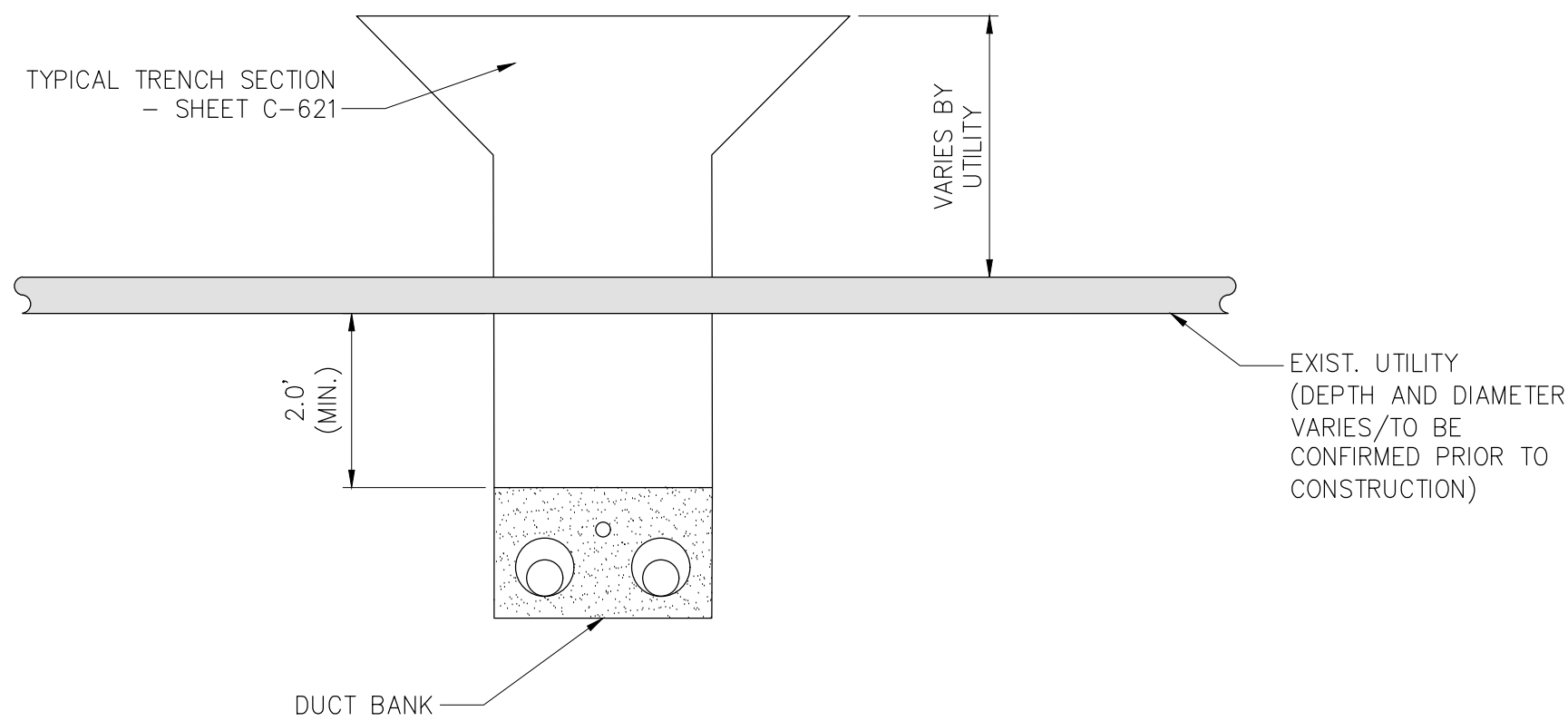
DRAWN BY: DRH DESIGNED BY: JNK APPROVED BY: OO SCALE AS SHOWN REV. NO.

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	S-770
DATE	04/15/2022
SH.NO.	XX OF XXX

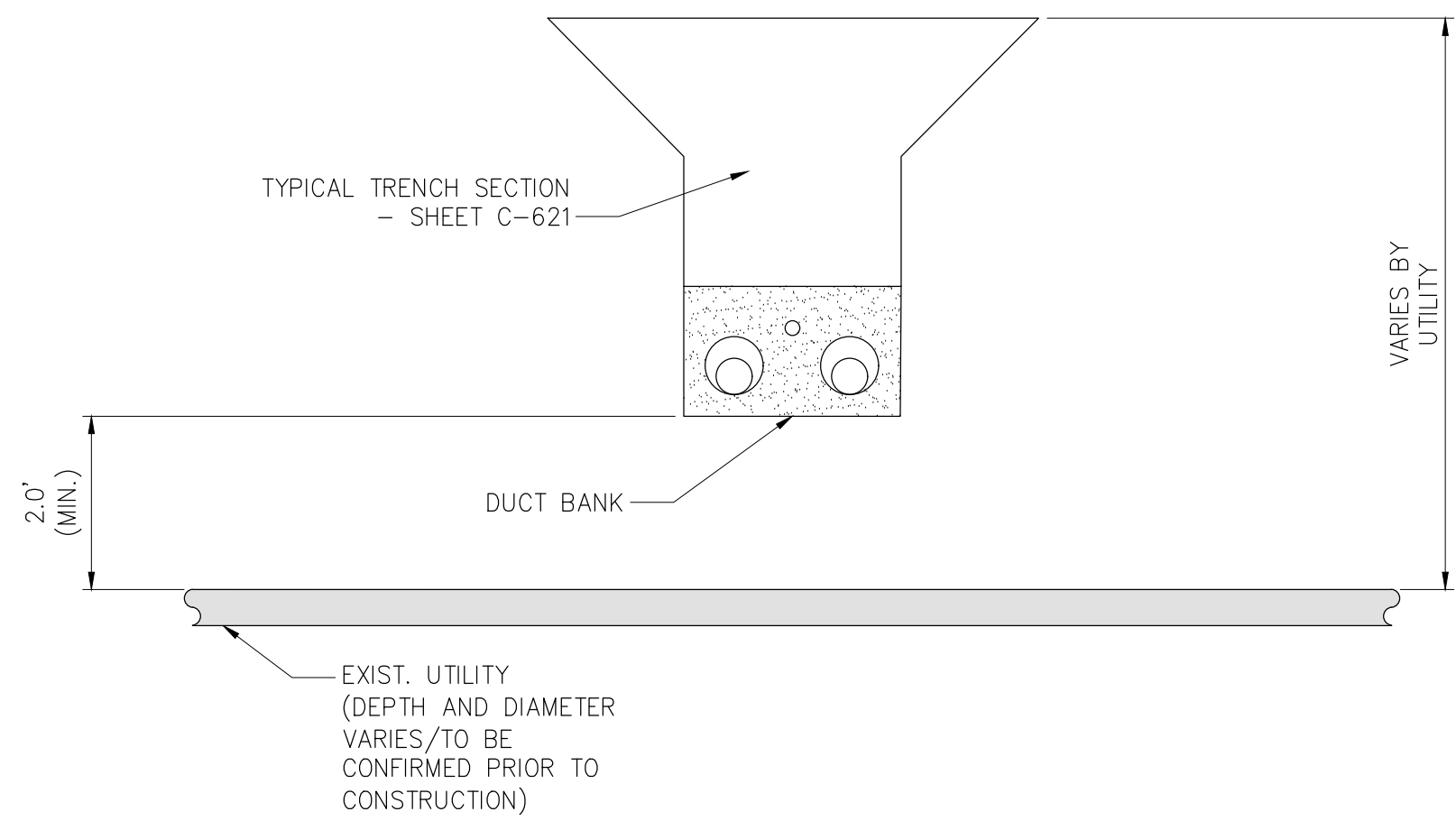
File: P:\PROJECTS\ME-CHPE\3-DES\95 WPT\3-OTERO\066076_P1A - C-901.DWG Saved: 4/7/2022 12:05:45 PM Plotted: 4/13/2022 10:23:45 AM Current User: William Otero LastSavedBy: William Otero



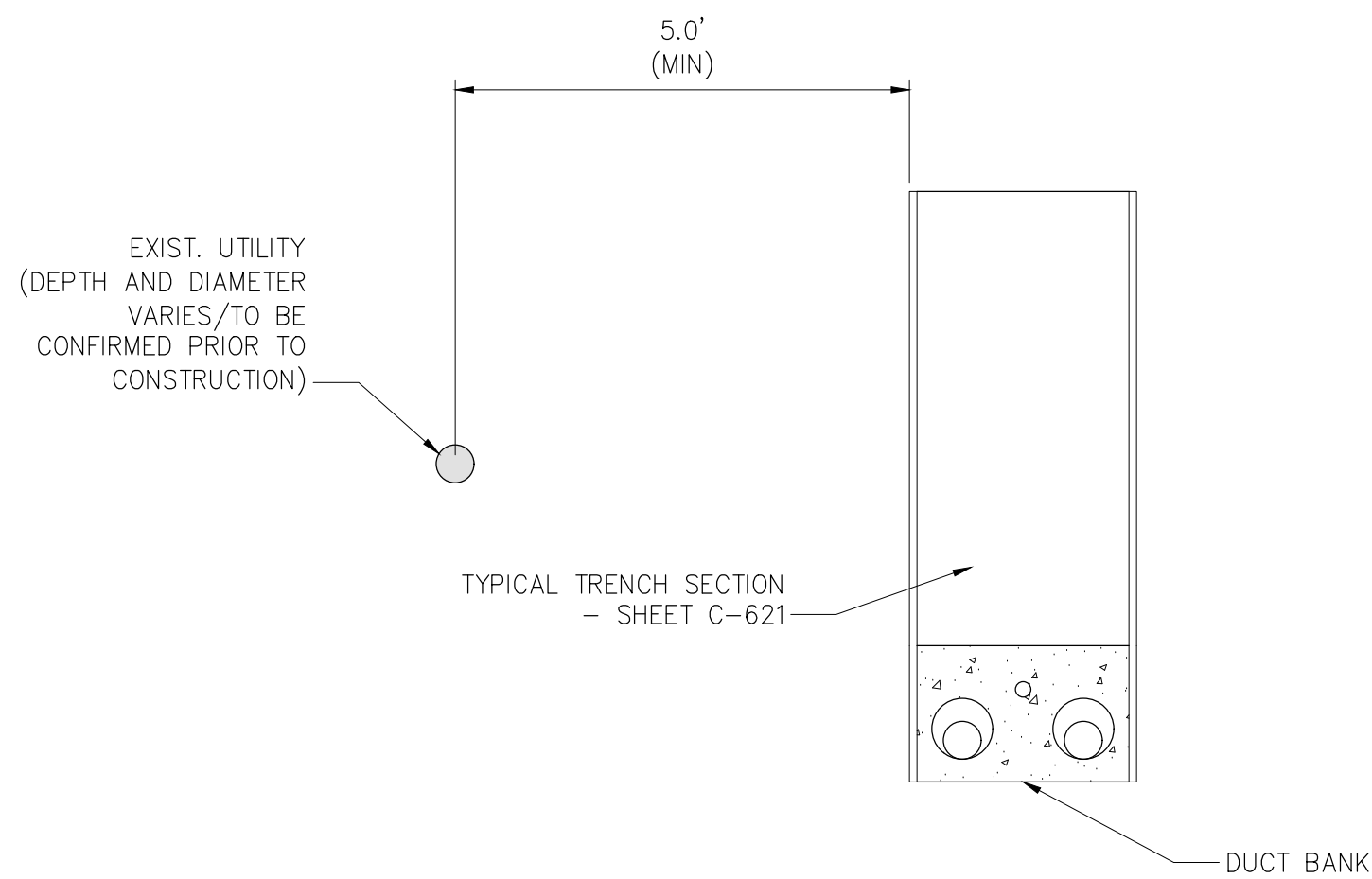
1 OPEN TRENCH - HORIZONTAL SEPARATION
NOT TO SCALE



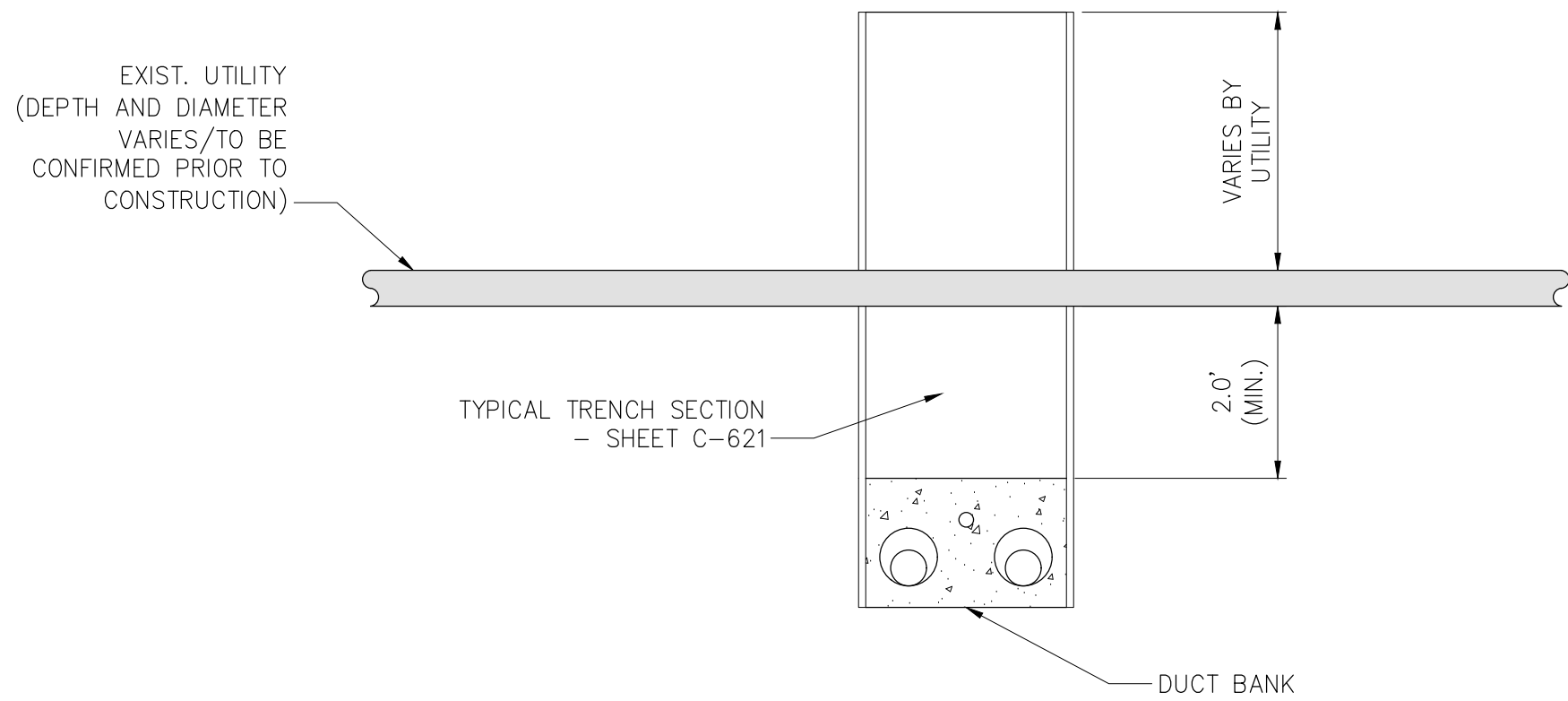
3 OPEN TRENCH - VERTICAL SEPARATION (UNDER)
NOT TO SCALE



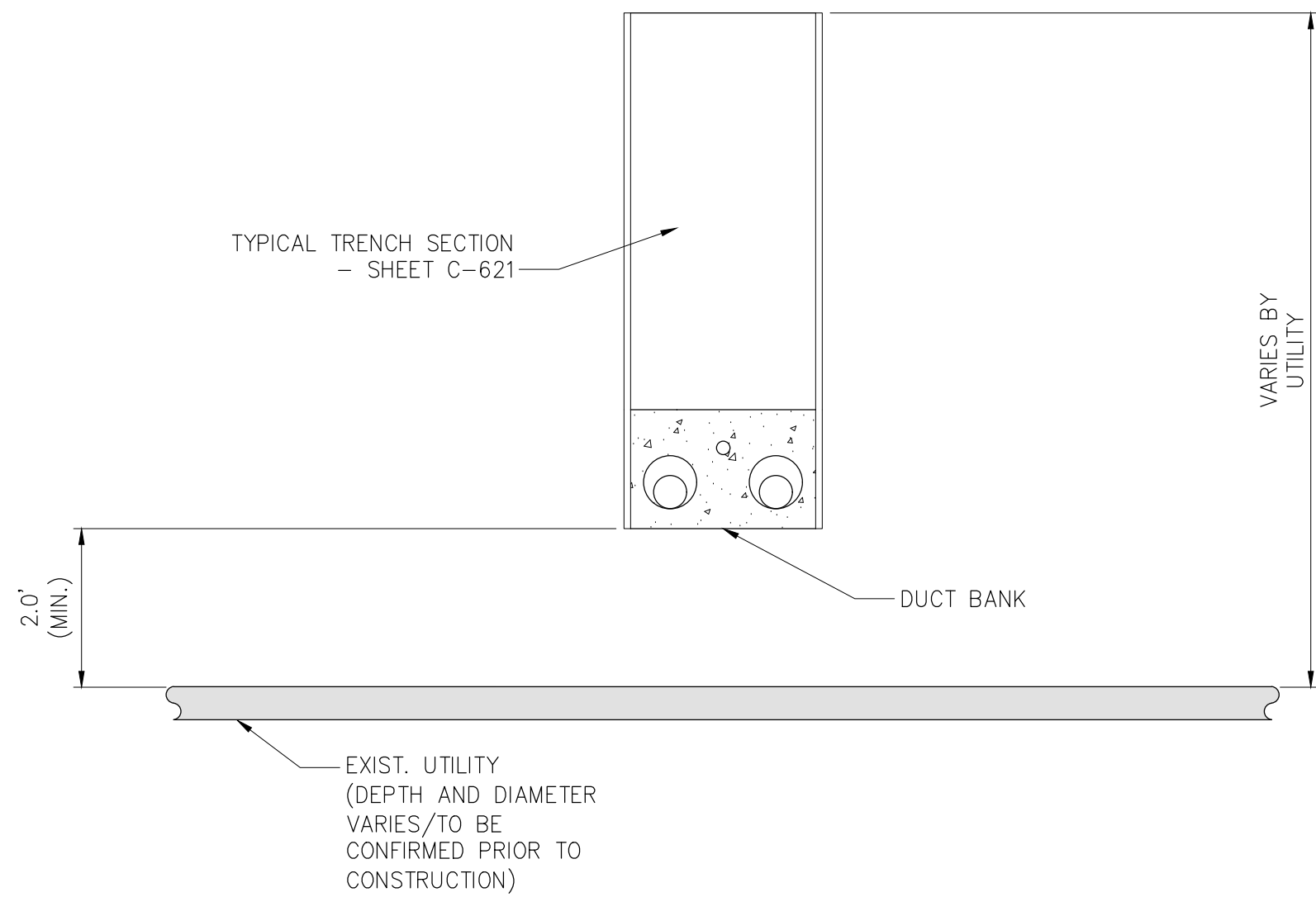
5 OPEN TRENCH - VERTICAL SEPARATION (OVER)
NOT TO SCALE



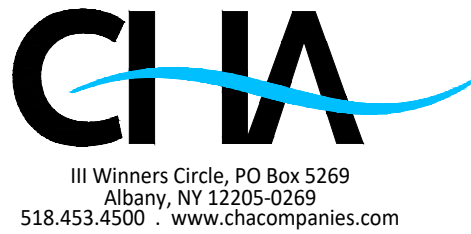
2 TYPICAL DUCTBANK IN ROADWAY - HORIZONTAL SEPARATION
NOT TO SCALE



4 TYPICAL DUCTBANK IN ROADWAY - VERTICAL SEPARATION (UNDER)
NOT TO SCALE



6 TYPICAL DUCTBANK IN ROADWAY - VERTICAL SEPARATION (OVER)
NOT TO SCALE



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 1- PUTNAM TO DRESDEN
UTILITY TYPICAL
SEPARATION DETAILS

DRAWN BY:	wso	DESIGNED BY:	wso	APPROVED BY:		SCALE	AS NOTED	DATE	04/15/2022
						REV. NO.		A	SH.NO. XXX OF XXX

KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	C-901

File: V:\PROJECTS\ANY\K6\066076.000\09_DESIGN\DRAWINGS\01_SHEETS\DESIGN PACKAGE 1B\066076_P1B - G-000 COVER SHEET.DWG Saved: 4/12/2022 4:58:45 PM Plotted: 4/13/2022 8:09:07 AM Current User: Evans, Josh LastSavedBy: 4893

A



NOT FOR CONSTRUCTION



CHA
III Winners Circle, PO Box 5269
Albany, NY 12205-0269
518.453.4500 • www.chacompanies.com

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

						CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL COVER SHEET					KIEWIT PROJECT NO. 21162	
											CHA PROJECT NO. 068076	
											DRAWING NO.	
											G-000	
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR								
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JTM	DESIGNED BY: JTM	APPROVED BY: JPR	SCALE REV. NO.	AS NOTED X	DATE SH.NO.	04/15/2022 XXX OF XXX	

File: V:\PROJECTS\ANY\6\066076.000\09_Design\Drawings\01_Sheets\Design Package \Design Package 1B\066076_P1B -- G-003 GENERAL NOTES.DWG Saved: 4/12/2022 2:30:02 PM Plotted: 4/14/2022 9:53:51 AM Current User: Evans, Josh Last Saved By: 4894

EROSION CONTROL NOTES

1. SEE C-400 SERIES OF SHEETS FOR EROSION AND SEDIMENT CONTROL SHEETS.
2. LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROVAL TO DO SO HAS BEEN RECEIVED BY GOVERNING AUTHORITIES.
3. THE GENERAL CONTRACTOR SHALL STRICTLY ADHERE TO THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) DURING CONSTRUCTION OPERATIONS.
4. NO LAND CLEARING OR GRADING SHALL BEGIN UNTIL ALL PERIMETER EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. (WETLAND PROTECTION FENCE, SILT FENCE AND STABILIZED CONSTRUCTION ENTRANCE)
5. SITE DISTURBANCE SHALL NOT EXCEED FIVE (5) ACRES OF SOIL AT ANY ONE TIME WITHOUT PRIOR WRITTEN AUTHORIZATION FROM NYSDEC.
6. ALL EXPOSED AREAS SHALL BE SEEDED AND MULCHED AS SPECIFIED WITHIN 14 DAYS OF FINAL GRADING.
7. INACTIVE PORTIONS OF THE SITE ARE TO BE SEEDED AND MULCHED AS SPECIFIED WITHIN 14 DAYS.
8. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) DAYS OR MORE FREQUENTLY IF REQUIRED. ALL MAINTENANCE REQUIRED BY INSPECTION SHALL COMMENCE WITHIN 24 HOURS AND BE COMPLETED WITHIN 48 HOURS OF REPORT.
9. THIS PLAN SHALL NOT BE CONSIDERED ALL INCLUSIVE AS THE GENERAL CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SOIL SEDIMENT FROM LEAVING THE SITE.
10. GENERAL CONTRACTOR SHALL COMPLY WITH ALL STATE AND LOCAL ORDINANCES THAT APPLY.
11. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES WILL BE INSTALLED IF DEEMED NECESSARY FOLLOWING SITE INSPECTION.
12. GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO TAKE WHATEVER MEANS NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION.
13. AT THE END OF EACH WORK DAY DISTURBED SOILS ARE TO BE REGRADED TO DRAIN INTO THE TEMPORARY DIVERSION SWALES AND DISCHARGES FROM DEWATERING ACTIVITIES ARE TO BE DIRECTED INTO CATCH BASINS OR SWALES.
14. CONCRETE WASHOUTS DEPICTED ON PLANS ARE FOR REFERENCE ONLY. CONTRACTOR TO FIELD LOCATE CONCRETE WASHOUTS AS NECESSARY. CONCRETE WASHOUTS SHALL NOT BE LOCATED WITHIN WETLANDS

GENERAL NOTES:

1. THE PLANS SHOW SUBSURFACE STRUCTURES, ABOVE GROUND STRUCTURES AND/OR UTILITIES FROM FIELD LOCATION AND RECORD MAPPING, EXACT LOCATION OF WHICH MAY VARY FROM THE LOCATIONS INDICATED. IN PARTICULAR, THE CONTRACTOR IS WARNED THAT THE EXACT OR EVEN APPROXIMATE LOCATION OF SUCH PIPELINES, SUBSURFACE STRUCTURES AND/OR UTILITIES IN THE AREA MAY BE DIFFERENT FROM THAT SHOWN OR MAY NOT BE SHOWN, AND IT SHALL BE HIS RESPONSIBILITY TO PROCEED WITH GREAT CARE IN EXECUTING ANY WORK. 48 HOURS BEFORE YOU DIG, DRILL, OR BLAST, CALL U.F.P.O. 1-(800)-962-7962 TOLL FREE.
2. THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY CONDITIONS THAT VARY FROM THOSE SHOWN ON THE PLANS. THE CONTRACTOR'S WORK SHALL NOT VARY FROM THE PLANS WITHOUT THE EXPRESSED APPROVAL OF THE ENGINEER.
3. THE CONTRACTOR SHALL RESTORE LAWNS, DRIVEWAYS, CULVERTS, SIGNS AND OTHER PUBLIC OR PRIVATE PROPERTY DAMAGED OR REMOVED TO AT LEAST AS GOOD A CONDITION AS BEFORE BEING DISTURBED AS DETERMINED BY THE ENGINEER.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND INCURRING THE COST OF ALL CONSTRUCTION PERMITS, INSPECTIONS, CERTIFICATES, ETC. AND SHALL COMPLY WITH ALL REQUIRED PERMITS.
5. ALL WORK SHALL BE DONE IN STRICT COMPLIANCE WITH ALL APPLICABLE NATIONAL, STATE, AND LOCAL CODES, STANDARDS, ORDINANCES, RULES, AND REGULATIONS.
6. ALL PROPOSED UTILITIES AND APPURTENANCES TO BE CONSTRUCTED IN COMPLIANCE WITH THE LOCAL MUNICIPALITIES' CODES AND REGULATIONS GOVERNING THE INSTALLATION OF SUCH UTILITIES.
7. THE ENGINEER RESERVES THE RIGHT TO EXAMINE ANY WORK DONE ON THIS PROJECT AT ANY TIME TO DETERMINE THE CONFORMANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS OF THIS PROJECT.
8. THE CONTRACTOR SHALL PROTECT EXISTING PROPERTY LINE MONUMENTATION. ANY MONUMENTATION DISTURBED OR DESTROYED, AS JUDGED BY THE ENGINEER OR OWNER, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE UNDER THE SUPERVISION OF A NEW YORK STATE LICENSED LAND SURVEYOR.
9. IT IS THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE ALL PLAN SHEETS.
10. THE CONTRACTOR SHALL:
 - A. VERIFY ALL CONDITIONS IN THE FIELD PRIOR TO COMMENCEMENT OF WORK AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
 - B. EXAMINE THE SITE AND INCLUDE IN HIS WORK THE EFFECT OF ALL EXISTING CONDITIONS ON THE WORK.
 - C. PROVIDE AND INSTALL ALL MATERIALS AND PERFORM ALL WORK IN ACCORDANCE WITH RECOGNIZED GOOD STANDARD PRACTICE.

NYSDOT NOTES

1. MAINTAIN EXISTING GUIDE RAIL, MEDIAN BARRIER, AND BRIDGE RAIL. WHEN CONSTRUCTION OPERATIONS REQUIRE THE TEMPORARY REMOVAL OF EXISTING BRIDGE RAIL, GUIDE RAIL OR MEDIAN BARRIER; OR WHEN EXISTING RAIL WILL BE REMOVED AND REPLACED WITH NEW RAIL, THE CONTRACTOR SHALL SCHEDULE OPERATIONS TO MINIMIZE THE TIME PERIOD THAT RAIL IS NOT INSTALLED.
2. UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, GUIDE RAIL OR MEDIAN BARRIER SHALL BE REPLACED OR THE LOCATION OTHERWISE PROTECTED WITHIN 14 CALENDAR DAYS. BRIDGE RAIL SYSTEMS SHALL BE MAINTAINED IN SERVICE AT ALL TIMES ON ANY STRUCTURE ON WHICH VEHICLE OR PEDESTRIAN TRAFFIC IS MAINTAINED, UNLESS A TEMPORARY BARRIER IS INSTALLED, OR OTHER MEANS ARE USED TO ENSURE THAT VEHICLES, BICYCLISTS AND PEDESTRIANS ARE NOT EXPOSED TO THE UNPROTECTED EDGE OF A BRIDGE.
3. DURING NON-WORK HOURS WHEN TRAFFIC IS BEING MAINTAINED ON THE FACILITY, ALL TEMPORARY ENDS (FREE ENDS) OF GUIDE RAIL, MEDIAN BARRIER AND BRIDGE RAIL SHALL BE TEMPORARILY TERMINATED AND MARKED WITH A CHANNELIZING DRUM OR OBJECT MARKER EQUIPPED WITH A TYPE A FLASHING WARNING LIGHT. CORRUGATED BEAM GUIDE RAIL AND MEDIAN BARRIER, AND HEAVY-POST, BLOCKED-OUT, CORRUGATED BEAM GUIDE RAIL AND MEDIAN BARRIER SHALL BE TEMPORARILY TERMINATED BY HAVING THE EXPOSED ENDS (FREE ENDS) DROPPED TO THE GROUND AND PINNED.
4. THE APPROACH ENDS OF BOX BEAM GUIDE RAIL, MEDIAN BARRIER AND BRIDGE RAIL SHALL BE TEMPORARILY TERMINATED WITH BOX BEAM GUIDE RAIL END ASSEMBLIES UTILIZING TWO SPLICE PLATES AND THE PROPER NUMBER OF BOLTS PER CONNECTION. NO POSTS FOR ANCHORAGES WILL BE REQUIRED. SPECIAL TEMPORARY SPLICE PLATES ARE REQUIRED TO ADAPT BOX BEAM GUIDE RAIL END ASSEMBLIES TO BOX BEAM MEDIAN BARRIERS.
5. DURING ANY OVERNIGHT PERIOD WHEN EXISTING GUIDE RAIL OR MEDIAN BARRIER IS TEMPORARILY REMOVED, THE CONTRACTOR SHALL INSTALL CHANNELIZING DEVICES IN THE LOCATION WHERE THE GUIDE RAIL OR MEDIAN BARRIER WAS REMOVED IN ACCORDANCE WITH §619-3.02J.6. REMOVED EXISTING GUIDE RAIL OR MEDIAN BARRIER.
6. GUIDERAIL REPLACEMENT TYPE TO BE COORDINATED WITH NYSDOT.
7. THE PLANS DEPICT AREAS OF EXPANDED LIMIT OF WORK AT EXISTING CULVERTS THAT WILL BE EVALUATED WITH NYSOT AND THE CERTIFICATE HOLDERS FOR POTENTIAL REPLACEMENT OR REPAIR. IF ANY REPAIRS OR REPLACEMENTS ARE DETERMINED, THE WORK WILL BE DONE IN ACCORDANCE WITH NYSDOT SPECIFICATIONS.

11. ALL TRENCH EXCAVATION AND ANY REQUIRED SHEETING AND SHORING SHALL BE DONE IN ACCORDANCE WITH THE LATEST REVISIONS OF NEW YORK STATE INDUSTRIAL CODE RULE 23 AND OSHA REGULATIONS FOR CONSTRUCTION. SHEET PILING SHALL BE DESIGNED AND SEALED BY A NEW YORK STATE PROFESSIONAL ENGINEER. WHERE WITHIN RAIL ROAD ROW, ANY EXCAVATION AND SHORING SHALL BE DESIGNED TO MINIMUM CP AND AREAMA REQUIREMENTS.
12. CONTRACTOR SHALL BE RESPONSIBLE FOR DEWATERING AND THE MAINTENANCE OF SURFACE DRAINAGE DURING THE COURSE OF WORK AND SHALL SUBMIT A DEWATERING PLAN. CONTRACTOR SHALL MAINTAIN EXISTING SITE DRAINAGE PATTERNS THROUGHOUT CONSTRUCTION UNLESS OTHERWISE SHOWN ON THE PLANS.
13. MAINTAIN FLOW FOR ALL EXISTING UTILITIES.
14. ALL FRAMES/COVERS WITHIN PAVED AREAS SHALL HAVE THE TOPS SET FLUSH WITH THE EXISTING PAVEMENT GRADE. IN LANDSCAPED AREAS, ALL FRAMES SHALL BE 0.1' ABOVE GRADE.
15. TEMPORARY PAVEMENT SHALL BE PLACED WITHIN 48 HOURS OF COMPLETION OF BACKFILL OPERATIONS WITHIN THE EXISTING PAVEMENT LIMITS.
16. CONTRACTOR SHALL MAINTAIN ALL TRAFFIC IN ALL AREAS IN ACCORDANCE WITH THE NYSDOT MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
17. ALL EXCAVATIONS SHALL BE PROTECTED AT THE END OF EACH WORK DAY PER OSHA AND NYSDOT REQUIREMENTS.
18. WITHIN NYSDOT ROW ALL OPEN EXCAVATIONS TO BE PROTECTED BY CONCRETE BARRIERS OR BE COVERED BY A STEEL PLATE, 3/4" THICK MINIMUM. A SINGLE PLATE SHOULD COVER THE ENTIRE EXCAVATION AND HAVE ENOUGH BEARING ON SURROUNDING SURFACES TO SUPPORT A VEHICLE.
19. CONTRACTOR SHALL TAKE CARE TO PREVENT DAMAGE TO EXISTING UTILITIES. UTILITIES DAMAGED BY CONTRACTOR SHALL BE IMMEDIATELY REPAIRED BY CONTRACTOR AT THE CONTRACTOR'S EXPENSE. IF DURING EXCAVATION PREVIOUSLY DAMAGED UTILITIES ARE UNCOVERED, CONTRACTOR SHALL DOCUMENT THE DAMAGE AND REPORT DAMAGE TO THE APPROPRIATE OWNER.
20. DEPTH OF BURY FOR EXISTING CABLED UTILITIES FIBER / ELECTRICAL / TELECOM AND WATERLINES UNKNOWN. ASSUMED DEPTH OF BURY FOR CABLED UTILITIES IS 30" UNLESS OTHERWISE SHOWN. ASSUMED DEPTH OF BURY FOR WATERLINES IS 5' UNLESS OTHERWISE SHOWN.
21. CONTRACTOR TO COORDINATE ALL DRIVEWAY CROSSINGS WITH THE PROPERTY OWNERS PRIOR TO EXCAVATING. ACCESS TO ALL DRIVEWAYS FOR THE RESIDENTS AND COMMERCIAL PROSPERITIES, WILL NEED TO BE MAINTAINED DURING THE PROJECT. ALL EXCAVATIONS IN THE ENTRANCES/DRIVEWAYS WILL NEED TO BE BACKFILLED AT THE END OF EACH WORKDAY, OR STEEL PLATES SHALL BE INSTALLED TO ALLOW ACCESS DURING CONSTRUCTION. REFER TO THE EM&CP FOR EMERGENCY ACCESS MANAGEMENT PLAN.

DISTURBANCE NOTE:

1. THE PROPOSED DISTURBANCE FOR THE TRENCH DETAIL WILL BE LIMITED TO THE WIDTH OF THE TRENCH SECTIONS DEPICTED ON C-621.
2. ARES OF THE LIMIT OF WORK ACROSS THE ALIGNMENT HAVE BEEN EXPANDED TO INCLUDE AREAS OFF THE ROADWAY WITHIN LAWN AREAS THAT ARE SUITABLE FOR POTENTIAL TEMPORARY STAGING AREAS DURING CONSTRUCTION. SHOULD THESE AREAS BE UTILIZED AND DISTURBED THE AREAS SHOULD BE RESTORED TO EXISTING CONDITION PER THE PROJECT RESTORATION NOTES AND DETAILS. EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN DEVELOPED TO INCORPORATE THESE POTENTIALLY DISTURBED AREAS.



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL
GENERAL NOTES

DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR SCALE AS NOTED
REV. NO. X

KIEWIT PROJECT NO.	
21162	
CHA PROJECT NO.	
066076	
DRAWING NO.	
G-003	
DATE	04/15/2022
SH.NO.	XXX OF XXX



File: V:\PROJECTS\ANY\6\066076.000\09_Design\Drawings\01_Sheets\Design Package 1B\066076_F1B - G-011-G-013.DWG Saved: 4/13/2022 10:15:34 PM Plotted: 4/13/2022 10:26:22 PM Current User: Evans, Josh LastSavedBy: 4894

TABLE 1.4: PHASE 1 SEGMENT 2 – AGRICULTURAL DISTRICT LANDS	
AGRICULTURAL LAND LOCATION	ANTICIPATED IMPACTS TO AGRICULTURAL ACTIVITIES/LAND
2	THERE ARE NO AGRICULTURAL LANDS LOCATED IN SEGMENT 2
WEST OF THE ALIGNMENT NORTH OF RYDER ROAD FROM 15273+50 TO 15280+50 (SHEET C–202)	THE NORTH RYDER ROAD STAGING AND LAYDOWN AREA IS LOCATED WITHIN SEGMENT 1C BUT WILL BE UTILIZED DURING CONSTRUCTION OF SEGMENTS 1 AND 2. ANTICIPATED IMPACTS TO AGRICULTURAL LANDS INCLUDE CONSTRUCTION ACTIVITY FROM THE STORAGE OF MATERIALS AND EQUIPMENT. ALL IMPACTS WILL BE TEMPORARY, AND THE AREA WILL BE FULLY RESTORED IN ACCORDANCE WITH SECTION 13.4.
WEST OF THE ALIGNMENT SOUTH OF RYDER ROAD FROM 15281+00 TO 15285+50 (SHEET C–201)	THE SOUTH RYDER ROAD STAGING AND LAYDOWN AREA IS LOCATED WITHIN SEGMENT 1C BUT WILL BE UTILIZED DURING CONSTRUCTION OF SEGMENTS 1 AND 2. ANTICIPATED IMPACTS TO AGRICULTURAL LANDS INCLUDE CONSTRUCTION ACTIVITY FROM THE STORAGE OF MATERIALS AND EQUIPMENT. ALL IMPACTS WILL BE TEMPORARY, AND THE AREA WILL BE FULLY RESTORED IN ACCORDANCE WITH SECTION 13.4.

NOTES:
1. SECTION 1.4.1 OF THE EM&CP SUMMARIZES THE MEASURES TO BE FOLLOWED WITHIN AGRICULTURAL AREAS. SECTION 7.1.4 OF THE EM&CP SUMMARIZES THE REQUIREMENTS AND PROCEDURES FOR ANY VEGETATION OR TREE CLEARING THAT MAY OCCUR WITHIN AGRICULTURAL LANDS. TABLE 13.1 IN SECTION 13.4 OF THE EM&CP SUMMARIZES THE LOCATION WHERE AGRICULTURAL LANDS POTENTIALLY REQUIRE RESTORATION WITHIN SEGMENT 2. SECTION 13.4 OF THE EM&CP AND DESCRIBES ALL CLEANUP AND RESTORATION PROCEDURES AND METHODS THAT WILL BE FOLLOWED TO RESTORE AGRICULTURAL LANDS.

SEGMENT 2 – AGRICULTURAL LANDS

TABLE 1.4: SEGMENT 2 – RECREATIONAL AREAS	
RECREATIONAL AREA	LOCATION
ADIRONDACK PARK	(DRESDEN TO 12938+00)
NYSDEC SOUTH BAY PIER AND BOAT LAUNCH	(DRESDEN TO 12938+00)
LAKES TO LOCKS PASSAGE SCENIC BYWAY (STATE HIGHWAY 22)	(12500+00 (C–101) TO 13040+00 (C–136))

NOTE: SECTION 1.4.2 OF THE EM&CP SUMMARIZES THE MEASURES TO BE FOLLOWED WITHIN RECREATIONAL AREAS.

SEGMENT 2 – RECREATIONAL AREAS

TABLE 4: SEGMENT 2 – CONSTRUCTION METHODS	
TOPIC	SECTION OF EM&CP
HORIZONTAL DIRECTIONAL DRILLING	4.2
INSTALLATION AND PERFORMANCE CONTROLS	4.2.1
DRILLING FLUIDS MANAGEMENT	4.2.2
ROAD CROSSING METHODS	4.2.3
TRENCHING	4.3
CABLE INSTALLATION REQUIREMENTS	4.3.1
DEWATERING METHODS	4.3.2
BEDDING AND BACKFILLING METHODS	4.3.3
DREDGING	NOT APPLICABLE TO SEGMENT 2
CONVERTER STATION AND SUBSTATION REQUIREMENTS	NOT APPLICABLE TO SEGMENT 2
RIGHT OF WAYS AND EASEMENTS	4.6
RIGHT OF WAY CLEARING	4.7 (See Section 7.0 for more details)
BUILDING AND STRUCTURE REMOVAL	NOT APPLICABLE TO SEGMENT 2
ACCESS ROADS	4.9
DRIVEWAY ACCESS DURING CONSTRUCTION	4.9.1
ACCESS THROUGH WETLANDS	4.9.2
ACCESS THROUGH AGRICULTURAL LANDS	4.9.3
DRAIN LINES AND UNDER DRAINS WITHIN AGRICULTURAL LANDS	4.9.4
SOIL MANAGEMENT PLAN	4.10
TRANSPORTATION AND UTILITY CROSSINGS	4.11 (SEE SECTION 11.0 AND 12.0 FOR MORE DETAILS)
CULVERT REPLACEMENT	4.12

NOTE: TABLE 4 SUMMARIZES THE CONSTRUCTION METHODS AND ASSOCIATED SUBSECTIONS THAT SUMMARIZE THE MEASURES AND STANDARDS THAT WILL BE FOLLOWED WITHIN THE SEGMENT 2.

SEGMENT 2 – CONSTRUCTION METHODS

TABLE 4.3: SEGMENT 2 –ACCESS ROADS				
ACCESS ROAD DESCRIPTION	TYPE OF ACCESS ROAD	LOCATION (APPROXIMATE – SEE DRAWINGS FOR DETAILS)	IMPACTS TO ENVIRONMENTALLY SENSITIVE AREAS	IMPACTS TO AGRICULTURAL LAND
GRAVEL ACCESS ROAD FOR LAKE ROAD HDD 2 ENTRY	TEMPORARY	12914+75 TO 12921+50 (C–128)	NONE	NONE

NOTE: TABLE 4.3 SUMMARIZES THE ACCESS ROADS IN THIS SEGMENT AND THEIR ASSOCIATED IMPACTS ON ENVIRONMENTALLY SENSITIVE AREAS AND AGRICULTURAL LANDS IF APPLICABLE. SECTION 4.8 OF THE EM&CP SUMMARIZES THE PROCEDURES THAT WILL BE FOLLOWED FOR THE CONSTRUCTION OF ALL ACCESS ROADS. ALL ACCESS ROADS WILL BE TEMPORARY AND RESTORED ACCORDING TO SECTION 13.2.4 AND 13.4 OF THE EM&CP AS APPLICABLE.

SEGMENT 2 – ACCESS ROADS

- NOTES:
- THE STORMWATER POLLUTION PREVENTION PLAN INCLUDED IN APPENDIX G OF THE EM&CP DESCRIBES THE EROSION AND SEDIMENT CONTROLS THAT WILL BE FOLLOWED FOR THIS SEGMENT.
 - THE EROSION AND SEDIMENT CONTROL PLANS CAN BE FOUND ON SHEETS C–401 TO SHEET C–418 FOR THIS SEGMENT.

SEGMENT 2 – EM&CP EROSION AND SEDIMENT CONTROL NOTES

- NOTE:
- THE NOISE RECEPTORS THAT OCCUR NEAR SEGMENT 2 AT VARIOUS POINTS INCLUDE RESIDENCES AND BUSINESSES. SECTION 9.2 OF THE EM&CP DESCRIBES THE NOISE CONTROL MEASURES THAT WILL BE EMPLOYED THROUGHOUT THIS SEGMENT.

SEGMENT 2 – EM&CP NOISE SENSITIVE AREAS NOTE

- NOTE:
- GIS FEMA DATA NOT AVAILABLE FOR WASHINGTON COUNTY, FEMA FIRM MAPS ARE PROVIDED IN APPENDIX D OF THE STORMWATER POLLUTION PREVENTION PLAN WHICH IS INCLUDED IN APPENDIX G OF THE EM&CP.

SEGMENT 2 – FEMA



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																							</
--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	----

TABLE 11.3 NYSDOT COORDINATION SUMMARY		
COORDINATING PARTIES	DESCRIPTION	CURRENT STATUS
CERTIFICATE HOLDERS, DPS STAFF, NYSDOT	ALL PLANS AND WORK TO BE PERFORMED IN STATE-OWNED ROW UNDER NYSDOT'S SUPERVISION AND MANAGEMENT.	ONGOING THROUGHOUT
CERTIFICATE HOLDERS, DPS STAFF, NYSDOT STAFF	CERTIFICATE HOLDERS SHALL PROVIDE DPS STAFF AND NYSDOT STAFF WITH A PRELIMINARY DESIGN MARKED TO AVOID CONFLICT WITH POTENTIAL TRANSPORTATION PROJECTS THAT NYSDOT STAFF MAY SEEK TO UNDERTAKE IN THE FUTURE AND SHALL OFFER TO CONSULT WITH NYSDOT STAFF CONCERNING ANY COMMENTS IT MAY OFFER AND SHALL USE REASONABLE EFFORTS TO ACCOMMODATE ANY NYSDOT CONCERNS (CC#68).	PRIOR TO FILING ANY SEGMENT EM&CP INVOLVING ANY SUCH STATE-OWNED ROW.
CERTIFICATE HOLDERS, NYSDOT, AGENCY CROSSED BY PROJECT	CERTIFICATE HOLDERS WILL CONSULT WITH EACH TRANSPORTATION DEPARTMENT OR AGENCY HAVING JURISDICTION OVER ANY ROADS, RELATED STRUCTURES, AND COMPONENTS THAT WILL BE CROSSED BY THE FACILITY OR USED FOR DIRECT ACCESS TO THE CONSTRUCTION ZONE. IF THE ACCESS ROAD TAKES DIRECT ACCESS FROM, OR LIES WITHIN THE LIMITS OF, SUCH ROADS, THE CERTIFICATE HOLDERS WILL NOTIFY EACH RELEVANT TRANSPORTATION DEPARTMENT OR AGENCY OF THE APPROXIMATE DATE WHEN WORK WILL BEGIN (CC#69A).	DURING PREPARATION OF THE EM&CP AND WHEN WORK BEGINS.
CERTIFICATE HOLDERS, NYSDOT, DPS STAFF, NYSDEC	THE CERTIFICATE HOLDERS WILL PROVIDE STATUS REPORTS SUMMARIZING CONSTRUCTION AND INDICATING CONSTRUCTION ACTIVITIES AND LOCATIONS SCHEDULED FOR THE NEXT MONTH (CC#47).	BI-WEEKLY.

SEGMENT 2 – NYSDOT COORDINATION SUMMARY

TABLE 12.1 SEGMENT 1 – CO-LOCATED INFRASTRUCTURE CONSULTATION SUMMARY					
OWNER	UTILITY	INITIAL CONTACT DATE	CI-OWNER RESPONSE	OUTREACH MAILING #2	OUTREACH MAILING #3
AT&T	FIBER/TELEPHONE	September 9, 2021	CROSSING CONDITIONS RECEIVED	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE.	4/13/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.
LEVEL 3 COMMUNICATIONS (NOW LUMEN TECHNOLOGIES)	FIBER	September 10, 2021	SUPPORT SERVICES AGREEMENT IN PLACE. CROSSING CONDITIONS RECEIVED. REIMBURSEMENT FUND ESTABLISHED.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE.	4/13/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.
NATIONAL GRID/EAST/ELECTRIC	ELECTRIC	September 10, 2021	CROSSING CONDITIONS RECEIVED.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE. RECEIVED LOCATIONS OF OVERHEAD ELECTRIC LINES.	4/14/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.
TIME WARNER CABLE (CHARTER COMMUNICATIONS/SPECTRUM)	FIBER/CATV	September 23, 2021	NO ACTION REQUIRED UNTIL PLANS ARE AVAILABLE FOR REVIEW.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE.	4/14/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.
VERIZON OR VERIZON/EAST	TELECOM; FIBER/TELEPHONE	September 10, 2021	NO ACTION REQUIRED UNTIL PLANS ARE AVAILABLE FOR REVIEW.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE.	4/14/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.
NYSDOT ALBANY REGION 1	TRAFFIC SIGNALS HIGHWAY	ONGOING FOR A NUMBER OF YEARS. SEE ALSO TABLE 11.3			1/27/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW.
TOWN OF DRESDEN	STORM SEWER/SANITARY SEWER	September 7, 2021	DRAFT PLAN & PROFILE SENT FOR REVIEW.		1/27/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW.
TOWN OF WHITEHALL	TRAFFIC SIGNALS/STORM SEWER/CULVERTS/SANITARY SEWER/WATER	September 7, 2021	DRAFT PLAN & PROFILE SENT FOR REVIEW.		1/27/2022 PLAN & PROFILES DRAWINGS SENT FOR REVIEW.
VILLAGE OF WHITEHALL	TRAFFIC SIGNALS/STORM SEWER/CULVERTS/SANITARY SEWER/WATER	September 8, 2021	DRAFT PLAN & PROFILE SENT FOR REVIEW.		1/27/2022 PLAN & PROFILE DRAWINGS SENT FOR REVIEW.

SEGMENT 2 – CO-LOCATED INFRASTRUCTURE CONSULTATION SUMMARY

TABLE 12.2 SEGMENT 2 – UTILITY CROSSINGS					
SEGMENT	CROSSING ID	UTILITY TYPE	UDERGROUND OR OVERHEAD	CROSSING TYPE	APROXIMATE STATION (DETAILS ON DRAWINGS)
2	N/A	ELECTRIC (SECTION 12.3.2)	OVERHEAD	OPEN CUT/TRENCH	12519+50 (C-102)
2	N/A	STORM (SECTION 12.3.3)	UNDERGROUND	OPEN CUT/TRENCH	12520+25 (C-102)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12530+25, 12536+00, 12538+25 (C-103)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12538+75, 12542+50 (C-103)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12550+00, 12556+75 (C-104)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12548+75, 12554+25 (C-104)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	102560+75, 12562+25, 12575+00 (C-105)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12565+75 (C-105)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12576+00 (C-106)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	102591+50 (C-107)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12592+50, 12599+25, 12604+50 (C-107)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12607+00, 12610+00 (C-108)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12610+25, 12612+75, 12619+50 (C-108)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12631+00 (C-109)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12649+50 (C-110)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12639+75 (C-110)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12652+00 (C-111)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12668+50, 12669+50, 12670+75, 12672+50 (C-112)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12666+50 to 12667+00, 12672+75, 12679+75 (C-112)
2	HDD 1	STORM	UNDERGROUND	HDD	12689+75 (C-113)
2	HDD 1	STORM	UNDERGROUND	HDD	12711+75, 12715+00, 12721+75 (C-115)
2	HDD 1	ELECTRIC	OVERHEAD	HDD	12727+75, 12739+00 (C-116)
2	HDD 1	STORM	UNDERGROUND	OPEN CUT/TRENCH	12730+75, 12737+00 (C-116)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12744+75 to 12745+00, 12755+00 (C-117)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12761+75 (C-118)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12781+25 (C-119)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12776+00 (C-119)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12785+00, 12790+50, 12796+25 (C-120)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12800+25, 12805+75 (C-121)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12802+25, 12805+75 (C-121)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12818+00, 12820+00, 12827+75 (C-122)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12842+00, 12843+50 (C-123)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12836+00, 12841+00 (C-123)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12852+00 (C-124)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12846+00, 12853+25, 12856+75 (C-124)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12861+75, 12863+25, 12867+00 (C-125)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12880+00, 12884+75 (C-126)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12893+25, 12900+00, 12902+75 (C-127)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12913+50, 12914+00 (C-128)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12907+75 (C-128)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12920+75(C-129)
2	HDD 2	STORM	UNDERGROUND	OPEN CUT/TRENCH	12920+50 (C-129)
2	HDD 2	FIBER OPTIC CABLE (SECTION 12.3.3)	UNDERGROUND	HDD	12925+00 (C-129)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12946+00 (C-130)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12954+25 (C-131)
2	N/A	WATER	UNDERGROUND	OPEN CUT/TRENCH	12951+75 (C-131)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12958+25 (C-131)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	12977+75 (C-132)
2	N/A	ELECTRIC	UNDERGROUND	OPEN CUT/TRENCH	12968+25, 12975+25 (C-132)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12966+75, 12977+75 (C-132)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	12980+25 (C-133)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	13002+75 to 13011+00 (C-134 to C-135)
2	N/A	ELECTRIC	UNDERGROUND	OPEN CUT/TRENCH	13001+75 (C-134)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	13001+75 (C-134)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	13022+75 (C-135)
2	N/A	ELECTRIC	UNDERGROUND	OPEN CUT/TRENCH	13029+75, 13035+50, 13037+25 (C-136)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	13030+25 (C-136)

NOTE: SECTION 12.3 OF THE EM&CP DESCRIBES THE UTILITY CROSSINGS THAT WILL OCCUR WITHIN SEGMENT 2 AS WELL AS THE MITIGATION MEASURES THAT WILL BE FOLLOWED FOR EACH TYPE OF UTILITY CROSSINGS. THERE ARE NO RAILROAD CROSSINGS IN SEGMENT 2.

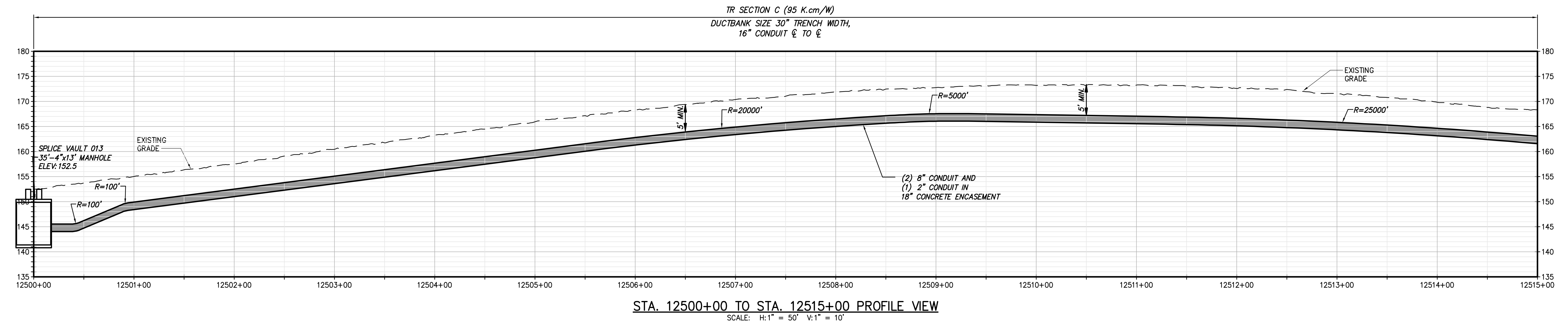
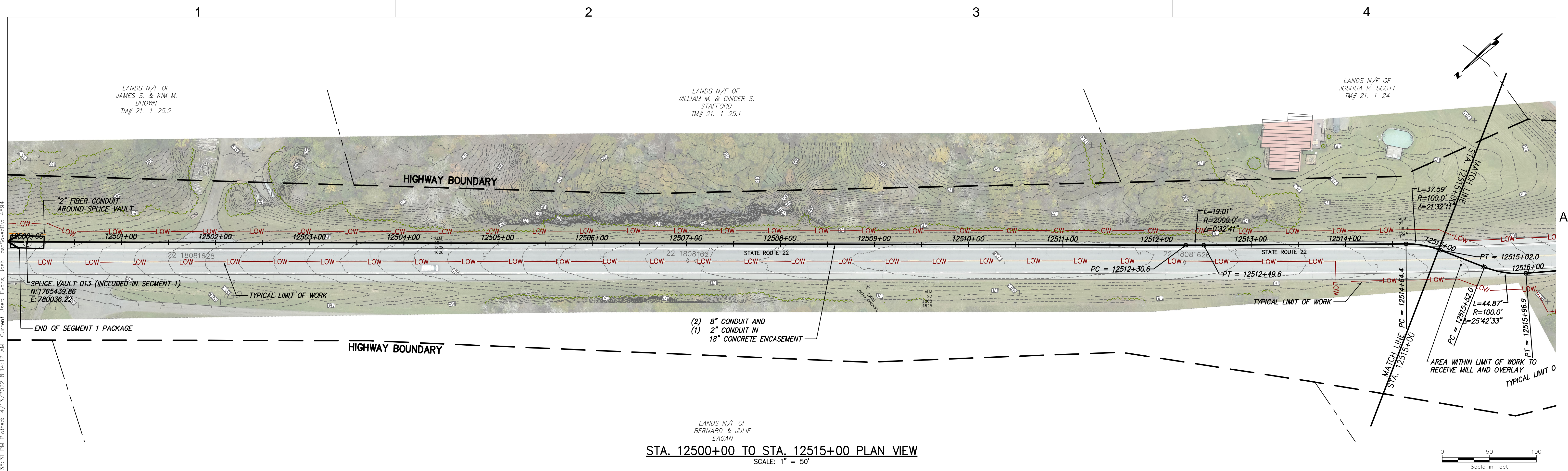
SEGMENT 2 – UTILITY CROSSINGS


A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL
EM&CP DATA TABLES


KIEWIT PROJECT NO.	21162
CHA PROJECT NO.	066076
DRAWING NO.	G-013
DATE	04/15/2022
SH.NO.	XXX OF XXX

DRAWN BY: xxxDESIGNED BY: xxxAPPROVED BY: xxxSCALE AS NOTEDREV.NO. X






Champlain Hudson Power Express



Kiewit



CHA

111 Winners Circle, PO Box 5269
Albany, NY 12205-0269
518.453.4500 • www.chacompanies.com

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL
STA. 12500+00 TO STA 12515+00 PLAN AND PROFILE

DRAWN BY: JTM

DESIGNED BY: JTM

APPROVED BY: JPR

SCALE AS NOTED
REV. NO. X

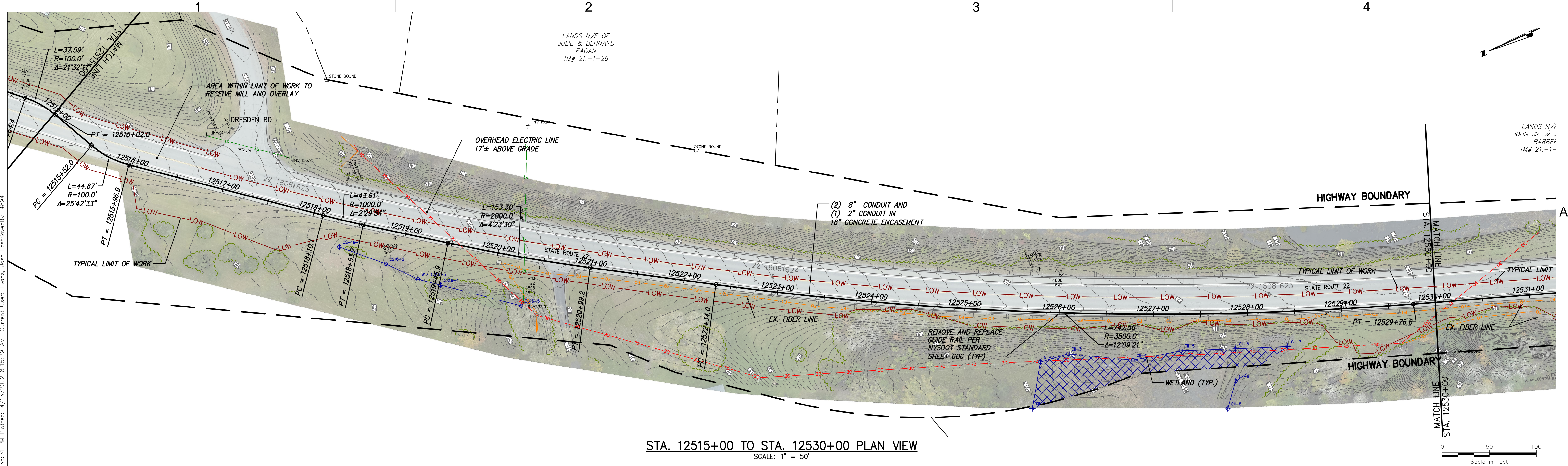
KIEWIT PROJECT NO.
21162

CHA PROJECT NO.
068076

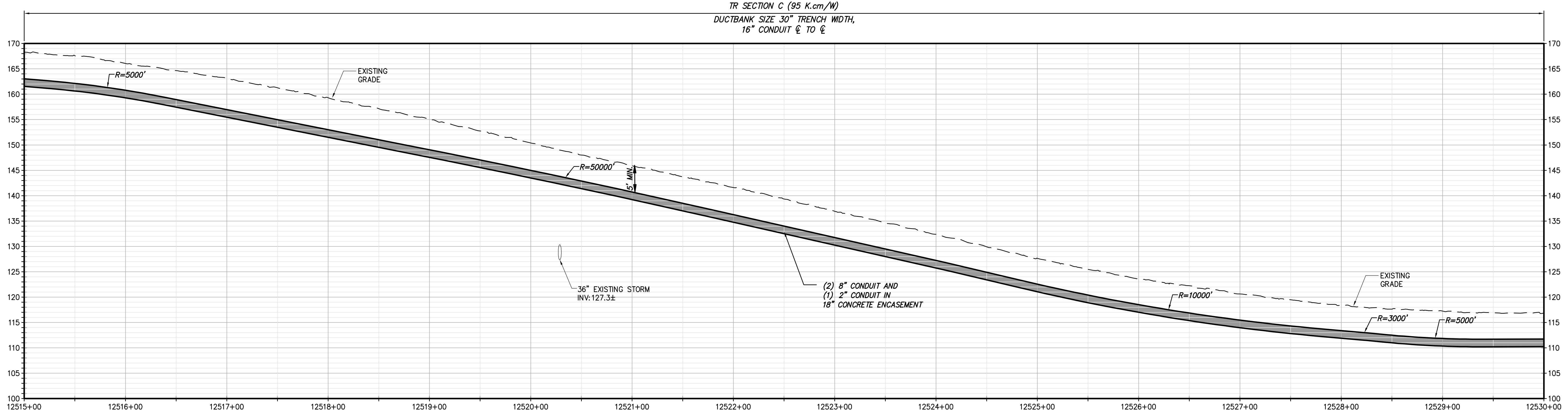
DRAWING NO.
C-101

DATE
04/15/2022




SH.NO.
XXX OF XXX



STA. 12515+00 TO STA. 12530+00 PLAN VIEW
SCALE: 1" = 50'



STA. 12515+00 TO STA. 12530+00 PROFILE VIEW
SCALE: H: 1" = 50' V: 1" = 10'



III Winners Circle, PO Box 5269
Albany, NY 12205-0269
518.453.4500 • www.chacompanies.com

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR

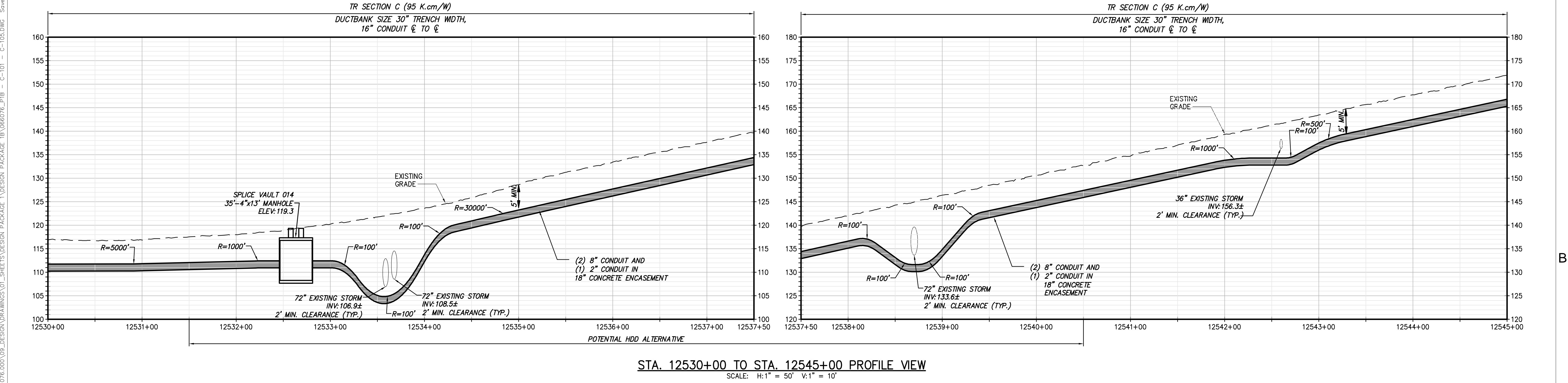
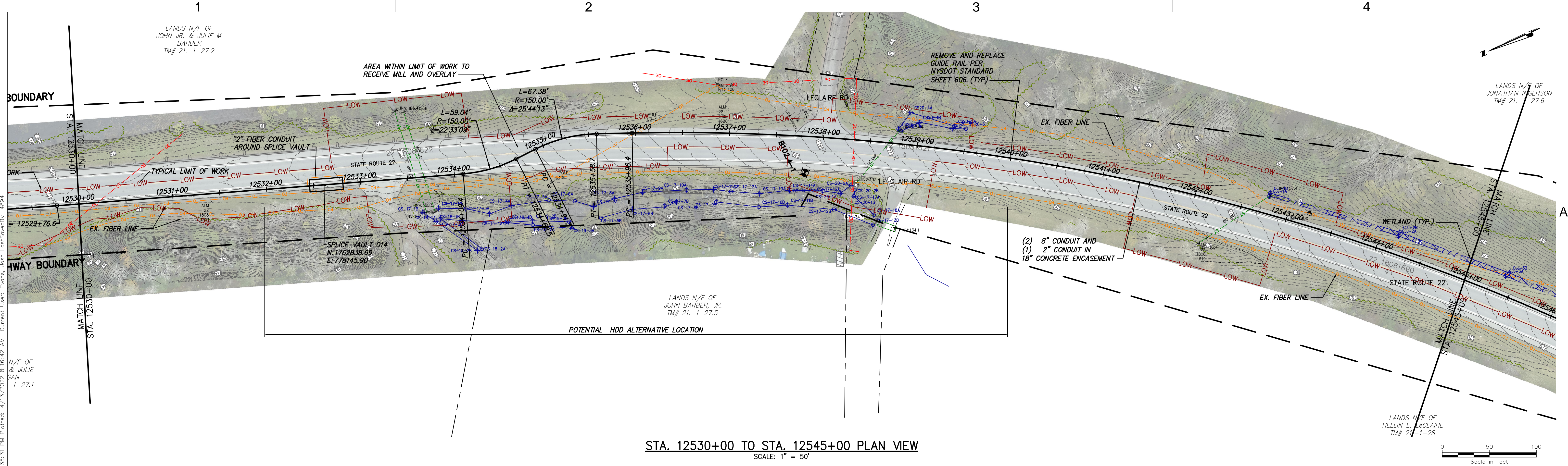
CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL
STA. 12515+00 TO STA. 12530+00 PLAN AND PROFILE




DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR

SCALE AS NOTED
REV. NO. X

KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 066076
DRAWING NO. C-102

DATE 04/15/2022
SH.NO. XXX OF XXX





IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL
STA. 12530+00 TO STA. 12545+00 PLAN AND PROFILE

DRAWN BY: JTM

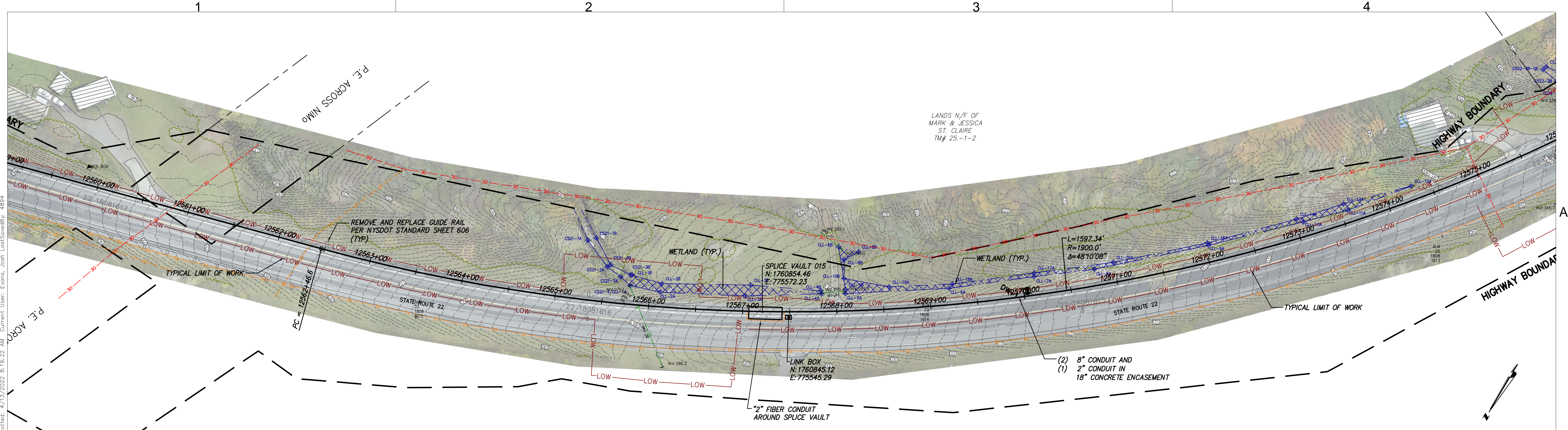
DESIGNED BY: JTM

APPROVED BY: JPR

SCALE AS NOTED
REV. NO. X

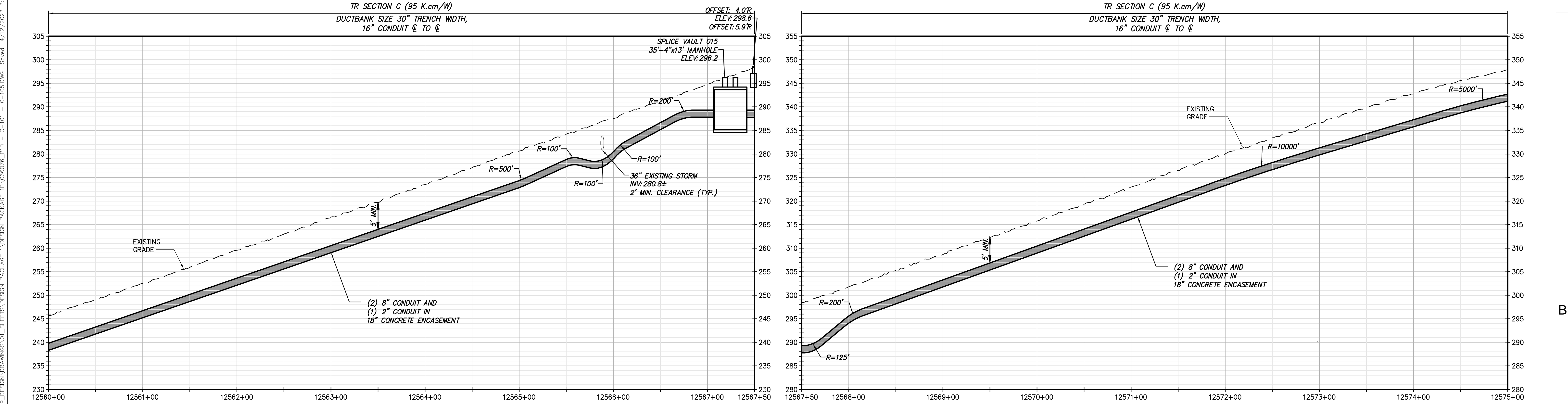
KIEWIT PROJECT NO.
21162
CHA PROJECT NO.
066076
DRAWING NO.
C-103

DATE
04/15/2022
SH.NO.
XXX OF XXX






STA. 12560+00 TO STA. 12575+00 PLAN VIEW

SCALE: 1" = 50'



STA. 12560+00 TO STA. 12575+00 PROFILE VIEW

SCALE: H:1" = 50' V:1" = 10'



III Winners Circle, PO Box 5269
Albany, NY 12205-0269
518.453.4500 • www.chacompanies.com

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR

CHAMPLAIN HUDSON POWER EXPRESS
SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL
STA. 12560+00 TO STA. 12575+00 PLAN AND PROFILE

DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR

SCALE AS NOTED
REV. NO. X

KIEWIT PROJECT NO. 21162
CHA PROJECT NO. 068076
DRAWING NO. C-105

DATE 04/15/2022
SH.NO. XXX OF XXX

