

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL

ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND

SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING

ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND

SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE

NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION

OF THE ALTERATION.

04/15/2022

EM&CP REGULATORY SUBMISSION

SUBMITTAL / REVISION DESCRIPTION

PROFESSIONAL SEAL

Place-holder on layer

"G-ANNO-RDME" a no-plot

Albany, NY 12205-0269 518.453.4500 • www.chacompanies.com

KIEWIT PROJECT NO.

21162

CHA PROJECT NO.

066076

DRAWING NO.

C-512

OF XXX

CHAMPLAIN HUDSON POWER EXPRESS

SEGMENT 1 - PUTNAM TO DRESDEN

WORK ZONE TRAFFIC CONTROL DETAILS

SHEET 10 OF 11

APPROVED BY:

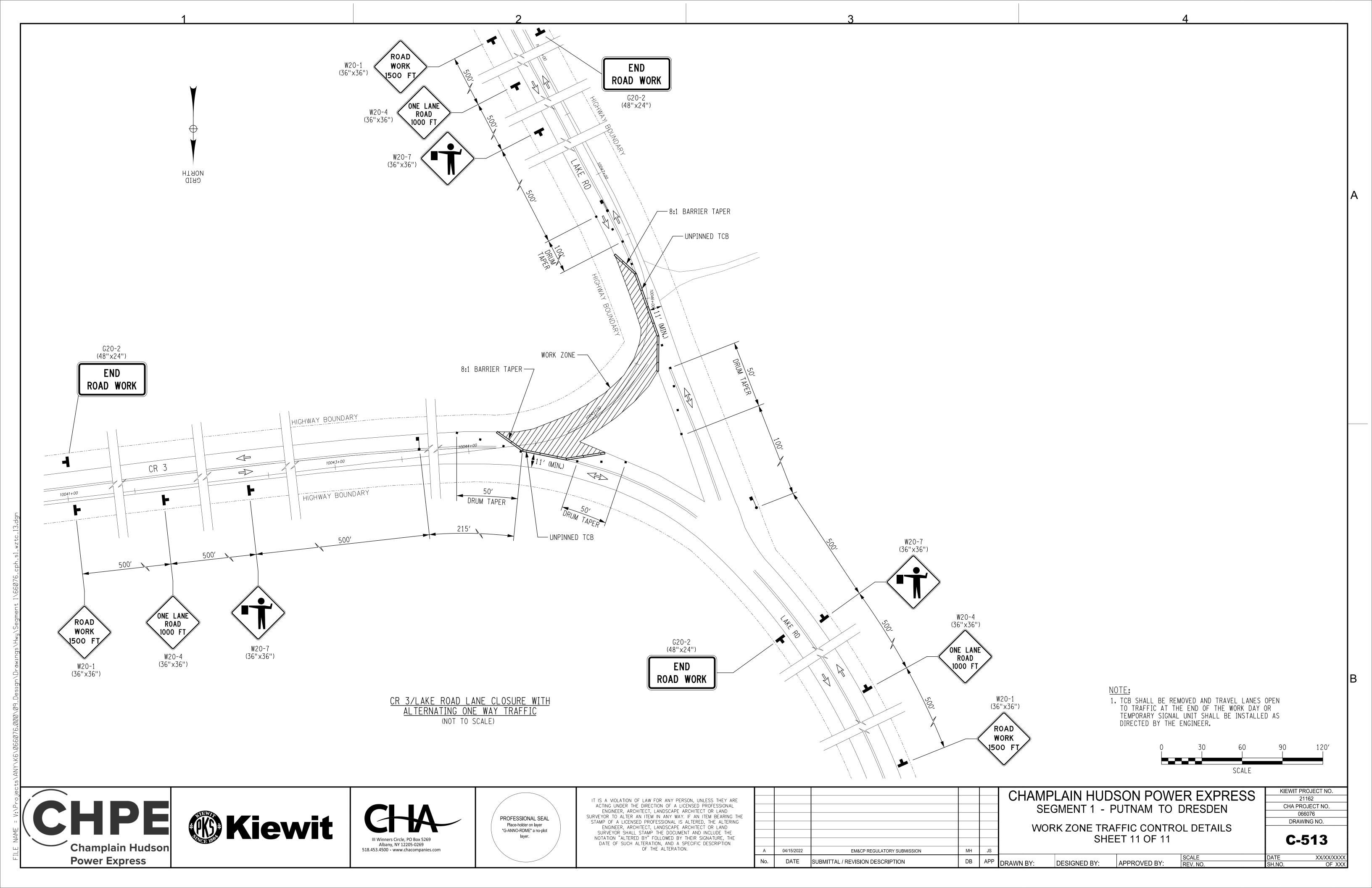
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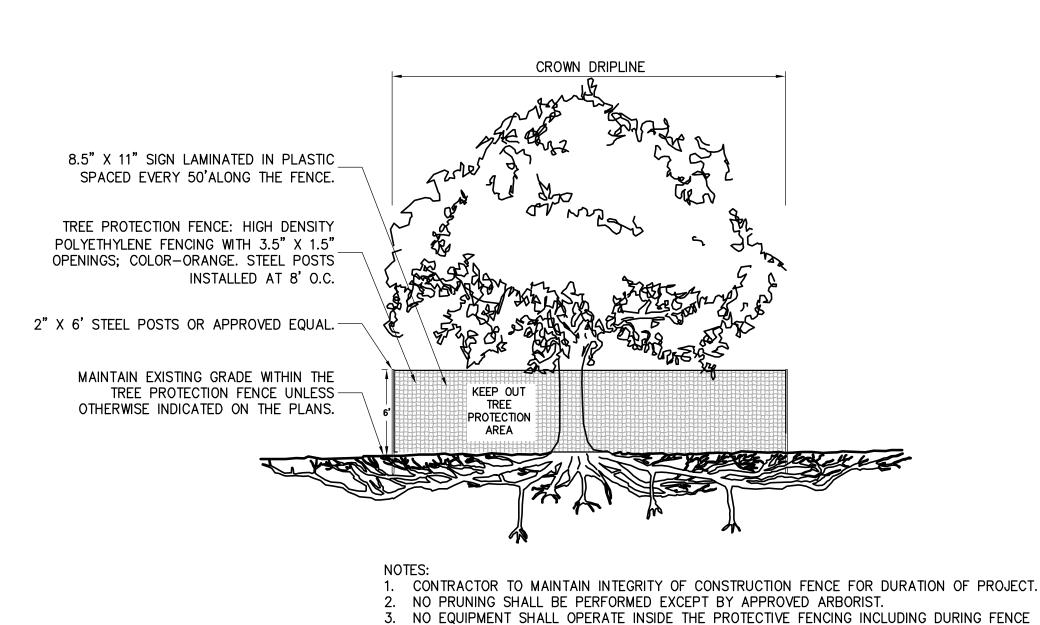
DRAWN BY:

CHPE Kiewit

Champlain Hudson

**Power Express** 





INSTALLATION AND REMOVAL.

TREE PROTECTION

POST 36" LONG 11/2"x11/2" -HARDWOOD SPACING 10' O.C. (SEE NOTE 2) 36" WIRE FENCING 14 GA. 6" SQ. STAPLED TO POST MIRAFI 100X (OR EQUIVALENT) FABRIC (FILTER CLOTH) 30#/IN. WRAP IN TRENCH AS SHOWN COMPACTED FLOW BACKFILL —

#### NOTES:

4. SEE EROSION CONTROL PLANS LOCATIONS OF TREE PROTECTION AREAS.

- TIE FABRIC TO WIRE FENCE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- 2. IF EXTRA STRENGTH FABRIC (GREATER THAN 50#/INCH) IS USED, WIRE CAN BE DELETED IF POST SPACING IS REDUCED TO 6' O.C.
- 3. AT THE ENDS OF THE FENCING THE FIRST 20' SHALL BE TURNED UP THE SLOPE 2'.
- 4. POSTS SHOULD BE INCLINED TOWARD THE DIRECTION FLOW CAME
- 5. OVERLAP FABRIC A MINIMUM OF 6" AND FOLDED AT JOINTS. ATTACH FILTER FABRIS TO STAKES ALLOWING EXTENSION INTO TRENCH AS SHOWN; SECURE TO STAKES AS NOTED.
- THE MAXIMUM AREA OF RUNOFF PER 100LF. OF FENCE SHALL NOT EXCEED 0.25 ACRES.
- MAINTENANCE SHALL BE PERFORMED AS NECESSARY. THE FENCING SHALL BE CHECKED AFTER EVERY STORM TO ENSURE THEIR PROPER FUNCTIONING.
- WHEN FENCE IS NO LONGER NEEDED, THE ACCUMULATED SILT, THE POSTS AND FABRIC SHALL BE REMOVED AND TRENCH BACK FILLED WITH TOPSOIL AND SEEDED.
- 9. FENCING SHOULD BE PLACED AS SHOWN ON THE DRAWING OR IF NOT SHOWN, 10' BEYOND THE TOE OF THE OF THE SLOPE AND AT A SPACING IN ACCORDANCE WITH THE TABLE.
- 10. EXCAVATE TRENCH AS PER DETAIL AND SET POSTS AT 10' O.C.
- 11. BACKFILL WITH COMPACTED, EXCAVATED SOIL FROM TRENCH.

SUBGRADE, TEMP. SEED MIXTURE FILTER SOCK, SIZED TO SUIT -HARDWOOD POST CONDITIONS. 10' O.C. WATER FLOW INLET SIDE

S75 OR APPROVED EQUAL ON

-NORTH AMERICAN GREEN

FILTER COMPOST MATERIAL AS PER SPECIFICATIONS.

#### 1. ALL MATERIAL TO MEET MANUFACTURER SPECIFICATIONS.

- 2. ALL FILTER SOCKS SHALL BE 12" DIAMETER UNLESS INDICATED
- 3. THE CONTRACTOR SHALL MAINTAIN THE COMPOST FILTER BERM CHANNEL PLACEMENT IN A FUNCTIONAL CONDITION AT ALL TIMES AND IT SHALL BE ROUTINELY INSPECTED.
- 4. WHERE THE BERM REQUIRES REPAIR, IT WILL BE ROUTINELY REPAIRED.
- 5. THE CONTRACTOR SHALL REMOVE SEDIMENTS COLLECTED AT THE BASE OF THE BERM WHEN THEY REACH 1/3 OF THE EXPOSED HEIGHT OF THE BERM, OR AS DIRECTED BY THE REPRESENTATIVE.
- 6. THE COMPOST FILTER BERM WILL BE DISPERSED ON SITE WHEN NO LONGER REQUIRED, AS DETERMINED BY THE REPRESENTATIVE.
- 7. INSTALL PERPENDICULAR TO FLOW.

MAINTENANCE NOTES:

TOP OF CHANNEL/BANK

ACROSS THE ENTIRE WIDTH OF

THE CHANNEL

FILTER

SOCK

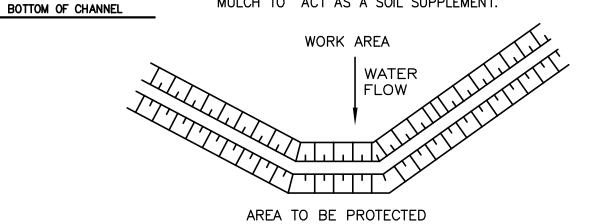
FILTER

SOCK

FILTER

SOCK

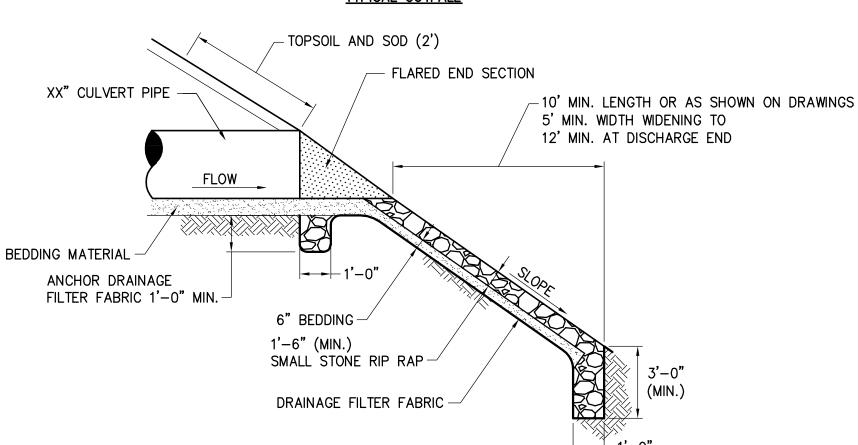
- 1. TRAFFIC SHALL NOT BE PERMITTED TO CROSS FILTER
- 2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN IT REACHES HALF THE ABOVE GROUND HEIGHT OF THE SOCK AND DISPOSED OF IN ACCORDANCE WITH THE PLAN.
- 3. SOCKS SHALL BE INSPECTED WEEKLY AND AFTER EACH RUNOFF EVENT. DAMAGED SOCKS SHALL BE REPAIRED IN THE MANNER REQUIRED BY THE MANUFACTURER OR REPLACED WITHIN 24 HOURS OF FILTER SOCK SHALL BE PLACED PERPENDICULAR TO THE FLOW INSPECTION NOTIFICATION.
  - BIODEGRADABLE FILTER SOCKS SHALL BE REPLACED AFTER 6 MONTHS; PHOTO-DEGRADABLE FILTER SOCKS AFTER 1 YEAR. POLY-PROPYLENE SOCKS SHALL BE REPLACED ACCORDING TO THE
  - MANUFACTURER'S RECOMMENDATIONS. UPON STABILIZATION OF THE AREA CONTRIBUTORY TO THE SOCK, STAKES SHALL BE REMOVED. THE SOCK SHALL BE REMOVED. FOR REMOVAL THE MESH CAN BE CUT AND COMPOST SPREAD AS AN ADDITIONAL MULCH TO ACT AS A SOIL SUPPLEMENT.



AT GRADE PLACEMENT

#### COMPOST FILTER SOCK DETAIL SCALE: N.T.S.

-10' MIN LENGTH OR AS SHOWN ON DRAWINGS TOPSOIL AND SOD (2') MIN WIDTH: OD+X' WIDENING TO OD+Y' AT - FLARED END SECTION DISCHARGE END XX" CULVERT PIPE MIN. SLOPE EQUAL TO CULVERT INVERT SLOPE > FLOW 060606060606 BEDDING MATERIAL - GEOTEXTILE SEPERATION **FABRIC** ANCHOR DRAINAGE └\_1'-6" (MIN.) SMALL FILTER FABRIC 1'-0" MIN. -6" BEDDING □ STONE RIP RAP TYPICAL OUTFALL



TYPICAL OUTFALL ON SLOPE

EXISTING OR PROPOSED FINISHED GRADE -1'-0" MIN. 4"-TOPSOIL, SEED SIDE SLOPES AND BOTTOM

## TYPICAL GRASS DRAINAGE SWALE

- EXISTING GRADE (TYP.) BLEND FINISHED GRADES TO EXISTING (TYP.) EROSION CONTROL BLANKET (TYP.) SEED AND MULCH ALL DISTRURBED AREAS SIDE SLOPES AS SHOWN ON GRADING PLANS (TYP.) ightharpoonupnysdot light stone fill GEOTEXTILE SEPARATION FABRIC UNDISTURBED SUBGRADE OR SUBGRADE COMPACTED TO 95% PROCTOR

## TYPICAL CULVERT OUTFALL RIP RAP

04/15/2022

EM&CP REGULATORY SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION

SCALE: N.T.S. CHAMPLAIN HUDSON POWER EXPRESS

<u> IGHT STONE-LINED DRAINAGE CHANNEL</u>

DB APP DRAWN BY: SKB DESIGNED BY: SKB APPROVED BY: JPR REV. NO.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. SEGMENT 1 - PUTNAM TO DRESDEN 066076 **EROSION AND SEDIMENT CONTROL DETAILS** DRAWING NO.

AS NOTED DATE

X SH.NO.

C-601

04/15/2022

XXX OF XXX

DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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# **Champlain Hudson**

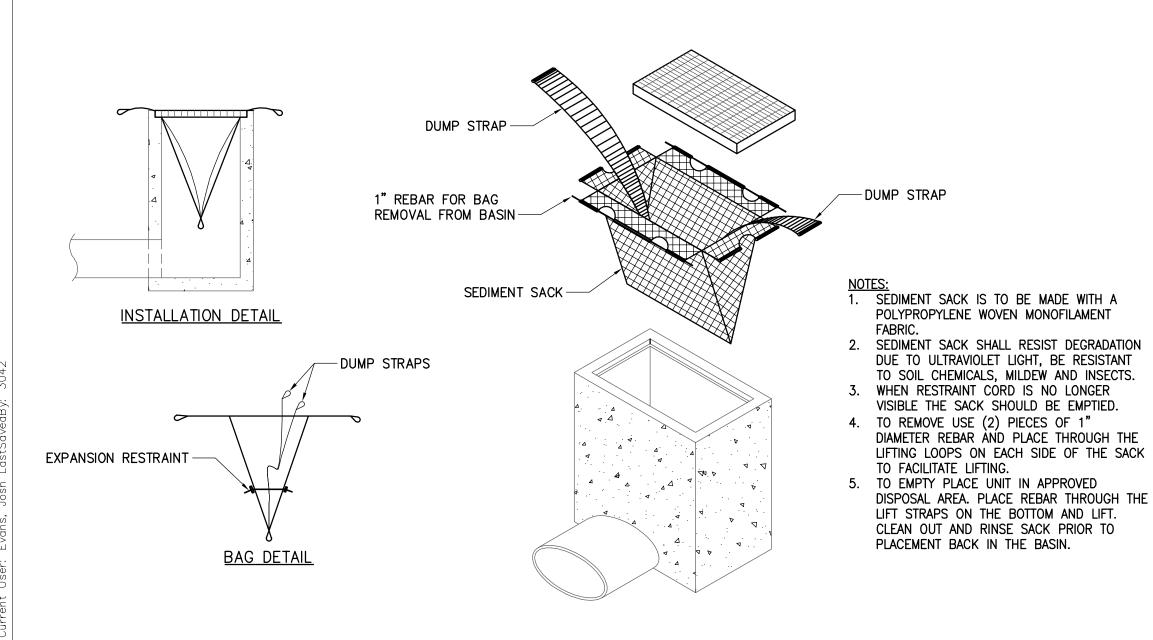
**Power Express** 

T FENCE



**ELEVATION** 





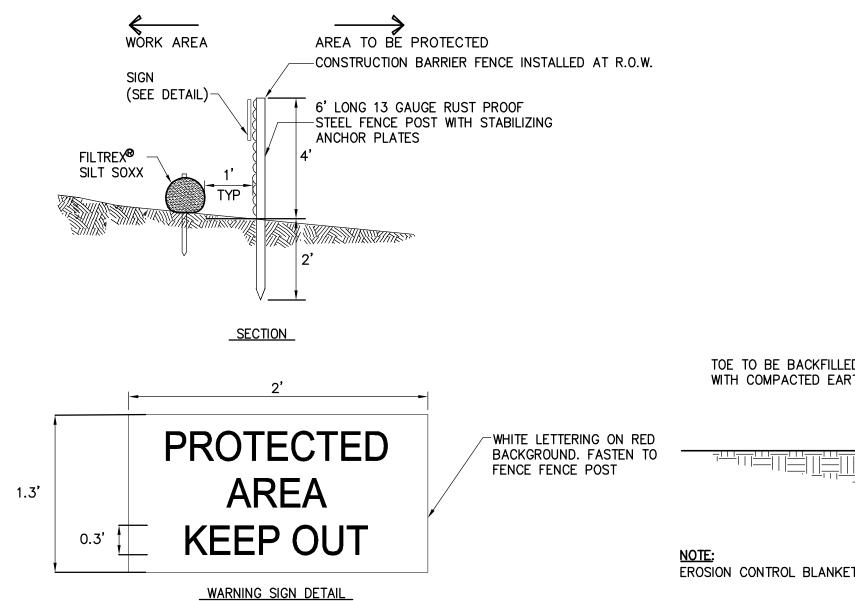
6' LONG 13 GAUGE RUST PROOF -STEEL FENCE POST WITH STABILIZING ANCHOR PLATES FLAT ORIENTED OR DIAMOND MESH -CONSTRUCTION BARRIER FENCE CONSTRUCTION FENCE BY RESINET OR EQUAL WARNING SIGN (SEE ADJACENT DETAIL) MAX. (SIGNS SPACED AT 48' ON CENTER±) -EXISTING GRADE ELEVATION

CONSTRUCTION BARRIER FENCE SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE PLANS PRIOR TO BEGINNING ANY WORK ADJACENT TO THESE AREAS.

2. THE CONTRACTOR SHALL INSTALL AT THE BEGINNING OF THE CONTRACT, AND MAINTAIN THROUGHOUT ITS DURATION.

3. SET BOTTOM OF CONSTRUCTION BARRIER FENCE FLUSH WITH EXISTING GRADE.

4. CONSTRUCTION BARRIER FENCE SHALL HAVE A MINIMUM TENSILE STRENGTH OF 2000 PSI.



STAPLE OR STAKE PER — MANUFACTURER'S RECOMMENDATIONS NORTH AMERICAN GREEN -ECB, MIRAFI MIRAMAT OR APPROVED EQUAL TOE TO BE BACKFILLED WITH COMPACTED EARTH EROSION CONTROL BLANKETS TO BE INSTALLED ON SLOPES 3:1 OR GREATER (TYP.)

SCALE: N.T.S.

EROSION CONTROL BANK STABILIZATION DETAIL

MAINTENACE NOTES:

FILTER STRIP.

LANDFILL.

DISPOSED OF OFF SITE.

1. ALL CONCRETE WASHOUT FACILITIES SHALL BE

INSPECTED DAIL.Y. DAMAGED OR LEAKING

FACILITATES SHALL BE DEACTIVATED AND

A STABILIZED AREA SUCH AS A GRASS

A CONTAINMENT VESSEL AND PROPERLY

OFF-SITE IN A CONSTRUCITON/DEMOLION

4. THE PLASTIC LINER SHALL BE REPLACED WITH

5. INSPECT THE PROJECT SITE FREQUENTLY TO

EACH CLEANING OF THE WASHOUT FACILITY.

ENSURE THAT NO CONCRETE DISCHARGES ARE

TAKING PLACE IN NON-DESIGNATED AREAS.

6. LOCATION(S) TO BE DETERMINED IN THE FIELD

BY THE OWNER'S REPRESENTATIVE

DISPOSE OF THE HARDENED MATERIAL

REPAIRED OR REPLACED IMMEDIATELY. EXCESS

RAINWATER THAT HAS ACCUMULATED OVER

HARDENED CONCRETE SHALL BE PUMPED TO

ACCUMULATED HARDENED MATERIAL SHALL BE REMOVED WHEN 75% OF THE STORAGE

CAPACITY OF THE STRUCTURE IS FILLED. ANY

EXCESS WASH WATER SHALL BE PUMPED INTO

**INLET PROTECTION** 

SCALE: N.T.S.

PUMP — - TIE DOWN DISCHARGE STRAP HOSE SEWN IN -SPOUT – WATER PUMP SEDIMENT\_ DEWATERING 2' WIDE x 1' HIGH FILTERED —V -2' WIDE x 1' HIGH #2 STONE BERM (TYP)— #2 STONE BERM AGGREGATE OR STRAW-SIDE VIEW UNDERLAY

(FOR ADDED FLOW) NOTE: THE SEDIMENT DEWATERING BAG WILL BE MANUFACTURED IN THE U.S.A. FROM A NONWOVEN POLYPROPYLENE FABRIC THAT MEETS OR EXCEEDS THE FOLLOWING SPECIFICATIONS:

#### SEDIMENT DEWATERING BAG SPECIFICATIONS

Mechanical Properties	Test Method	Units	MARV
· ·			
Grab Tensile Strength	ASTM D 4632	kN (lbs)	0.9 (205) × 0.9 (205)
Grab Tensile Elongation	ASTM D 4632	%	50 x 50
Puncture Strength	ASTM D 4833	kN (lbs)	0.58 (130)
Mullen Burst Strength	ASTM D 3786	kPa (psi)	2618 (380)
Trapezoid Tear Strength	ASTM D 4533	kN (lbs)	0.36 (80) X 0.36 (80)
UV Resistence	ASTM D 4355	%	70
Apparent Opening Size	ASTM D 4751	Mm (US Std Sieve)	0.180 (80)
Flow Rate	ASTM D 4491	1/min/m² (gal/min/ft²)	3866 (95)
Permittivity	ASTM D 4491	Sec <sup>-1</sup>	1.2

SEDIMENT DEWATERING BAG

2 WETLAND PROTECTION FENCE SCALE: N.T.S.

PER PLAN OR 50' MIN.

<u>PROFILE</u>

PLAN VIEW

STABILIZED CONSTRUCTION ACCESS

20'

**EXISTING** 

PAVEMEN<sup>\*</sup>

WOVEN GEOTEXTILE

- EXISTING GROUND

12' MIN.

PAVEMENT

1. STONE SIZE-USE AASHTO M43 SIZE 3 COARSE AGGREGATE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT. 2. LENGTH - NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE

3. THICKNESS - NOT LESS THAN 12".

4. WIDTH - TWELVE (12) FOOT MINIMUM, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. TWENTY-FOUR (24) FOOT IF SINGLE ACCESS TO SITE.

RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD

5. WOVEN GEOTEXTILE FABRIC WILL BE PLACED OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE.

6. EXISTING ROAD SIDE DRAINAGE SHALL BE MAINTAINED.

7. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED BENEATH THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.

8. MAINTENANCE-THE ACCESS SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT OR STONE SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY.

9. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON A AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.

10. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

18**"** ALL CONCRETE TRUCKS BLACK LETTERS ANCHOR BALES WITH (2) 2"x2"x4' STAKES PER BALE

BACKGROUND CHANNEL POST POLYETHYLENE SIGN SHALL BE PLACED IN A PROMINENT LOCATION —AGGREGATE — AT WASHOUT AREA

18**"**± - 6" MIN IMBEDMENT - BINDING WIRE (TYPICAL) 24" MIN EXISTING GRADE — STRAW BALE (TYPICAL) POLYETHYLENE SHEETING └---<del>-</del> ---WOOD STAKE (TYPICAL) GROUNDWATER TABLE 6" MIN DEPTH AGGREGATE

CONCRETE WASHOUT AREA

ON WHITE <u>PLAN</u> WASHOUT SIGN

ALL AROUND TYPICAL SECTION

EM&CP REGULATORY SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION

**Champlain Hudson Power Express** 





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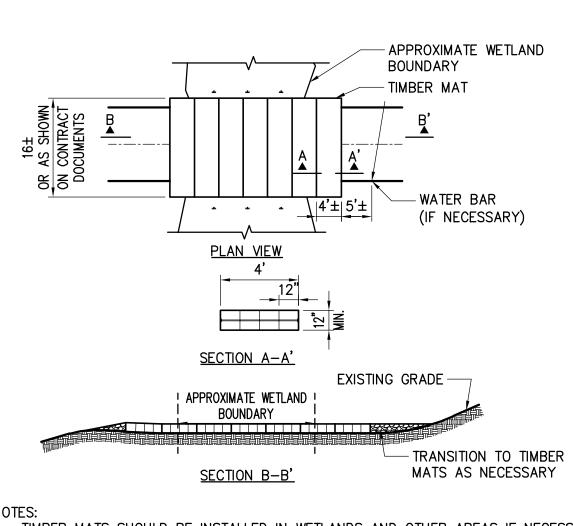
### CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN **EROSION AND SEDIMENT CONTROL DETAILS**

DB APP DRAWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

C-602

AS NOTED DATE 04/15/2022 X SH.NO. XXX OF XXX

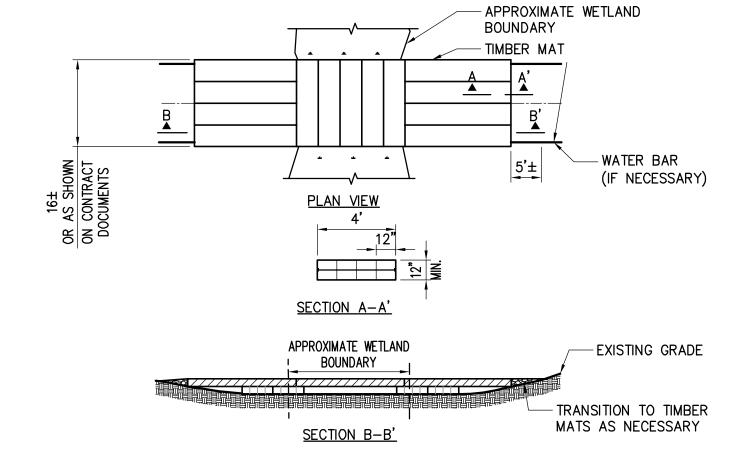


1. TIMBER MATS SHOULD BE INSTALLED IN WETLANDS AND OTHER AREAS IF NECESSARY TO

- PREVENT RUTTING. 2. BASED ON ACTUAL SITE CONDITIONS, MULTIPLE LAYERS OF TIMBER MATS MAY BE REQUIRED. 3. TIMBER MAT SURFACE SHOULD BE LEVEL TO PREVENT EQUIPMENT AND VEHICLES FROM
- SLIDING OFF DURING MUDDY OR ICING CONDITIONS, AND PREVENT TIMBERS FROM BREAKING. 4. SEDIMENT TRACKED ONTO TIMBER MATTING SHOULD BE REMOVED AS NECESSARY TO PREVENT SEDIMENT FROM ENTERING WETLAND DURING RAIN EVENTS. SEDIMENT SHOULD BE
- REMOVED TO A STABILIZED SOIL STOCKPILE OR OTHER APPROVED LOCATION. 5. PERIMETER EROSION AND SEDIMENT CONTROL ARE REQUIRED TO BE INSTALLED PRIOR TO
- PLACING TIMBER MATTING. 6. UNLESS PERMITTED FROM REMOVAL, STUMPS WITHIN THE WETLAND SHOULD REMAIN. THIS
- MAY REQUIRE ADDITIONAL TIMBERS TO BRIDGE ABOVE.
- 7. UPON REMOVAL OF TIMBER MATTING ALL SPLINTERED WOOD SHOULD BE REMOVED. IF EXPOSED SOILS ARE PRESENT STRAW MULCH SHOULD BE APPLIED.

NOTE: GEOTEXTILE FABRIC TO BE INSTALLED UNDER MATTING (TYP)

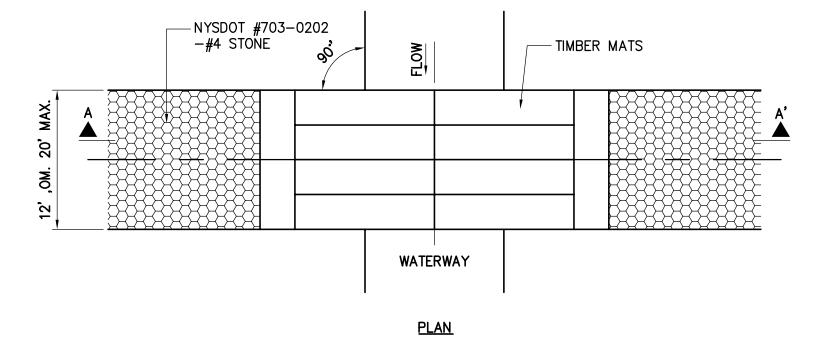


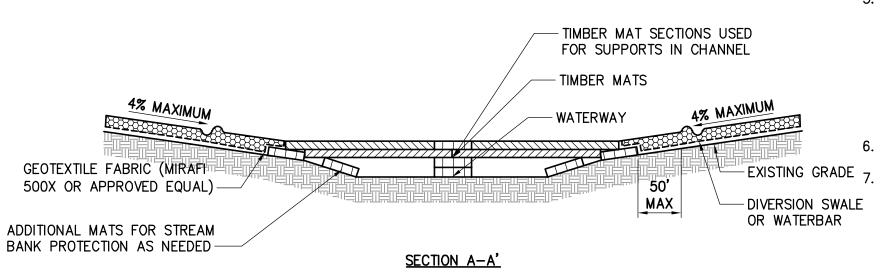


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NOTE: GEOTEXTILE FABRIC TO BE INSTALLED UNDER MATTING (TYP)

NOT TO SCALE





- 1. IN-STREAM EXCAVATION SHOULD BE COMPLETED IN ACCORDANCE WITH "TEMPORARY ACCESS WATERWAY CROSSING" ON PAGE 2.32 OF THE 2016 NYSDEC STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (OR NEWEST VERSION).
- 2. THE CONSTRUCTION OF ANY CROSSING SHOULD NOT CAUSE A SIGNIFICANT WATER LEVEL DIFFERENCE BETWEEN THE UPSTREAM AND DOWNSTREAM WATER SURFACE ELEVATIONS. FISH SPAWNING OR MIGRATION DATES CAN VARY ACROSS NEW YORK, AND RESTRICTIONS IMPOSED BY THE NYSDEC MY VARY AND MUST BE
- 3. THE TEMPORARY WATERWAY CROSSING SHOULD BE AT RIGHT ANGLES TO THE STREAM WHERE APPROACH CONDITIONS DICTATE, THE CROSSINGS MAY VARY 15 DEGREES FROM A LINE DRAWN PERPENDICULAR TO THE CENTERLINE OF THE STREAM AT THE INTENDED CROSSING LOCATION.
- 4. ALL FILL MATERIALS ASSOCIATED WITH THE ROADWAY APPROACH SHOULD BE LIMITED TO A MAXIMUM HEIGHT OF 2 FEET ABOVE THE EXISTING FLOOD PLAIN ELEVATION.
- 5. A WATER DIVERTING STRUCTURE SUCH AS A SWALE OR WARE BAR SHOULD BE CONSTRUCTED (ACROSS THE ROADWAY ON BOTH ROADWAY APPROACHES) 50 FEET (MAXIMUM) ON EITHER SIDE OF THE WATERWAY CROSSING. THIS WILL PREVENT ROADWAY SURFACE RUNOFF FROM DIRECTLY ENTERING THE WATERWAY. THE 50 FEET MEASURED IS MEASURED FROM THE TOP OF THE WATERWAY BANK. IF THE ROADWAY APPROACH IS CONSTRUCTED WITH A REVERSE GRADE AWAY FROM THE WATERWAY, A SEPARATE DIVERTING STRUCTURE IS NOT REQUIRED.
- ALL CROSSINGS SHOULD HAVE ONE TRAFFIC LANE. THE MINIMUM WIDTH SHOULD BE 12 FEET WITH A MAXIMUM WIDTH OF 20 FEET. ANCHORS: TIMBER MATS SHOULD BE SECURELY ANCHORED AT ONLY ONE END, USING STEEL CABLE OR CHAIN. ANCHORING AT ONLY ONE END WILL PREVENT CHANNEL OBSTRUCTION IN THE EVENT THAT FLOODWATERS FLOAT THE BRIDGE. ACCEPTABLE ANCHORS ARE LARGE TREES, LARGE BOULDERS, OR DRIVEN STEEL ANCHORS. ANCHORING SHOULD BE SUFFICIENT TO PREVENT THE TIMBER MATS FORM FLOATING DOWNSTREAM AND POSSIBLY CAUSING AN OBSTRUCTION TO THE FLOW.

WETLAND CROSSING SCALE: N.T.S.







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No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP
Α	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR

### CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN WETLAND CROSSING DETAILS

DRAWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

C-611

AS NOTED DATE 04/15/2022 X SH.NO. XXX OF XXX 1. SLOPE SHALL BE BASED ON OSHA SHEETING AND SHORING REQUIREMENTS TO BE DETERMINED BY THE COMPETENT PERSON IN THE FIELD.

1 OPEN TRENCH (30" WIDTH, 16" CENTER TO CENTER SPACING)
NOT TO SCALE

2 TYPICAL DUCTBANK IN ROADWAY (30" WIDTH, 16" CENTER TO CENTER SPACING)
NOT TO SCALE

1'-4" 6"

2'-4"

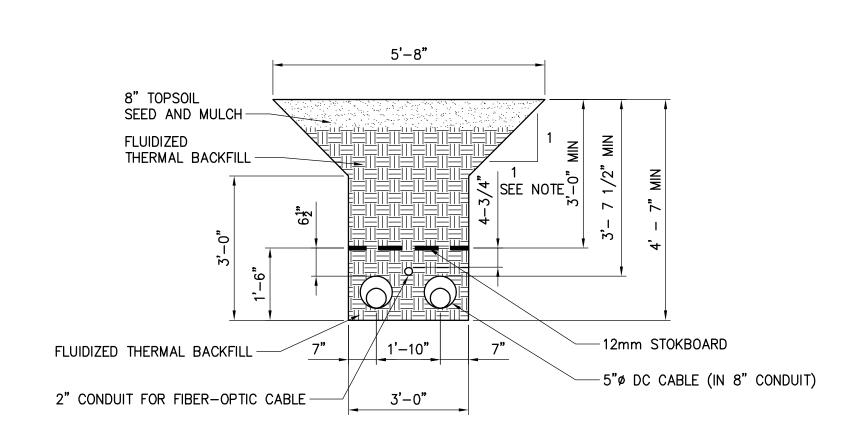
(MIN)

TEMPORARY SHORING

-2,500 PSI CONCRETE

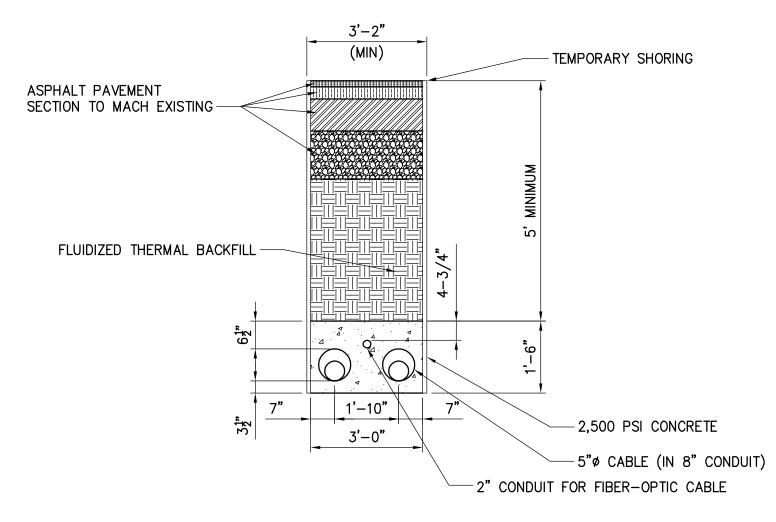
- 2" CONDUIT FOR FIBER-OPTIC CABLE

-5"ø CABLE (IN 8" CONDUIT)



1. SLOPE SHALL BE BASED ON OSHA SHEETING AND SHORING REQUIREMENTS TO BE DETERMINED BY THE COMPETENT PERSON IN THE FIELD.

3 OPEN TRENCH (36" WIDTH. 22" CENTER TO CENTER SPACING)
NOT TO SCALE



TYPICAL DUCTBANK IN ROADWAY (36" WIDTH. 22" CENTER TO CENTER SPACING)
NOT TO SCALE







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A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	CHAMPLAIN HUDSON POWER EX SEGMENT 1 - PUTNAM TO DRESI TRENCHING DETAILS
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO.

CHAMPLAIN HUDSON POWER EXPRE	S
SEGMENT 1 - PUTNAM TO DRESDEN	
TRENCHING DETAILS	

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

XXX OF XXX

AS NOTED DATE X SH.NO.

-1 1/2" ASPHALT CONCRETE TOP COURSE (NYS DOT TYPE 6, ITEM NO. 402.098204) -3" ASPHALT CONCRETE BINDER COURSE (NYS DOT TYPE 3, ITEM NO. 402.198904)

-12" SUBBASE MATERIAL, MATCH EXISTING THICKNESS NYSDOT TYPE 2 (ITEM NO. 304.12) - GEOTEXTILE STABILIZATION FABRIC (NYSDOT ITEM NO. 207.24)

1. ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION IF EXISTING THICKNESS IS GREATER

2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

ASPHALT CONCRETE PAVEMENT DETAIL (PRIVATE DRIVEWAY)

-1 1/2" ASPHALT CONCRETE TOP COURSE (NYS DOT TYPE 6, ITEM NO. 402.128204) 3" ASPHALT CONCRETE BINDER COURSE (NYS DOT TYPE 3, ITEM NO. 402.258904) -8" ASPHALT CONCRETE BASE COURSE (NYS DOT ITEM NO. 402.378904) -12" SUBBASE MATERIAL, MATCH EXISTING THICKNESS NYSDOT TYPE 2 (ITEM NO. 304.12) - GEOTEXTILE STABILIZATION FABRIC (NYS DOT ITEM NO. 207.24) - COMPACTED SUBGRADE ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION

IF EXISTING THICKNESS IS GREATER 2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

ASPHALT CONCRETE PAVEMENT

(WITHIN NYSDOT ROADWAYS)

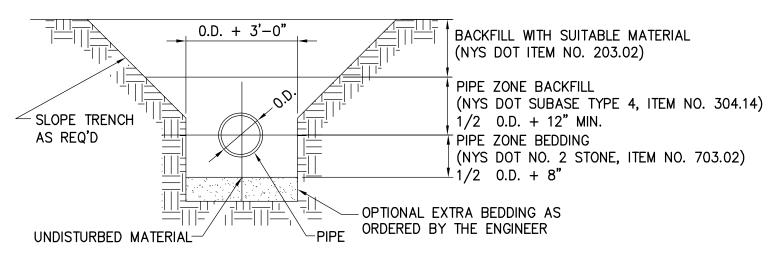
-SLOPE AS INDICATED ON PLAN -LAYER THICKNESS PER TABLE COMPACTED SUBGRADE - STABILIZATION FABRIC OR GEOGRID

TEMPORARY ACCESS ROAD SECTION					
CBR	180N GEOTEXTILE	BX1200 GEOGRID <sup>2</sup>	RD580I GEOTEXTILE		
0.5	24 INCH RIP RAP <sup>3</sup> + 6 INCH AGGREGATE <sup>4</sup>		24 INCH RIP RAP + 4 INCH AGGREGATE		
1.0	20 INCH AGGREGATE	12 INCH AGGREGATE	9 INCH AGGREGATE		
1.5	15 INCH AGGREGATE	9 INCH AGGREGATE			
2.0	12 INCH AGGREGATE	6 INCH AGGREGATE			
3.0+	9 INCH AGGREGATE	6 INCH AGGREGATE			

ACCESS ROAD SECTIONS PER KIEWIT GEOTECHNICAL

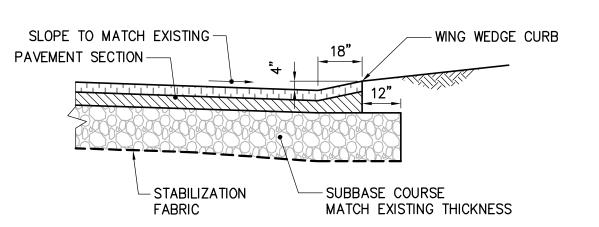
TEMPORARY ACCESS ROAD

NOTES:			
1 ESTIMATE CBR IN THE FIELD U	SING A DYNAMIC CONE PENETROI	METER.	
<sup>2</sup> SEPARATOR GEOTEXTILE FABRIC S	SUCH AS MIRAFI 140N SHOULD BE I	NCLUDED BENEATH GEOGRID LAYER	ON COHESIVE SUBGRADE
3 RIP RAP TO BE NYSDOT LIGHT S'	TONE FALL		
4 A LAYER OF #57 STONE MAY BE	REQUIRED ON TOP OF GEOTEXTILE	TO PREVENT DAMAGING OR PUNCHI	NG OF THE FABRIC.
5 GEOGRID AND GEOTEXTILES SHOU	LD BE INSTALLED PER MANUFACTUR	ES SPECIFICATION INCLUDING OVERL	AP AND EDGE DETAILS



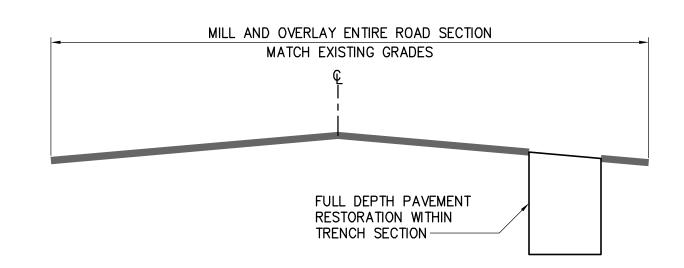
1. WHERE IDENTIFIED ON PLANS, CULVERT REPLACEMENTS AND/OR REPAIR TO BE COMPLETED IN ACCORDANCE WITH NYSDOT STANDARD SHEETS (NYSDOT STANDARD SHEET GROUP 603 CULVERTS AND STORM DRAINS AND NYSDOT BRIDGE DETAIL SHEETS BD-CB1 THRU BD-CB13)

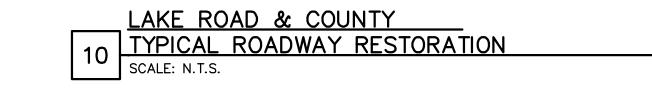
CULVERT REPLACEMENT SCALE: N.T.S.

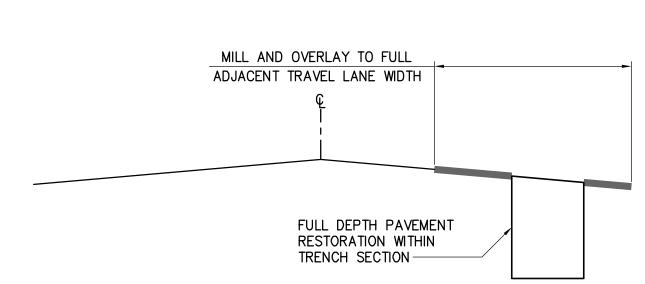


1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

WING WEDGE CURB DETAIL







MILL AND OVERLAYASPHALT CONCRETE PAVEMENT DETAIL

COLD MILL EXISTING PAVEMENT

COAT AND INSTALL NEW TOP COURSE

1. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

PAVEMENT TRANSITION DETAIL

1. APPLY TACKCOAT TO MILLED SURFACE PRIOR TO PLACING ASPHALT

2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

CONCRETE TOP COURSE.

- EXISTING PAVEMENT

- EXISTING PAVEMENT

TO REMAIN

SUBBASE

-MILL AND OVERLAY 1 1/2" ASPHALT

ITEM NO. 402.128203)

- EXISTING SUBBASE

-EXISTING ASPHALT COURSE(S)

CONCRETE TOP COURSE (NYS DOT TYPE 6,

COLD MILL EXISTING

PAVEMENT 18" MIN. BEYOND

THE TRENCH WALL

TOP COURSE, TACK

TOP COURSE PER

PAVEMENT DETAIL -

-1 1/2" ASPHALT CONCRETE TOP COURSE

(NYS DOT TYPE 6, ITEM NO. 402.097203)

-2-1/2" ASPHALT CONCRETE BINDER COURSE

(NYS DOT TYPE 3, ITEM NO. 402.197903)

-3" ASPHALT CONCRETE BASE COURSE

(NYS DOT ITEM NO. 402.377203)

NYSDOT TYPE 2 (ITEM NO. 304.12)

GEOTEXTILE STABILIZATION FABRIC

-12" SUBBASE MATERIAL,

COMPACTED SUBGRADE

SLOPE AS INDICATED ON PLAN

-8" SUBBASE

— COMPACTED SUBGRADE

STABILAZATION FABRIC

1. ABOVE SECTION IS THE MINIMUM FOR INSTALLATION. MATCH EXISTING SECTION

2. ALL MATERIALS TO MEET NYSDOT STANDARD SPECIFICATIONS

ASPHALT CONCRETE PAVEMENT 3 (WITHIN COUNTY OR TOWN ROADWAYS)

IF EXISTING THICKNESS IS GREATER

MATCH EXISTING THICKNESS

(NYS DOT ITEM NO. 207.24)

NOTE: TWO LANE SECTION TO RECEIVE MILL AND OVERLAY TO CENTER LINE

NYSDOT ROUTE 22 TYPICAL ROADWAY RESTORATION SCALE: N.T.S.







IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

o.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRA
`	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
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CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN SURFACE RESTORATION DETAILS

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

C-631

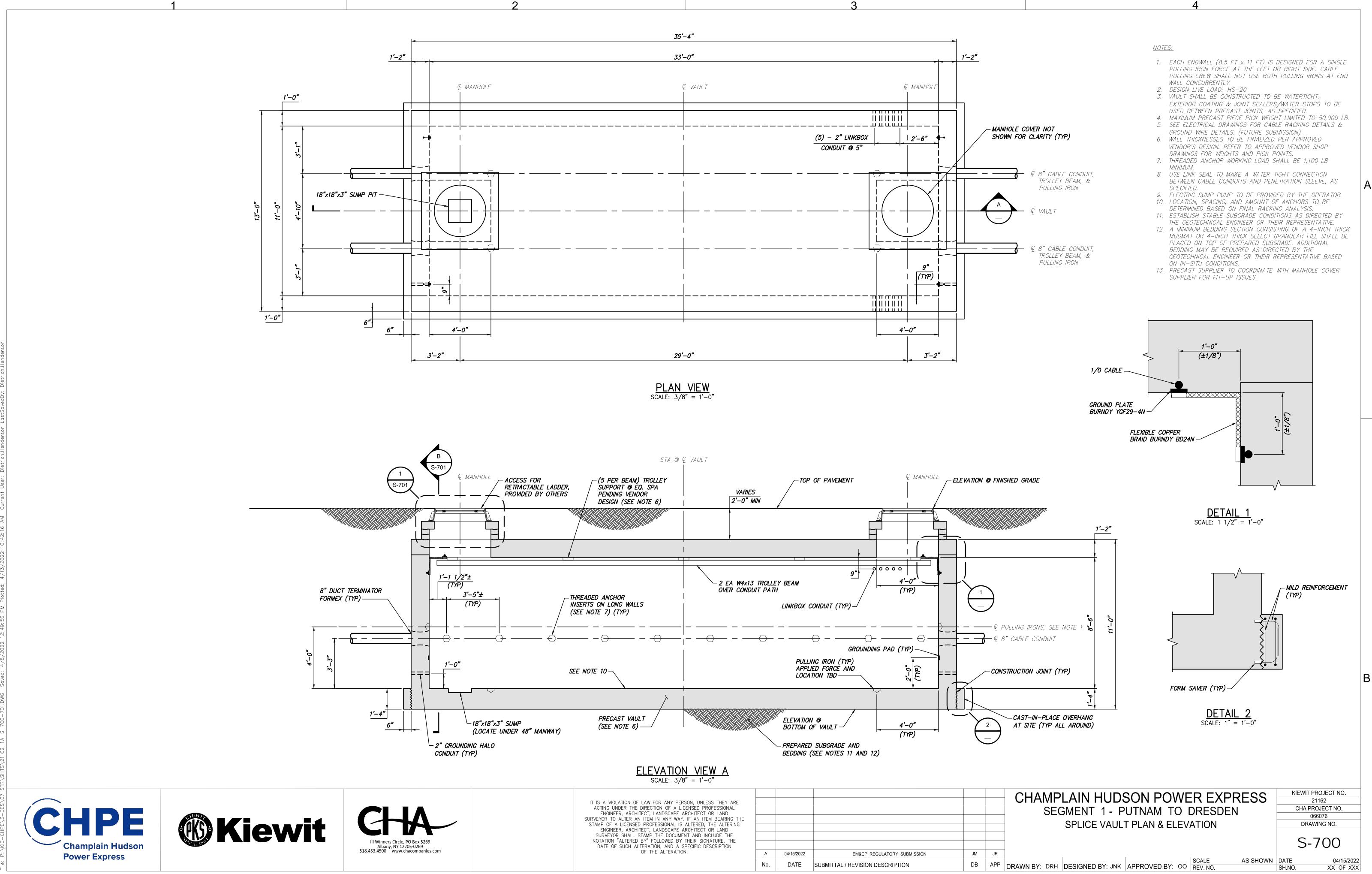
AS NOTED DATE

AWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO.

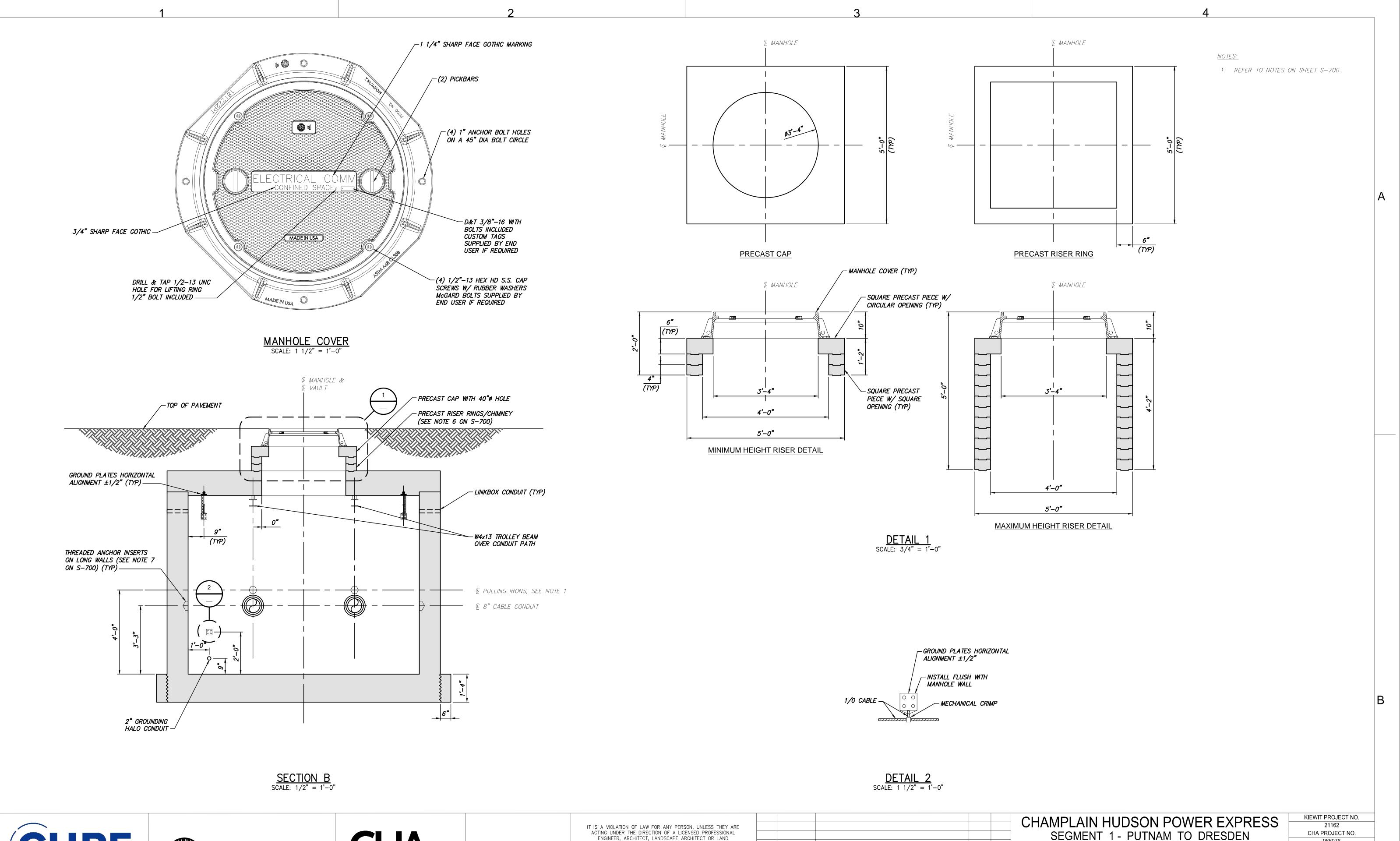
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XXX OF XXX

04/15/2022



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ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND
SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE
STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING

ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

EM&CP REGULATORY SUBMISSION

SUBMITTAL / REVISION DESCRIPTION

A 04/15/2022

DATE

066076

DRAWING NO.

S-701

XX OF XXX

AS SHOWN DATE

SPLICE VAULT SECTION & DETAILS

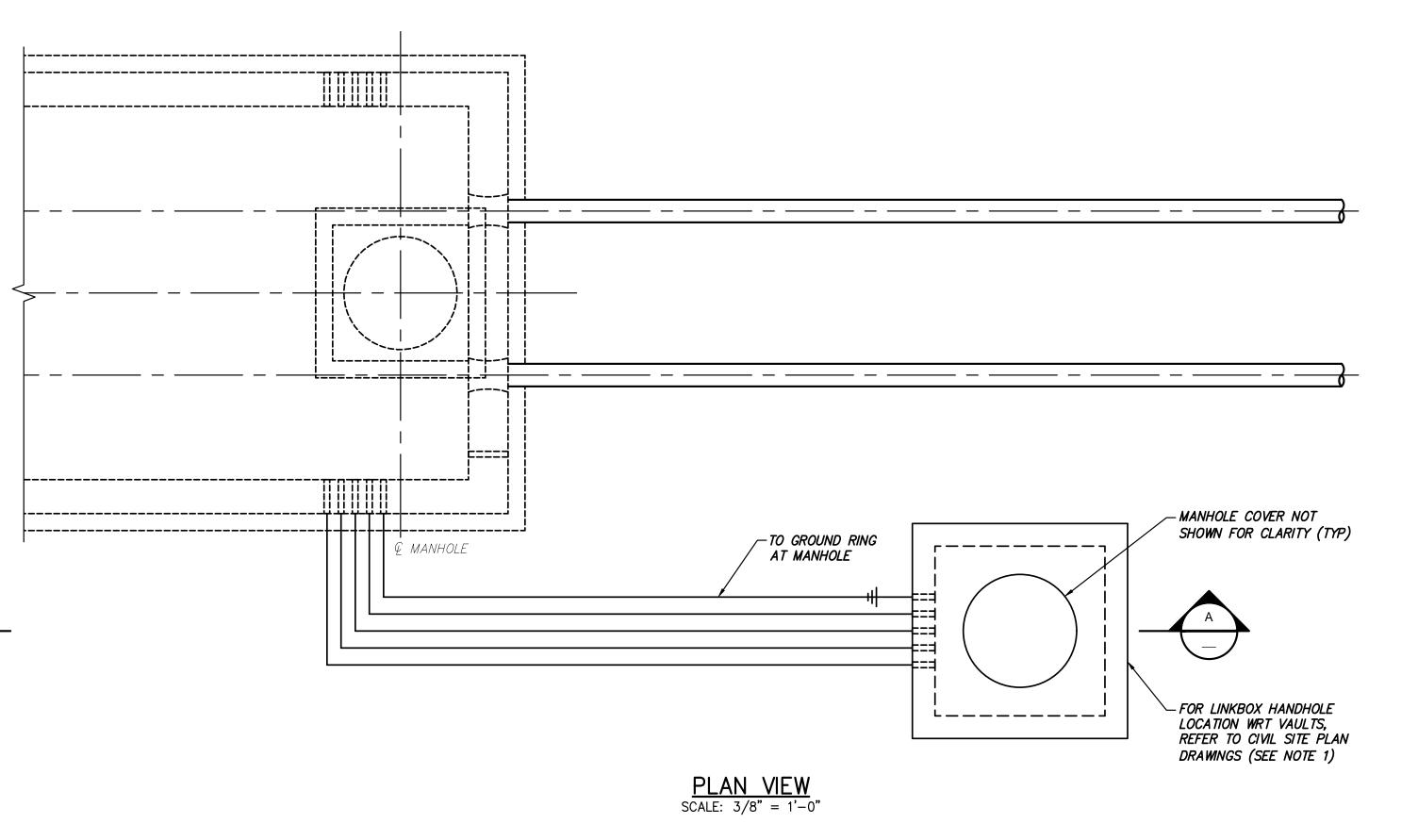
DB APP DRAWN BY: DRH DESIGNED BY: JNK APPROVED BY: OO REV. NO.

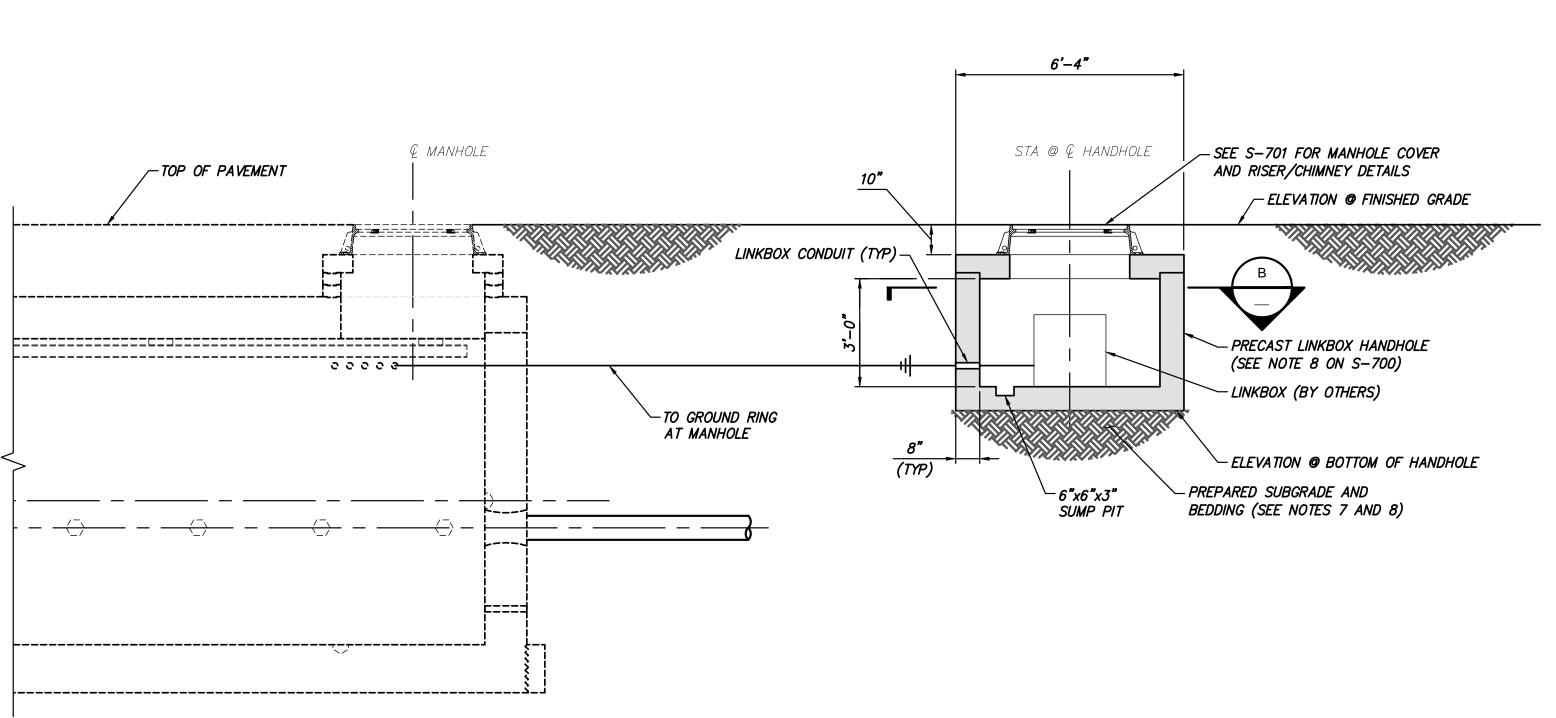
Champlain Hudson

**Power Express** 

Kiewit

Albany, NY 12205-0269 518.453.4500 . www.chacompanies.com







— LINKBOX (BY OTHERS)

- 1. CLEAR SPACE BETWEEN SPLICE VAULT AND LINKBOX HANDHOLE TO BE 5 FT MAXIMUM.
- 2. FOR LINK BOX DETAILS SEE ELECTRICAL DRAWINGS. 3. DESIGN LIVE LOAD: HS-20.
- 4. WALL THICKNESSES TO BE FINALIZED PER APPROVED VENDOR'S DESIGN. REFER TO APPROVED VENDOR
- SHOP DRAWINGS FOR WEIGHTS AND PICK POINTS. 5. ELECTRIC SUMP PUMP TO BE PROVIDED BY OTHERS. SEE S-700 FOR VAULT DETAILS.
- 7. ESTABLISH STABLE SUBGRADE CONDITIONS AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE. 8. A MINIMUM BEDDING SECTION CONSISTING OF A
- 4-INCH THICK MUDMAT OR 4-INCH THICK SELECT GRANULAR FILL SHALL BE PLACED ON TOP OF PREPARED SUBGRADE. ADDITIONAL BEDDING MAY BE REQUIRED AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE BASED ON IN-SITU CONDITIONS.

LINK BOX HANDHOLE

5**'**-0"

SECTION A
SCALE: 3/8" = 1'-0"







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SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE
NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE
DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

	04/45/2000	FIMOD DEGULATORY QUIDMICOLON			SEC	SMENT 1 - F	SON POWE PUTNAM TO I PLAN ELEVATI	DRESE
А	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR				
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: DRH	DESIGNED BY: JNK	APPROVED BY: 00	SCALE REV. NO.

LINKBOX CONDUIT (TYP) —

TO GROUND RING

AT MANHOLE ----

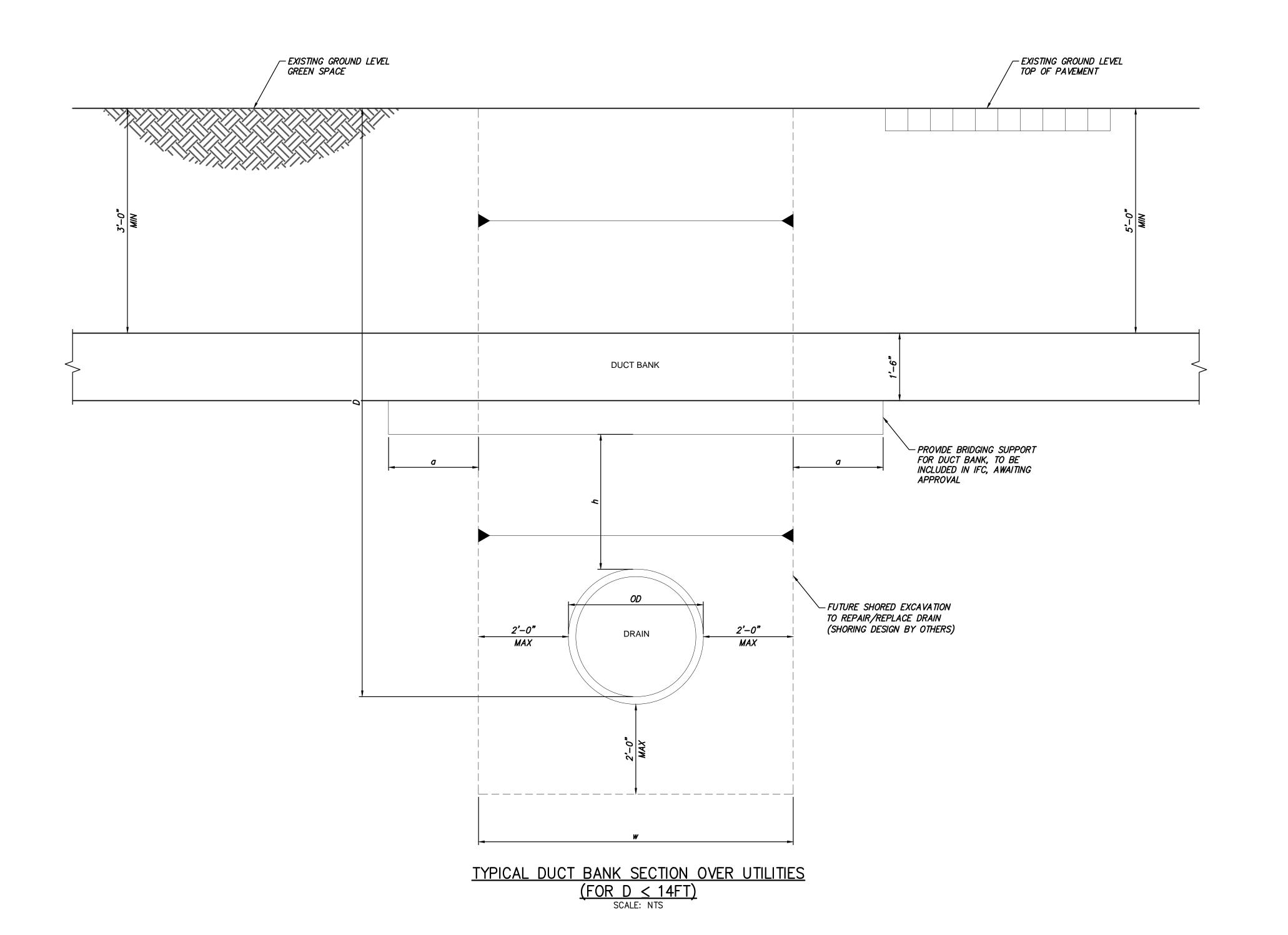
6"x6"x3" SUMP PIT

CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN LINK BOX HANDHOLE PLAN ELEVATION & SECTION

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. DRAWING NO.

S-710

AS SHOWN DATE XX OF XXX



<u>NOTES:</u>

- 1. TYPICAL SECTION AND FOLLOWING NOTES PROVIDED PROPOSED DESIGN CRITERIA TO BE APPLIED FOR DRAINS LOCATED BELOW THE DUCT BANK WITHIN THE NYSDOT RIGHT OF WAY, PENDING APPROVAL.
- 2. EXCAVATION FOR / CONSTRUCTION OF FUTURE DRAIN REPAIRS/REPLACEMENTS, AND RELATED SHORING CALCULATIONS BY OTHERS.
- 3. D = DISTANCE FROM EXISTING GROUND LEVEL TO DRAIN INVERT.
- 4. h = DISTANCE FROM BOTTOM OF TRENCH PROTECTIVE CONCRETE/BOTTOM OF BRIDGING SUPPORT, TO TOP OF EXISTING DRAINAGE PIPE.
- 5. FOR SINGLE PIPE, OD = OUTSIDE DIAMETER OF PIPE; FOR MULTIPLE PIPES IN PARALLEL, OD = DISTANCE BETWEEN OUTSIDE EDGES OF EXTERIOR PIPES.
- 6. h IS ASSUMED TO BE 2FT MINIMUM.
- 7. FOR D≤14FT, ALL FUTURE EXCAVATIONS TO REPAIR/REPLACE EXISTING DRAINAGE PIPES WILL BE SHORED EXCAVATION, WITH MAXIMUM EXCAVATION WIDTH OF w=2FT+OD+2FT (DESIGN/CONSTRUCTION BY OTHERS).
- 8. FOR D>14FT, TRENCH-LESS METHOD WILL BE USED TO REPAIR/REPLACE EXISTING DRAINAGE PIPES (DESIGN/CONSTRUCTION BY OTHERS).
- 9. BRIDGING SUPPORT IS PROVIDED OVER LENGTH "w+2a".
- 10. BRIDGING SUPPORT IS NOT PROVIDED WHEN D>14FT.
- 11. BRIDGING SUPPORT IS DESIGNED TO CARRY DUCT BANK WEIGHT, ONLY. SOIL ABOVE THE DUCT BANK IS REMOVED BEFORE EXCAVATING UNDER THE DUCT BANK.
- 12. "a" DIMENSION AS REQUIRED BY DESIGN.

**Power Express** 





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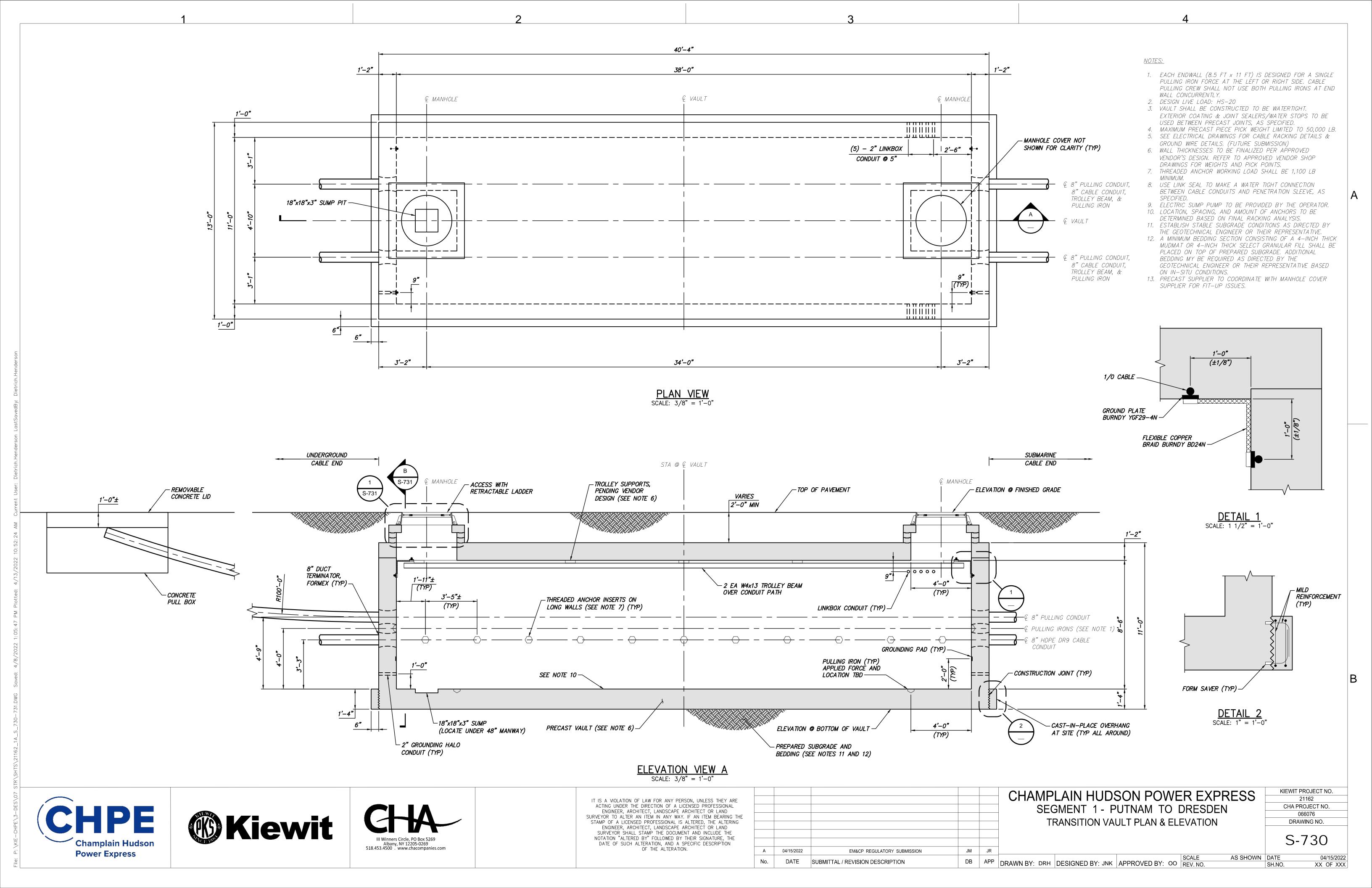
RE THE NG						SEC	LAIN HUDS SMENT 1 - P SUPPORTING S	PUTNAM TO	DRESE
Ξ	A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR				
	No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: DRH	DESIGNED BY: JNK	APPROVED BY:	OO REV. NO.

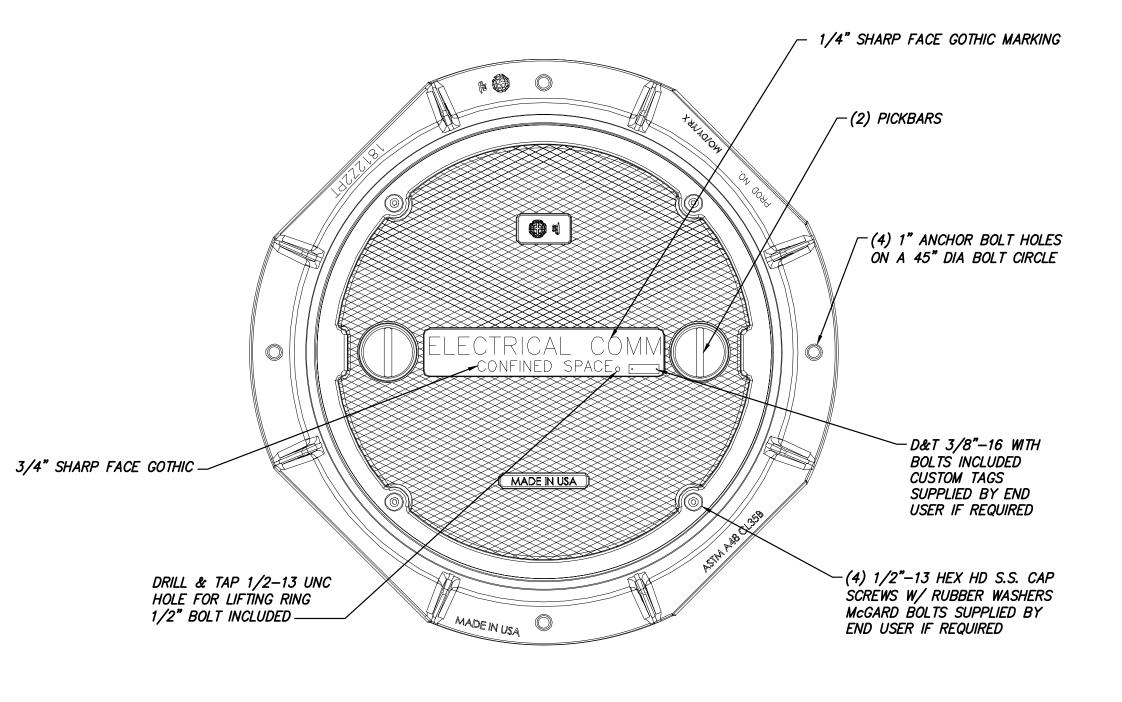
CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN SELF-SUPPORTING STRUCTURE OVER UTILITIES

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

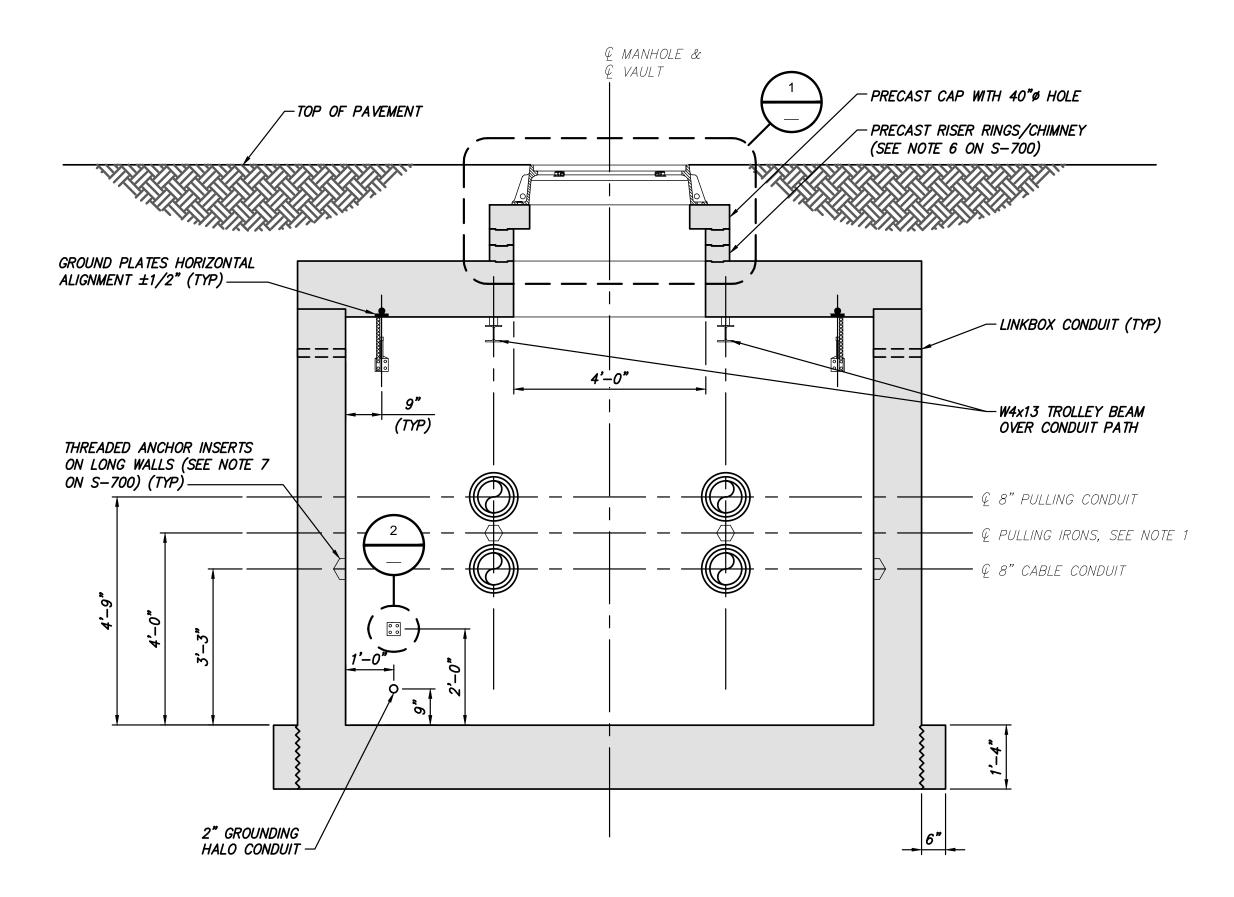
S-720

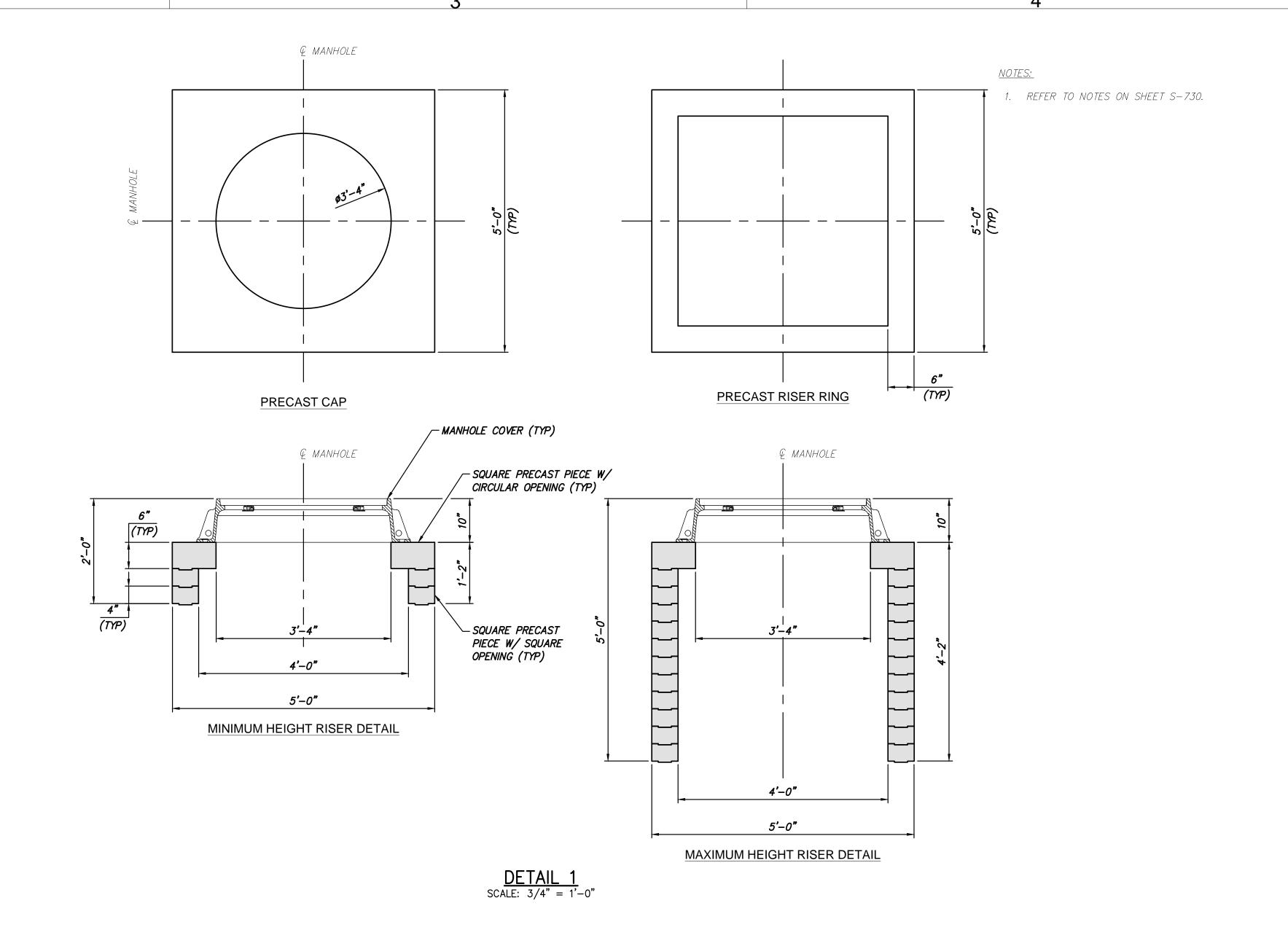
AS SHOWN DATE XX OF XXX

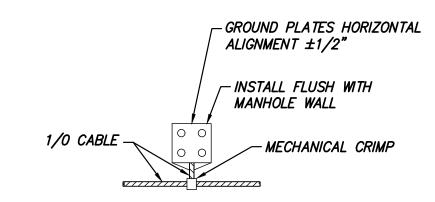




# MANHOLE COVER SCALE: 1 1/2" = 1'-0"







DETAIL 2 SCALE: 1 1/2" = 1'-0"

SECTION B
SCALE: 1/2" = 1'-0"

Champlain Hudson **Power Express** 





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OF THE ALTERATION.

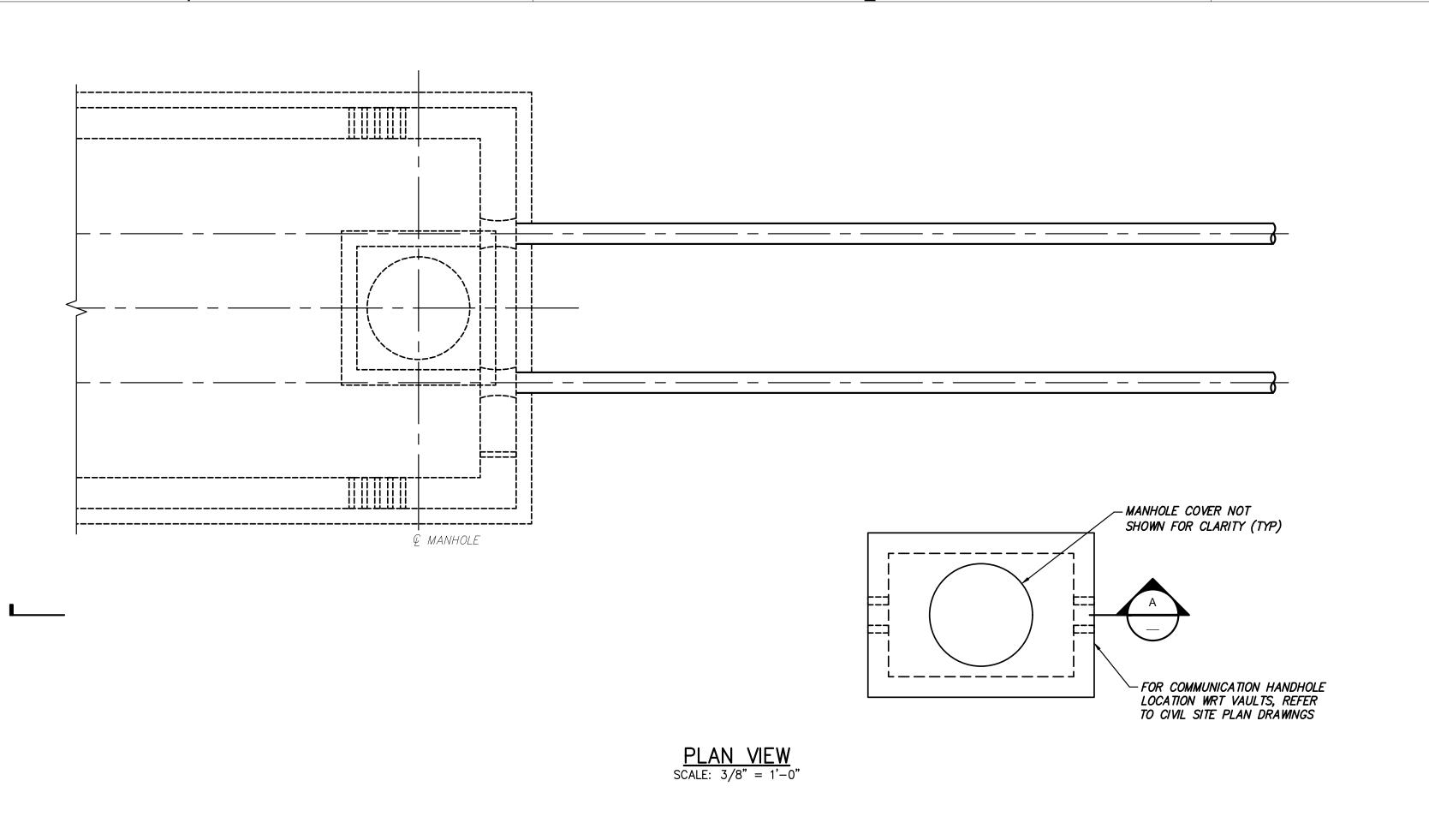
HE						CHAMPLAIN HUDSON POWER EX SEGMENT 1 - PUTNAM TO DRES
•						TRANSITION VAULT SECTION & DETA
	Α	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	
	No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: DRH DESIGNED BY: JNK APPROVED BY: OO REV. NO.

CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 1 - PUTNAM TO DRESDEN TRANSITION VAULT SECTION & DETAILS

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

S-731 AS SHOWN DATE

04/15/2022 XX OF XXX



6'-0" FIBER CONDUIT (TYP) -6"x6"x3" SUMP PIT - COMMUNICATION HANDHOLE

------FIBER CONDUIT (TYP) -PRECAST COMMUNICATION HANDHOLE (SEE NOTE 8 ON S-700) -ELEVATION @ BOTTOM OF HANDHOLE PREPARED SUBGRADE AND BEDDING (SEE NOTES 5 AND 6)

7'-4"

STA @ @ HANDHOLE

SECTION A SCALE: 3/8" = 1'-0"





\_\_\_\_\_\_\_

& MANHOLE

\_TOP OF PAVEMENT



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NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION
OF THE ALTERATION.

- SEE S-701 FOR MANHOLE COVER

\_ ELEVATION @ FINISHED GRADE

AND RISER/CHIMNEY DETAILS

					CHAMPLA SEGME
					COMMUNICATION
Α	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	

DATE SUBMITTAL / REVISION DESCRIPTION

AIN HUDSON POWER EXPRESS MENT 1 - PUTNAM TO DRESDEN TION HANDHOLE PLAN ELEVATION & SECTION

DB APP DRAWN BY: DRH DESIGNED BY: JNK APPROVED BY: OO REV. NO.

<u>NOTES:</u>

1. DESIGN LIVE LOAD: HS-20.

REPRESENTATIVE.

IN-SITU CONDITIONS.

4. SEE S-700 FOR VAULT DETAILS.

2. WALL THICKNESSES TO BE FINALIZED PER APPROVED VENDOR'S DESIGN. REFER TO APPROVED VENDOR SHOP DRAWINGS FOR WEIGHTS AND PICK POINTS. 3. ELECTRIC SUMP PUMP TO BE PROVIDED BY OTHERS.

DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR

4-INCH THICK MUDMAT OR 4-INCH THICK SELECT GRANULAR FILL SHALL BE PLACED ON TOP OF PREPARED SUBGRADE. ADDITIONAL BEDDING MAY BE REQUIRED AS DIRECTED BY THE GEOTECHNICAL ENGINEER OR THEIR REPRESENTATIVE BASED ON

5. ESTABLISH STABLE SUBGRADE CONDITIONS AS

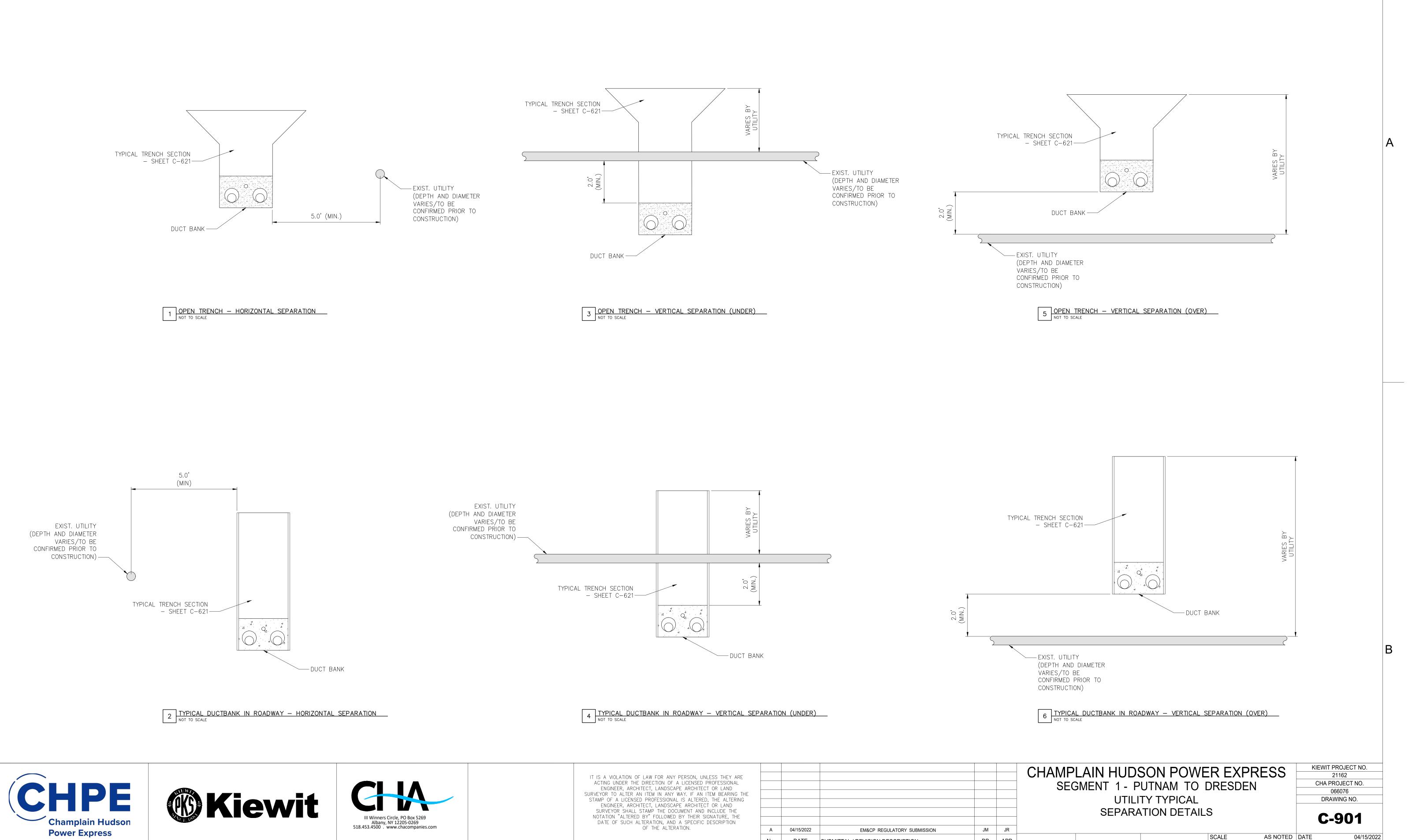
6. A MINIMUM BEDDING SECTION CONSISTING OF A

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

S-770

AS SHOWN DATE

XX OF XXX



No.

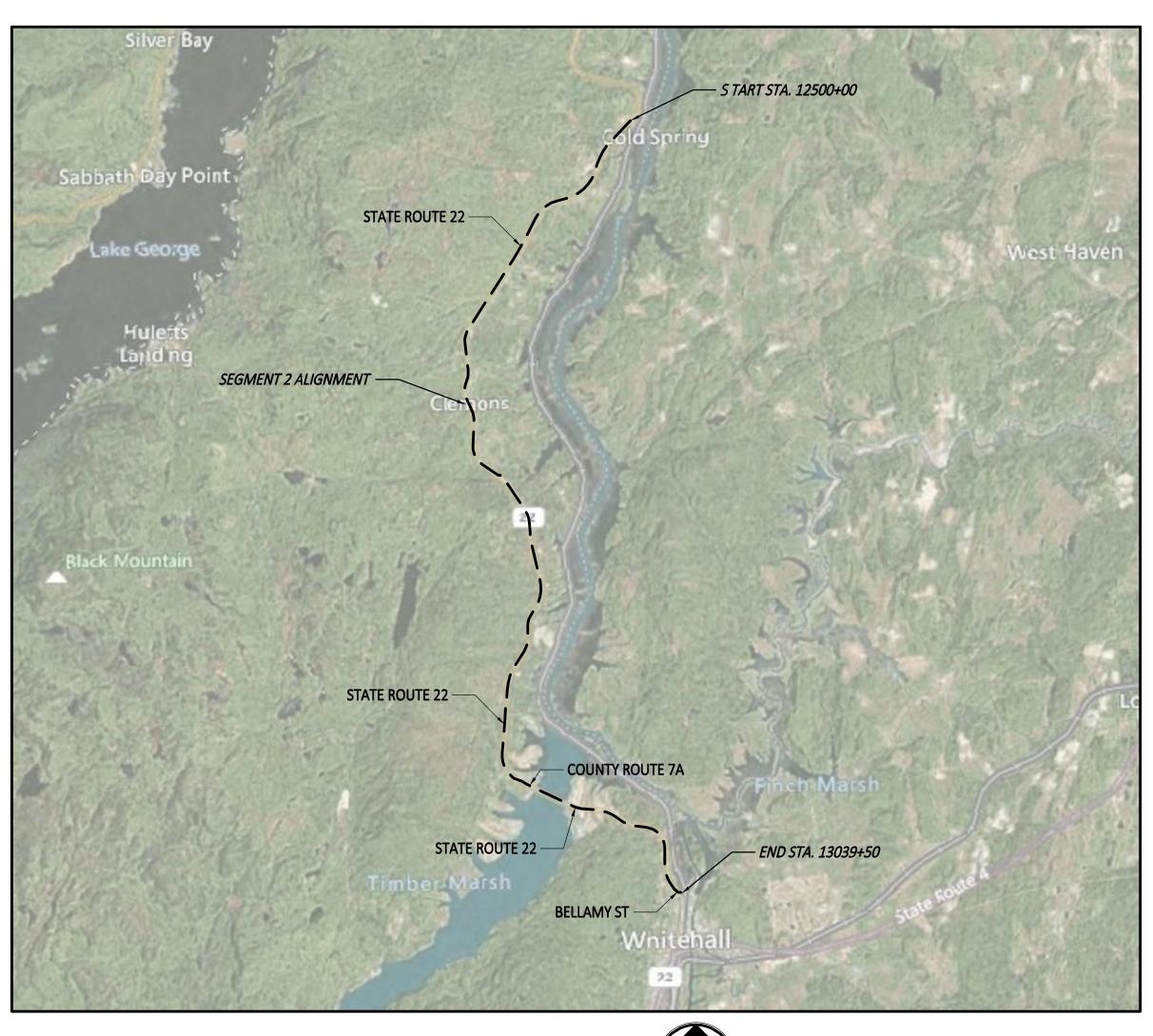
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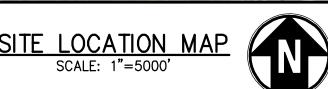
DB | APP | DRAWN BY: wso DESIGNED BY: wso APPROVED BY:

XXX OF XXX

# CHAMPLAIN HUDSON POWER EXPRESS

SEGMENT 2 - DRESDEN TO WHITEHALL WASHINGTON COUNTY, NEW YORK 100% FINAL SUBMISSION (APRIL 15, 2022)





NOT FOR CONSTRUCTION







IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE
ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL
ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND
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DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION
OF THE ALTERATION.

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FESSIONAL R LAND BEARING THE HE ALTERING R LAND LUDE THE TURE, THE CRIPTION		
S THEY ARE		

EM&CP REGULATORY SUBMISSION

SUBMITTAL / REVISION DESCRIPTION

$\frac{1}{1}$	CHAMPLAIN HUDSON POWER EXPRESS
	SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL

KIEWIT PROJECT NO. CHA PROJECT NO. DRAWING NO.

**COVER SHEET** 

G-000

DB APP DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

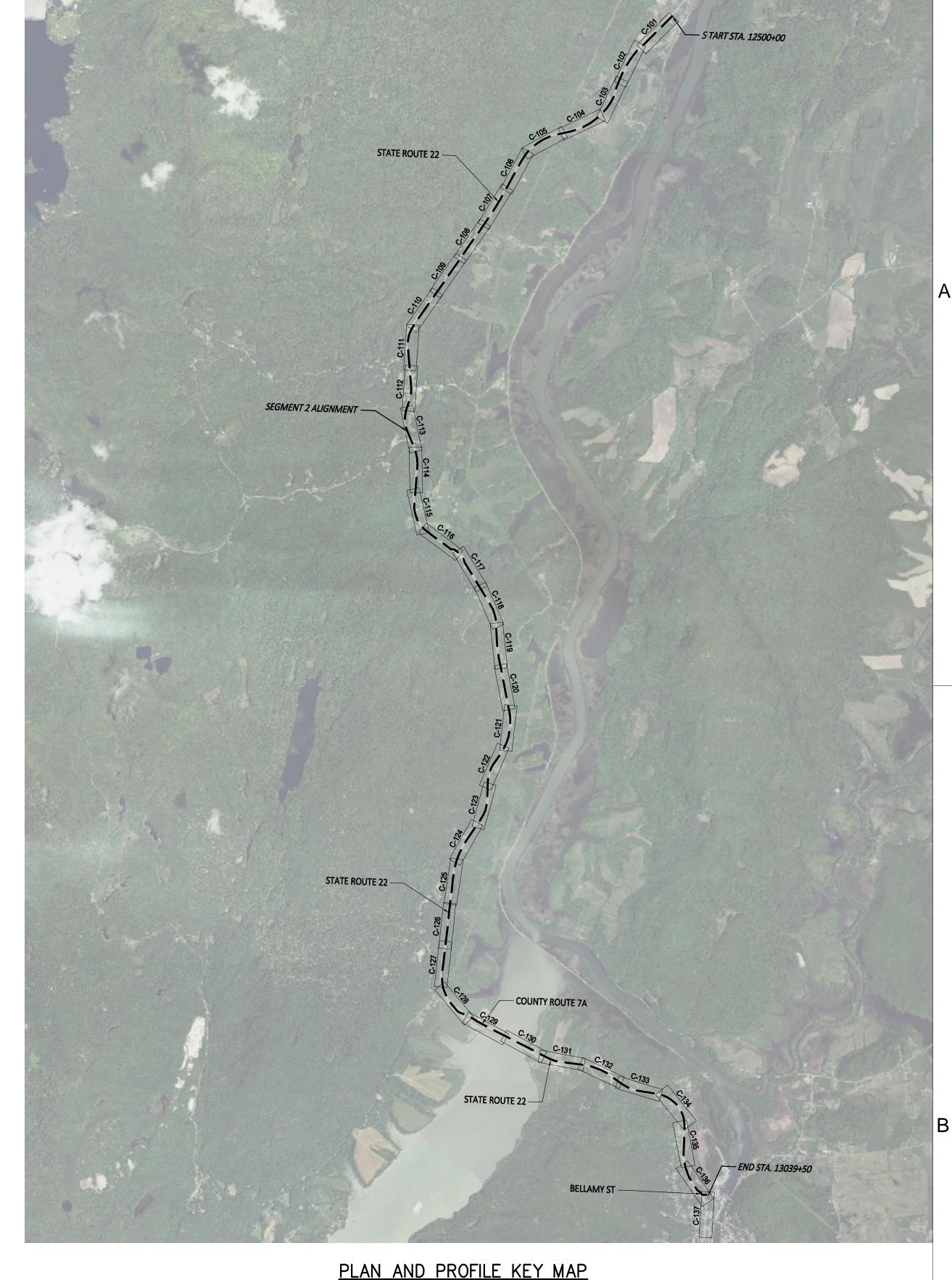
AS NOTED DATE

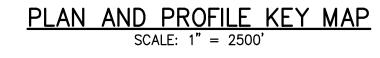
FIBER SPLICE HANDHOLE 004	12588+66.26	RIM = 350.80	346.804	4.00			
FIBER SPLICE HANDHOLE 005	12750+65.07	RIM = 380.35	376.347	4.00			
FIBER SPLICE HANDHOLE 006	12910+64.46	RIM = 162.24	158.237	4.00			
LINK BOX HANDHOLE 015	12567+48.65	RIM = 298.61	292.842	4.50			
LINK BOX HANDHOLE 019	12697+25.99	RIM = 418.32	414.319	4.50			
LINK BOX HANDHOLE 023	12822+06.82	RIM = 213.95	209.950	4.50			
LINK BOX HANDHOLE 027	12956+25.73	RIM = 131.53	127.419	4.50			
SPLICE VAULT 013	12500+00.00	RIM = 152.46	148.463	11.67			
SPLICE VAULT 014	12532+63.51	RIM = 119.34	115.338	11.67			
SPLICE VAULT 015	12567+24.27	RIM = 296.22	290.688	11.67			
SPLICE VAULT 016	12598+77.53	RIM = 329.66	321.738	11.67			
SPLICE VAULT 017	12630+00.00	RIM = 384.30	380.302	11.67			
SPLICE VAULT 018	12664+25.41	RIM = 411.88	407.882	11.67			
SPLICE VAULT 019	12697+01.46	RIM = 418.53	414.531	11.67			
SPLICE VAULT 020	12727+00.00	RIM = 322.20	318.200	11.67			
SPLICE VAULT 021	12758+32.64	RIM = 400.98	396.000	11.67			
SPLICE VAULT 022	12791+84.10	RIM = 307.53	303.533	11.67			
SPLICE VAULT 023	12821+82.64	RIM = 213.80	208.500	11.67			
SPLICE VAULT 024	12857+03.96	RIM = 309.28	305.282	11.67			
SPLICE VAULT 025	12886+15.15	RIM = 265.79	261.793	11.67			
SPLICE VAULT 026	12914+68.32	RIM = 147.00	143.000	11.67			
SPLICE VAULT 027	12956+01.21	RIM = 131.43	127.405	11.67			
SPLICE VAULT 028	12984+50.30	RIM = 174.64	170.643	11.67			
SPLICE VAULT 029	13017+21.66	RIM = 127.54	123.545	11.67			
SPLICE VAULT 030	13034+09.85	RIM = 114.50	110.500	11.67			
POTENTIAL ADDITIONAL SPLICE VAULT 14A @ STA. 12559+00							

Structure Table

Structure Name

Station | Finished Grade | Bottom of Structure | Difference





DB APP DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL KEYPLAN

EM&CP REGULATORY SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

G-001 AS NOTED DATE

X SH.NO.

XXX OF XXX





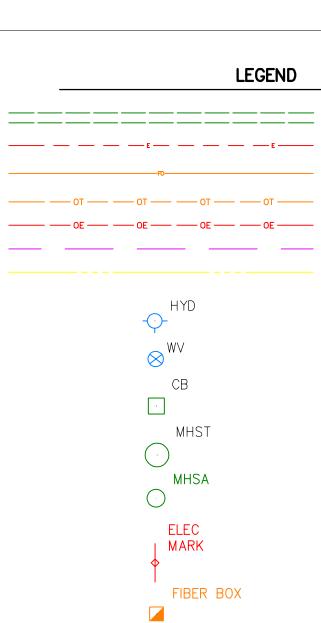


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A 04/15/2022







UNDERGROUND STORM UNDERGROUND ELECTRIC

UNDERGROUND FIBER OPTIC OVERHEAD TELEPHONE OVERHEAD ELECTRIC RIGHT OF WAY

EASEMENT

HYDRANT

WATER VALVE CATCH BASIN

MANHOLE STORM MANHOLE SANITARY

FIBER BOX FIBER MARKER

TELEPHONE PEDESTAL

ELECTRIC MARKER

UTILITY POLE CAPPED IRON ROD CAPPED IRON ROD

> IRON PIPE CONCRETE BOUND

POST WETLAND FLAG

NYSDOT REFERENCE MARKER PROVIDED BY NYSDOT KMZ

MILEPOST NUMBER

CERTIFIED ROUTE PROVIDED BY CHPE KMZ

> **TRANSPORTATION** BRIDGE DECK PAVED DRIVEWAY UNPAVED DRIVEWAY PAVED PARKING

UNPAVED PARKING ABANDONDED ROAD CURBED ROAD PAVED ROAD ----- UNPAVED ROAD RAILROAD INACTIVE RAILROAD PAVED SHOULDER - UNPAVED SHOULDER

SIDEWALK TRAIL <u>UTLITIES</u> **ANTENNA** UTILITY BOX CATCH BASIN HANDHOLE FIRE HYDRANT LIGHT POLE MANHOLE MISCELLANEOUS POLE PIPELINE UTILITY POLE GUYWIRE

#### **SURVEY NOTES:**

- 1. A ROW, UTILITY & WETLAND SURVEY PREPARED IN THE FIELD BY CONTROL POINT ASSOCIATES, INC., AND OTHER REFERENCE MATERIAL AS
- ELEVATIONS REFER TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), BASED ON GPS OBSERVATIONS TAKEN AT THE TIME OF THE SURVEY.
- NORTH ORIENTATION SHOWN REFERS TO THE NAD83 STATE PLANE COORDINATE SYSTEM - NEW YORK EAST ZONE BASED ON GPS OBSERVATIONS TAKEN AT THE TIME OF THE FIELD SURVEY.
- 4. LOCATION OF UNDERGROUND UTILITIES ARE APPROXIMATE. LOCATIONS AND SIZES ARE BASED ON UTILITY MARK-OUTS, ABOVE GROUND STRUCTURES THAT WERE VISIBLE & ACCESSIBLE IN THE FIELD, AND THE MAPS AS LISTED IN THE REFERENCES AVAILABLE AT THE TIME OF THE SURVEY. AVAILABLE ASBUILT PLANS AND UTILITY MARKOUT DOES NOT ENSURE MAPPING OF ALL UNDERGROUND UTILITIES AND STRUCTURES. BEFORE ANY EXCAVATION IS TO BEGIN, ALL UNDERGROUND UTILITIES SHOULD BE VERIFIED AS TO THEIR LOCATION, SIZE AND TYPE BY THE PROPER UTILITY COMPANIES. CONTROL POINT ASSOCIATES, INC. DOES NOT GUARANTEE THE UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA EITHER IN SERVICE OR ABANDONED.

#### SUBSURFACE UTILITY ENGINEERING NOTES:

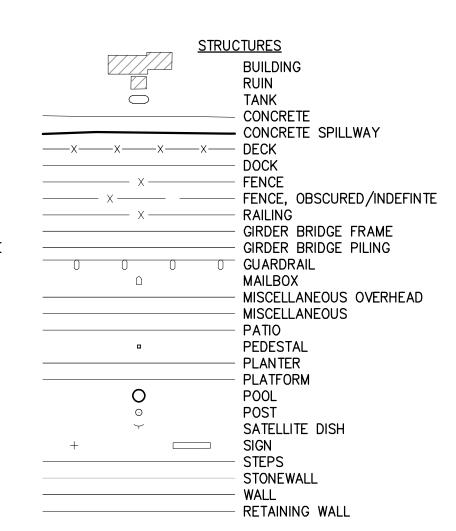
A QUALITY LEVEL SYSTEM IS UTILIZED TO IDENTIFY THE SOURCE OF THE UNDERGROUND UTILITY INFORMATION. THE METHOD OF DETERMINATION IS BASED ON CONTRACTUAL AGREEMENT WITH THE CLIENT AND IS DEPICTED ON THE SURVEY BY THE LINE TYPES SHOWN IN THE DRAWING LEGEND. FOR REFERENCE, THE QUALITY LEVELS ARE AS FOLLOWS:

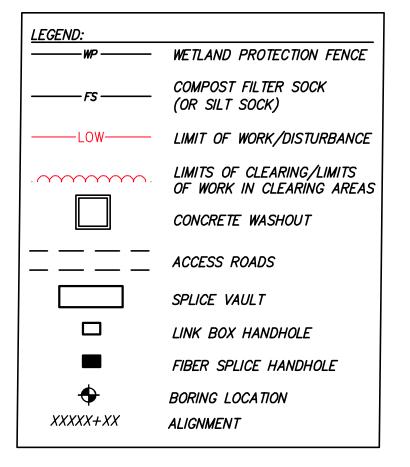
QUALITY LEVEL B - UTILITY LOCATION DATA IS COLLECTED THROUGH GEOPHYSICAL SENSING TECHNOLOGY TO SUPPLEMENT SURFACE FEATURES AND OR REFERENCE MAPPING. INCLUDES MARKOUT BY CONTROL POINT ASSOCIATES, INC.

WETLAND COMMUNITY TYPES LEGEND PEM – PALUSTRINE EMERGENT PSS - PALUSTRINE \_\_\_\_\_SCRUB-SHRUB PFO - PALUSTRINE PUB - PALUSTRINE UNCONSOLIDATED BOTTOM L1 – LACUSTRINE LIMNETIC L2 - LACUSTRINE LITTORAL NYSDEC FWW 100-F00T ADJACENT AREA GIS - WETLAND

WILLIAMS AERIAL & MAPPING, INC. MAPPING FEATURES LEGEND

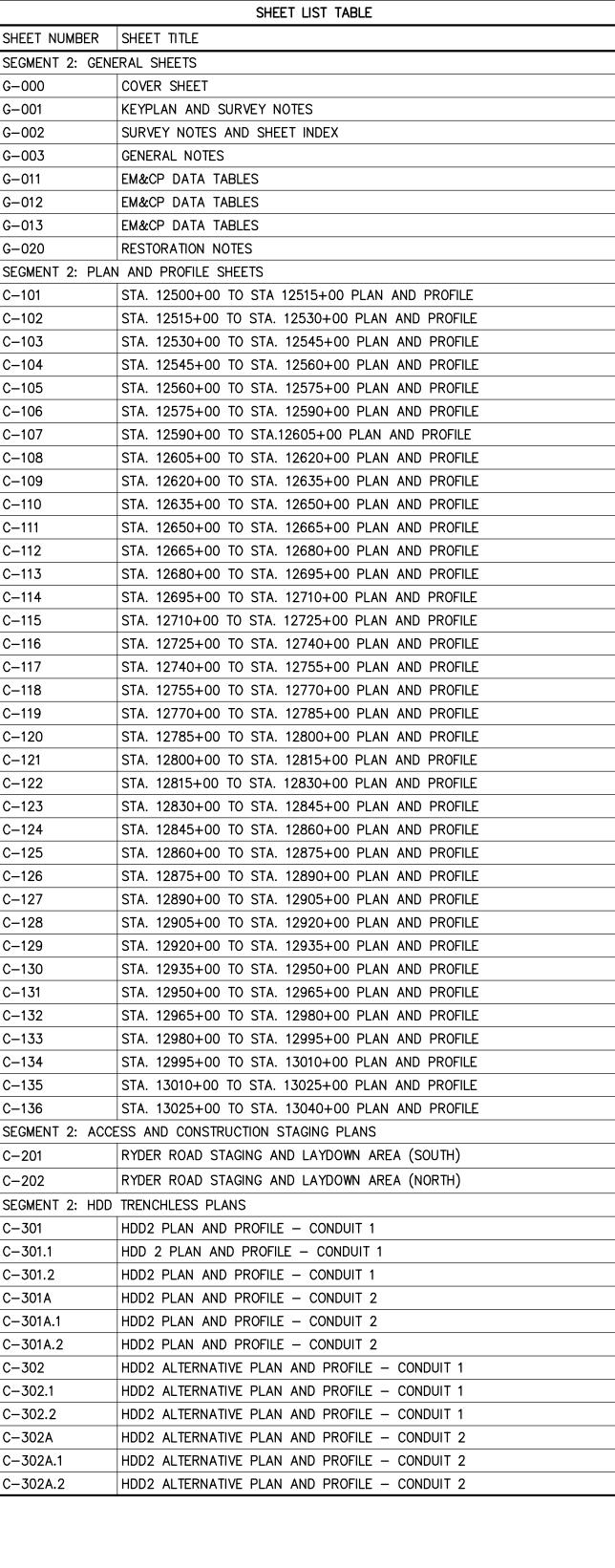
JD BOUNDARY





**TOPOGRAPHY** CONTOUR, INDEX CONTOUR, DEPRESSION INDEX CONTOUR, INTERMEDIATE CONTOUR, DEPRESSION INTERMEDIATE SPOT ELEVATION <u>CULTURAL</u> \_\_\_\_\_ DEBRIS FIELD LINE LANDSCAPE AREA PILE STORAGE AREA **HYDROGRAPHIC** HYDROGRAPHIC CULVERT CULVERT, OBSCURED INUNDATED AREA RIP-RAP STREAM SWAMP WATER LEVEL **BOULDER** SCRUB TREE LINE ○ ○ ○ SINGLE TREE/BUSH

MAPPING BOUNDARY GROUND CONTROL



C-400	KEYPLAN E&S						
C-401	STA. 12500+00 TO STA. 12530+00 EROSION AND SEDIMENT CONTROL PLAN						
C-402	STA. 12530+00 TO STA. 12560+00 EROSION AND SEDIMENT CONTROL PLAN						
C-403	STA. 12560+00 TO STA. 12590+00 EROSION AND SEDIMENT CONTROL PLAN						
C-404	STA. 12590+00 TO STA. 12620+00 EROSION AND SEDIMENT CONTROL PLAN						
C-405	STA. 12620+00 TO STA. 12650+00 EROSION AND SEDIMENT CONTROL PLAN						
C-406	STA. 12650+00 TO STA. 12680+00 EROSION AND SEDIMENT CONTROL PLAN						
C-407	STA. 12680+00 TO STA. 12710+00 EROSION AND SEDIMENT CONTROL PLAN						
C-408	STA. 12710+00 TO STA. 12740+00 EROSION AND SEDIMENT CONTROL PLAN						
C-409	STA. 12740+00 TO STA. 12770+00 EROSION AND SEDIMENT CONTROL PLAN						
C-410	STA. 12770+00 TO STA. 12800+00 EROSION AND SEDIMENT CONTROL PLAN						
C-411	STA. 12800+00 TO STA. 12830+00 EROSION AND SEDIMENT CONTROL PLAN						
C-412	STA. 12830+00 TO STA. 12860+00 EROSION AND SEDIMENT CONTROL PLAN						
C-413	STA. 12860+00 TO STA. 12890+00 EROSION AND SEDIMENT CONTROL PLAN						
C-414	STA. 12890+00 TO STA. 12920+00 EROSION AND SEDIMENT CONTROL PLAN						
C-415	STA. 12920+00 TO STA. 12950+00 EROSION AND SEDIMENT CONTROL PLAN						
C-416	STA. 12950+00 TO STA. 12980+00 EROSION AND SEDIMENT CONTROL PLAN						
C-417	STA. 12980+00 TO STA. 13010+00 EROSION AND SEDIMENT CONTROL PLAN						
C-418	STA. 13010+00 TO STA. 13040+00 EROSION AND SEDIMENT CONTROL PLAN						
SEGMENT 2: MAIN	TENANCE AND PROTECTION OF TRAFFIC PLANS						
C-501	WORK ZONE TRAFFIC CONTROL NOTES, LEGEND AND ABBREVIATIONS						
C-502	WORK ZONE TRAFFIC CONTROL						
C-503	WORK ZONE TRAFFIC CONTROL						
C-504	WORK ZONE TRAFFIC CONTROL						
C-505	WORK ZONE TRAFFIC CONTROL						
C-506	WORK ZONE TRAFFIC CONTROL						
C-507	WORK ZONE TRAFFIC CONTROL						
C-508	WORK ZONE TRAFFIC CONTROL						
C-509	WORK ZONE TRAFFIC CONTROL						
C-510	WORK ZONE TRAFFIC CONTROL						
C-511	WORK ZONE TRAFFIC CONTROL						
SEGMENT 2: DETA	ILS: ESC DETAILS						
C-601	EROSION AND SEDIMENT CONTROL DETAILS						
C-602	EROSION AND SEDIMENT CONTROL DETAILS						
SEGMENT 2: DETA	NILS: EM&CP DETAILS						
C-611	WETLAND CROSSING DETAILS						
SEGMENT 2: DETA	NILS: CONSTRUCTION AND INSTALLATION DETAILS						
C-621	TRENCHING DETAILS						
SEGMENT 2: DETAILS: RESTORATION DETAILS							
C-631	SEGMENT 2: STRUCTURAL DETAILS						
SEGMENT 2: STRU	JCTURAL DETAILS						
SEGMENT 2: STRU S-701	SPLICE VAULT SECTION & DETAILS						
SEGMENT 2: STRU S-701 S-702	SPLICE VAULT SECTION & DETAILS SPLICE VAULT SECTION & DETAILS						
SEGMENT 2: STRU S-701 S-702 S-710	SPLICE VAULT SECTION & DETAILS SPLICE VAULT SECTION & DETAILS LINK BOX HANDHOLE PLAN ELEVATION AND SECTION						
SEGMENT 2: STRU S-701 S-702 S-710 S-720	SPLICE VAULT SECTION & DETAILS  SPLICE VAULT SECTION & DETAILS  LINK BOX HANDHOLE PLAN ELEVATION AND SECTION  TRANSITION VAULT SECTION & DETAILS  COMMUNMICATION HANDHOLE PLAN ELEVATION AND SECTION						

Champlain Hudson **Power Express** 





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A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	SEGMENT 2 - ROUTE 22: DRESDEN - WH SURVEY NOTES AND SHEET INDE
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.
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CHAMPLAIN HUDSON POWER EXPRESS **SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL** SURVEY NOTES AND SHEET INDEX

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

G-002

AS NOTED DATE 04/15/2022 X SH.NO. XXX OF XXX

SEGMENT 2: EROSION AND SEDIMENT CONTROL PLANS

- 3. THE GENERAL CONTRACTOR SHALL STRICTLY ADHERE TO THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) DURING CONSTRUCTION OPERATIONS.
- 4. NO LAND CLEARING OR GRADING SHALL BEGIN UNTIL ALL PERIMETER EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. (WETLAND PROTECTION FENCE, SILT FENCE AND STABILIZED CONSTRUCTION ENTRANCE)
- 5. SITE DISTURBANCE SHALL NOT EXCEED FIVE (5) ACRES OF SOIL AT ANY ONE TIME WITHOUT PRIOR WRITTEN AUTHORIZATION FROM NYSDEC.
- 6. ALL EXPOSED AREAS SHALL BE SEEDED AND MULCHED AS SPECIFIED WITHIN 14 DAYS OF FINAL GRADING.
- 7. INACTIVE PORTIONS OF THE SITE ARE TO BE SEEDED AND MULCHED AS SPECIFIED WITHIN 14 DAYS.
- 8. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) DAYS OR MORE FREQUENTLY IF REQUIRED. ALL MAINTENANCE REQUIRED BY INSPECTION SHALL COMMENCE WITHIN 24 HOURS AND BE COMPLETED WITHIN 48 HOURS OF REPORT.
- 9. THIS PLAN SHALL NOT BE CONSIDERED ALL INCLUSIVE AS THE GENERAL CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SOIL SEDIMENT FROM LEAVING THE SITE.
- 10. GENERAL CONTRACTOR SHALL COMPLY WITH ALL STATE AND LOCAL ORDINANCES THAT APPLY.
- 11. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES WILL BE INSTALLED IF DEEMED NECESSARY FOLLOWING SITE INSPECTION.
- 12. GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO TAKE WHATEVER MEANS NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION.
- 13. AT THE END OF EACH WORK DAY DISTURBED SOILS ARE TO BE REGRADED TO DRAIN INTO THE TEMPORARY DIVERSION SWALES AND DISCHARGES FROM DEWATERING ACTIVITIES ARE TO BE DIRECTED INTO CATCH BASINS OR SWALES.
- 14. CONCRETE WASHOUTS DEPICTED ON PLANS ARE FOR REFERENCE ONLY. CONTRACTOR TO FIELD LOCATE CONCRETE WASHOUTS AS NECESSARY. CONCRETE WASHOUTS SHALL NOT BE LOCATED WITHIN WETLANDS

#### **GENERAL NOTES:**

- 1. THE PLANS SHOW SUBSURFACE STRUCTURES, ABOVE GROUND STRUCTURES AND/OR UTILITIES FROM FIELD LOCATION AND RECORD MAPPING, EXACT LOCATION OF WHICH MAY VARY FROM THE LOCATIONS INDICATED. IN PARTICULAR. THE CONTRACTOR IS WARNED THAT THE EXACT OR EVEN APPROXIMATE LOCATION OF SUCH PIPELINES, SUBSURFACE STRUCTURES AND/OR UTILITIES IN THE AREA MAY BE DIFFERENT FROM THAT SHOWN OR MAY NOT BE SHOWN, AND IT SHALL BE HIS RESPONSIBILITY TO PROCEED WITH GREAT CARE IN EXECUTING ANY WORK. 48 HOURS BEFORE YOU DIG, DRILL, OR BLAST, CALL U.F.P.O. 1-(800)-962-7962 TOLL FREE.
- 2. THE ENGINEER SHALL BE NOTIFIED IN WRITING OF ANY CONDITIONS THAT VARY FROM THOSE SHOWN ON THE PLANS. THE CONTRACTOR'S WORK SHALL NOT VARY FROM THE PLANS WITHOUT THE EXPRESSED APPROVAL OF THE ENGINEER.
- 3. THE CONTRACTOR SHALL RESTORE LAWNS. DRIVEWAYS. CULVERTS. SIGNS AND OTHER PUBLIC OR PRIVATE PROPERTY DAMAGED OR REMOVED TO AT LEAST AS GOOD A CONDITION AS BEFORE BEING DISTURBED AS DETERMINED BY THE ENGINEER.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND INCURRING THE COST OF ALL CONSTRUCTION PERMITS, INSPECTIONS, CERTIFICATES, ETC. AND SHALL COMPLY WITH ALL REQUIRED PERMITS.
- 5. ALL WORK SHALL BE DONE IN STRICT COMPLIANCE WITH ALL APPLICABLE NATIONAL, STATE, AND LOCAL CODES, STANDARDS, ORDINANCES, RULES, AND REGULATIONS.
- 6. ALL PROPOSED UTILITIES AND APPURTENANCES TO BE CONSTRUCTED IN COMPLIANCE WITH THE LOCAL MUNICIPALITIES' CODES AND REGULATIONS GOVERNING THE INSTALLATION OF SUCH UTILITIES.
- 7. THE ENGINEER RESERVES THE RIGHT TO EXAMINE ANY WORK DONE ON THIS PROJECT AT ANY TIME TO DETERMINE THE CONFORMANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS OF THIS PROJECT.
- 8. THE CONTRACTOR SHALL PROTECT EXISTING PROPERTY LINE MONUMENTATION. ANY MONUMENTATION DISTURBED OR DESTROYED, AS JUDGED BY THE ENGINEER OR OWNER, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE UNDER THE SUPERVISION OF A NEW YORK STATE LICENSED LAND SURVEYOR.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE ALL PLAN SHEETS.
- 10. THE CONTRACTOR SHALL:
  - A. VERIFY ALL CONDITIONS IN THE FIELD PRIOR TO COMMENCEMENT OF
  - WORK AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES. EXAMINE THE SITE AND INCLUDE IN HIS WORK THE EFFECT OF ALL
  - EXISTING CONDITIONS ON THE WORK. C. PROVIDE AND INSTALL ALL MATERIALS AND PERFORM ALL WORK IN ACCORDANCE WITH RECOGNIZED GOOD STANDARD PRACTICE.

#### NYSDOT NOTES

- MAINTAIN EXISTING GUIDE RAIL, MEDIAN BARRIER, AND BRIDGE RAIL. WHEN CONSTRUCTION OPERATIONS REQUIRE THE TEMPORARY REMOVAL OF EXISTING BRIDGE RAIL, GUIDE RAIL OR MEDIAN BARRIER; OR WHEN EXISTING RAIL WILL BE REMOVED AND REPLACED WITH NEW RAIL, THE CONTRACTOR SHALL SCHEDULE OPERATIONS TO MINIMIZE THE TIME PERIOD THAT RAIL IS NOT INSTALLED.
- 2. UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, GUIDE RAIL OR MEDIAN BARRIER SHALL BE REPLACED OR THE LOCATION OTHERWISE PROTECTED WITHIN 14 CALENDAR DAYS. BRIDGE RAIL SYSTEMS SHALL BE MAINTAINED IN SERVICE AT ALL TIMES ON ANY STRUCTURE ON WHICH VEHICLE OR PEDESTRIAN TRAFFIC IS MAINTAINED, UNLESS A TEMPORARY BARRIER IS INSTALLED, OR OTHER MEANS ARE USED TO ENSURE THAT VEHICLES. BICYCLISTS AND PEDESTRIANS ARE NOT EXPOSED TO THE UNPROTECTED EDGE OF A BRIDGE.
- 3. DURING NON-WORK HOURS WHEN TRAFFIC IS BEING MAINTAINED ON THE FACILITY, ALL TEMPORARY ENDS (FREE ENDS) OF GUIDE RAIL, MEDIAN BARRIER AND BRIDGE RAIL SHALL BE TEMPORARILY TERMINATED AND MARKED WITH A CHANNELIZING DRUM OR OBJECT MARKER EQUIPPED WITH A TYPE A FLASHING WARNING LIGHT, CORRUGATED BEAM GUIDE RAIL AND MEDIAN BARRIER, AND HEAVY-POST, BLOCKED-OUT, CORRUGATED BEAM GUIDE RAIL AND MEDIAN BARRIER SHALL BE TEMPORARILY TERMINATED BY HAVING THE EXPOSED ENDS (FREE ENDS) DROPPED TO THE GROUND AND PINNED.
- 4. THE APPROACH ENDS OF BOX BEAM GUIDE RAIL, MEDIAN BARRIER AND BRIDGE RAIL SHALL BE TEMPORARILY TERMINATED WITH BOX BEAM GUIDE RAIL END ASSEMBLIES UTILIZING TWO SPLICE PLATES AND THE PROPER NUMBER OF BOLTS PER CONNECTION. NO POSTS FOR ANCHORAGES WILL BE REQUIRED. SPECIAL TEMPORARY SPLICE PLATES ARE REQUIRED TO ADAPT BOX BEAM GUIDE RAIL END ASSEMBLIES TO BOX BEAM MEDIAN BARRIERS.
- 5. DURING ANY OVERNIGHT PERIOD WHEN EXISTING GUIDE RAIL OR MEDIAN BARRIER IS TEMPORARILY REMOVED, THE CONTRACTOR SHALL INSTALL CHANNELIZING DEVICES IN THE LOCATION WHERE THE GUIDE RAIL OR MEDIAN BARRIER WAS REMOVED IN ACCORDANCE WITH §619-3.02J.6. REMOVED EXISTING GUIDE RAIL OR MEDIAN BARRIER.
- 6. GUIDERAIL REPLACEMENT TYPE TO BE COORDINATED WITH NYSDOT.
- 7. THE PLANS DEPICT AREAS OF EXPANDED LIMIT OF WORK AT EXISTING CULVERTS THAT WILL BE EVALUATED WITH NYSDOT AND THE CERTIFICATE HOLDERS FOR POTENTIAL REPLACEMENT OR REPAIR. IF ANY REPAIRS OR REPLACEMENTS ARE DETERMINED, THE WORK WILL BE DONE IN ACCORDANCE WITH NYSDOT SPECIFICATIONS.

- 11. ALL TRENCH EXCAVATION AND ANY REQUIRED SHEETING AND SHORING SHALL BE DONE IN ACCORDANCE WITH THE LATEST REVISIONS OF NEW YORK STATE INDUSTRIAL CODE RULE 23 AND OSHA REGULATIONS FOR CONSTRUCTION. SHEET PILING SHALL BE DESIGNED AND SEALED BY A NEW YORK STATE PROFESSIONAL ENGINEER. WHERE WITHIN RAIL ROAD ROW, ANY EXCAVATION AND SHORING SHALL BE DESIGNED TO MINIMUM CP AND AREAMA REQUIREMENTS.
- 12. CONTRACTOR SHALL BE RESPONSIBLE FOR DEWATERING AND THE MAINTENANCE OF SURFACE DRAINAGE DURING THE COURSE OF WORK AND SHALL SUBMIT A DEWATERING PLAN. CONTRACTOR SHALL MAINTAIN EXISTING SITE DRAINAGE PATTERNS THROUGHOUT CONSTRUCTION UNLESS OTHERWISE SHOWN ON THE PLANS.
- 13. MAINTAIN FLOW FOR ALL EXISTING UTILITIES.
- 14. ALL FRAMES/COVERS WITHIN PAVED AREAS SHALL HAVE THE TOPS SET FLUSH WITH THE EXISTING PAVEMENT GRADE. IN LANDSCAPED AREAS, ALL FRAMES SHALL BE 0.1' ABOVE GRADE.
- 15. TEMPORARY PAVEMENT SHALL BE PLACED WITHIN 48 HOURS OF COMPLETION OF BACKFILL OPERATIONS WITHIN THE EXISTING PAVEMENT LIMITS.
- 16. CONTRACTOR SHALL MAINTAIN ALL TRAFFIC IN ALL AREAS IN ACCORDANCE

WITH THE NYSDOT MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

- 17. ALL EXCAVATIONS SHALL BE PROTECTED AT THE END OF EACH WORK DAY PER OSHA AND NYSDOT REQUIREMENTS.
- 18. WITHIN NYSDOT ROW ALL OPEN EXCAVATIONS TO BE PROTECTED BY CONCRETE BARRIERS OR BE COVERED BY A STEEL PLATE, 3/4" THICK MINIMUM. A SINGLE PLATE SHOULD COVER THE ENTIRE EXCAVATION AND HAVE ENOUGH BEARING ON SURROUNDING SURFACES TO SUPPORT A VEHICLE.
- 19. CONTRACTOR SHALL TAKE CARE TO PREVENT DAMAGE TO EXISTING UTILITIES. UTILITIES DAMAGED BY CONTRACTOR SHALL BE IMMEDIATELY REPAIRED BY CONTRACTOR AT THE CONTRACTOR'S EXPENSE, IF DURING EXCAVATION PREVIOUSLY DAMAGED UTILITIES ARE UNCOVERED, CONTRACTOR SHALL DOCUMENT THE DAMAGE AND REPORT DAMAGE TO THE APPROPRIATE OWNER.
- 20. DEPTH OF BURY FOR EXISTING CABLED UTILITIES FIBER / ELECTRICAL , TELECOM AND WATERLINES UNKNOWN. ASSUMED DEPTH OF BURY FOR CABLED UTILITIES IS 30" UNLESS OTHERWISE SHOWN. ASSUMED DEPTH OF BURY FOR WATERLINES IS 5' UNLESS OTHERWISE SHOWN.
- 21. CONTRACTOR TO COORDINATE ALL DRIVEWAY CROSSINGS WITH THE PROPERTY OWNERS PRIOR TO EXCAVATING. ACCESS TO ALL DRIVEWAYS FOR THE RESIDENTS AND COMMERCIAL PROSPERITIES, WILL NEED TO BE MAINTAINED DURING THE PROJECT. ALL EXCAVATIONS IN THE ENTRANCES/DRIVEWAYS WILL NEED TO BE BACKFILLED AT THE END OF EACH WORKDAY, OR STEEL PLATES SHALL BE INSTALLED TO ALLOW ACCESS DURING CONSTRUCTION. REFER TO THE EM&CP FOR EMERGENCY ACCESS MANAGEMENT PLAN.

#### **DISTURBANCE NOTE:**

- 1. THE PROPOSED DISTURBANCE FOR THE TRENCH DETAIL WILL BE LIMITED TO THE WIDTH OF THE TRENCH SECTIONS DEPICTED ON C-621.
- 2. ARES OF THE LIMIT OF WORK ACROSS THE ALIGNMENT HAVE BEEN EXPANDED TO INCLUDE AREAS OFF THE ROADWAY WITHIN LAWN AREAS THAT ARE SUITABLE FOR POTENTIAL TEMPORARY STAGING AREAS DURING CONSTRUCTION. SHOULD THESE AREAS BE UTILIZED AND DISTURBED THE AREAS SHOULD BE RESTORED TO EXISTING CONDITION PER THE PROJECT RESTORATION NOTES AND DETAILS. EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN DEVELOPED TO INCORPORATE THESE POTENTIALLY DISTURBED AREAS.









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04/15/2022

		CHAMPLAIN HUDSON POWER EX
		CHAMILAM HODOCH I CWEN EX
		SEGMENT 2 - ROUTE 22: DRESDEN - WH
		CENEDAL NOTES
		GENERAL NOTES

JM JR

EM&CP REGULATORY SUBMISSION

DATE SUBMITTAL / REVISION DESCRIPTION

XPRESS HITEHALL

DB | APP | DRAWN BY: JTM | DESIGNED BY: JTM | APPROVED BY: JPR | REV. NO.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. DRAWING NO.

G-003

AS NOTED DATE X SH.NO. XXX OF XXX

1. SECTION 1.4.1 OF THE EM&CP SUMMARIZES THE MEASURES TO BE FOLLOWED WITHIN AGRICULTURAL AREAS, SECTION 7.1.4 OF THE EM&CP SUMMARIZES THE REQUIREMENTS AND PROCEDURES FOR ANY VEGETATION OR TREE CLEARING THAT MAY OCCUR WITHIN AGRICULTURAL LANDS. TABLE 13.1 IN SECTION 13.4 OF THE EM&CP SUMMARIZES THE LOCATION WHERE AGRICULTURAL LANDS POTENTIALLY REQUIRE RESTORATION WITHIN SEGMENT 2. SECTION 13.4 OF THE EM&CP AND DESCRIBES ALL CLEANUP AND RESTORATION PROCEDURES AND METHODS THAT WILL BE FOLLOWED TO RESTORE AGRICULTURAL LANDS.

AND THE AREA WILL BE FULLY RESTORED IN ACCORDANCE WITH SECTION 13.4.

<u>SEGMENT 2 – AGRICULTURAL LANDS</u>

TABLE 1.4: SEGMENT 2 — RECREATIONAL AREAS				
RECREATIONAL AREA LOCATION				
ADIRONDACK PARK	(DRESDEN TO 12938+00)			
NYSDEC SOUTH BAY PIER AND BOAT LAUNCH	(DRESDEN TO 12938+00)			
LAKES TO LOCKS PASSAGE SCENIC BYWAY (STATE HIGHWAY 22)	(12500+00 (C-101) TO 13040+00 (C-136))			

NOTE: SECTION 1.4.2 OF THE EM&CP SUMMARIZES THE MEASURES TO BE FOLLOWED WITHIN RECREATIONAL AREAS.

#### <u>SEGMENT 2 - RECREATIONAL AREAS</u>

TABLE 4: SEGMENT 2 - CONSTRUCTION METHODS				
TOPIC	SECTION OF EM&CP			
HORIZONTAL DIRECTIONAL DRILLING	4.2			
INSTALLATION AND PERFORMANCE CONTROLS	4.2.1			
DRILLING FLUIDS MANAGEMENT	4.2.2			
ROAD CROSSING METHODS	4.2.3			
TRENCHING	4.3			
CABLE INSTALLATION REQUIREMENTS	4.3.1			
DEWATERING METHODS	4.3.2			
BEDDING AND BACKFILLING METHODS	4.3.3			
DREDGING	NOT APPLICABLE TO SEGMENT 2			
CONVERTER STATION AND SUBSTATION REQUIREMENTS	NOT APPLICABLE TO SEGMENT 2			
RIGHT OF WAYS AND EASEMENTS	4.6			
RIGHT OF WAY CLEARING	4.7 (See Section 7.0 for more details)			
BUILDING AND STRUCTURE REMOVAL	NOT APPLICABLE TO SEGMENT 2			
ACCESS ROADS	4.9			
DRIVEWAY ACCESS DURING CONSTRUCTION	4.9.1			
ACCESS THROUGH WETLANDS	4.9.2			
ACCESS THROUGH AGRICULTURAL LANDS	4.9.3			
DRAIN LINES AND UNDER DRAINS WITHIN AGRICULTURAL LANDS	4.9.4			
SOIL MANAGEMENT PLAN	4.10			
TRANSPORTATION AND UTILITY CROSSINGS	4.11 (SEE SECTION 11.0 AND 12.0 FOR MORE DETAILS)			
CULVERT REPLACEMENT	4.12			

NOTE: TABLE 4 SUMMARIZES THE CONSTRUCTION METHODS AND ASSOCIATED SUBSECTIONS THAT SUMMARIZE THE MEASURES AND STANDARDS THAT WILL BE FOLLOWED WITHIN THE SEGMENT 2.

#### SEGMENT 2 - CONSTRUCTION METHODS







IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PRO ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM STAMP OF A LICENSED PROFESSIONAL IS ALTERED, T ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT ( SURVEYOR SHALL STAMP THE DOCUMENT AND INC NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNA DATE OF SUCH ALTERATION, AND A SPECIFIC DES OF THE ALTERATION.

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CHAMPLAIN HUDSON POWER EXPRESS EGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL **EM&CP DATA TABLES** 

DB APP DRAWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO.

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

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TABLE 4.3: SEGMENT 2 -ACCESS ROADS LOCATION (APPROXIMATE - SEE | IMPACTS TO ENVIRONMENTALLY ACCESS ROAD DESCRIPTION TYPE OF ACCESS ROAD IMPACTS TO AGRICULTURAL LAND DRAWINGS FOR DETAILS) SENSITIVE AREAS GRAVEL ACCESS ROAD FOR **TEMPORARY** 12914+75 TO 12921+50 (C-128) NONE NONE LAKE ROAD HDD 2 ENTRY

NOTE: TABLE 4.3 SUMMARIZES THE ACCESS ROADS IN THIS SEGMENT AND THEIR ASSOCIATED IMPACTS ON ENVIRONMENTALLY SENSITIVE AREAS AND AGRICULTURAL LANDS IF APPLICABLE. SECTION 4.8 OF THE EM&CP SUMMARIZES THE PROCEDURES THAT WILL BE FOLLOWED FOR THE CONSTRUCTION OF ALL ACCESS ROADS. ALL ACCESS ROADS WILL BE TEMPORARY AND RESTORED ACCORDING TO SECTION 13.2.4 AND 13.4 OF THE EM&CP AS APPLICABLE.

#### SEGMENT 2 - ACCESS ROADS

1. THE STORMWATER POLLUTION PREVENTION PLAN INCLUDED IN APPENDIX G OF THE EM&CP DESCRIBES THE EROSION AND SEDIMENT CONTROLS THAT WILL BE FOLLOWED FOR THIS

2. THE EROSION AND SEDIMENT CONTROL PLANS CAN BE FOUND ON SHEETS C-401 TO SHEET C-418 FOR THIS SEGMENT.

## <u>SEGMENT 2 - EM&CP EROSION AND SEDIMENT CONTROL NOTES</u>

1. THE NOISE RECEPTORS THAT OCCUR NEAR SEGMENT 2 AT VARIOUS POINTS INCLUDE RESIDENCES AND BUSINESSES. SECTION 9.2 OF THE EM&CP DESCRIBES THE NOISE CONTROL MEASURES THAT WILL BE EMPLOYED THROUGHOUT THIS SEGMENT.

#### <u>SEGMENT 2 - EM&CP NOISE SENSITVE AREAS NOTE</u>

1. GIS FEMA DATA NOT AVAILABLE FOR WASHINGTON COUNTY, FEMA FIRM MAPS ARE PROVIDED IN APPENDIX D OF THE STORMWATER POLLUTION PREVENTION PLAN WHICH IS INCLUDED IN APPENDIX G OF THE EM&CP.

SEGMENT 2 - FEMA

TABLE 7.1: SEGMENT 2 — VEGETATION AND TREE CLEARING LOCATIONS TABLE				
DESCRIPTION	LOCATION (APPROXIMATE-SEE DRAWINGS FOR DETAILS)	VEGETATION/TREE CLEARING METHOD TYPE		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12598+43 TO 12599+15 (C-107)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12664+00 TO 12664+65 (C-111)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12678+28 TO 12679+30 (C-112)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12758+00 TO 12759+12 (C-118)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12842+50 TO 12843+00 (C-123)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12843+77 TO 12880+27 (C-123, 124, 125, 126)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12884+08 TO 12884+38 (C-126)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	12890+19 TO 12896+50, 12898+00 TO 12896+30, 12902+82 TO 12903+38 (C-127)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
ACCESS ROAD AND CONDUIT INSTALLATION. TREE CLEARING AND VEGETATION CLEARING.	12917+00 TO 12917+79, 12919+64 TO 12921+00 (C-128, 129)	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
ENTRY AND EXIT PITS FOR HDD-2. TREE AND VEGETATION CLEARING.	12913+00 TO 12921+00	TYPE I AND TYPE II. WITHIN APA BOUNDARY.		
TREE TRIMMING AND VEGETATION CLEARING. NO TREE REMOVAL ANTICIPATED.	13035+74 TO 13037+50 (C-136)	TYPE I AND TYPE II		

NOTE: TABLE 7.1 SUMMARIZES THE LOCATION AND CLEARING TYPE THAT WILL OCCUR WITHIN THIS SEGMENT. SECTION 7.0 OF THE EM&CP SUMMARIZES THE CLEARING METHODS AND PROCEDURES FOR VEGETATION AND TREES.

#### <u>SEGMENT 2 - FEDERALLY LISTED AND STATE LISTED SPECIES</u>

TABL	TABLE 7.2 SEGMENT 2 - TREE AND VEGETATION CLEARING METHODS			
METHOD TYPE	METHOD TITLE	METHOD DESCRIPTION		
TYPE I	HAND CUTTING (HC)	THIS METHOD EMPLOYS A HAND—HELD CHAIN SAW. IT IS SELECTIVE BUT IS SLOWER AND MORE EXPENSIVE THAN MOTORIZED MECHANICAL DEVICES. RESIDENTIAL AREAS, BUFFER ZONES, WETLANDS, AND HIGHWAY SCREENS ARE AREAS WHERE HAND CUTTING IS TYPICALLY PRESCRIBED.		
TYPE II	MECHANICAL CLEARING MACHINE (HA)	THIS TERM USUALLY REFERS TO A MACHINE KNOWN AS THE HYDRO—AX OR KERSHAW MOWER. THIS MACHINE CAN CUT TREES UP TO TEN (10) INCHES IN DIAMETER AT THE RATE OF SEVERAL ACRES A DAY, DEPENDING ON STEM DENSITY AND TERRAIN. IT IS ESSENTIALLY NONSELECTIVE AND A GOOD DEVICE FOR CLEARING RIGHTS—OF—WAY THAT ARE COMPOSED OF YOUNG UNDESIRABLE SPECIES IN A RELATIVELY UNIFORM STAND.		
TYPE III	MOWING	THIS TECHNIQUE IS PRIMARILY USED IN AREAS OF HERBACEOUS VEGETATION. TERRAIN MUST BE RELATIVELY FLAT WITH NO GULLIES OR ROCKS.		
TYPE IV	MECHANICAL WHOLE—TREE FELLING EQUIPMENT	THIS METHOD ALLOWS CONTROLLED FELLING AND LOADING OF WHOLE TREES WHILE MINIMIZING DAMAGE TO ADJACENT TREES.  WHERE VEGETATION IS CLEARED, EROSION AND SEDIMENT CONTROL MEASURES WILL BE INSTALLED AND MONITORED UNTIL THE TOPSOIL IS STABILIZED AND CAN SUPPORT GRASSY VEGETATION.		

NOTE: TABLE 7.2 SUMMARIZES THE CLEARING TYPES THAT MAY OCCUR WITHIN THIS SEGMENT. SECTION 7.0 AND 7.1 OF THE EM&CP SUMMARIZES THE CLEARING METHODS AND PROCEDURES FOR VEGETATION AND TREE CLEARING AND REMOVAL INCLUDING STANDARDS AND SPECIFICATIONS FOR CLEARING IN ENVIRONMENTALLY SENSITIVE AREAS.

- 1. WETLANDS: SECTION 7.1.5 AND SECTION 8.1 OF THE EM&CP
- 2. STREAM CROSSING: SECTION 7.1.5 AND SECTION 8.1 OF THE EM&CP.
- 3. THREATENED AND ENDANGERED SPECIES/SENSITIVE HABITATS: SECTION 8.2 OF THE EM&CP.
- 4. ADIRONDACK PARK: SECTION 7.1.4 OF THE EM&CP
- 5. AGRICULTURAL LANDS: SECTION 7.1.6 OF THE EM&CP.

#### <u>SEGMENT 2 - TREE AND VEGETATION CLEARING METHODS</u>

TABL	TABLE 7.2 SEGMENT 2 - TREE AND VEGETATION DISPOSAL METHODS			
METHOD TYPE	METHOD TITLE	METHOD DESCRIPTION		
TYPE A	CONSTRUCTION USE	LOGS MAY BE UTILIZED AS NEEDED DURING CONSTRUCTION FOR WETLAND ACCESS, CRIBBING, RETAINING WALLS, OR OTHER USES. FOLLOWING USE, ANY LOGS UNSUITABLE FOR FIREWOOD, SAW LOGS, OR CHIPPING WILL BE TRANSPORTED OFF THE RIGHT-OF-WAY TO AN APPROVED DISPOSAL SITE		
TYPE B	LOG PILES	LGS NOT NEEDED FOR CONSTRUCTION WILL BE REMOVED FROM THE RIGHT-OF-WAY TO AN APPROVED DISPOSAL AREA.		
TYPE C	SALE	WHERE SUFFICIENT MERCHANTABLE VOLUME EXISTS ON THE SITE, LOGS MAY BE SOLD TO A THIRD PARTY. WHERE APPROPRIATE AND PRACTICAL, AND WITH THE AGREEMENT OF LANDOWNERS, UNSOLD LOGS WILL BE HAULED TO ACCESSIBLE LOCATIONS FOR SALVAGE BY THE GENERAL PUBLIC IN ACCORDANCE WITH THE SUBSTANTIVE REQUIREMENTS OF 6 NYCRR PART 192.5, FIREWOOD RESTRICTIONS TO PROTEST FORESTS FROM INVASIVE SPECIES.		
TYPE D	TREE/LOG CHIPPING	WHEN LOGS CANNOT BE REUSED OR SOLD, THEY WILL BE CHIPPED ON SITE. THE RESULTING WOOD CHIPS WILL BE PILED IN UPLAND AREAS WITIN THE RIGHT-OF-WAY OR TRANSPORTED OFF RIGHT-OF-WAY TO AN APPROVED DISPOSAL SITE. WOOD CHIPS WILL BE SPREAD THREE (3) TO FIVE (5) INCHES THICK WITH FERTILIZER SPREAD OVER THE CHIPS TO MINIMIZE SOIL NITROGEN DEPLETION DUE TO CELLULOSE DECOMPOSITION.		
TYPE E	VEGETATION CHIPPING	VEGETATION MAY BE CHIPPED TO REDUCE DEBRIS VOLUME. SEE TABLE 7.3 ABOVE FOR THE HANDLING OF CHIPS.		
TYPE F	VEGETATION HAULING	VEGETATION AND STUMPS MAY BE HAULED TO A NYSDEC APPROVED LANDFILL OR OTHER SUITABLE OFF—SITE LOCATION WITH THE APPROVAL OF THE LANDOWNER AND ALL APPLICABLE PERMITTING AGENCIES.		
TYPE G	VEGETATION BURIAL	STUMPS MAY BE BURIED ON THE RIGHT-OF-WAY WITH LANDOWNER AGREEMENT. THE BURIAL AREAS WILL BE SUFFICIENTLY COMPACTED AND MONITORED AFTER CONSTRUCTION TO ASSURE THAT SETTLING DOES NOT OCCUR. WHERE SIGNIFICANT SETTLING AFTER CONSTRUCTION HAS BEEN IDENTIFIED BY THE CONSTRUCTION INSPECTOR ET.AL., FINISHED GRADE WILL BE RE-ESTABLISHED USING LOCALLY OBTAINED RUN-OF-BANK MATERIAL AND/OR TOPSOIL AND RE-SEEDED AS APPROPRIATE AS SPECIFIED IN SECTIONS 14.2. AREAS WHERE SIGNIFICANT AMOUNTS OF STUMP BURIAL OCCURS WILL BE NOTED ON AS-BUILT DRAWINGS, AND MONITORED FOR SETTLING DURING ROW CONDITION SURVEYS AND MAINTENANCE ACTIVITIES.		

NOTE: SECTION 7.3 OF THE EM&CP SUMMARIZES THE TREE AND VEGETATION DISPOSAL PROCEDURES FOR THIS SEGMENT. NO BURNING OF ANY VEGETATIVE OR TREE DEBRIS IS PERMITTED WITHIN THE WORK AREAS OF THE SEGMENT 1 AND 2. ALL APPLICABLE NYSDEC REGULATIONS REGARDING INVASIVE SPECIES WILL BE FOLLOWED WHEN DISPOSING OF VEGETATION.

### SEGMENT 2 - TREE AND VEGETATION CLEARING METHODS

1. SECTION 8.1 OF THE EM&CP AND THE WETLAND DELINEATION REPORT IN APPENDIX M OF THE EM&CP DESCRIBE THE IMPACTS TO WETLANDS WITHIN THIS SEGMENT AS WELL AS THE AVOIDANCE AND MINIMIZATION MEASURES. TABLE 8.2 ABOVE SUMMARIZES THE PERMANENT ROW IMPACTS (0.008 ACRES) AND TEMPORARY CONSTRUCTION IMPACTS (0.04 ACRES) IN SEGMENT 1.

2. SECTION 8.1 OF THE EM&CP AND THE WATERBODY INVENTORY IN APPENDIX M OF THE EM&CP DESCRIBE THE IMPACTS TO STREAMS AND WATERBODIES WITHIN THIS SEGMENT AS WELL AS THE AVOIDANCE AND MINIMIZATION MEASURES. THERE ARE NO IMPACTS TO STREAMS OR WATERBODIES WITHIN SEGMENT 1.

SEGMENT 2 - SUMMARY OF WETLAND IMPACT

	SUMMARY OF WETLAND IMPACT				
WETLAND ID	JURISDICTION	APPROXIMATE STATION & SHEET NUMBER	ECOLOGICAL COMMUNITY TYPE	CONDUIT (CABLE) RIGHT OF WAY (SQUARE FEET)	CONSTRUCTION CORRIDOR IMPACTS* (SQUARE FEET)
WETLAND G-P	USACE	12905+75 TO 12906+00 C-414	PEM	0	0
WETLAND G-N	USACE	12913+50 TO 12921+00 C-414 TO C-415	PEM	0	2,431
	TOTAL		PEM	0	2,431

1. SECTION 8.1 OF THE EM&CP AND THE WETLAND DELINEATION REPORT IN APPENDIX J OF THE EM&CP DESCRIBE THE IMPACTS TO WETLANDS WITHIN THE SEGMENT 1 SEGMENT AS WELL AS THE AVOIDANCE AND MINIMIZATION MEASURES. THERE WERE NO IDENTIFIED IMPACTS TO WETLANDS WITHIN SEGMENT 2.

2. SECTION 8.1 OF THE EM&CP AND THE WATERBODY INVENTORY IN APPENDIX J OF THE EM&CP DESCRIBE THE IMPACTS TO STREAMS AND WATERBODIES WITHIN THE SEGMENT 1 SEGMENT AS WELL AS THE AVOIDANCE AND MINIMIZATION MEASURES. THERE WERE NO IDENTIFIED IMPACTS TO STREAMS AND WATERBODIES WITHIN SEGMENT 2.

#### SEGMENT 2 IMPACTS TO WETLANDS AND WATERBODIES

TABLE 8.3: SEGMENT 2 — FEDERALLY LISTED SPECIES TABLE				
SPECIES NAME (SCIENTIFIC NAME)	LOCATION	AVOIDANCE	MINIMIZATION MEASURES	IMPACTS
INDIANA BAT (Myotis sodalis)	THROUGHOUT SEGMENT 2 WITHIN FORESTED AREAS	NO TREE CUTTING ANTICIPATED FOR SEGMENT 2 EXCEPT FOR AS NOTED IN MINIMIZATION COLUMN	TREE REMOVAL WILL BE LIMITED TO OCTOBER 31 THROUGH MARCH 31.  TREE REMOVAL REQUIRED FROM STATION 12913+00 TO 12921+00 (SEGMENT 2) SEE TABLE 7.1 IN SECTION 7.0	NONE
NORTHERN LONG—EARED BAT (Myotis septentrionalis)	THROUGHOUT SEGMENT 2 WITHIN FORESTED AREAS	NO TREE CUTTING ANTICIPATED FOR SEGMENT 2 EXCEPT FOR AS NOTED IN MINIMIZATION COLUMN	TREE REMOVAL WILL BE LIMITED TO OCTOBER 31 THROUGH MARCH 31.  TREE REMOVAL REQUIRED FROM STATION 12913+00 TO 12921+00 (SEGMENT 2) SEE TABLE 7.1 IN SECTION 7.0.	NONE

TABLE 8.4: SEGMENT 2 - STATE LISTED SPECIES TABLE				
SPECIES NAME (SCIENTIFIC NAME)	LOCATION	AVOIDANCE	MINIMIZATION MEASURES	IMPACTS
NORTHERN LONG—EARED BAT (Myotis septentrionalis)	THROUGHOUT SEGMENT 2 WITHIN FORESTED AREAS	NO TREE CUTTING ANTICIPATED FOR SEGMENT 2 EXCEPT FOR AS NOTED IN MINIMIZATION COLUMN	TREE REMOVAL WILL BE LIMITED TO OCTOBER 31 THROUGH MARCH 31.TREE REMOVAL WILL BE LIMITED TO OCTOBER 31 THROUGH MARCH 31.  TREE REMOVAL REQUIRED FROM STATION 12913+00 TO 12921+00 (SEGMENT 2) SEE TABLE 7.1 IN SECTION 7.0	NONE
BALD EAGLE (Haliaeetus leucocephalus)	SEGMENT 2- DOCUMENTED  1 MILE FROM THE PROJECT IN PUTNAM STATION	NO IMPACT	100% AVOIDABLE	NONE

NOTE: TABLE 8.3 AND TABLE 8.4 ABOVE SUMMARIZE THE LOCATIONS, AVOIDANCE, AND MINIMIZATION MEASURES, AND IMPACTS FOR THE FEDERALLY LISTED SPECIES AND STATE-LISTED SPECIES THAT MAY OCCUR ON OR WITHIN THE VICINITY OF THE SEGMENT 1. SECTION 8.2 OF THE EM&CP SUMMARIZES THE HABITAT DESCRIPTIONS AND MITIGATION AND PROTECTION MEASURES FOR THREATENED AND ENDANGERED SPECIES; RARE, THREATENED, AND ENDANGERED PLANTS; AND SIGNIFICANT NATURAL

#### <u>SEGMENT 2 - FEDERALLY LISTED AND STATE LISTED SPECIES</u>

TABLE 8.5: SEGMENT 2 - AQUATIC INVASIVE SPECIES / LOCATOIN		
SPECIES NAME (SCIENTIFIC NAME)	Location (Approximate —see Drawings for Details)	COMMENTS
REED CANARY GRASS (Phalaris arundinacea)	WETLAND G-N (1-16) 12913+00 -12921+25	ROUTE 22

NOTE: TABLE 8.5 SUMMARIZES THE INVASIVE AQUATIC SPECIES IDENTIFIED WITHIN SEGMENT 2. SECTION 8.3 OF THE EM&CP DESCRIBES THE INVASIVE SPECIES MANAGEMENT PLAN FOR THE PROJECT. SECTION 8.3.2 AND 8.3.3 OF THE EM&CP DESCRIBE THE MEASURES TO PREVENT OR CONTROL THE TRANSPORT OF INVASIVE PLANT AND INSECT SPECIES AS WELL AS THE NECESSARY REPORTING REQUIREMENTS TO NYSDEC REGIONAL FORESTER IF THESE SPECIES ARE ENCOUNTERED.

#### SEGMENT 2 — FEDERALLY LISTED AND STATE LISTED SPECIES

NOTE: THERE WERE NO CULTURAL RESOURCES IDENTIFIED WITHIN SEGMENT 2. SECTION 10.0 OF THE EM&CP AND THE CULTURAL RESOURCE MANAGEMENT PLAN IN APPENDIX O OF THE EM&CP DESCRIBE THE PROCEDURES THAT SHOULD BE FOLLOWED DURING THE UNANTICIPATED DISCOVERY OF ARCHEOLOGICAL RESOURCES DURING CONSTRUCTIONS.

#### <u>SEGMENT 2 - CULTURAL RESOURCES</u>

**Champlain Hudson Power Express** 





IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSI ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAN SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEAF STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE AL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAN SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPT OF THE ALTERATION.

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## CHAMPLAIN HUDSON POWER EXPRESS **SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL EM&CP DATA TABLES**

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EM&CP REGULATORY SUBMISSION AS NOTED DATE DB APP DRAWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO. DATE SUBMITTAL / REVISION DESCRIPTION X SH.NO.

TABLE 11.3 NYSDOT COORDINATION SUMMARY						
COORDINATING PARTIES	DESCRIPTION	CURRENT STATUS				
CERTIFICATE HOLDERS, DPS STAFF, NYSDOT	ALL PLANS AND WORK TO BE PERFORMED IN STATE-OWNED ROW UNDER NYSDOT'S SUPERVISION AND MANAGEMENT.	ONGOING THROUGHOUT				
CERTIFICATE HOLDERS, DPS STAFF, NYSDOT STAFF	CERTIFICATE HOLDERS SHALL PROVIDE DPS STAFF AND NYSDOT STAFF WITH A PRELIMINARY DESIGN MARKED TO AVOID CONFLICT WITH POTENTIAL TRANSPORTATION PROJECTS THAT NYSDOT STAFF MAY SEEK TO UNDERTAKE IN THE FUTURE AND SHALL OFFER TO CONSULT WITH NYSDOT STAFF CONCERNING ANY COMMENTS IT MAY OFFER AND SHALL USE REASONABLE EFFORTS TO ACCOMMODATE ANY NYSDOT CONCERNS (CC#68).	PRIOR TO FILING ANY SEGMENT EM&CP INVOLVING ANY SUCH STATE—OWNED ROW.				
CERTIFICATE HOLDERS, NYSDOT, AGENCY CROSSED BY PROJECT	CERTIFICATE HOLDERS WILL CONSULT WITH EACH TRANSPORTATION DEPARTMENT OR AGENCY HAVING JURISDICTION OVER ANY ROADS, RELATED STRUCTURES, AND COMPONENTS THAT WILL BE CROSSED BY THE FACILITY OR USED FOR DIRECT ACCESS TO THE CONSTRUCTION ZONE. IF THE ACCESS ROAD TAKES DIRECT ACCESS FROM, OR LIES WITHIN THE LIMITS OF, SUCH ROADS, THE CERTIFICATE HOLDERS WILL NOTIFY EACH RELEVANT TRANSPORTATION DEPARTMENT OR AGENCY OF THE APPROXIMATE DATE WHEN WORK WILL BEGIN (CC#69A).	DURING PREPARATION OF THE EM&CP AND WHEN WORK BEGINS.				
CERTIFICATE HOLDERS, NYSDOT, DPS STAFF, NYSDEC	THE CERTIFICATE HOLDERS WILL PROVIDE STATUS REPORTS SUMMARIZING CONSTRUCTION AND INDICATING CONSTRUCTION ACTIVITIES AND LOCATIONS SCHEDULED FOR THE NEXT MONTH (CC#47).	BI-WEEKLY.				

#### SEGMENT 2 - NYSDOT COORDINATION SUMMARY

TABLE 12.1 SEGMENT 1 — CO-LOCATED INFRASTRUCTURE CONSULTATION SUMMARY							
OWNER	UTILITY	INITIAL CONTACT DATE	CI-OWNER RESPONSE	OUTREACH MAILING #2	OUTREACH MAILING #3		
AT&T	FIBER/TELEPHONE	September 9, 2021	CROSSING CONDITIONS RECEIVED	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CL ALONG PROJECT ROUTE.	4/13/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.		
LEVEL 3 COMMUNICATIONS (NOW LUMEN TECHNOLOGIES)	FIBER	September 10, 2021	SUPPORT SERVICES AGREEMENT IN PLACE. CROSSING CONDITIONS RECEIVED. REIMBURSEMENT FUND ESTABLISHED.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CL ALONG PROJECT ROUTE.	4/13/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.		
NATIONAL GRID/EAST/ELECTRIC	ELECTRIC	September 10, 2021	CROSSING CONDITIONS RECEIVED.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE. RECEIVED LOCATIONS OF OVERHEAD ELECTRIC LINES.	4/14/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.		
TIME WARNER CABLE (CHARTER COMMUNICATIONS/SPE CTRUM)	FIBER/CATV	September 23, 2021	NO ACTION REQUIRED UNTIL PLANS ARE AVAILABLE FOR REVIEW.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CLALONG PROJECT ROUTE.	4/14/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.		
VERIZON OR VERIZON/EAST	TELECOM; FIBER/TELEPHONE	September 10, 2021	NO ACTION REQUIRED UNTIL PLANS ARE AVAILABLE FOR REVIEW.	1/27/2022 EMAIL SENT WITH .KMZ DIGITAL MAP OF ROUTE AND REQUEST FOR RECORDS, DATA & INFORMATION OF CI ALONG PROJECT ROUTE.	4/14/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW. RESPONSES REQUESTED BY 4/29/2022.		
NYSDOT ALBANY REGION 1	TRAFFIC SIGNALS HIGHWAY	ONGOING FOR A NUMBER OF YEARS. SEE ALSO TABLE 11.3			1/27/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW.		
TOWN OF DRESDEN	STORM SEWER/SANITARY SEWER	September 7, 2021	DRAFT PLAN & PROFILE SENT FOR REVIEW.		1/27/2022 PLAN & PROFILE DRAWINGS PROVIDED FOR REVIEW.		
TOWN OF WHITEHALL	TRAFFIC SIGNALS/STORM SEWER/CULVERTS/S ANITARY SEWER/WATER	September 7, 2021	DRAFT PLAN & PROFILE SENT FOR REVIEW.		1/27/2022 PLAN & PROFILES DRAWINGS SENT FOR REVIEW.		
VILLAGE OF WHITEHALL	TRAFFIC SIGNALS/STORM SEWER/CULVERTS/S ANITARY SEWER/WATER	September 8, 2021	DRAFT PLAN & PROFILE SENT FOR REVIEW.		1/27/2022 PLAN & PROFILE DRAWINGS SENT FOR REVIEW.		

SEGMENT 2 - CO-LOCATED INFRASTRUCTURE CONSULTATION SUMMARY

SEGMENT	CROSSING ID	UTILITY TYPE	UDERGROUND OR OVERHEAD	CROSSING TYPE	APROXIMATE STATION (DETAILS ON DRAWINGS)
2	N/A	ELECTRIC (SECTION 12.3.2)	OVERHEAD	OPEN CUT/TRENCH	12519+50 (C-102)
2	N/A	STORM (SECTION 12.3.3)	UNDERGROUND	OPEN CUT/TRENCH	12520+25 (C-102)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12530+25, 12536+00, 12538+25 (C-103)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12538+75, 12542+50 (C-103)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12550+00, 12556+75 (C-104)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12548+75, 12554+25 (C-104)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	102560+75, 12562+25, 12575+00 (C-105)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12565+75 (C-105)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12576+00 (C-106)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	102591+50 (C-107)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12592+50, 12599+25, 12604+50 (C-107)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12607+00, 12610+00 (C-108)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12610+25, 12612+75, 12619+50 (C-108)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12631+00 (C-109)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12649+50 (C-110)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12639+75 (C-110)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12652+00 (C-111)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12668+50, 12669+50, 12670+75, 12672+50 (C-112
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12666+50 to 12667+00, 12672+75, 12679+75 (C-112)
2	HDD 1	STORM	UNDERGROUND	HDD	12689+75 (C-113)
2	HDD 1	STORM	UNDERGROUND	HDD	12711+75, 12715+00, 12721+75 (C-115)
2	HDD 1	ELECTRIC	OVERHEAD	HDD	12727+75, 12739+00 (C-116)
2	HDD 1	STORM	UNDERGROUND	OPEN CUT/TRENCH	12730+75, 12737+00 (C-116)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12744+75 to 12745+00, 12755+00 (C-117)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12761+75 (C-118)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12781+25 (C-119)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12776+00 (C-119)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12785+00, 12790+50, 12796+25 (C-120)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12800+25, 12805+75 (C-121)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12802+25, 12805+75 (C-121)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12818+00, 12820+00, 12827+75 (C-122)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12842+00, 12843+50 (C-123)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12836+00, 12841+00 (C-123)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12852+00 (C-124)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12846+00, 12853+25, 12856+75 (C-124)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12861+75, 12863+25, 12867+00 (C-125)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12880+00, 12884+75 (C-126)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12893+25, 12900+00, 12902+75 (C-127)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12913+50, 12914+00 (C-128)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12907+75 (C-128)
2	N/A	ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	12920+75(C-129)
2	HDD 2	STORM FIBER OPTIC CABLE	UNDERGROUND UNDERGROUND	OPEN CUT/TRENCH HDD	12920+50 (C-129) 12925+00 (C-129)
		(SECTION 12.3.3)			12925+00 (C-129) 12946+00 (C-130)
2	N/A	ELECTRIC ELECTRIC	OVERHEAD	OPEN CUT/TRENCH	· · ·
2	N/A N/A	WATER	OVERHEAD UNDERGROUND	OPEN CUT/TRENCH OPEN CUT/TRENCH	12954+25 (C-131) 12951+75 (C-131)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12951+75 (C=131) 12958+25 (C=131)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	12977+75 (C-132)
2	N/A	ELECTRIC	UNDERGROUND	OPEN CUT/TRENCH	12968+25, 12975+25 (C-132)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	12966+75, 12977+75 (C-132)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	12980+25 (C-133)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	13002+75 to 13011+00 (C-134 to C-135)
2	N/A	ELECTRIC	UNDERGROUND	OPEN CUT/TRENCH	13001+75 (C-134)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	13001+75 (C-134)
2	N/A	FIBER OPTIC	UNDERGROUND	OPEN CUT/TRENCH	13022+75 (C-135)
2	N/A	ELECTRIC	UNDERGROUND	OPEN CUT/TRENCH	13029+75, 13035+50, 13037+25 (C-136)
2	N/A	STORM	UNDERGROUND	OPEN CUT/TRENCH	13030+25 (C-136)

NOTE: SECTION 12.3 OF THE EM&CP DESCRIBES THE UTILITY CROSSINGS THAT WILL OCCUR WITHIN SEGMENT 2 AS WELL AS THE MITIGATION MEASURES THAT WILL BE FOLLOWED FOR EACH TYPE OF UTILITY CROSSINGS. THERE ARE NO RAILROAD CROSSINGS IN SEGMENT 2.

## SEGMENT 2 - UTILITY CROSSINGS

Kiewit	
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION
OF THE ALTERATION.

No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	DF
A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	_

CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL EM&CP DATA TABLES

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

G-013

XXX OF XXX

Champlain Hudson **Power Express** 

DRAWN BY: XXX DESIGNED BY: XXX APPROVED BY: XXX REV. NO.

TABLE 13: SEGMENT 1A - RESTORATION METHODS

SECTION OF EMCP

NOT APPLICABLE TO SEGMENT 2

SECTION 13.2.3

**SECTION 13.2.4** 

SECTION 13.2.5

SECTION 13.2.6

SECTION 13.4

13.4.2

13.4

LAND USE

LANDSCAPING

STREAMS AND WATERBODIES

CONSTRUCTION MATERIALS AND EQUIPMENT STAGING LOCATIONS

AND TEMPORARY ACCESS ROADS PAVEMENT

RECREATIONAL AREAS

ACCESS ROADS AND LAYDOWN AREAS WITHIN AGRICULTURAL

LANDS

DRAINAGE FEATURES

GENERAL AGRICULTURAL LANDS

1. SECTION 13.0 OF THE EM&CP DESCRIBES THE CLEANUP STANDARDS AND PRACTICES THAT WILL BE FOLLOWED THROUGHOUT SEGMENT 1. TABLE 13 SUMMARIZES THE

APPROPRIATE SUBSECTION WITHIN SECTION 13 THAT INCLUDES

THE RESTORATION PROCEDURES FOR EACH TYPE OF LAND USE.

SEGMENT 2 — RESTORATION METHODS

AS NOTED DATE X SH.NO.

#### CLEANUP STANDARDS AND PRACTICES

FROM THE BMPS', CLEAN-UP, RESTORATION, AND REVEGETATION PROCEDURES WILL BE ONGOING DURING CONSTRUCTION AS EACH PACKAGE IS COMPLETED. DURING CONSTRUCTION, ROAD AND CONSTRUCTION ROWS WILL BE KEPT FREE OF DEBRIS AND DISCARDED MATERIAL TO THE GREATEST EXTENT POSSIBLE. AS CONSTRUCTION CONTINUES, EACH SECTION OF THE ROWS WILL BE THOROUGHLY CLEANED AFTER CONSTRUCTION IS COMPLETED ON THAT PARTICULAR SECTION. ALL FABRICATED DEBRIS RESULTING FROM CONSTRUCTION WILL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE IN COMPLIANCE WITH ALL APPROPRIATE ENVIRONMENTAL REGULATIONS. FABRICATED DEBRIS GENERATED DURING CONSTRUCTION INCLUDES PIPING, FENCING, WIRING, AND ANY OTHER MATERIALS USED DURING CONSTRUCTION. NO FABRICATED DEBRIS BE BURNED OR BURIED. ALL TRUCKS LEAVING THE CONSTRUCTION AREA WILL BE LOADED AND COVERED IN ACCORDANCE WITH APPLICABLE REGULATIONS AS NEEDED AS DESCRIBED IN THE SOIL MANAGEMENT PLAN OF THE EM&CP IN APPENDIX L.

#### RESTORATION AND PLANTING

THE FINAL STAGE OF CONSTRUCTION WILL CONSIST OF RESTORING THE ROAD AND CONSTRUCTION ROWS AND PACKAGE 1B TO ITS ORIGINAL CONDITION AND CHARACTER AS MUCH AS POSSIBLE/IS PRACTICAL, UNLESS DOING SO WOULD INTERFERE WITH THE SAFE OR RELIABLE OPERATION AND MAINTENANCE OF THE PROJECT. RESTORATION ACTIVITIES MAY VARY WITH THE SPECIFIC AREA TO BE RESTORED BUT WILL CONSIST PREDOMINANTLY OF RESTORING TOPOGRAPHY TO ORIGINAL GRADIENTS AND RESEEDING EXCAVATED AREAS OVER THE TRENCH AS IDENTIFIED HEREIN.

#### SITE PREPARATION FOR REVEGETATION

THE SURFACE OF THE ROAD AND CONSTRUCTION ROWS DISTURBED B' CONSTRUCTION ACTIVITIES WILL BE GRADED TO MATCH THE ORIGINAL TOPOGRAPHIC CONTOURS AND TO BE COMPATIBLE WITH SURROUNDING DRAINAGE PATTERNS WHERE APPROPRIATE. IT SHOULD BE NOTED THAT SUBCONTRACTORS WILL TYPICALLY LIMIT GRUBBING (THE REMOVAL OF STUMPS AND ROOTS) TO THE FOOTPRINT OF THE EXCAVATED TRENCH AND ACCESS ROADS TO ALLOW RE-SPROUTING AND ASSIST IN THE RECOVERY OF WOODY SPECIES, EXCEPT WHERE REMOVAL IS REQUIRED FOR SAFE CONSTRUCTION. WHERE NEEDED, IT MAY BE NECESSARY TO IMPORT TOPSOIL TO RETURN AN AREA TO GRADE. IMPORTED TOPSOIL WILL FOLLOW CLASSIFICATION AND CHARACTERIZATION MEASURES OUTLINED IN THE SOIL MANAGEMENT PLAN IN APPENDIX L. HDD ENTRY AND EXIT PITS WILL BE BACKFILLED AND THE DISTURBED GROUND SURFACE WILL BE SIMILARLY GRADED. TRENCHES WILL BE BACKFILLED IN ACCORDANCE WITH THE MEASURES OUTLINED IN SECTION 4.2.3 OF THE EM&CP.

THE CERTIFICATE HOLDER WILL BE RESPONSIBLE FOR WILL CHECKING ALL CULVERTS AND ASSURE THAT THEY ARE NOT CRUSHED OR BLOCKED DURING CONSTRUCTION AND RESTORATION OF THE PACKAGE 1A/1B AND. IF A CULVERT IS BLOCKED OR CRUSHED, TAKE IMMEDIATE STEPS TO REPLACE OR REPAIR THE CULVERT IN ACCORDANCE WITH APPLICABLE STATE OR LOCAL STANDARDS.

#### RESTORATION OF NYS RT 22

THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS. TRENCHES WILL BE BACKFILLED PER THE APPROPRIATE DETAIL, FULL WIDTH OF THE TRENCH WILL BE RESTORED TO MATCH THE EXISTING PAVEMENT SECTION, THE FULL WIDTH OF THE TRAVEL LANE TO THE CENTERLINE WILL BE MILLED AND OVERLAYED WITH A TOP COURSE OF ASPHALT. ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE-APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDERS.

#### SEEDING AND PLANTING

NOTES: SEEDING OPERATIONS ACROSS THE 35-FOOT-WIDE CONSTRUCTION AND FACILITY ROWS AS WELL AS WITHIN STAGING AND LAYDOWN AREAS WILL COMMENCE ONLY AFTER AN ACCEPTABLE SEEDBED HAS BEEN ESTABLISHED. AS DESCRIBED ABOVE. SEED WILL BE APPLIED BY HAND, OR VIA HYDRO-SEEDERS. THE ENTIRE SEEDED AREA WILL BE WATERED WITH A FINE SPRAY UNTIL A UNIFORM MOISTURE DEPTH OF APPROXIMATELY ONE (1) INCH HAS BEEN OBTAINED, AS APPLICABLE. MULCHING AND ANCHORING OF THE MULCH MAY BE NECESSARY IN SOME AREAS UNLESS A HYDROMULCH/SEED SLURRY IS USED. ON STEEP SLOPES, JUTE NET WILL BE USED TO PROVIDE STABILIZATION. FERTILIZER WILL BE ADDED, AS APPLICABLE, AT THE APPROPRIATE RATES AFTER SEED IS APPLIED AND/OR TO A HYDROMULCH/SEED SLURRY TO BUFFER AREAS BUT NOT FOR WETLAND RESOURCE AREAS. SEEDING/MULCHING WILL TAKE PLACE UNDER THE SUPERVISION OF THE ENVIRONMENTAL INSPECTOR. THE SEED MIXTURE AND RATE OF APPLICATION WILL DEPEND ON THE SOIL TYPE. LAND USE. AVAILABLE MOISTURE. AND SEASON AT THE TIME OF APPLICATION. THE LOCAL SOIL AND WATER CONSERVATION DISTRICT AND THE LANDOWNER/OPERATOR WILL BE CONSULTED REGARDING APPROPRIATE SEED MIXTURES AND APPLICATION RATES. ALL SEED MIXES WILL BE FREE OF INVASIVE SPECIES. ALL SEED BAG TAGS WILL BE PROVIDED TO THE ENVIRONMENTAL INSPECTOR EITHER ORIGINAL TAGS OR SCANNED COPIES. SEEDED AREAS WILL BE MONITORED FOLLOWING RESTORATION UNTIL A MINIMUM VEGETATIVE COVER OF EIGHTY (80) PERCENT IS ACHIEVED. FOR WETLAND RESOURCE AREAS, EMERGENT COMMUNITIES SHOULD BE REVEGETATED WITH AN ERNST FACW WETLAND MEADOW MIX (ERNMX-122) OR EQUIVALENT, AND FOR SHADED SITES WITHIN FORESTED/SHRUB-SHRUB WETLAND COMMUNITIES. DISTURBED AREAS SHOULD BE REVEGETATED WITH ERNST SPECIALIZED WETLAND MIX FOR SHADED AREAS (ERNMX-137) OR EQUIVALENT.

THE MAJORITY OF SOIL EXCAVATION FOR PACKAGE 1A AND 1B WILL BE FOR THE TRENCH-LINE FOR THE CONDUIT/CABLE. HOWEVER, SOME EXCAVATION WILL BE PERFORMED TO CONSTRUCTION TEMPORARY ACCESS ROADS AS DESCRIBE IN SECTION 4.8 OF THE EM&CP. ALL TRENCH AREAS AND OTHER EXCAVATED AREAS WILL BE RESEEDED WITH AN APPROPRIATE SEED MIX AS IDENTIFIED ABOVE. VEGETATION THROUGHOUT THE CONSTRUCTION ROW WILL BE CUT TO GROUND LEVEL AND ROOT SYSTEMS WILL REMAIN INTACT TO ALLOW FOR RESPROUTING FOLLOWING CONSTRUCTION, UNLESS RESPROUTING WOULD INTERFERE WITH THE SAFE AND RELIABLE OPERATION OF THE PROJECT.

ALL TREES OVER TWO (2) INCHES IN DIAMETER AT BREAST HEIGHT OR SHRUBS OVER FOUR (4) FEET IN HEIGHT DAMAGED OR DESTROYED BY ACTIVITIES DURING CONSTRUCTION, OPERATION, OR MAINTENANCE, ASSOCIATED URBAN. RESIDENTIAL OR ADIRONDACK PARK LANDSCAPED AREAS, WILL BE REPLACED WITHIN THE FOLLOWING YEAR BY THE CERTIFICATE HOLDER WITH THE EQUIVALENT TYPE OF TREES OR SHRUBS EXCEPT IF:

a) EQUIVALENT TYPE REPLACEMENT TREES OR SHRUBS WOULD INTERFERE WITH THE PROPER CLEARING, CONSTRUCTION, OPERATION, OR MAINTENANCE OF THE PROJECT OR WOULD BE INCONSISTENT WITH STATE-INVASIVE SPECIES POLICY; OR

b) REPLACEMENT WOULD BE CONTRARY TO SOUND ROW MANAGEMENT PRACTICES, OR TO ANY APPROVED LONG-RANGE ROW MANAGEMENT PLAN APPLICABLE TO THE FACILITY OR ADJOINING ROW: OR

c) THE OWNER OF LAND WHERE THE DAMAGED OR DESTROYED TREES OR SHRUBS WERE LOCATED (OR OTHER RECORDED EASEMENT OR LICENSE HOLDERS WITH THE RIGHT TO CONTROL REPLACEMENT) DECLINES REPLACEMENT.

#### RESTORATION OF WATERBODIES

THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS. ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDER. ALL MOTORIZED CONSTRUCTION EQUIPMENT WILL BE TRANSPORTED TO OFF--SITE FACILITIES. ALL WETLAND MATS AND MATERIALS WILL BE COLLECTED. PACKED. AND TRANSPORTED TO OFF-SITE STORAGE FACILITY OR TO THE NEXT PACKAGE'S STAGING AREAS NEEDED.

#### RESTORATION OF WETLANDS

THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS. ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE-APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDERS. ALL MOTORIZED CONSTRUCTION EQUIPMENT WILL BE TRANSPORTED TO OFF-SITE FACILITIES. ALL WETLAND MATS AND MATERIALS WILL BE COLLECTED, PACKED, AND TRANSPORTED TO OFF-SITE STORAGE FACILITIES OR TO THE NEXT SEGMENT'S STAGING AREA AS NEEDED.

FOR WETLAND RESOURCE AREAS, EMERGENT COMMUNITIES SHOULD BE REVEGETATED WITH AN ERNST FACW WETLAND MEADOW MIX (ERNMX-122) OR EQUIVALENT, AND FOR SHADED SITES WITHIN FORESTED/SHRUB-SHRUB WETLAND COMMUNITIES. DISTURBED AREAS SHOULD BE REVEGETATED WITH ERNST SPECIALIZED WETLAND MIX FOR SHADED AREAS (ERNMX-137) OR EQUIVALENT. FOR UPLANDS AND WETLAND BUFFER ZONES LOCATED WITHIN THE ADIRONDACK PARK SHOULD BE ADIRONDACK

#### RESTORATION OF COUNTY RT 3 AND LAKE ROAD

THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS. TRENCHES WILL BE BACKFILLED PER THE APPROPRIATE DETAIL, FULL WIDTH OF THE TRENCH WILL BE RESTORED TO MATCH THE EXISTING PAVEMENT SECTION, THE FULL WIDTH OF THE ROAD WILL BE MILLED AND OVERLAYED WITH A TOP COURSE OF ASPHALT. ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE-APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDERS.

#### RESTORATION OF CONSTRUCTION MATERIALS AND EQUIPMENT STAGING AND TEMPORARY ACCESS ROADS

THE CONSTRUCTION MATERIALS EQUIPMENT STAGING LOCATIONS AND ACCESS ROADS FOR THIS PACKAGE ARE SHOWN IN THE PLAN AND PROFILE DRAWINGS AND EROSION AND SEDIMENT CONTROL PLAN. THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS, EXCEPT WHERE THE UNDERLYING LANDOWNER REQUESTS THE ABILITY TO RETAIN ACCESS ROADS OR OTHER STAGING LOCATIONS AS PROPERTY IMPROVEMENTS.

ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE-APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDER. ALL MOTORIZED CONSTRUCTION EQUIPMENT WILL BE TRANSPORTED TO OFF-SITE FACILITIES. ALL OTHER USABLE CONSTRUCTION EQUIPMENT AND MATERIALS WILL BE COLLECTED, PACKED, AND TRANSPORTED TO OFF-SITE STORAGE FACILITIES OR TO THE NEXT SEGMENT'S STAGING AREA AS NEEDED. ALL UNUSABLE EQUIPMENT AND MATERIALS WILL BE REMOVED FROM THE LAYDOWN YARD AND DISPOSED OF APPROPRIATELY.

#### PLANT INSPECTION, GUARANTEE AND MAINTENANCE

VEGETATION RESTORATION ALSO INCLUDES THE MAINTENANCE OF PLANTINGS FOR SPECIFIED TIME PERIODS AND THE REPLACEMENT OF UNSUCCESSFUL PLANTINGS. ALL VEGETATION REPLACED WILL HAVE A MINIMUM TWO (2) YEAR SURVIVAL GUARANTEE. TEMPORARY EROSION CONTROLS WILL BE REMOVED ONCE REVEGETATION IS ESTABLISHED. REVEGETATION WILL BE MONITORED UNTIL THERE IS A MINIMUM OF EIGHTY (80) PERCENT REGROWTH. WHERE TREE OR SHRUB PLANTINGS ARE NEEDED. A POST CONSTRUCTION SURVIVAL SURVEY WILL BE PERFORMED ONE YEAR AFTER THE PLANTINGS. IF ANY TREE OR SHRUB HAS NOT SURVIVED OR IS IN POOR HEALTH, THE TREE/SHRUB WILL BE REPLACED.

THE ENVIRONMENTAL INSPECTOR WILL INSPECT ALL PLANTS IN CONTAINERS PRIOR TO PLANTING. PLANTINGS WILL BE PERFORMED BY A QUALIFIED LANDSCAPE OR NURSERY CONTRACTOR. THE ENVIRONMENTAL INSPECTOR WILL INSPECT ALL PLANTS PRIOR TO PLANTING AND AFTER COMPETITION OF PLANTING TO ENSURE PROPER PLANTING PROCEDURES AND THE CORRECT PLANT SPECIES WERE USED. ADDITIONALLY, THE ENVIRONMENTAL INSPECTOR WILL CONDUCT A FINAL INSPECTION OF ALL REVEGETATED AREAS AFTER THE END OF THE MONITORING PERIOD TO ENSURE FINAL STABILIZATION.

RESTORATION OR SEEDED AREAS WILL BE CONSIDERED TO HAVE MET FINAL STABILIZATION WHEN ALL SOIL DISTURBING ACTIVITIES HAVE CEASED AND A UNIFORM, PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 80 PERCENT.

STORMWATER POLLUTION PREVENTION PLAN (SWPPP) INSPECTIONS WILL BE PERFORMED BY THE ENVIRONMENTAL INSPECTOR ON A WEEKLY BASIS UNTIL ALL DISTURBED AREAS HAVE ACHIEVED THE 80% REVEGETATION REQUIRED FOR FINAL RESTORATION. FOLLOWING FINAL RESTORATION. EROSION AND SEDIMENT CONTROL MEASURES WILL BE REMOVED FROM THE SITE AND DISPOSED OF APPROPRIATELY.

#### RESTORATION WITHIN NYSDOT ROW

L RESTORATION WITHIN THE NYSDOT ROW SHALL BE DONE IN ACCORDANCE WITH THE LATEST VERSION OF THE NYSDOT STANDARD SPECIFICATIONS AND STANDARD

#### ROADWAY RESTORATION

#### (STRIPING, SIGNAGE, AUDIBLE ROADWAY DELINEATORS)

STRIPING IMPACTED OR REMOVED FROM CONSTRUCTION WITHIN THE LIMITS OF WORK, INCLUDING AREAS OF MILL AND OVERLAYS TO BE INSTALLED PER EXISTING STRIPING PATTERNS. CONTRACTOR SHALL INVENTORY ALL STRIPING PRIOR TO WORK. WORK TO BE COMPLETED IN ACCORDANCE WITH NYSDOT STANDARD SHEETS AND SPECIFICATIONS (SEE 685 SERIES STANDARD SHEETS)

SIGNAGE IMPACTED DAMAGED OR REMOVED DURING CONSTRUCTION SHALL BE REPLACED IN KIND OR REINSTALLED IN ACCORDANCE WITH NYSDOT STANDARD SHEETS AND SPECIFICATIONS (SEE 645 SERIES STANDARD SHEETS).

AUDIBLE ROADWAY DELINEATORS DAMAGED OR REMOVED DUE TO CONSTRUCTION SHALL BE INSTALLED DURING RE-PAVING OPERATIONS IN ACCORDANCE WITH NYSDOT STANDARD SHEET 649-03.

#### RESTORATION OF ROADWAY SHOULDER

THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS, TRENCHES WILL BE BACKFILLED THE APPROPRIATE DETAIL, FULL WIDTH OF THE TRENCH WILL BE RESTORED TO MATCH THE EXISTING PAVEMENT SHOULDER SECTION, THE FULL WIDTH OF THE SHOULDER WILL BE MILLED AND OVERLAYED WITH A TOP COURSE OF ASPHALT. ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE-APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDERS.

#### RESTORATION OF ROADWAY

THESE AREAS WILL BE RESTORED AS CLOSE AS PRACTICABLE TO PRE-CONSTRUCTION CONDITIONS AND CONTOURS. TRENCHES WILL BE BACKFILLED THE APPROPRIATE DETAIL, FULL WIDTH OF THE TRENCH WILL BE RESTORED TO MATCH THE EXISTING PAVEMENT SHOULDER SECTION, THE FULL WIDTH OF THE TRAVEL LANE WILL BE MILLED AND OVERLAYED WITH A TOP COURSE OF ASPHALT. ALL TEMPORARY FENCING AND EROSION CONTROLS WILL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER AT A STATE-APPROVED DISPOSAL FACILITY APPROVED BY DPS STAFF AND THE CERTIFICATE HOLDERS.

#### RESTORATION OF ROADWAY

FOLLOWING CONSTRUCTION, THE CERTIFICATE HOLDER WILL RESEED THE CONSTRUCTION AREA WITHIN RECREATIONAL AREAS SUCH AS ADIRONDACK PARK USING THE PROCEDURES AND METHODS SPECIFIED IN THE SECTIONS ABOVE WHERE NEEDED. IF NECESSARY, ADDITIONAL REVEGETATION AND TREE PLANTING MAY BE PERFORMED DEPENDING ON THE IMPACT OF CONSTRUCTION.

#### RESTORATION OF ROADWAY

CURRENTLY, NO RESTORATION NEEDS IN URBAN/RESIDENTIAL AREAS HAVE BEEN IDENTIFIED WITHIN PACKAGE 1A/1B. AS DESCRIBED IN THE CERTIFICATE CONDITIONS, CURBS, SIDEWALKS, AND STREETS DAMAGED BY CONSTRUCTION WILL BE RESTORED TO PRE-EXISTING CONDITION OR BETTER. THE CERTIFICATE HOLDER WILL CONSULT, WHERE APPLICABLE, THE MUNICIPAL ROAD OR HIGHWAY DEPARTMENT AND/OR THE REGIONAL OFFICE OR COUNTY ENGINEER OF THE NYSDOT IN ORDER TO IDENTIFY AND INCORPORATE APPLICABLE SPECIFICATIONS FOR CURB. SIDEWALK, OR STREET RESTORATION. ALL SURFACE RESTORATION WILL FOLLOW THE APPLICABLE DETAIL.. GUIDE RAILS WILL BE REMOVED AND REPLACED IN ACCORDANCE WITH NYSDOT STANDARD SHEET 606-01. COUNTY ROAD 3 IN PACKAGE 1A WILL BE RESTORED UNDER A DIFFERENT COUNTY RESTORATION REQUIREMENT AS SHOWN IN THE APPLICABLE DETAIL.

#### PLANT INSPECTION, GUARANTEE AND MAINTENANCE

VEGETATION RESTORATION ALSO INCLUDES THE MAINTENANCE OF PLANTINGS FOR SPECIFIED TIME PERIODS AND THE REPLACEMENT OF UNSUCCESSFUL PLANTINGS. ALL VEGETATION REPLACED WILL HAVE A MINIMUM TWO (2) YEAR SURVIVAL GUARANTEE. TEMPORARY EROSION CONTROLS WILL BE REMOVED ONCE REVEGETATION IS ESTABLISHED. REVEGETATION WILL BE MONITORED UNTIL THERE IS A MINIMUM OF EIGHTY (80) PERCENT REGROWTH. WHERE TREE OR SHRUB PLANTINGS ARE NEEDED, A POST CONSTRUCTION SURVIVAL SURVEY WILL BE PERFORMED ONE YEAR AFTER THE PLANTINGS. IF ANY TREE OR SHRUB HAS NOT SURVIVED OR IS IN POOR HEALTH, THE TREE/SHRUB WILL BE REPLACED.

THE ENVIRONMENTAL INSPECTOR WILL INSPECT ALL PLANTS IN CONTAINERS PRIOR TO PLANTING. PLANTINGS WILL BE PERFORMED BY A QUALIFIED LANDSCAPE OR NURSERY CONTRACTOR. THE ENVIRONMENTAL INSPECTOR WILL INSPECT ALL PLANTS PRIOR TO PLANTING AND AFTER COMPETITION OF PLANTING TO ENSURE PROPER PLANTING PROCEDURES AND THE CORRECT PLANT SPECIES WERE USED. ADDITIONALLY, THE ENVIRONMENTAL INSPECTOR WILL CONDUCT A FINAL INSPECTION OF ALL REVEGETATED AREAS AFTER THE END OF THE MONITORING PERIOD TO ENSURE FINAL STABILIZATION.

RESTORATION OR SEEDED AREAS WILL BE CONSIDERED TO HAVE MET FINAL STABILIZATION WHEN ALL SOIL DISTURBING ACTIVITIES HAVE CEASED AND A UNIFORM, PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 80 PERCENT.

SWPPP INSPECTIONS WILL BE PERFORMED BY THE ENVIRONMENTAL INSPECTOR ON A WEEKLY BASIS UNTIL ALL DISTURBED AREAS HAVE ACHIEVED THE 80% REVEGETATION REQUIRED FOR FINAL RESTORATION. FOLLOWING FINAL RESTORATION. EROSION AND SEDIMENT CONTROL MEASURES WILL BE REMOVED FROM THE SITE AND DISPOSED OF APPROPRIATELY.

#### SEED MIXES

A. GENERAL SEED:

1. PIPELINE MIX W/SWITCHGRASS (ERNMX-102-1)

2. MIX COMPOSITION

33.0% PANICUM VIRGATUM, 'SHAWNEE' (SWITCHGRASS, 'SHAWNEE') 25.0% FESTUCA RUBRA (CREEPING RED FESCUE)

18.0% LOLIUM MULTIFLORUM (ANNUAL RYEGRASS)

16.0% PHLEUM PRATENSE, CLIMAX (TIMOTHY, CLIMAX)

5.0% TRIFOLIUM HYBRIDUM (ALSIKE CLOVER)

3.0% AGROSTIS ALBA (REDTOP) 3. APPLIED AT A RATE OF 40 LBS/ACRE

B. ADIRONDACK SEED: 1. MIX COMPOSITION

25% VIRGINIA WILD RYE (ELYMUS VIRGINICUS VAR. VIRGINICUS)

25% CANADA WILD RYE (ELYMUS CANADENSIS) 25% AUTUMN BENTGRASS (AGROSTIS PERENNANS)

25% CAMPER LITTLE BLUESTEM (SCHIZACHYRIUM SCOPARIUM) 2. APPLIED AT A RATE OF 40 LBS/ACRE.

#### C. SPECIALIZED WETLAND MIX FOR SHADED OBL-FACW AREAS (ERNMX-137) 1. MIX COMPOSITION

35.0% CAREX VULPINOIDEA, PA ECOTYPE (FOX SEDGE, PA ECOTYPE) 20.0% ELYMUS VIRGINICUS, MADISON-NY ECOTYPE (VIRGINIA WILDRYE,

MADISON-NY ECOTYPE)

15.0% CAREX SCOPARIA, PA ECOTYPE (BLUNT BROOM SEDGE, PA ECOTYPE)

12.8% CAREX LURIDA, PA ECOTYPE (LURID SEDGE, PA ECOTYPE)

5.0% CAREX LUPULINA, PA ECOTYPE (HOP SEDGE, PA ECOTYPE)

4.0% VERBENA HASTATA, PA ECOTYPE (BLUE VERVAIN, PA ECOTYPE)

2.0% HELIOPSIS HELIANTHOIDES, PA ECOTYPE (OXEYE SUNFLOWER, PA ECOTYPE)

1.0% CAREX INTUMESCENS, PA ECOTYPE (STAR SEDGE, PA ECOTYPE)

1.0% SPARGANIUM AMERICANUM (EASTERN BUR REED)

0.7% IRIS VERSICOLOR (BLUEFLAG) 0.5% BIDENS CERNUA, PA ECOTYPE (NODDING BUR MARIGOLD, PA ECOTYPE)

0.5% CAREX CRINITA, PA ECOTYPE (FRINGED SEDGE, PA ECOTYPE)

0.5% CAREX STIPATA, PA ECOTYPE (AWL SEDGE, PA ECOTYPE)

0.5% EUPATORIUM PERFOLIATUM, PA ECOTYPE (BONESET, PA ECOTYPE) 0.5% SCIRPUS CYPERINUS, PA ECOTYPE (WOOLGRASS, PA ECOTYPE)

0.5% VERNONIA NOVEBORACENSIS, PA ECOTYPE (NEW YORK IRONWEED, PA

0.3% LOBELIA SIPHILITICA, PA ECOTYPE (GREAT BLUE LOBELIA, PA ECOTYPE) 0.2% PENTHORUM SEDOIDES, PA ECOTYPE (DITCH STONECROP, PA ECOTYPE)

2. APPLIED AT A RATE OF APPROXIMATELY 20 LBS/ACRE, ALONG WITH THE COVER

3. COVER CROP SHOULD BE APPLIED AT A RATE OF 60-80 LBS/ACRE. COVER CROP TO BE AN ANNUAL RYE. COVER CROP BASED ON SEASON OF RESTORATION AN ANNUAL RYE THROUGH SPRING AND SUMMER AND WINTER RYE FOR LATE FALL.

#### D. FACW WETLAND MEADOW MIX (ERNMX-122)

#### 1. MIX COMPOSITION

21.0% CAREX VULPINOIDEA, PA ECOTYPE (FOX SEDGE, PA ECOTYPE) 20.0% ELYMUS VIRGINICUS, PA ECOTYPE (VIRGINIA WILDRYE, PA ECOTYPE)

16.0% CAREX LURIDA, PA ECOTYPE (LURID SEDGE, PA ECOTYPE)

12.0% CAREX LUPULINA, PA ECOTYPE (HOP SEDGE, PA ECOTYPE)

12.0% CAREX SCOPARIA, PA ECOTYPE (BLUNT BROOM SEDGE, PA ECOTYPE)

3.0% VERBENA HASTATA, PA ECOTYPE (BLUE VERVAIN, PA ECOTYPE)

2.4% ASCLEPIAS INCARNATA, PA ECOTYPE (SWAMP MILKWEED, PA ECOTYPE) 2.0% JUNCUS EFFUSUS (SOFT RUSH)

2.0% ZIZIA AUREA, PA ECOTYPE (GOLDEN ALEXANDERS, PA ECOTYPE)

1.6% ASTER NOVAE-ANGLIAE, PA ECOTYPE (NEW ENGLAND ASTER, PA ECOTYPE)

1.3% CAREX STIPATA, PA ECOTYPE (AWL SEDGE, PA ECOTYPE)

1.0% BIDENS CERNUA, PA ECOTYPE (NODDING BUR MARIGOLD, PA ECOTYPE)

1.0% JUNCUS TENUIS, PA ECOTYPE (PATH RUSH, PA ECOTYPE) 0.8% SOLIDAGO RUGOSA, PA ECOTYPE (WRINKLELEAF GOLDENROD, PA ECOTYPE)

0.6% VERBENA URTICIFOLIA, PA ECOTYPE (WHITE VERVAIN, PA ECOTYPE)

0.5% CAREX CRINITA, PA ECOTYPE (FRINGED SEDGE, PA ECOTYPE)

0.5% EUPATORIUM PERFOLIATUM, PA ECOTYPE (BONESET, PA ECOTYPE) 0.5% HELENIUM AUTUMNALE, PA ECOTYPE (COMMON SNEEZEWEED, PA ECOTYPE)

0.5% MIMULUS RINGENS, PA ECOTYPE (SQUARE STEMMED MONKEYFLOWER, PA

0.3% LOBELIA SIPHILITICA, PA ECOTYPE (GREAT BLUE LOBELIA, PA ECOTYPE)

0.3% SCIRPUS CYPERINUS, PA ECOTYPE (WOOLGRASS, PA ECOTYPE) 0.2% ALISMA SUBCORDATUM, PA ECOTYPE (MUD PLANTAIN, PA ECOTYPE)

0.2% ASTER PUNICEUS, PA ECOTYPE (PURPLESTEM ASTER, PA ECOTYPE) 0.2% ASTER UMBELLATUS, PA ECOTYPE (FLAT TOPPED WHITE ASTER, PA ECOTYPE)

0.1% PENTHORUM SEDOIDES, PA ECOTYPE (DITCH STONECROP, PA ECOTYPE) 2. APPLIED AT A RATE OF APPROXIMATELY 20 LBS/ACRE, ALONG WITH THE COVER

3. COVER CROP SHOULD BE APPLIED AT A RATE OF 60-80 LBS/ACRE. COVER CROP TO BE AN ANNUAL RYE. COVER CROP BASED ON SEASON OF RESTORATION AN ANNUAL RYE THROUGH SPRING AND SUMMER AND WINTER RYE FOR LATE FALL.

ALL SEED MIXES WILL BE FREE OF INVASIVE SPECIES.

CHAMPLAIN HUDSON POWER EXPRESS

**SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL** RESTORATION NOTES

CHA PROJECT NO. DRAWING NO.

G-020

KIEWIT PROJECT NO.

21162

**W**Kiewit



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

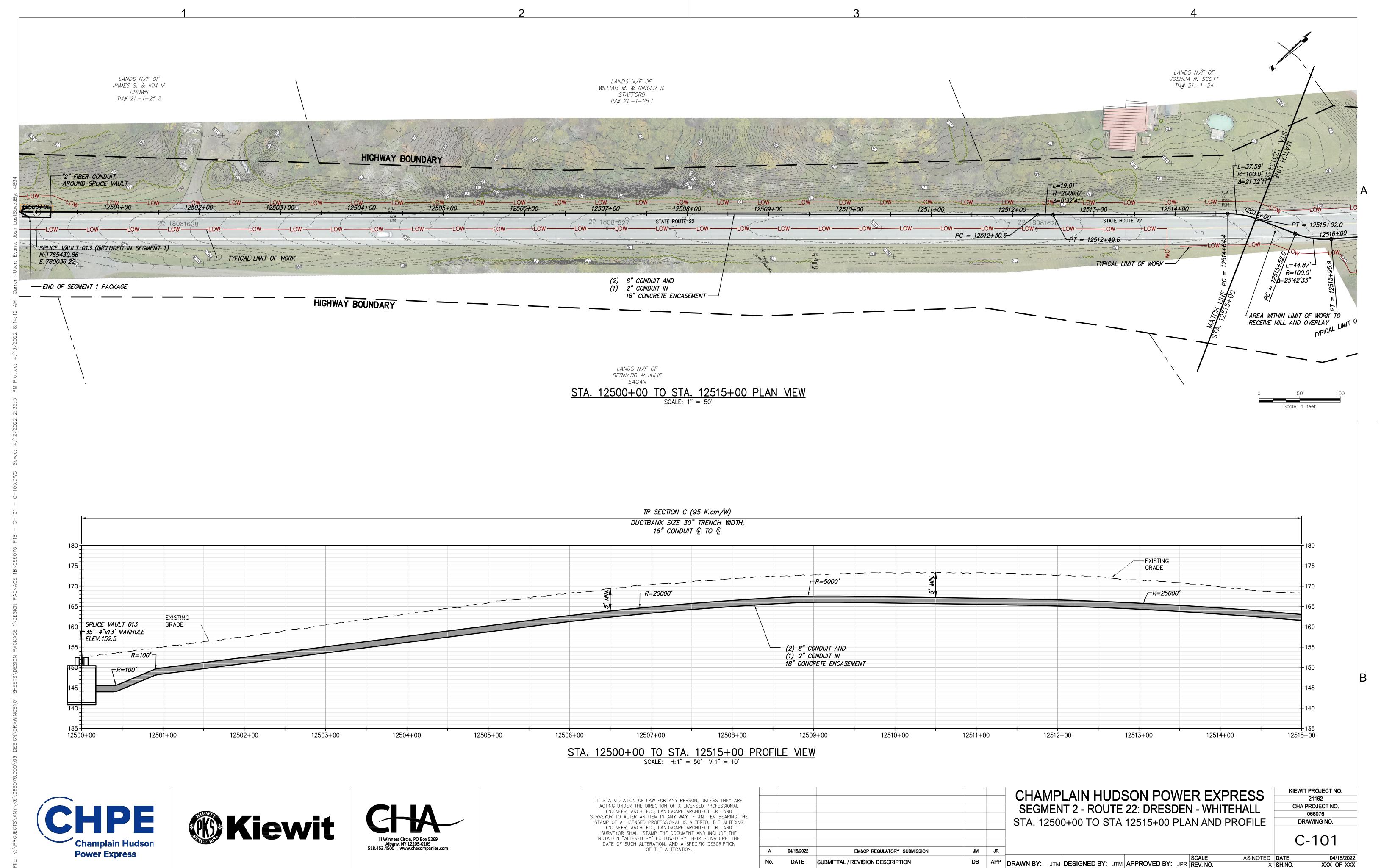
Α 04/15/2022 EM&CP REGULATORY SUBMISSION SUBMITTAL / REVISION DESCRIPTION

DB | APP | DRAWN BY: XXX | DESIGNED BY: XXX | APPROVED BY: XXX | REV. NO.

AS NOTED DATE X SH.NO.

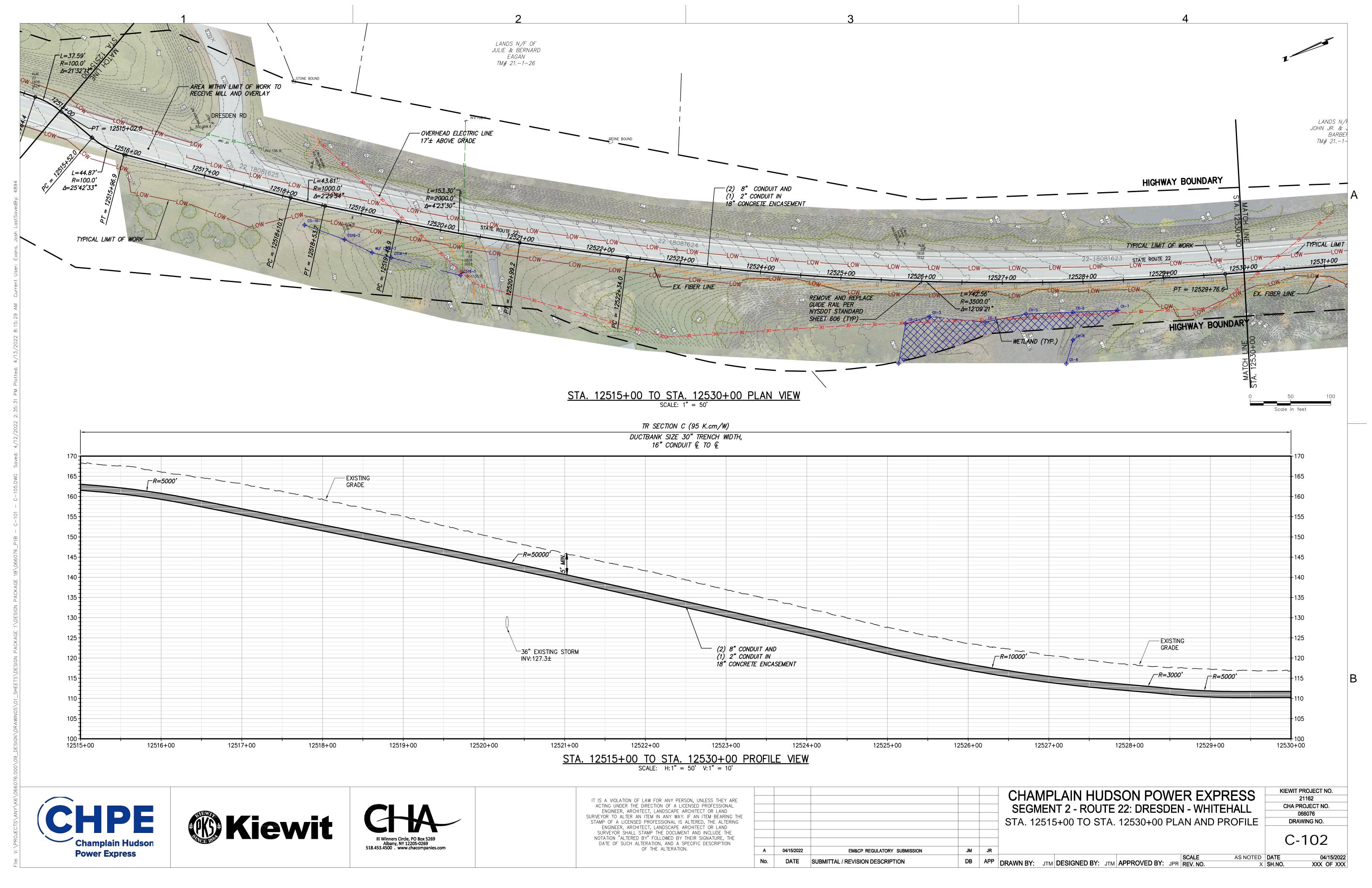
04/15/2022 XXX OF XXX

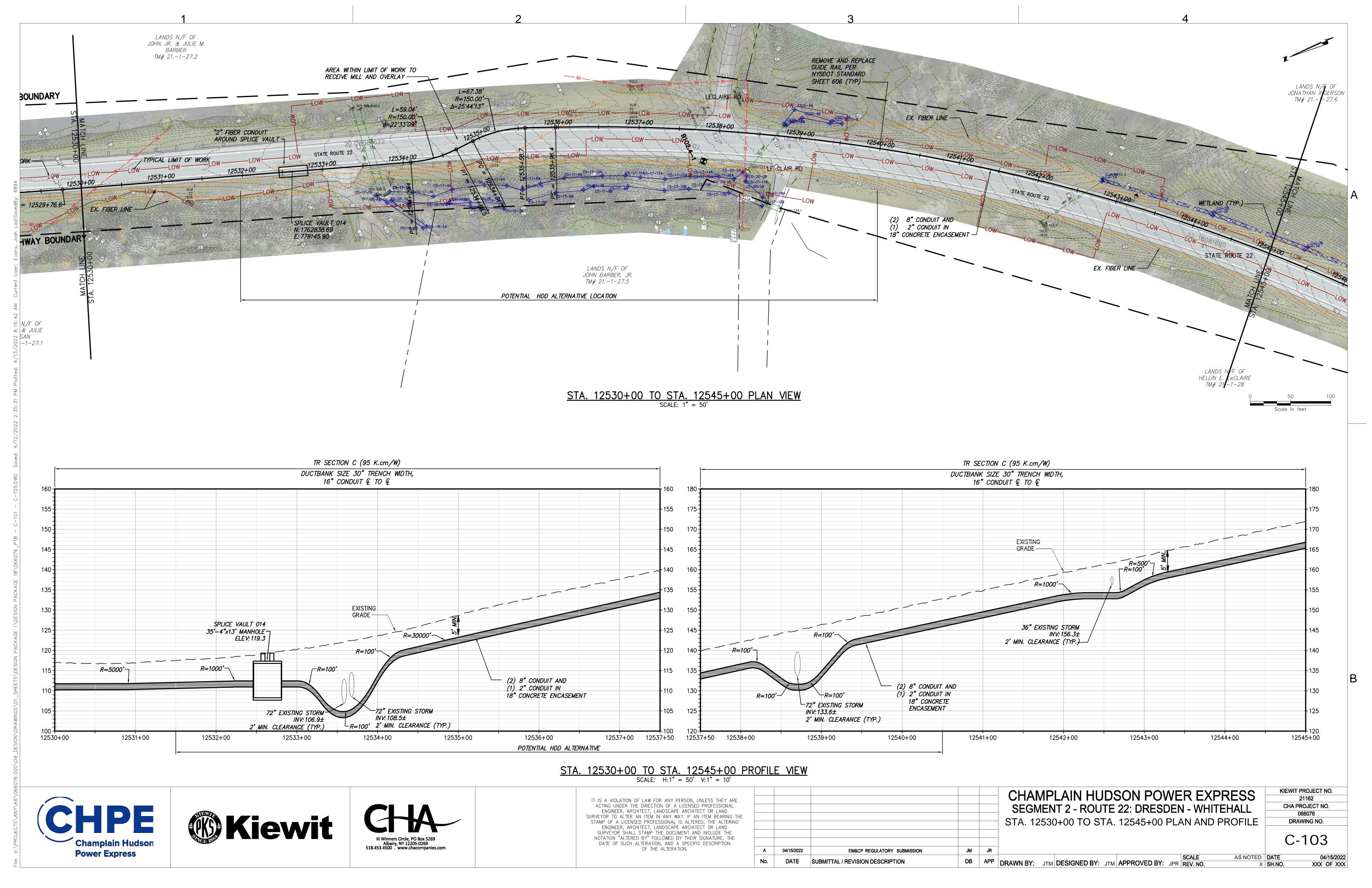
Champlain Hudson **Power Express** 

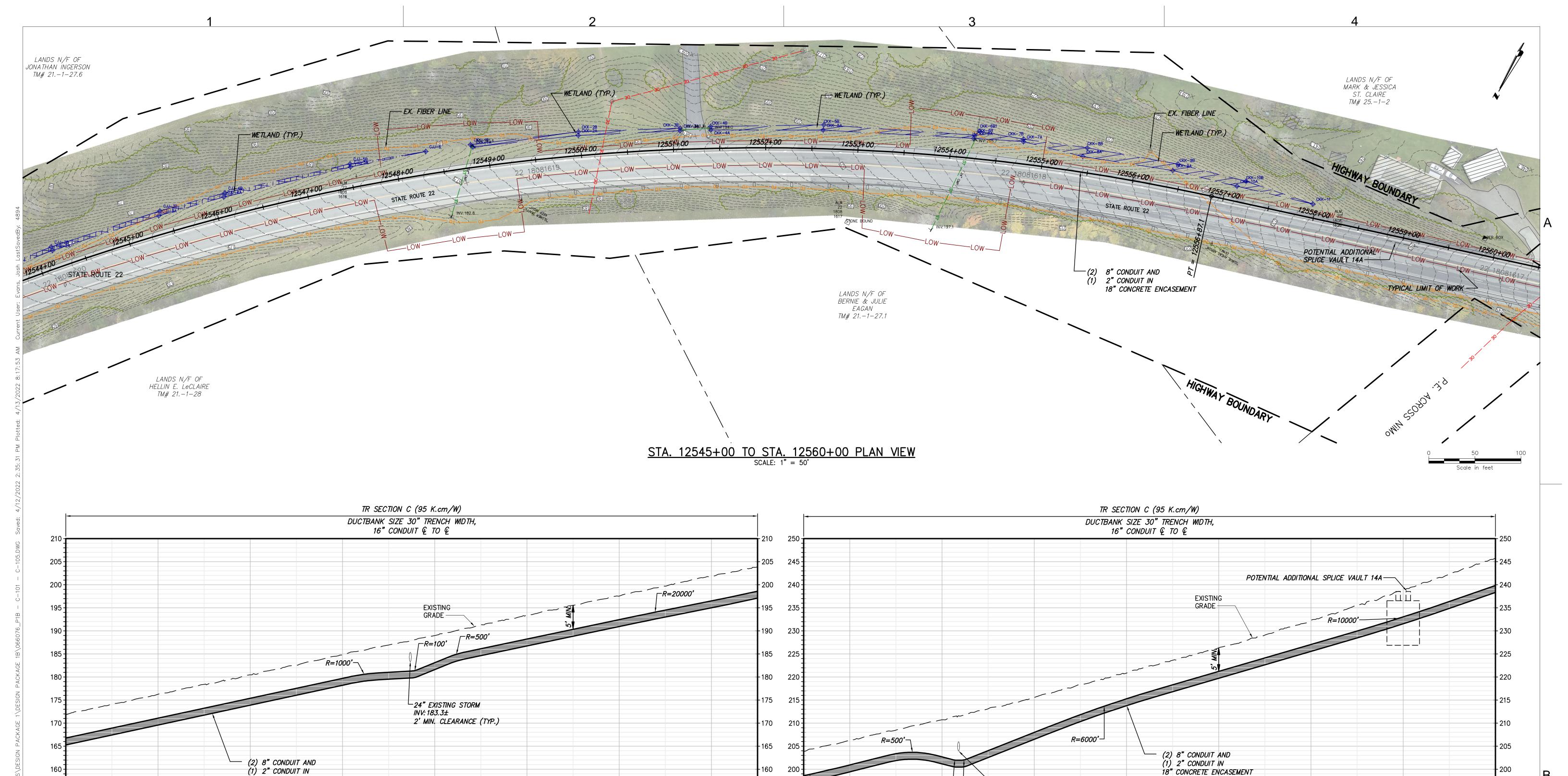


DATE SUBMITTAL / REVISION DESCRIPTION

XXX OF XXX







# STA. 12545+00 TO STA. 12560+00 PROFILE VIEW SCALE: H: 1" = 50' V: 1" = 10'

12552+50 12553+00

-160

-155

150 12552+00 12552+50



12546+00

12545+00



18" CONCRETE ENCASEMENT

12548+00

12547+00



12550+00

12551+00

12549+00

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SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE
STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION
OF THE ALTERATION.

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Α	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	1					
No.	DATE	CURNITTAL / DEVICION DESCRIPTION	DB	ADD						SCALE
INO.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	<b>DRAWN BY:</b> JTM	DESIGNED BY:	JTM	APPROVED	<b>) BY:</b> JPF	REV. NO.

-24" EXISTING STORM

2' MIN. CLEARANCE (TYP.)

12555+00

INV: 204.0±

R=100'-

12554+00

CHAMPLAIN HUDSON POWER EXPRESS SEGMENT 2 - ROUTE 22: DRESDEN - WHITEHALL STA. 12545+00 TO STA. 12560+00 PLAN AND PROFILE

12558+00

12559+00

18" CONCRETE ENCASEMENT

12556+00

12557+00

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. 066076 DRAWING NO.

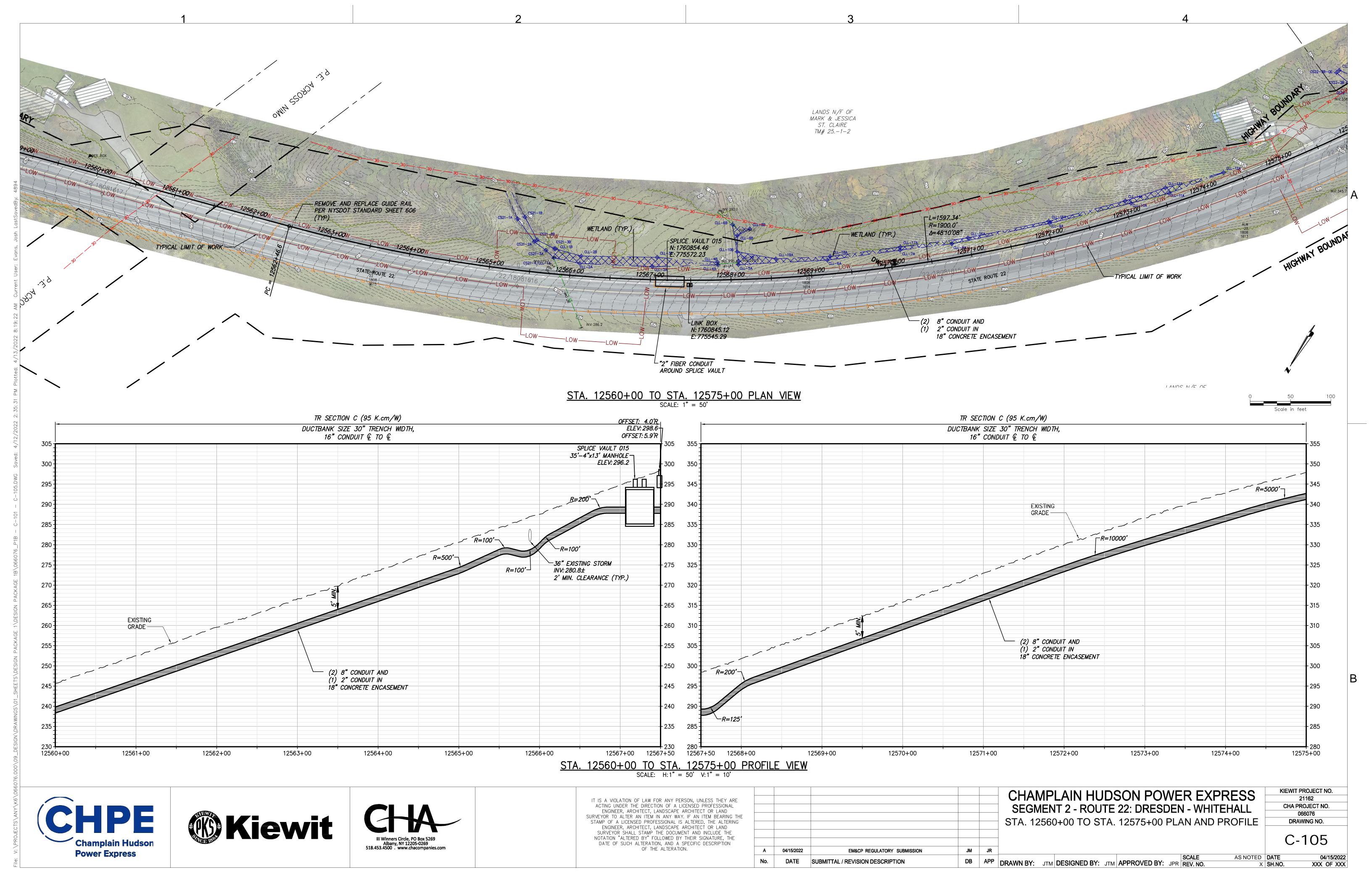
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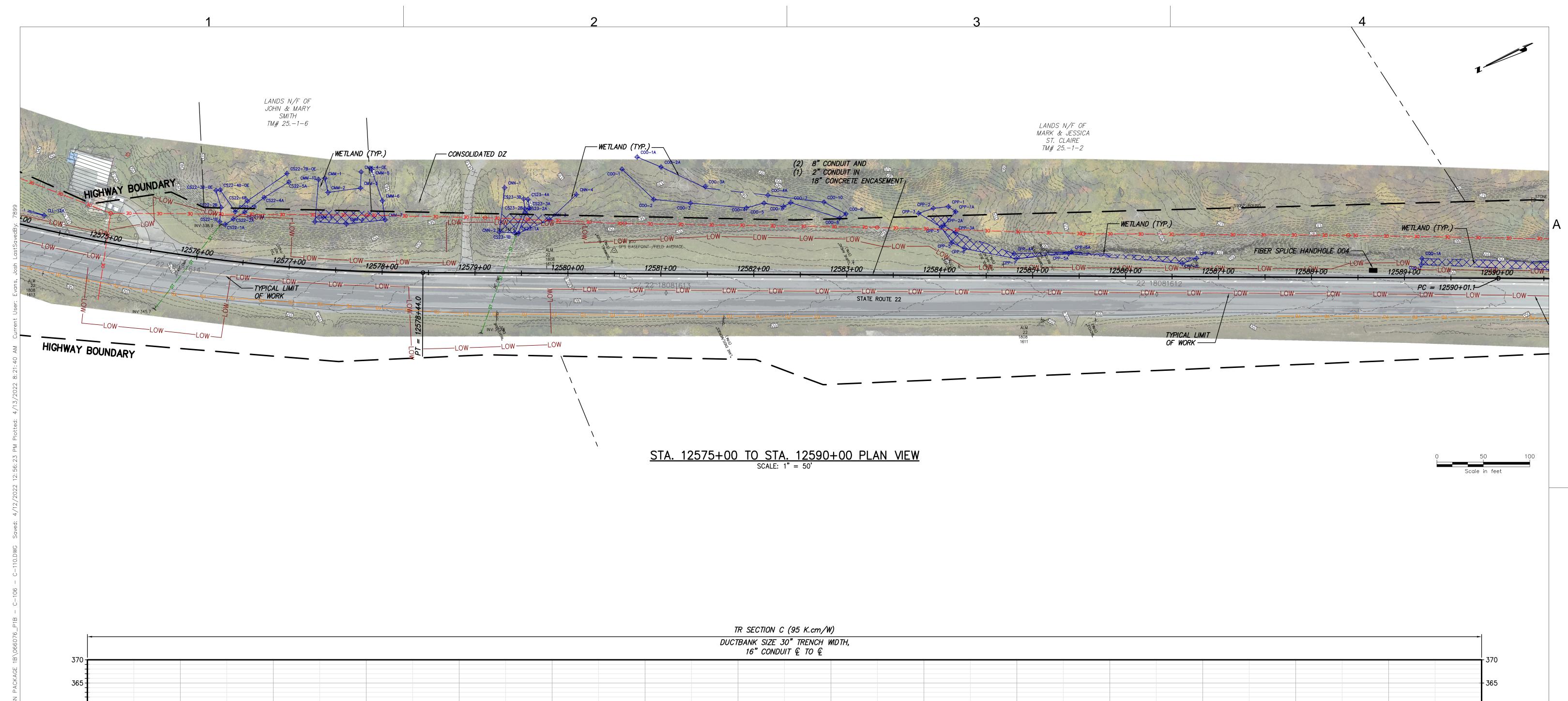
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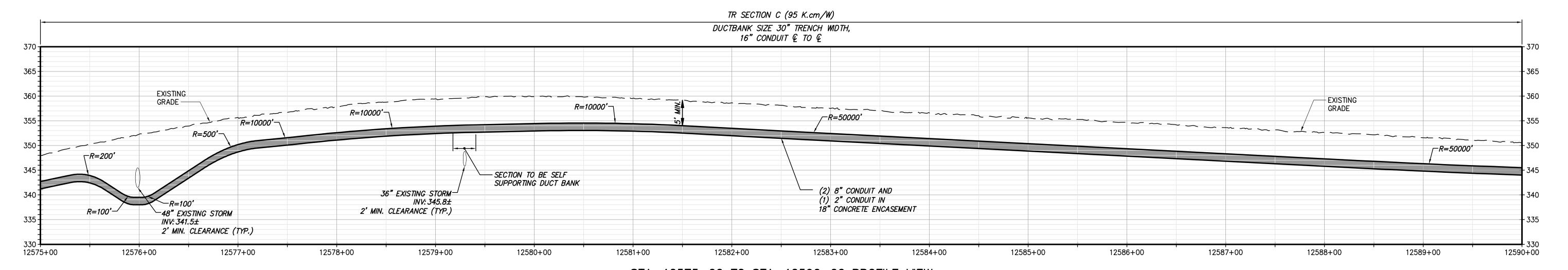
12560+00

C-104

AS NOTED DATE 04/15/2022 XXX OF XXX







STA. 12575+00 TO STA. 12590+00 PROFILE VIEW

SCALE: H: 1" = 50' V: 1" = 10'







IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT OR LAND
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STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING
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SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE
NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE
DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION
OF THE ALTERATION.

A	04/15/2022	EM&CP REGULATORY SUBMISSION	JM	JR	CHAMPLAIN HUDSON POWER EX SEGMENT 2 - ROUTE 22: DRESDEN - WH STA. 12575+00 TO STA. 12590+00 PLAN AND
				4.00	SCALE
No.	DATE	SUBMITTAL / REVISION DESCRIPTION	DB	APP	P DRAWN BY: JTM DESIGNED BY: JTM APPROVED BY: JPR REV. NO.

N HUDSON POWER EXPRESS - ROUTE 22: DRESDEN - WHITEHALL TO STA. 12590+00 PLAN AND PROFILE

KIEWIT PROJECT NO. 21162 CHA PROJECT NO. DRAWING NO.

C-106

AS NOTED DATE 04/15/2022 X SH.NO. XXX OF XXX